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ANDREW COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	FHWA	Bridge Name	Description
ANDR01	J 201	Rosendale Bridge	1- 100' riveted Warren pony truss 1930 Fred M. Clark
ANDR02	K 84	Platte River Bridge	1-120' riveted Pratt through truss 1932 Fred M. Clark and Sons
*ANDR03	L 202A	102 River Bridge	1-120' riveted polyg. Warren pony truss 1947 Clark & Runquist Constr. Co.
*ANDR04	007001.0	Arapahoe Creek Bridge	1- 90' riveted Pratt pony truss c1915
ANDR05	014000.9	Arapahoe Creek Bridge	1- 45' pinned Pratt pony truss c1910
ANDR06	027000.9	Arapahoe Creek Bridge	1- 80' riveted Camelback pony truss c1925
ANDR07	027002.0	Arapahoe Creek Bridge	1- 40' pinned Pratt pony truss c1910
ANDR08	029000.6	Arapahoe Creek Bridge	1-100' riveted Camelback pony truss c1925
ANDR09	040000.0	Pedlar Creek Bridge	1- 40' pinned Pratt pony truss c1910
*ANDR10	048000.0	Leach Mill Bridge	2- 70' pinned Pratt through truss 1913 A.S. Seybert
ANDR11	062001.0	Bridge	1- 45' pinned Pratt pony truss c1915
ANDR12	067000.4	Lincoln Creek Bridge	1- 40' pinned Pratt pony truss c1910
ANDR13	077000.6	Lincoln Creek Bridge	1- 50' pinned Pratt pony truss c1910
*ANDR14	080001.4	White Cloud Creek Bridge	(replaced)
*ANDR15	092001.0	Cawood Bridge	(replaced)
ANDR16	096000.6	Hickory Creek Bridge	(replaced)
ANDR17	143000.2	Bridge	1- 45' pinned Pratt pony truss c1905
ANDR18	150000.3	Kellogg Branch Bridge	1- 40' pinned Pratt pony truss c1910
*ANDR19	164001.0	Bridge	1- 80' 2-angle Camelback pony truss c1925
*ANDR20	198001.9	Fish Trap Bridge	(replaced)
ANDR21	206002.7	Crooked Creek Bridge	1- 40' pinned Pratt pony truss c1915
ANDR22	207000.5	Agee Creek Bridge	1- 40' pinned Pratt pony truss 1902 John Clark
ANDR23	209001.6	Crooked Creek Bridge	1- 45' pinned Pratt pony truss c1910
ANDR24	272000.9	Third Fork Bridge	1- 56' pinned Pratt pony truss 1908 Standard Bridge Co., Omaha

ANDREW COUNTY

INCLUDED (cont.):

ANDR25	275001.3	Muddy Creek Bridge			(replaced)
*ANDR26	276001.1	Muddy Creek Bridge	1- 70'	c1925	2-angle Camelback pony truss
*ANDR27	337000.0	Daily Bridge	1-100'	1901	pinned Pratt through truss J.H. Sparks
ANDR28	374000.1	Dillon Creek Bridge	1- 50'	c1910	pinned Pratt pony truss
ANDR29	375000.7	Dillon Creek Bridge	1- 56'	c1905	pinned Pratt pony truss
*ANDR30	377000.3	Dillon Creek Bridge	1- 60'	c1925	2-angle Pratt pony truss
*ANDR31	392002.2	Mace Creek Bridge	1-120'	c1925	2-angle Parker through truss
ANDR32	418000.8	Caples Creek Bridge	1- 45'	c1910	riveted lattice bedstead Illinois Stl. Bridge Co. (prob.)
ANDR33	422000.2	Mill Creek Bridge	1- 45'	c1910	pinned Pratt pony truss

EXCLUDED:

Pratt pony truss
013001.6 027002.8 399000.6 402000.0

Warren pony truss
K 349 311000.8 446000.0

Steel stringer

K 85	K 131	K 350	S 190	006001.0	016002.5	018000.4
025000.2	031000.2	034000.6	036000.4	045000.0	050000.4	050001.2
052000.2	054001.5	054001.7	056001.0	062000.1	063001.0	073000.5
078000.4	078000.7	091001.5	092000.9	094000.0	096003.6	098500.1
098500.2	109001.3	122003.1	125002.4	125003.0	128000.2	130000.7
133000.2	137000.7	145000.9	156000.5	181000.7	186001.9	210000.8
216001.0	220000.6	221000.2	231000.7	235000.5	235002.8	235004.8
238000.4	245001.4	249000.4	257000.1	259000.8	264000.2	266R01.0
295000.3	297R00.4	304001.3	315001.2	315001.6	321001.2	325000.7
362000.1	365000.5	366000.2	372002.8	377000.1	401001.5	404003.5
408001.0	410R02.1	411000.1	418002.5	420001.0	422000.5	423001.2
423002.5						

Steel girder
423002.3

Concrete girder
H 910 H1000 J 3R 446001.3

Concrete box culvert
G 925 K 86 308R01.0

ANDREW COUNTY

SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	3	25	0	0	28
Excluded	10	83	0	0	93
<hr/>					
	13	108	0	0	121 structures

Rosendale Bridge

ANDR01

GENERAL DATA

structure no.:	J 201	city/town:	Rosendale
county:	Andrew	feature inters.:	102 River
		cadastral grid:	S34, T61N, R35W
		highway route:	Missouri State Highway 48
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Warren pony truss; 10 concrete deck girder approach spans		
substructure:	concrete abutments and wingwalls; hammerhead spill-through piers		
span number:	1	condition:	good
span length:	100.0'	alterations:	none
total length:	577.0'	floor/decking :	concrete deck over steel stringers
roadway width:	20.0'	other features:	steel guardrails

HISTORICAL DATA

erection date:	1929-30
erection cost:	\$49,087.01
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	Fred M. Clark
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. J 201; Primary System Bridge Files - located at the Missouri Highway and Transportation Department, Jefferson City MO.
sign. rating:	43
evaluation:	NRHP non-eligible (typical example of MSHD standard truss design)

inventoried by: Michelle Crow-Dolby 27 May 1993

Platte River Bridge

ANDR02

GENERAL DATA

structure no.: K 84	city/town: 2.0 miles east of Whitesville
county: Andrew	feature inters.: Platte River
	cadastral grid: S27, T61N, R34W
	highway route: Missouri State Highway 48
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 6-panel, rigid-connected Pratt through truss, with steel stringer approach spans	
substructure: concrete abutments and wingwalls; hammerhead spill-through piers	
span number: 1	condition: good
span length: 120.0'	alterations: none
total length: 404.0'	floor/decking : concrete deck over steel stringers
roadway width: 22.0'	other features: steel angle guardrails

HISTORICAL DATA

erection date: 1932	
erection cost: \$26,574.64	
designer: Missouri State Highway Department	
fabricator : unknown	
contractor: Fred M. Clark and Sons	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. K 84; Primary System Bridge Files - located at the Missouri Highway and Transportation Department, Jefferson City MO.	
sign. rating: 41	
evaluation: NRHP non-eligible (typical example of MSHD standard truss design)	

inventoried by: Michelle Crow-Dolby 27 May 1993

102 River Bridge

ANDR03

GENERAL DATA

structure no.: L 202A	city/town: 3.3 miles east of Savannah
county: Andrew	feature inters.: 102 River
	cadastral grid: S12, T59N, R35W
	highway route: State Secondary Route E
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 12-panel, rigid-connected Warren pony truss, skewed, with polygonal upper chords; 5 steel stringer approach spans on east end

substructure: concrete abutments and wingwalls; steel pile bent piers with concrete caps; 1 solid concrete pier

span number: 1	condition: good
span length: 120.0'	alterations: none
total length: 331.0'	floor/decking : concrete over steel stringers
roadway width: 22.0'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates; vertical: I-beam; diagonal: I-beam; lateral bracing: angle; floor beam: I-beam; guardrail: 2 angles

HISTORICAL DATA

erection date: 1947

erection cost: \$69,276.30

designer: Missouri State Highway Department

fabricator : Illinois Steel Company, Chicago IL

contractor: Clark and Runquist Construction Company

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. L 202A; Primary System Bridge Files - located at the Missouri Highway and Transportation Department, Jefferson City, MO; field inspection by Mitzi Rossillon, 7 September 1990.

sign. rating: 64

evaluation: NRHP possibly eligible (well-preserved, long-span, relatively late example of uncommon structural type)

inventoried by: Clayton B. Fraser 27 May 1993

Arapahoe Creek Bridge

ANDR04

GENERAL DATA

structure no.:	007001.0	city/town:	3.8 miles north of Fillmore
county:	Andrew	feature inters.:	Arapahoe Creek
		cadastral grid:	S18, T61N, R36W
		highway route:	County Road 7
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	90.0'	alterations:	none
total length:	91.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.3'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 angles with batten plates; vertical: 2 angles with batten plates; diagonal: 2 angles with batten plates (diagonals and verticals attached with bolt through gusset plates); lateral bracing: angle attached to gusset plate, between floor beam and lower chord (2 angle knee-braces); floor beam: I-beam bolted to vertical; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	Illinois Steel Company, Chicago IL
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 007001.0; field inspection by Mitzi Rossillon, 7 September 1990.
sign. rating:	32
evaluation:	NRHP non-eligible (typical example of common structural type, largely undocumented)

Inventoried by: Michelle Crow-Dolby 27 May 1993

Arapahoe Creek Bridge

ANDR05

GENERAL DATA

structure no.:	014000.9	city/town:	3.9 miles north of Fillmore
county:	Andrew	feature inters.:	Arapahoe Creek
		cadastral grid:	S19, T61N, R36W
		highway route:	County Road 14
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss; timber stringer approach span		
substructure:	steel pile bent pier and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	45.0'	alterations:	unknown
total length:	76.0'	floor/decking :	timber deck
roadway width:	11.9'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 014000.9.

sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type, largely undocumented)

Inventoried by: Michelle Crow-Dolby 27 May 1993

Arapahoe Creek Bridge

ANDR06

GENERAL DATA

structure no.:	027000.9	city/town:	5.8 miles west of Bolckow
county:	Andrew	feature inters.:	Arapahoe Creek
		cadastral grid:	S3/10, T61N, R36W
		highway route:	County Road 27
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure: steel, rigid-connected Camelback pony truss
substructure: steel pile bent piers with timber wingwalls

span number:	1	condition:	fair
span length:	80.0'	alterations:	unknown
total length:	81.0'	floor/decking :	timber deck
roadway width:	15.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date: c1925
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 027000.9.

sign. rating: 44
evaluation: NRHP non-eligible (poorly documented example of Pratt truss subtype)

inventoried by: Michelle Crow-Dolby 27 May 1993

Arapahoe Creek Bridge

ANDR07

GENERAL DATA

structure no.:	027002.0	city/town:	5.6 miles west of Bolckow
county:	Andrew	feature inters.:	Arapahoe Creek
		cadastral grid:	S2/11, T61N, R36W
		highway route:	County Road 27
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent pier and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	57.0'	floor/decking :	timber deck
roadway width:	13.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 027002.0.

sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby 27 May 1993

Arapahoe Creek Bridge

ANDR08

GENERAL DATA

structure no.:	029000.6	city/town:	7.1 miles west of Bolckow
county:	Andrew	feature inters.:	Arapahoe Creek
		cadastral grid:	S9/10, T61N, R36W
		highway route:	County Road 29
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure: steel, 5-panel, rigid-connected Camelback pony truss
substructure: steel pile bent abutments with timber wingwalls

span number:	1	condition:	fair
span length:	100.0'	alterations:	unknown
total length:	102.0'	floor/decking :	timber deck
roadway width:	17.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date: c1925
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 029000.6.

sign. rating: 44
evaluation: NRHP non-eligible (poorly documented example of Pratt truss subtype)

inventoried by: Michelle Crow-Dolby 27 May 1993

Pedlar Creek Bridge

ANDR09

GENERAL DATA

structure no.:	040000.0	city/town:	4.5 miles northwest of Rosendale
county:	Andrew	feature inters.:	Pedlar Creek
		cadastral grid:	S25, T61N, R36W
		highway route:	County Road 40
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss
substructure: concrete-filled steel cylinder piers

span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	53.0'	floor/decking :	timber deck
roadway width:	11.6'	other features:	unknown

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 040000.0.

sign. rating: 25
evaluation: NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby 27 May 1993

Leach Mill Bridge

ANDR10

GENERAL DATA

structure no.: 048000.0	city/town: 1.8 miles west of Fillmore
county: Andrew	feature inters.: Nodaway River
	cadastral grid: S12, T60N, R37W
	highway route: County Road 48
	highway distr.: 1
	current owner: Andrew County / Holt County

STRUCTURAL DATA

superstructure: steel, 8-panel, pin-connected Pratt through truss on east end; steel, 3-panel, pin-connected Pratt pony truss on west end
substructure: concrete-filled steel cylinder piers; steel pile bent abutments

span number: 2	condition: fair
span length: 70.0'	alterations: none
total length: 130.0'	floor/decking : timber deck over steel stringers
roadway width: 15.7'	other features: through truss: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round rod with turnbuckle; floor beam: I-beam, field-bolted to verticals; guardrail: 1 angle; pony truss: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: round rod with turnbuckle; floor beam: I-beam, field-bolted to verticals; guardrail: steel channel

HISTORICAL DATA

erection date: 1913
erection cost: \$4225.00 (contract amount)
designer: unknown
fabricator : Inland Steel Company, East Chicago IN (top chord); Lackawanna Steel Company, Pittsburgh PA (verticals)
contractor: A.S. Seybert
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 048000.0; original plans "for Bridge across Nodaway River at Leach Mills" (N^o 30, 1913), and contract with A.S. Seybert (1913) - located in old bridge specifications box **Bridge Contracts and Bonds 1911**, at Andrew County Courthouse, Savannah MO; field inspection by Mark Hufstetler, 7 September 1990.

Leach Mill Bridge

sign. rating: 44
evaluation: NRHP non-eligible (typically configured, relatively late example of main-stay structural type, distinguished somewhat by its through/pony truss configuration.)

inventoried by: Michelle Crow-Dolby 27 May 1993

Bridge

ANDR11

GENERAL DATA

structure no.:	062001.0	city/town:	5.1 miles southwest of Rosendale
county:	Andrew	feature inters.:	branch of Lincoln Creek
		cadastral grid:	S14, T60N, R36W
		highway route:	County Road 62
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	45.0'	alterations:	unknown
total length:	46.0'	floor/decking :	timber deck
roadway width:	18.0'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 062001.0.

sign. rating:	20
evaluation:	NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby 27 May 1993

Lincoln Creek Bridge

ANDR12

GENERAL DATA

structure no.:	067000.4	city/town:	5.0 miles southwest of Rosendale
county:	Andrew	feature inters.:	Lincoln Creek
		cadastral grid:	S15, T60N, R36W
		highway route:	County Road 67
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	88.0'	floor/decking :	timber deck
roadway width:	15.3'	other features:	unknown

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 067000.4.

sign. rating:	25
evaluation:	NRHP determined non-eligible (typically configured, poorly documented example of common structural type)

inventoried by: Michelle Crow-Dolby 27 May 1993

Lincoln Creek Bridge

ANDR13

GENERAL DATA

structure no.: 077000.6	city/town: 1.7 miles southwest of Fillmore
county: Andrew	feature inters.: Lincoln Creek
	cadastral grid: S24, T60N, R37W
	highway route: County Road 77
	highway distr.: 1
	current owner: Andrew County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: timber pile bent abutments, wingwalls and piers	
span number: 1	condition: fair
span length: 50.0'	alterations: truss probably moved to this location
total length: 107.0'	floor/decking : timber deck
roadway width: 13.6'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 077000.6.	
sign. rating: 16	
evaluation: NRHP determined non-eligible (undistinguished example of common structural type, probably moved)	

inventoried by: Michelle Crow-Dolby 27 May 1993

Bridge

ANDR17

GENERAL DATA

structure no.:	143000.2	city/town:	1.8 miles west of Bolckow
county:	Andrew	feature inters.:	branch of 102 River
		cadastral grid:	S8, T61N, R35W
		highway route:	County Road 143
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	timber pile bent abutments, wingwalls and piers		
span number:	1	condition:	fair
span length:	45.0'	alterations:	unknown
total length:	58.0'	floor/decking :	timber deck
roadway width:	14.0'	other features:	none

HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 143000.2.

sign. rating:	26
evaluation:	NRHP non-eligible (undistinguished example of common structural type, inadequately documented)

inventoried by: Michelle Crow-Dolby 27 May 1993

Kellogg Branch Bridge

ANDR18

GENERAL DATA

structure no.: 150000.3	city/town: 2.0 miles southwest of Rosendale
county: Andrew	feature inters.: Kellogg Branch
	cadastral grid: S17, T60N, R35W
	highway route: County Road 150
	highway distr.: 1
	current owner: Andrew County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: concrete-filled steel cylinder piers	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 66.0'	floor/decking : timber deck
roadway width: 12.0'	other features: none

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 150000.3.

sign. rating: 25
evaluation: NRHP non-eligible (undistinguished example of common structural type, inadequately documented)

inventoried by: Michelle Crow-Dolby 27 May 1993

Bridge

ANDR19

GENERAL DATA

structure no.:	164001.0	city/town:	2.4 miles south of Rosendale
county:	Andrew	feature inters.:	branch of 102 River
		cadastral grid:	S22, T60N, R35W
		highway route:	County Road 164
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure:	steel, 5-panel, 2-angle, rigid-connected Camelback pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	80.0'	alterations:	none
total length:	81.0'	floor/decking :	concrete deck over steel stringers
roadway width:	15.8'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; counter: 1 angle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 steel angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN; Bethlehem Steel Company, Bethlehem PA
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 164001.0; field inspection by Mitzi Rossillon, 11 September 1990.
sign. rating:	43
evaluation:	NRHP non-eligible (undistinguished example of uncommon structural type, inadequately documented)

inventoried by: Michelle Crow-Dolby 27 May 1993

Crooked Creek Bridge

ANDR21

GENERAL DATA

structure no.: 206002.7	city/town: 1.3 miles southwest of Flag Springs
county: Andrew	feature inters.: Crooked Creek
	cadastral grid: S14, T60N, R34W
	highway route: County Road 206
	highway distr.: 1
	current owner: Andrew County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 41.0'	floor/decking : timber deck
roadway width: 11.7'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1915
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 206002.7.
sign. rating: 20
evaluation: NRHP non-eligible (undistinguished example of common structural type, inadequately documented)

inventoried by: Michelle Crow-Dolby 27 May 1993

Agee Creek Bridge

ANDR22

GENERAL DATA

structure no.:	207000.5	city/town:	2.8 miles west of Flag Springs
county:	Andrew	feature inters.:	Agee Creek
		cadastral grid:	S9, T60N, R34W
		highway route:	County Road 207
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	64.0'	floor/decking :	timber deck
roadway width:	12.0'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1901-02
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	John Clark

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 207000.5.

sign. rating:	37
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 27 May 1993

Crooked Creek Bridge

ANDR23

GENERAL DATA

structure no.:	209001.6	city/town:	0.9 mile northwest of Flag Creek
county:	Andrew	feature inters.:	Crooked Creek
		cadastral grid:	S11, T60N, R34W
		highway route:	County Road 209
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	45.0'	alterations:	unknown
total length:	71.0'	floor/decking :	timber deck
roadway width:	11.9'	other features:	none

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 209001.6.

sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 27 May 1993

Third Fork Bridge

ANDR24

GENERAL DATA

structure no.:	272000.9	city/town:	4.4 miles southeast of Cosby
county:	Andrew	feature inters.:	Third Fork of Platte River
		cadastral grid:	S16/21, T58N, R33W
		highway route:	County Road 272
		highway distr.:	1
		current owner:	Andrew County / DeKalb County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	56.0'	alterations:	unknown
total length:	90.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1908
erection cost:	\$6335.95 (multi-bridge contract)
designer:	Standard Bridge Company, Omaha NE (probable)
fabricator :	Standard Bridge Company, Omaha NE
contractor :	Standard Bridge Company, Omaha NE

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 272000.9; original bridge drawings for "Steel Bridge Across Third Fork" (N^o 2, 1908) - located in old bridge-specifications box at Andrew County Courthouse, Savannah MO; contract with Standard Bridge Company for 10 bridges in the amount of \$6335.95 (1908) - located at the Andrew County Courthouse, Savannah MO.

sign. rating:	39
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 27 May 1993

Muddy Creek Bridge

ANDR26

GENERAL DATA

structure no.: 276001.1	city/town: 2.6 miles southeast of Cosby
county: Andrew	feature inters.: Muddy Creek
	cadastral grid: S18/19, T58N, R33W
	highway route: County Road 276
	highway distr.: 1
	current owner: Andrew County

STRUCTURAL DATA

superstructure: steel, 5-panel, 2-angle, rigid-connected Camelback pony truss	
substructure: steel pile bent abutments with timber back- and wingwalls	
span number: 1	condition: fair
span length: 70.0'	alterations: unknown
total length: 71.0'	floor/decking : timber deck over steel and timber stringers
roadway width: 15.8'	other features: upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date: c1925	
erection cost: unknown	
designer: unknown	
fabricator : Inland Steel Company, East Chicago IN	
contractor : unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 276001.1; field inspection by Mitzi Rossillon, 8 September 1990.	
sign. rating: 43	
evaluation: NRHP non-eligible (undistinguished example of uncommon structural type, inadequately documented)	

inventoried by: Michelle Crow-Dolby 27 May 1993

Daily Bridge

ANDR27

GENERAL DATA

structure no.: 337000.0	city/town: 2.2 miles east of Savannah
county: Andrew	feature inters.: 102 River
	cadastral grid: S14, T59N, R35W
	highway route: County Road 337
	highway distr.: 1
	current owner: Andrew County

STRUCTURAL DATA

superstructure: steel, 6-panel, pin-connected Pratt through truss; 2 steel, 3-panel, pin-connected Pratt pony approach spans

substructure: ashlar sandstone abutments; concrete-filled steel cylinder piers

span number: 1	condition: good
span length: 100.0'	alterations: repair work to the concrete abutments with concrete on top and south side
total length: 204.0'	floor/decking: timber deck over steel and timber stringers
roadway width: 15.5'	other features: through truss: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (4 angles with continuous plate at end posts); diagonal: 2 punched rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles; portal strut: lattice; floor beam: I-beam with plates, field-bolted to vertical; guardrail: 2 angles;
	pony trusses: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: round rod with turnbuckles; lateral bracing: round rod with threaded ends; floor beam: I-beam with plates, field-bolted to vertical; guardrail: 2 angles

HISTORICAL DATA

erection date: 1901
erection cost: \$1469.00 (superstructure contract)
designer: unknown
fabricator : Lackawanna Steel Company, Pittsburgh PA
contractor : superstructure: J.H. Sparks;
substructure: W.E. Scott

Daily Bridge

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 337000.0; Andrew County Court Record, Book N: page 628 (5 August 1901), page 628 (7 August 1901)- located at the Andrew County Courthouse, Savannah MO; field inspection by Mitzi Rossillon, 7 September 1990.

sign. rating: 51

evaluation: NRHP possibly eligible (well-preserved, multiple-span example of main-stay structural type)

inventoried by: Clayton B. Fraser 27 May 1993

Dillon Creek Bridge

ANDR28

GENERAL DATA

structure no.:	374000.1	city/town:	2.2 miles north of St. Joseph
county:	Andrew	feature inters.:	Dillon Creek
		cadastral grid:	S8, T58N, R35W
		highway route:	County Road 374
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	85.0'	floor/decking :	timber deck
roadway width:	11.5'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 374000.1.

sign. rating:	25
evaluation:	NRHP non-eligible (undistinguished example of common structural type, inadequately documented)

inventoried by: Michelle Crow-Dolby 27 May 1993

Dillon Creek Bridge

ANDR29

GENERAL DATA

structure no.:	375000.7	city/town:	3.0 miles northwest of St. Joseph
county:	Andrew	feature inters.:	Dillon Creek
		cadastral grid:	S7, T58N, R35W
		highway route:	County Road 375
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	56.0'	alterations:	unknown
total length:	97.0'	floor/decking :	timber deck
roadway width:	11.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 375000.7.
sign. rating:	25
evaluation:	NRHP non-eligible (undistinguished example of common structural type, inadequately documented)

Inventoried by: Michelle Crow-Dolby 27 May 1993

Dillon Creek Bridge

ANDR30

GENERAL DATA

structure no.: 377000.3	city/town: 2.0 miles northwest of St. Joseph
county: Andrew	feature inters.: Dillon Creek
	cadastral grid: S17, T58N, R35W
	highway route: County Road 377
	highway distr.: 1
	current owner: Andrew County

STRUCTURAL DATA

superstructure: steel, 4-panel, 2-angle, rigid-connected Pratt pony truss
substructure: steel pile bent abutments with timber wingwalls

span number: 1	condition: fair
span length: 60.0'	alterations: moderate collision damage
total length: 62.0'	floor/decking : timber deck over steel stringers
roadway width: 13.7'	other features: upper chord, inclined end post, lower chord, vertical: 2 angles; diagonal: 1 angle; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: 2 angles

HISTORICAL DATA

erection date: c1925
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 377000.3; field inspection by Mitzi Rossillon, 7 September 1990.

sign. rating: 32
evaluation: NRHP non-eligible (undistinguished example of uncommon structural type, inadequately documented)

inventoried by: Michelle Crow-Dolby 27 May 1993

Mace Creek Bridge

ANDR31

GENERAL DATA

structure no.: 392002.2	city/town: 2.0 miles south of Amazonia
county: Andrew	feature inters.: Mace Creek
	cadastral grid: S12, T58N, R36W
	highway route: County Road 392
	highway distr.: 1
	current owner: Andrew County

STRUCTURAL DATA

superstructure: steel, 7-panel, 2-angle, rigid-connected Parker through truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 120.0'	alterations: none
total length: 120.0'	floor/decking : timber deck over steel stringers
roadway width: 16.0'	other features: upper chord: 2 angles; inclined end post: 2 angles with gusset plates; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles on end posts (1 angle on others); lateral bracing: round rod, top and bottom; floor beam: I-beam; guardrail: 2 angles

HISTORICAL DATA

erection date: c1925	
erection cost: unknown	
designer: unknown	
fabricator : Carnegie-Illinois, Pittsburgh PA	
contractor: unknown	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 392002.2; field inspection by Mitzi Rossillon, 7 September 1990.
sign. rating: 46	
evaluation:	NRHP non-eligible (undistinguished example of uncommon structural type, inadequately documented)

inventoried by: Michelle Crow-Dolby 27 May 1993

Caples Creek Bridge

ANDR32

GENERAL DATA

structure no.: 418000.8	city/town: 3.2 miles southwest of Savannah
county: Andrew	feature inters.: Caples Creek
	cadastral grid: S24, T59N, R36W
	highway route: County Road 418
	highway distr.: 1
	current owner: Andrew County

STRUCTURAL DATA

superstructure: steel, 2-panel, lattice truss-leg bedstead	
substructure: steel pile bent piers with timber wingwalls	
span number: 1	condition: fair
span length: 45.0'	alterations: unknown
total length: 79.0'	floor/decking : timber deck
roadway width: 13.7'	other features: outriders at center panel point

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: Illinois Steel Bridge Company, Jacksonville IL (probable)	
fabricator : Illinois Steel Bridge Company, Jacksonville IL (probable)	
contractor: Illinois Steel Bridge Company, Jacksonville IL (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 418000.8.	
sign. rating: 33	
evaluation: NRHP non-eligible (undistinguished, small-scale bridge, poorly documented)	

inventoried by: Michelle Crow-Dolby 27 May 1993

Mill Creek Bridge

ANDR33

GENERAL DATA

structure no.:	422000.2	city/town:	5.0 miles west of Savannah
county:	Andrew	feature inters.:	Mill Creek
		cadastral grid:	S10, T59N, R36W
		highway route:	County Road 422
		highway distr.:	1
		current owner:	Andrew County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	45.0'	alterations:	unknown
total length:	64.0'	floor/decking :	timber deck
roadway width:	13.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 422000.2.

sign. rating:	25
evaluation:	NRHP non-eligible (typically configured, poorly documented example of common structural type)

inventoried by: Michelle Crow-Dolby 27 May 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

102 River Bridge
MHTD: L 202A

ANDR03

DATE(S) OF CONSTRUCTION

1947

LOCATION

State Secondary Route E over 102 River; S12, T59N, R35W
3.3 miles east of Savannah; Andrew County, Missouri

USE (ORIGINAL / CURRENT)

highway bridge / highway bridge

RATING NRHP possibly eligible (score: 64)

CONDITION

good

OWNER

Missouri Highway and Transportation Department

span number: 1
span length: 120.0'
total length: 331.0'
roadway wdt.: 22.0'

superstructure: steel, 12-panel, rigid-connected Warren pony truss, skewed, with polygonal upper chords; 5 steel stringer approach spans on east end
substructure: concrete abutments and wingwalls; steel pile bent piers with concrete caps; 1 solid concrete pier
floor/decking: concrete over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates; vertical: I-beam; diagonal: I-beam; lateral bracing: angle; floor beam: I-beam; guardrail: 2 angles

Located east of Savannah, this long-span pony truss carries State Secondary Route E over the 102 River. The channel span consists of a single rigid-connected Warren pony truss, with polygonal upper chords, carried on a skew by a concrete substructure. A series of five steel stringer spans make up the east approach. The 102 River Bridge was designed in the summer of 1947 by engineers for the Missouri State Highway Department. On July 11th a contract to build the bridge was awarded to the Clark and Runquist Construction Company. The contractors used steel rolled by the Illinois Steel Company for the truss, completing the bridge later that year. Total cost: \$69,276.30. Since its completion, the 102 River Bridge has functioned in place, with no serious alterations.

The Missouri State Highway Department used riveted Warren configurations for its pony trusses almost from the time the agency developed its first bridge standards around 1920. Structurally straightforward and versatile, these ubiquitous trusses were erected by the hundreds throughout the state in span lengths ranging from 40 to 100 feet. In the early 1930s the highway department began designing Warren trusses with polygonal upper chords, a variation that was more materially conservant than the straight-chorded Warren for long-span applications. Relatively few of these Warren subtypes were built during the decade, due more to their extreme span length than to their utility. Fewer than ten of these polygonal Warren pony trusses have been identified as extant by the statewide bridge inventory. Fabricated from essentially the same drawings, their superstructures were virtually identical, other than minor details, such as the skew on the 102 River Bridge. With a construction date of 1947, this span in Andrew County is one of the last of its kind built. And with a span length of 120 feet, it is the longest.

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Daily Bridge
MHTD: 337000.0

ANDR27

DATE(S) OF CONSTRUCTION

1901

LOCATION

County Road 337 over 102 River; S14, T59N, R35W
2.2 miles east of Savannah; Andrew County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 51)

CONDITION

good

OWNER

Andrew County

span number: 1
span length: 100.0'
total length: 204.0'
roadway wdt.: 15.5'

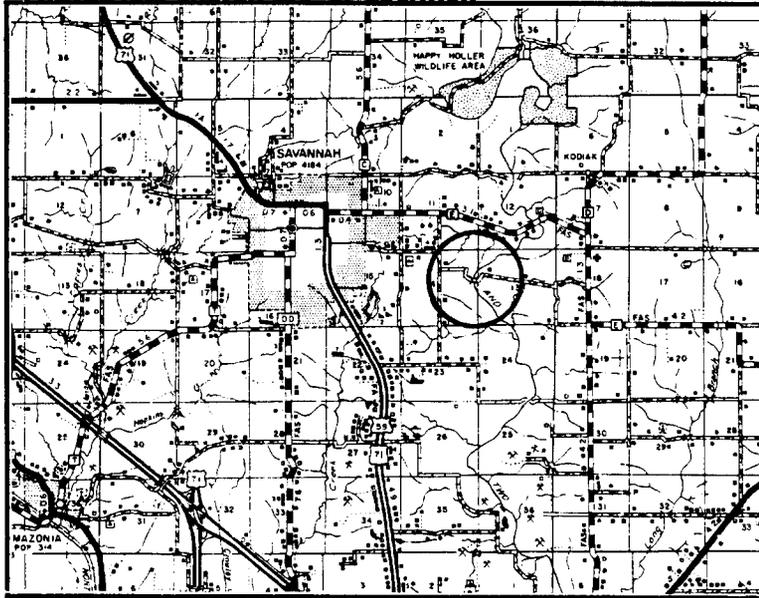
superstructure: steel, 6-panel, pin-connected Pratt through truss; 2 steel, 3-panel, pin-connected Pratt pony approach spans
substructure: ashlar sandstone abutments; concrete-filled steel cylinder piers
floor/decking: timber deck over steel and timber stringers
other features: **through truss:** upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (4 angles with continuous plate at end posts); diagonal: 2 punched rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles; portal strut: lattice; floor beam: I-beam with plates, filed-bolted to vertical; guardrail: 2 angles; **pony trusses:** upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: round rod with turnbuckles; lateral bracing: round rod with threaded ends; floor beam: I-beam with plates, field-bolted to vertical; guardrail: 2 angles

This three-span crossing of the 102 River is located some two miles east of Savannah, the Andrew County seat. A pin-connected Pratt through truss, the bridge is supported by concrete-filled cylinder piers and stone masonry abutments. On August 5, 1901, J.E. Schnitzius, the Andrew County Road and Bridge Commissioner, presented to the court several competitive proposals for a bridge to be built in Section 14, Township 59 North, Range 35 West. Low bidder at \$1469.00, J.H. Sparks was awarded the contract to furnish and erect the superstructure. Local contractor W.E. Scott was hired to build the stone substructure at \$4.75 per cubic yard. It was estimated that 150 cubic yards were needed, which brought the cost to about \$712.00. Completed that year, the Daily Bridge - as it is known locally - remains otherwise intact, as it continues to carry vehicular traffic in its rural setting. With a span length of 100 feet, a pinned Pratt configuration and a construction date of 1901, the Daily Bridge fits squarely within the mainstream of early county bridge construction in Missouri.

NAME(S) OF STRUCTURE

Daily Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 337000.0; Andrew County Court Record, Book N: page 628 (5 August 1901), page 628 (7 August 1901)- located at the Andrew County Courthouse, Savannah MO; field inspection by Mitzi Rossillon, 7 September 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

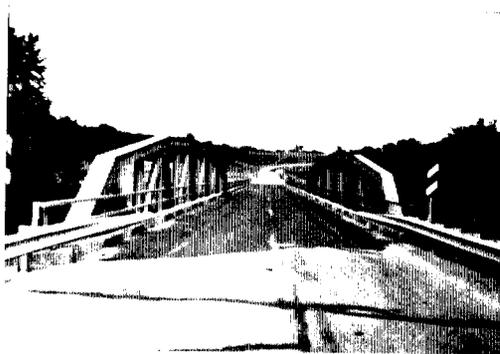
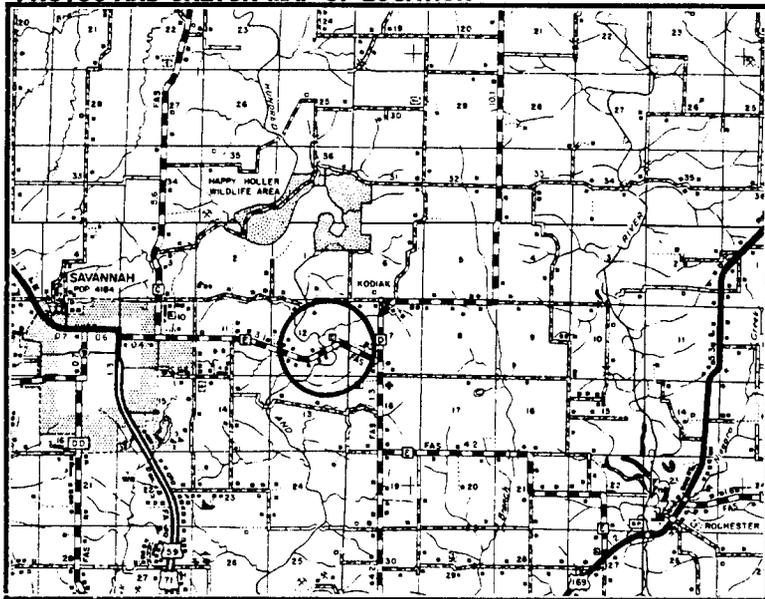
Fraserdesign, Loveland CO

DATE

27 May 1993

NAME(S) OF STRUCTURE

102 River Bridge

PHOTOS AND SKETCH MAP OF LOCATION**LOCATION MAP**TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. L 202A; Primary System Bridge Files - located at the Missouri Highway and Transportation Department, Jefferson City, MO; field inspection by Mitzi Rossillon, 7 September 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE27 May 1993

ATCHISON COUNTY

INCLUDED (cont.):

ATCH24	167000.5	Rock Creek Bridge	1- 70' c1910	pinned Pratt pony truss
*ATCH25	203001.9	Bridge	1- 60' c1925	riveted 2-angle Pratt pony truss
ATCH26	208000.4	Bridge	1- 40' c1910	pinned Pratt pony truss
ATCH27	223000.1	Rock Creek Bridge	1- 60' c1910	pinned Pratt pony truss
ATCH28	230001.3	Cow Branch Bridge	1- 50' c1910	pinned Pratt pony truss
ATCH29	231001.4	Cow Branch Bridge	1- 80' c1920	riveted Camelback pony truss
ATCH30	232000.8	Cow Branch Bridge	1- 80' c1920	riveted Camelback pony truss
*ATCH31	264001.0	Tarkio River Bridge	1-130' c1920	riveted 2-angle Parker through
*ATCH32	265000.3	Bridge	1- 65' c1925	riveted 2-angle Pratt pony truss
*ATCH33	275001.2	Deadman's Hollow Bridge	1- 60' c1925	riveted 2-angle Pratt pony truss
*ATCH34	276000.8	Deadman's Hollow Bridge	1-100' c1910	pinned Pratt through truss
*ATCH35	291000.3	Tarkio River Bridge	1- 70' c1925	riveted 2-angle Camelback pony
ATCH36	305001.7	Long Branch Bridge	1- 52' c1910	pinned Pratt pony truss
ATCH37	309001.2	Squaw Creek Bridge	1- 40' c1915	pinned Pratt pony truss
ATCH38	312001.3	Squaw Creek Bridge	1- 40' c1910	pinned Pratt pony truss
*ATCH39	343000.7	Little Tarkio Creek Bridge	1- 70' 1925	riveted 2-angle Camelback pony
*ATCH40	343001.9	Little Tarkio Creek Bridge	1- 60' c1925	riveted 2-angle Pratt pony truss
*ATCH41	357001.6	Little Tarkio Creek Bridge		(replaced)
*ATCH42	365000.6	Little Tarkio Creek Bridge	1- 50' c1925	riveted 2-angle Camelback pony
*ATCH43	371000.1	Tarkio Creek Bridge	1- 90' c1925	riveted 2-angle Camelback pony
*ATCH44	375000.2	Little Tarkio Creek Bridge	1-100' c1925	riveted 2-angle Camelback pony
ATCH45	378000.2	Little Tarkio Creek Bridge	1- 75' c1915	pinned Pratt pony truss
ATCH46	416000.4	Rock Creek Bridge	1- 70' c1910	pinned Pratt pony truss
ATCH47	438R00.1	Mill Creek Bridge	1- 80' 1928	riveted Camelback pony truss

ATCHISON COUNTY

INCLUDED (cont.):

*ATCH48	453000.4	Tarkio River Bridge	1- 90' 1920	pinned Pratt pony truss
*ATCH49	463002.5	Tarkio River Bridge	1- 90' c1910	pinned Pratt through truss
*ATCH50	481001.5	Little Tarkio Creek Bridge	2- 72' c1910	pinned Pratt pony truss
*ATCH51	481002.7	Little Tarkio Creek Bridge	1- 84' c1925	riveted 2-angle Camelback pony
*ATCH52	485000.8	Little Tarkio Creek Bridge	1-100' c1925	riveted 2-angle Camelback pony
*ATCH53	490000.9	Tarkio Creek Bridge	1- 70' c1925	riveted 2-angle Camelback pony
ATCH54	491000.8	Tarkio Creek Bridge	1- 50' c1910	pinned Pratt pony truss
*ATCH55	501000.8	Little Tarkio Creek Bridge	1-102' c1910	pinned Pratt through truss
*ATCH56	502000.7	Hickory Branch Bridge	1-100' c1925	riveted 2-angle Camelback pony
*ATCH57	512000.0	Minnesota Valley Ck. Bridge	1- 80' c1925	riveted 2-angle Camelback pony
*ATCH58	522000.9	Little Tarkio Creek Bridge	1-100' c1925	riveted 2-angle Camelback pony
*ATCH59	524000.5	East Little Tarkio Ck. Bridge	1-100' c1925	riveted 2-angle Camelback pony
*ATCH60	525000.2	Little Tarkio Creek Bridge	1- 80' c1925	riveted 2-angle Camelback pony
*ATCH61	526000.3	Little Tarkio Creek Bridge	1- 70' c1925	riveted 2-angle Camelback pony
*ATCH62	530000.5	Minnesota Valley Ck. Bridge	1- 60' c1925	riveted 2-angle Pratt pony truss
ATCH63	554000.4	Rock Creek Bridge		(replaced)

EXCLUDED:

Pratt pony truss						
082000.7	116000.2	122000.9	227000.1	290001.0	308000.2	308002.0
346001.9	369000.8	375500.1	471000.6			

Warren pony truss
J 177

Steel stringer

H 973R	J 838	K 812	S 106	T 47	T 103	T 813
T 814	X 168	X 169	X 170	X 171	X 576	042000.2
065000.7	248000.8	284000.2	315000.2	343000.4	368000.0	417000.0
421000.0						

ATCHISON COUNTY

EXCLUDED (cont.):

Steel girder

K 827A	X 172	092000.6	282000.2	360001.3	438002.2	475000.5
519000.3	554000.4					

Concrete girder

G 674R	J 23	J 219	J 221	J 222	J 232
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Concrete box culvert

G 354	G 374R	H 545	H 545R	J 839	K 826R
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Timber stringer

177001.2

SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	3	56	0	0	59
Excluded	28	23	0	0	51
	<hr/>				
	31	79	0	0	110 structures

Big Tarkio River Bridge

ATCH01

GENERAL DATA

structure no.:	G 355R	city/town:	6.5 miles southeast of Rock Port
county:	Atchison	feature inters.:	Big Tarkio River
		cadastral grid:	S3, T64N, R40W
		highway route:	U.S. Highway 59
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Pratt through truss, skewed; steel, 7-panel, rigid-connected Pratt pony truss and steel stringer approach spans		
substructure:	concrete abutments, wingwalls and piers		
span number:	1	condition:	good
span length:	140.0'	alterations:	minor repairs, 1985
total length:	348.0'	floor/decking :	concrete deck over steel stringers
roadway width:	22.0'	other features:	steel guardrails

HISTORICAL DATA

erection date:	1923
erection cost:	\$23,361.60
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	W.R. Larsen
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number G 355R; Primary System Bridges Files located at the Missouri Highway and Transportation Department, Jefferson City MO.
sign. rating:	53
evaluation:	NRHP possibly eligible (atypically configured example of MSHD truss bridge design)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Tarkio Creek Bridge

ATCH02

GENERAL DATA

structure no.:	J 176	city/town:	1.0 mile southeast of Tarkio
county:	Atchison	feature inters.:	East Fork of Tarkio Creek
		cadastral grid:	S13/24, T65N, R39W
		highway route:	U.S. Highway 136
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 5-panel, rigid-connected Warren pony truss, skewed
substructure: concrete abutments, wingwalls and piers

span number:	5	condition:	good
span length:	90.0'	alterations:	none
total length:	465.0'	floor/decking :	asphalt on concrete over steel stringers
roadway width:	20.0'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates; vertical: 4 angles with continuous plate; diagonal: 4 angles with continuous plate or 2 angles with batten plates; lateral bracing: 1 angle; guardrail: steel pipe; bridge plate: Missouri / Highway Dept / Bridge / N° J. 176 / 1930

HISTORICAL DATA

erection date: 1929-30
erection cost: \$58,989.84
designer: Missouri State Highway Department
fabricator : Inland Steel Company, East Chicago IN
contractor: George W. Condon Company

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number J 176; Primary System Bridge Files, located at the Missouri Highway and Transportation Department, Jefferson City MO; field inspection by Mark Hufstetler, 17 September 1990.

sign. rating: 61
evaluation: NRHP possibly eligible (typically configured MSHD standard truss, distinguished by its multiplicity of spans and skewed configuration)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Brownville Bridge

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number L 98; files on primary system bridges located at the Missouri Highway and Transportation Department, Jefferson City MO; Nebraska Department of Roads and Irrigation, **Twenty-Third Biennial Report, 1939-1940**, p. 23; Ned L. Ashton, "Comments on Design and Construction of Various Mississippi River Bridges," lecture to the American Society of Civil Engineers, Tri-City Section, Davenport, 6 April 1944, copy at the Iowa State Historical Society, Des Moines IA; Henry G. Schlitt, "Missouri River Bridges, Report #2," 7 November 1945, located in Bridge Department Files, Nebraska Department of Roads; **Atchison County Mail** (newspaper): "To Get Bridge at Brownville" (26 August 1938), "Brownville Bridge to Become a Reality" (28 October 1938), 2 small articles (11 November 1938), "Brownville Bridge Bid \$591,321.50" (18 November 1938), "PWA Engineer is Here for Bridge" (2 December 1938), "Weekly Bridge Pay Roll of \$6,000" (16 June 1939), "Prepare for Steel Work on Bridge" (30 June 1939), "State Highway Dept - bids on road to bridge" (25 August 1939), "Steel on Brownville Bridge Going Up" (1 September 1939), "Here is how the Brownville Bridge Looms on the Skyline" (3 November 1939), "Brownville Bridge Open Next Week" (1 December 1939), "Brownville Bridge is accomplished Fact", and advertisement regarding dedication on page 7 (8 December 1939), "Huge Throng at Bridge Dedication" (15 December 1939); field inspection by Mark Hofstetler, 18 September 1990.

sign. rating: 73

evaluation: NRHP listed (outstanding large-scale truss bridge located at an important interstate crossing)

inventoried by: Jeffrey A. Hess and Michelle Crow-Dolby 7 August 1992

Brownville Bridge

ATCH03

GENERAL DATA

structure no.:	L 98	city/town:	7.2 miles west of Rock Port / Brownville, Nebraska
county:	Atchison	feature inters.:	Missouri River / Burlington Northern Railroad
		cadastral grid:	S33, T65N, R42W
		highway route:	U.S. Highway 136
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department / Nebraska Department of Roads

STRUCTURAL DATA

superstructure:	steel, 12-panel, rigid-connected cantilevered Warren through truss; steel plate girder approach at east end; steel plate girder approach at west end		
substructure:	concrete abutments, wingwalls and 9 hammerhead spill-through piers		
span number:	2	condition:	fair
span length:	420.0'	alterations:	truss painted and concrete repairs made, 1990
total length:	1142.0'	floor/decking:	asphalt over concrete deck
roadway width:	22.5'	other features:	upper chord and inclined end post: 2 channels with cover plate and ventilated bottom; lower chord: 2 channels with cover plate and ventilated bottom; vertical: wide flange; diagonal: 2 ventilated channels with continuous plate; lateral bracing: cross-braced I-beam; bottom lateral: cross-braced back-to-back angle sections; strut: I-beams; guard-rail: steel balustrade; bridge plate: Federal Emergency / Administration of Public Works / Franklin D. Roosevelt / President of the United States / Harold L. Ickes / Administrator of Public Works / Brownville Bridge / 1939 Constructed by / Atchison County Missouri / Under Direction of the County Court / General Contractors / C.F. Lytle Company, Substructure / Bethlehem Steel Company, Superstructure / G.H. Atkinson Paving Company, Embankments

HISTORICAL DATA

erection date:	1939
erection cost:	\$708,878.54
designer:	Ash, Howard, Needles and Tammen, Kansas City MO and New York City NY
fabricator:	Bethlehem Steel Company, Bethlehem PA
contractor:	Bethlehem Steel Company, Bethlehem PA (superstructure); C.F. Lytle Company, Sioux City IA (substructure); C.W. Atkinson Paving Company, St. Joseph MO (approaches)

High Creek Bridge

ATCH04

GENERAL DATA

structure no.:	025002.0	city/town:	2.3 miles north of Watson
county:	Atchison	feature inters.:	High Creek
		cadastral grid:	S23/26, T66N, R42W
		highway route:	County Road 25
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, 2-angle, rigid-connected Camelback pony truss, with steel stringer approaches at each end		
substructure:	steel pile bent piers and abutments with concrete back- and wingwalls		
span number:	1	condition:	fair
span length:	90.0'	alterations:	none
total length:	134.0'	floor/decking :	timber deck over steel stringers
roadway width:	17.0'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 025002.0; field inspection by Mark Hufstetler, 16 September 1990.
sign. rating:	34
evaluation:	NRHP non-eligible (uncommon truss type, inadequately documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

High Creek Bridge

ATCH05

GENERAL DATA

structure no.:	029000.6	city/town:	2.4 miles northeast of Watson
county:	Atchison	feature inters.:	High Creek
		cadastral grid:	S24/25, T66N, R42W
		highway route:	County Road 29
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected 2-angle Camelback pony truss, with timber stringer approaches at each end		
substructure:	steel pile bent piers with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	90.0'	alterations:	none
total length:	125.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.0'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 029000.6; field inspection by Mark Hufstetler, 16 September 1990.
sign. rating:	34
evaluation:	NRHP non-eligible (uncommon truss type, inadequately documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

West High Creek Bridge

ATCH06

GENERAL DATA

structure no.:	039001.3	city/town:	4.7 miles southwest of Farme City
county:	Atchison	feature inters.:	West High Creek
		cadastral grid:	S10/15, T66N, R41W
		highway route:	County Road 39
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 7-panel, pin-connected Pratt through truss	condition:	poor
substructure:	timber pile bent abutments and timber back- and wingwalls	alterations:	bridge closed
span number:	1	floor/decking :	timber deck over timber stringers
span length:	105.0'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round rod with turn-buckle; lateral bracing: round rod with threaded ends; strut: I-beam (angle lattice at portal); floor beam: I-beam, U-bolted to lower chord pins
total length:	107.0'		
roadway width:	14.0'		

HISTORICAL DATA

erection date:	c1895
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 039001.3; field inspection by Mark Hufstetler, 16 September 1990.
sign. rating:	25
evaluation:	NRHP non-eligible (typically configured example of common structural type)
inventoried by:	Michelle Crow-Dolby 7 August 1993

McElroy Creek Bridge

ATCH07

GENERAL DATA

structure no.:	044000.2	city/town:	4.0 miles northeast of Watson
county:	Atchison	feature inters.:	McElroy Creek
		cadastral grid:	S20, T66N, R41W
		highway route:	County Road 44
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, 2-angle, rigid-connected Camelback pony truss		
substructure:	timber pile abutments, back- and wingwalls		
span number:	1	condition:	fair
span length:	80.0'	alterations:	none
total length:	82.0'	floor/decking:	timber deck over steel stringers
roadway width:	16.0'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 044000.2; field inspection by Mark Hufstetler, 16 September 1990.
sign. rating:	34
evaluation:	NRHP non-eligible (uncommon truss type, inadequately documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

High Creek Bridge

ATCH08

GENERAL DATA

structure no.:	045001.0	city/town:	3.0 miles northeast of Watson
county:	Atchison	feature inters.:	High Creek
		cadastral grid:	S19/30, T66N, R41W
		highway route:	County Road 45
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, 2-angle, rigid-connected Camelback pony truss; 2-span, timber stringer approaches at each end		
substructure:	main span: steel pile bent piers; approach spans: timber pile bent piers, back- and wingwalls		
span number:	1	condition:	fair
span length:	90.0'	alterations:	unknown
total length:	143.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.0'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 045001.0; field inspection by Mark Hufstetler, 16 September 1990.
sign. rating:	34
evaluation:	NRHP non-eligible (uncommon truss type, inadequately documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Rose Branch Bridge

ATCH09

GENERAL DATA

structure no.:	056000.1	city/town:	3.0 miles northwest of Rock Port
county:	Atchison	feature inters.:	Rose Branch
		cadastral grid:	S17, T65N, R41W
		highway route:	County Road 56
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	42.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.5'	other features:	steel lattice guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 056000.1.

sign. rating: 20
evaluation: NRHP non-eligible (typical example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby 7 August 1993

High Creek Bridge

ATCH10

GENERAL DATA

structure no.:	068001.1	city/town:	4.0 miles northwest of Farme City
county:	Atchison	feature inters.:	High Creek
		cadastral grid:	S13/24, T66N, R41W
		highway route:	County Road 68
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	79.0'	floor/decking:	timber deck over steel stringers
roadway width:	13.6'	other features:	steel lattice guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator:	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 068001.1.

sign. rating: 25
evaluation: NRHP non-eligible (typical, small-scale example of common structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

West High Creek Bridge

ATCH11

GENERAL DATA

structure no.:	071001.7	city/town:	3.3 miles northwest of Farme City
county:	Atchison	feature inters.:	West High Creek
		cadastral grid:	S1/36, T66/67N, R41W
		highway route:	County Road 71
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss	condition:	fair
substructure:	steel pile bent piers with timber wingwalls	alterations:	unknown
span number:	1	floor/decking :	timber deck over steel stringers
span length:	40.0'	other features:	steel lattice guardrails
total length:	56.0'		
roadway width:	13.6'		

HISTORICAL DATA

erection date:	1911
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 071001.7; Atchison County Record, Book 11: page 462 (3 May 1911) - located at the Atchison County Courthouse, Rock Port MO.
sign. rating:	29
evaluation:	NRHP non-eligible (typical, small-scale example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

West Tarkio Creek Bridge

ATCH12

GENERAL DATA

structure no.:	091001.1	city/town:	3.6 miles northwest of Westboro
county:	Atchison	feature inters.:	West Tarkio Creek
		cadastral grid:	S6/31, T66/67N, R39W
		highway route:	County Road 91
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Pratt pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	100.0'	alterations:	unknown
total length:	129.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1940
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 091001.1.

sign. rating:	26
evaluation:	NRHP non-eligible (long-span example of common structural type)

inventoried by: Michelle Crow-Dolby 7 August 1993

West Tarkio Creek Bridge

ATCH13

GENERAL DATA

structure no.:	105000.6	city/town:	7.7 miles north of Tarkio
county:	Atchison	feature inters.:	West Tarkio Creek
		cadastral grid:	S1/12, T66N, R40W
		highway route:	County Road 105
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Pratt pony truss		
substructure:	steel pile bent pier and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	100.0'	alterations:	unknown
total length:	116.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1940
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 105000.6.

sign. rating:	26
evaluation:	NRHP non-eligible (long-span example of common structural type)

inventoried by: Michelle Crow-Dolby 7 August 1993

Middle Tarkio Creek Bridge

ATCH14

GENERAL DATA

structure no.:	110000.4	city/town:	0.9 mile southwest of Westboro
county:	Atchison	feature inters.:	Middle Tarkio Creek
		cadastral grid:	S16/21, T66N, R39W
		highway route:	County Road 110
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss	condition:	fair
substructure:	timber pile abutments, wingwalls and piers	alterations:	unknown
span number:	1	floor/decking :	timber deck over steel stringers
span length:	60.0'	other features:	timber guardrails
total length:	106.0'		
roadway width:	14.0'		

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 110000.4.
sign. rating:	28
evaluation:	NRHP non-eligible (typically configured example of common structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

West Tarkio Creek Bridge

ATCH16

GENERAL DATA

structure no.:	114000.3	city/town:	5.8 miles north of Tarkio
county:	Atchison	feature inters.:	West Tarkio Creek
		cadastral grid:	S13/24, T66N, R40W
		highway route:	County Road 114
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Pratt pony truss, skewed		
substructure:	unknown		
span number:	1	condition:	fair
span length:	100.0'	alterations:	unknown
total length:	152.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date: c1940
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 114000.3.

sign. rating: 46
evaluation: NRHP non-eligible (long-span example of common structural type, somewhat noteworthy for its skewed configuration but inadequately documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Middle Tarkio Creek Bridge

ATCH17

GENERAL DATA

structure no.:	124000.5	city/town:	3.0 miles northeast of Tarkio
county:	Atchison	feature inters.:	Middle Tarkio Creek
		cadastral grid:	S1/31, T65/66N, R39W
		highway route:	County Road 124
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, 2-angle, rigid-connected Camelback pony truss, with timber stringer approach span at the west end		
substructure:	concrete abutments and piers with steel reinforcement; timber back-and wingwalls at the east end		
span number:	1	condition:	fair
span length:	90.0'	alterations:	deck recently replaced
total length:	106.0'	floor/decking:	timber deck over steel stringers
roadway width:	14.0'	other features:	upper chord, inclined end post, lower chord, vertical: 2 angles; diagonal: 1 angle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator:	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 124000.5; field inspection by Mark Hufstetler, 15 September 1990.

sign. rating:	34
evaluation:	NRHP non-eligible (uncommon truss type, inadequately documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

West Tarkio Creek Bridge

ATCH18

GENERAL DATA

structure no.:	137000.3	city/town:	2.0 miles north of Tarkio
county:	Atchison	feature inters.:	West Tarkio Creek
		cadastral grid:	S2/11, T65N, R40W
		highway route:	County Road 137
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt through truss, with steel stringer approach span at each end		
substructure:	steel pile bent piers and abutments with timber back-and wingwalls		
span number:	1	condition:	fair
span length:	90.0'	alterations:	diagonals reinforced; west approach span replaced, 1988
total length:	122.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.0'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles with continuous plate; guardrail: steel cable

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 137000.3; field inspection by Mark Hufstetler, 16 September 1990.
sign. rating:	21
evaluation:	NRHP non-eligible (typical example of common structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Rock Creek Bridge

ATCH20

GENERAL DATA

structure no.:	145R01.1	city/town:	6.2 miles northeast of Rock Port
county:	Atchison	feature inters.:	Rock Creek
		cadastral grid:	S31/32, T66N, R40W
		highway route:	County Road 145
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, 2-angle, rigid-connected Camelback pony truss, with steel stringer approach span at the north end		
substructure:	timber pile abutment - north; timber wingwalls; steel pile bent piers		
span number:	1	condition:	fair
span length:	70.0'	alterations:	truss moved to this location
total length:	82.0'	floor/decking :	timber deck over steel stringers
roadway width:	17.6'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	unknown
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 145R01.1; field inspection by Mark Hufstetler, 16 September 1990.
sign. rating:	29
evaluation:	NRHP non-eligible (uncommon truss type, inadequately documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Rock Creek Bridge

ATCH21

GENERAL DATA

structure no.:	146000.7	city/town:	5.8 miles northeast of Rock Port
county:	Atchison	feature inters.:	Rock Creek
		cadastral grid:	S31, T66N, R40W
		highway route:	County Road 146
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss	condition:	fair
substructure:	steel pile bent abutments with timber wingwalls	alterations:	bridge closed, 1990
span number:	1	floor/decking :	timber deck over steel stringers
span length:	60.0'	other features:	steel lattice guardrails
total length:	61.0'		
roadway width:	13.5'		

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 146000.7.

sign. rating: 28

evaluation: NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Rock Creek Bridge

ATCH22

GENERAL DATA

structure no.:	147000.2	city/town:	5.2 miles northeast of Rock Port
county:	Atchison	feature inters.:	Rock Creek
		cadastral grid:	S1/31, T65/66N, R41W
		highway route:	County Road 147
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss	condition:	fair
substructure:	steel pile bent pier and abutments with timber wingwalls	alterations:	unknown
span number:	1	floor/decking :	timber deck over steel stringers
span length:	80.0'	other features:	unknown
total length:	96.0'		
roadway width:	14.0'		

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 147000.2.

sign. rating: 30
evaluation: NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Rock Creek Bridge

ATCH23

GENERAL DATA

structure no.:	148R00.2	city/town:	5.0 miles northwest of Tarkio
county:	Atchison	feature inters.:	Rock Creek
		cadastral grid:	S1/2, T65N, R41W
		highway route:	County Road 148
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, rigid-connected Camelback pony truss	condition:	fair
substructure:	unknown	alterations:	truss moved to this location from Oklahoma
span number:	1	floor/decking:	timber deck over steel stringers
span length:	80.0'	other features:	steel guardrails
total length:	80.0'		
roadway width:	22.8'		

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator:	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 148R00.2.
sign. rating:	23
evaluation:	NRHP non-eligible (uncommon Pratt subtype, moved from out of state)

inventoried by: Michelle Crow-Dolby 7 August 1993

Rock Creek Bridge

ATCH24

GENERAL DATA

structure no.:	167000.5	city/town:	1.5 miles north of Rock Port
county:	Atchison	feature inters.:	Rock Creek
		cadastral grid:	S22, T65N, R41W
		highway route:	County Road 167
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	timber pile abutments, wingwalls and piers		
span number:	1	condition:	fair
span length:	70.0'	alterations:	unknown
total length:	90.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.0'	other features:	unknown

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 167000.5.

sign. rating: 28
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 7 August 1993

Bridge

ATCH25

GENERAL DATA

structure no.:	203001.9	city/town:	1.4 miles southeast of Langdon
county:	Atchison	feature inters.:	drainage ditch
		cadastral grid:	S19/30, T64N, R41W
		highway route:	County Road 203
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 4-panel, 2-angle, rigid-connected Pratt pony truss, with timber stringer approach span at east end		
substructure:	timber pile abutments; timber pile bent pier at east end		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	80.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord, inclined end post, lower chord, vertical: 2 angles; diagonal: 1 angle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Phoenix USA
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 203001.9; field inspection by Mark Hufstetler, 17 September 1990.

sign. rating:	32
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Bridge

ATCH26

GENERAL DATA

structure no.:	208000.4	city/town:	0.5 mile southwest of Rock Port
county:	Atchison	feature inters.:	drainage ditch
		cadastral grid:	S19, T64N, R41W
		highway route:	County Road 208
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss	condition:	fair
substructure:	steel pile bent piers and abutments with timber wingwalls	alterations:	none
span number:	1	floor/decking :	timber deck over steel stringers
span length:	40.0'	other features:	steel angle guardrails
total length:	89.0'		
roadway width:	13.6'		

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 208000.4.
sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 7 August 1993

Rock Creek Bridge

ATCH27

GENERAL DATA

structure no.:	223000.1	city/town:	2.5 miles south of Rock Port
county:	Atchison	feature inters.:	Rock Creek
		cadastral grid:	S9, T64N, R41W
		highway route:	County Road 223
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	118.0'	floor/decking:	timber deck over steel stringers
roadway width:	13.0'	other features:	steel lattice guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator:	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 223000.1.

sign. rating: 28
evaluation: NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Cow Branch Bridge

ATCH28

GENERAL DATA

structure no.:	230001.3	city/town:	4.1 miles east of Rock Port
county:	Atchison	feature inters.:	Cow Branch
		cadastral grid:	S29/32, T65N, R40W
		highway route:	County Road 230
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	timber pile abutments, wingwalls and piers		
span number:	1	condition:	fair
span length:	50.0'	alterations:	approach span replaced, 1988
total length:	98.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	unknown

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 230001.3.

sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Cow Branch Bridge

ATCH29

GENERAL DATA

structure no.:	231001.4	city/town:	4.8 miles east of Rock Port
county:	Atchison	feature inters.:	Cow Branch
		cadastral grid:	S32/33, T65N, R40W
		highway route:	County Road 231
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, rigid-connected Camelback pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	80.0'	alterations:	none
total length:	80.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1920
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 231001.4.
sign. rating:	38
evaluation:	NRHP non-eligible (poorly documented example of uncommon structural type)

inventoried by: Michelle Crow-Dolby 7 August 1993

Cow Branch Bridge

ATCH30

GENERAL DATA

structure no.:	232000.8	city/town:	4.5 miles northwest of Fairfax
county:	Atchison	feature inters.:	Cow Branch
		cadastral grid:	S33, T65N, R40W
		highway route:	County Road 232
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, rigid-connected Camelback pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	80.0'	alterations:	unknown
total length:	103.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1920
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 232000.8.

sign. rating: 38
evaluation: NRHP non-eligible (poorly documented example of uncommon structural type)

inventoried by: Michelle Crow-Dolby 7 August 1993

Tarkio River Bridge

ATCH31

GENERAL DATA

structure no.:	264001.0	city/town:	2.2 miles northeast of Tarkio
county:	Atchison	feature inters.:	Tarkio River
		cadastral grid:	S7/18, T65N, R39W
		highway route:	County Road 264
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 8-panel, 2-angle, rigid-connected Parker through truss, with timber stringer approach spans at each end		
substructure:	timber pile abutments and wingwalls; steel pile bent piers		
span number:	1	condition:	fair
span length:	130.0'	alterations:	none
total length:	194.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord and inclined end post: 2 angles with cover plate; lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, field-bolted to verticals; guard-rail: 2 angles

HISTORICAL DATA

erection date:	c1920
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 264001.0; field inspection by Mark Hufstetler, 16 September 1990.
sign. rating:	45
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Bridge

ATCH32

GENERAL DATA

structure no.:	265000.3	city/town:	3.0 miles northeast of Tarkio
county:	Atchison	feature inters.:	branch of Tarkio River
		cadastral grid:	S8/17, T65N, R39W
		highway route:	County Road 265
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure: steel, 4-panel, 2-angle, rigid-connected Pratt pony truss
substructure: steel pile bent abutments with concrete/timber back- and wingwalls

span number:	1	condition:	fair
span length:	65.0'	alterations:	none
total length:	66.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord, inclined end post, lower chord, vertical: 2 angles; diagonal: 1 angle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guard- rail: 2 angles

HISTORICAL DATA

erection date: c1925
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 265000.3; field inspection by Mark Hufstetler, 15 September 1990.

sign. rating: 41
evaluation: NRHP non-eligible (uncommon structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Deadman's Hollow Bridge

ATCH33

GENERAL DATA

structure no.:	275001.2	city/town:	6.0 miles northeast of Tarkio
county:	Atchison	feature inters.:	Deadman's Hollow
		cadastral grid:	S26/27, T66N, R39W
		highway route:	County Road 275
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 4-panel, 2-angle, rigid-connected Pratt pony truss		
substructure:	steel pile bent piers with concrete/timber back- and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	60.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord, inclined end post, lower chord, vertical: 2 angles; diagonal: 1 angle; floor beam: I-beam, field-bolted to verticals; guard-rail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 275001.2; field inspection by Mark Hufstetler, 15 September 1990.
sign. rating:	36
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Deadman's Hollow Bridge

ATCH34

GENERAL DATA

structure no.:	276000.8	city/town:	2.0 miles southeast of Westboro
county:	Atchison	feature inters.:	Deadman's Hollow
		cadastral grid:	S22/27, T66N, R39W
		highway route:	County Road 276
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, pin-connected Pratt through truss		
substructure:	timber pile abutments and wingwalls		
span number:	1	condition:	fair
span length:	100.0'	alterations:	none
total length:	101.0'	floor/decking :	timber deck over timber stringers
roadway width:	13.6'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 276000.8; field inspection by Mark Hufstetler, 15 September 1990.
sign. rating:	27
evaluation:	NRHP non-eligible (typically configured, inadequately documented example of common structural type)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Tarkio River Bridge

ATCH35

GENERAL DATA

structure no.:	291000.3	city/town:	5.3 miles northeast of Westboro
county:	Atchison	feature inters.:	branch of Tarkio River
		cadastral grid:	S32/33, T67N, R38W
		highway route:	County Road 291
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, 2-angle, rigid-connected Camelback pony truss, with steel stringer approach span at the south end		
substructure:	steel pile bent abutments with timber wingwalls at north end; timber pile abutment at south end		
span number:	1	condition:	fair
span length:	70.0'	alterations:	substructure partially replaced
total length:	102.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal, guardrail: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guard-rail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 291000.3; field inspection by Mark Hufstetler, 15 September 1990.
sign. rating:	32
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Long Branch Bridge

ATCH36

GENERAL DATA

structure no.:	305001.7	city/town:	6.0 miles southeast of Westboro
county:	Atchison	feature inters.:	Long Branch
		cadastral grid:	S28/29, T66N, R38W
		highway route:	County Road 305
		highway distr.:	I
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	52.0'	alterations:	unknown
total length:	71.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 305001.7.

sign. rating: 25

evaluation: NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Michelle Crow-Dolby 7 August 1993

Squaw Creek Bridge

ATCH37

GENERAL DATA

structure no.:	309001.2	city/town:	4.3 miles southeast of Westboro
county:	Atchison	feature inters.:	Squaw Creek
		cadastral grid:	S31/36, T66N, R38/39W
		highway route:	County Road 309
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	106.0'	floor/decking:	timber deck over steel stringers
roadway width:	13.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator:	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 309001.2.

sign. rating: 25
evaluation: NRHP non-eligible (typically configured, inadequately documented example of common structural type)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Squaw Creek Bridge

ATCH38

GENERAL DATA

structure no.:	312001.3	city/town:	3.4 miles southeast of Westboro
county:	Atchison	feature inters.:	Squaw Creek
		cadastral grid:	S25/26, T66N, R39W
		highway route:	County Road 312
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss	condition:	fair
substructure:	steel pile bent piers with timber wingwalls	alterations:	unknown
span number:	1	floor/decking :	timber deck over steel stringers
span length:	40.0'	other features:	steel angle guardrails
total length:	42.0'		
roadway width:	13.6'		

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 312001.3.

sign. rating: 25
evaluation: NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Michelle Crow-Dolby 7 August 1993

Little Tarkio Creek Bridge

ATCH39

GENERAL DATA

structure no.:	343000.7	city/town:	6.3 miles southeast of Tarkio
county:	Atchison	feature inters.:	Little Tarkio Creek
		cadastral grid:	S23/26, T65N, R39W
		highway route:	County Road 343
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, 2-angle, rigid-connected Camelback pony truss, with timber stringer approach at west end		
substructure:	timber pile abutments and wingwalls; steel pile bent pier		
span number:	1	condition:	fair
span length:	70.0'	alterations:	none
total length:	86.0'	floor/decking:	timber deck over steel stringers
roadway width:	15.6'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator:	unknown
contractor:	Inland Steel Company, East Chicago IN
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 343000.7; field inspection by Mark Hufstetler, 16 September 1990.
sign. rating:	32
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Little Tarkio Creek Bridge

ATCH40

GENERAL DATA

structure no.:	343001.9	city/town:	8.0 miles southeast of Tarkio
county:	Atchison	feature inters.:	Little Tarkio Creek
		cadastral grid:	S26/26, T65N, R39W
		highway route:	County Road 343
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 4-panel, 2-angle, rigid-connected Pratt pony truss		
substructure:	steel pile bent abutments with concrete back- and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	62.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord, inclined end post, lower chord, vertical: 2 angles; diagonal: 1 angle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guard-rail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	Inland Steel Company, East Chicago IN
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 343001.9; field inspection by Mark Hufstetler, 16 September 1990.
sign. rating:	28
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Little Tarkio Creek Bridge

ATCH42

GENERAL DATA

structure no.:	365000.6	city/town:	5.6 miles northeast of Fairfax
county:	Atchison	feature inters.:	Little Tarkio Creek
		cadastral grid:	S4/9, T64N, R39W
		highway route:	County Road 365
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, 2-angle, rigid-connected Camelback pony truss		
substructure:	steel pile bent piers and abutments with concrete back- and wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	substructure partially replaced
total length:	91.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	Inland Steel Company, East Chicago IN
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 365000.6; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	29
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Tarkio Creek Bridge

ATCH43

GENERAL DATA

structure no.:	371000.1	city/town:	4.0 miles southeast of Tarkio
county:	Atchison	feature inters.:	branch of Tarkio Creek
		cadastral grid:	S5/6, T64N, R39W
		highway route:	County Road 371
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, 2-angle, rigid-connected Camelback pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	90.0'	alterations:	none
total length:	91.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	Inland Steel Company, East Chicago IN
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 371000.1; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	34
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Little Tarkio Creek Bridge

ATCH44

GENERAL DATA

structure no.:	375000.2	city/town:	6.8 miles southeast of Tarkio
county:	Atchison	feature inters.:	Little Tarkio Creek
		cadastral grid:	S17/20, T64N, R39W
		highway route:	County Road 375
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, 2-angle, rigid-connected Camelback pony truss with timber stringer approach span at east end		
substructure:	steel pile bent piers; timber back- and wingwalls at west end; concrete wingwalls at east end		
span number:	1	condition:	fair
span length:	100.0'	alterations:	substructure and approaches replaced
total length:	121.0'	floor/decking:	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator:	Inland Steel Company, East Chicago IN
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 375000.2; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	30
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Little Tarkio Creek Bridge

ATGH45

GENERAL DATA

structure no.:	378000.2	city/town:	2.5 miles northeast of Fairfax
county:	Atchison	feature inters.:	branch of Little Tarkio Creek
		cadastral grid:	S18/19, T64N, R39W
		highway route:	County Road 378
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss	condition:	fair
substructure:	steel pile bent abutments with timber wingwalls	alterations:	unknown
span number:	1	floor/decking :	timber deck over steel stringers
span length:	75.0'	other features:	steel angle guardrails
total length:	75.0'		
roadway width:	13.6'		

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 378000.2.
sign. rating:	28
evaluation:	NRHP non-eligible (typically configured, poorly documented example of common structural type)

inventoried by: Michelle Crow-Dolby 7 August 1993

Bridge

ATCH46

GENERAL DATA

structure no.:	416000.4	city/town:	4.5 miles southwest of Rock Port
county:	Atchison	feature inters.:	Rock Creek Drainage Ditch
		cadastral grid:	S20/21, T64N, R41W
		highway route:	County Road 416
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	70.0'	alterations:	deck and stringers replaced, 1981
total length:	103.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.5'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 416000.4.

sign. rating:	28
evaluation:	NRHP non-eligible (typically configured, poorly documented example of common structural type)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Mill Creek Bridge

ATCH47

GENERAL DATA

structure no.:	438R00.1	city/town:	3.8 miles northwest of Milton
county:	Atchison	feature inters.:	Mill Creek
		cadastral grid:	S2, T63N, R41W
		highway route:	County Road 438
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, rigid-connected Camelback pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	80.0'	alterations:	bridge moved to this site from Oklahoma, 1981
total length:	80.0'	floor/decking :	unknown
roadway width:	22.5'	other features:	unknown

HISTORICAL DATA

erection date:	1928
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 438R00.1.
sign. rating:	24
evaluation:	NRHP non-eligible (uncommon structural type, moved from out of state)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Tarkio River Bridge

ATCH48

GENERAL DATA

structure no.:	453000.4	city/town:	0.5 mile east of Milton
county:	Atchison	feature inters.:	Tarkio River
		cadastral grid:	S9/10, T63N, R40W
		highway route:	County Road 453
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss, with timber stringer approach spans		
substructure:	timber pile abutments and wingwalls; steel pile bent piers		
span number:	1	condition:	fair
span length:	90.0'	alterations:	none
total length:	128.0'	floor/decking :	timber deck over timber stringers
roadway width:	18.0'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with batten plates; diagonal: 2 punched rectangular eyebars; counter: 2 square eyebars with turnbuckles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	Bethlehem Steel Company, Bethlehem PA; Inland Steel Company, East Chicago IN
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 453000.4; Atchison County Court Record, Book 13: page 192 (3 May 1920) - located at the Atchison County Courthouse, Rock Port MO; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	32
evaluation:	NRHP non-eligible (undistinguished structure, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Tarkio River Bridge

ATCH49

GENERAL DATA

structure no.:	463002.5	city/town:	1.4 miles northeast of Milton
county:	Atchison	feature inters.:	branch of Tarkio River
		cadastral grid:	S3, T63N, R40W
		highway route:	County Road 463
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, pin-connected Pratt through truss, with timber stringer approach span at south end		
substructure:	timber pile abutments with timber back- and wingwalls; steel pile bent pier		
span number:	1	condition:	fair
span length:	90.0'	alterations:	none
total length:	106.0'	floor/decking :	timber deck over timber stringers
roadway width:	13.6'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles with continuous plate; floor beam: I-beam, field-bolted to verticals; guardrail: steel lattice

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	Cambria Steel Company, Pittsburgh PA
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 463002.5; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	27
evaluation:	NRHP non-eligible (typical example of common structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Little Tarkio Creek Bridge

ATCH50

GENERAL DATA

structure no.:	481001.5	city/town:	4.3 miles southeast of Milton
county:	Atchison	feature inters.:	Little Tarkio Creek
		cadastral grid:	S7/18, T63N, R39W
		highway route:	County Road 481
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss;	steel, 3-panel, pin-connected Pratt pony truss	
substructure:	steel pile bent piers with timber wingwalls		
span number:	2	condition:	fair
span length:	72.0'	alterations:	unknown
total length:	unknown	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 481001.5.
sign. rating:	30
evaluation:	NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Little Tarkio Creek Bridge

ATCH51

GENERAL DATA

structure no.:	481002.7	city/town:	4.5 miles southeast of Milton
county:	Atchison	feature inters.:	East Fork of Little Tarkio Creek
		cadastral grid:	S18/19, T63N, R39W
		highway route:	County Road 481
		highway distr.:	1
		current owner:	Atchison County / Holt County

STRUCTURAL DATA

superstructure:	steel, 6-panel, 2-angle, rigid-connected Camelback pony truss, skewed, with steel stringer approach spans at each end		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	84.0'	alterations:	none
total length:	84.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; guardrail: main span: steel cable, approach spans: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 481002.7; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	36
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Little Tarkio Creek Bridge

ATCH52

GENERAL DATA

structure no.:	485000.8	city/town:	3.6 southwest of Daleview
county:	Atchison	feature inters.:	East Fork of Little Tarkio Creek
		cadastral grid:	S9/10, T63N, R39W
		highway route:	County Road 485
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, 2-angle, rigid-connected Camelback pony truss with timber stringer approach span at the north end		
substructure:	timber abutment and wingwalls - south; steel pile bent piers		
span number:	1	condition:	fair
span length:	100.0'	alterations:	none
total length:	124.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 485000.8; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	41
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Tarkio Creek Bridge

ATCH53

GENERAL DATA

structure no.:	490000.9	city/town:	4.5 miles southeast of Fairfax
county:	Atchison	feature intera.:	branch of Tarkio Creek
		cadastral grid:	S29/32, T64N, R39W
		highway route:	County Road 490
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, 2-angle, rigid-connected Camelback pony truss with timber stringer approach span at the west end		
substructure:	timber pile bent abutments and wingwalls; steel pile bent pier		
span number:	1	condition:	fair
span length:	70.0'	alterations:	none
total length:	95.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.5'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 490000.9; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	34
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Tarkio Creek Bridge

ATCH54

GENERAL DATA

structure no.:	491000.8	city/town:	4.2 miles northwest of Daleview
county:	Atchison	feature inters.:	branch of Tarkio Creek
		cadastral grid:	S28/29, T64N, R39W
		highway route:	County Road 491
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent pier and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	bridge closed, 1987
total length:	69.0'	floor/decking:	timber deck over steel stringers
roadway width:	13.6'	other features:	steel lattice guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator:	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 491000.8.

sign. rating: 25
evaluation: NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Little Tarkio Creek Bridge

ATCH55

GENERAL DATA

structure no.:	501000.8	city/town:	1.6 miles northwest of Daleview
county:	Atchison	feature inters.:	East Fork of Little Tarkio Creek
		cadastral grid:	S26/35, T64N, R39W
		highway route:	County Road 501
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, pin-connected Pratt through truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	102.0'	alterations:	none
total length:	104.0'	floor/decking :	timber deck over steel stringers
roadway width:	15.6'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beams: I-beam, field-bolted to verticals; strut: 2 angles with continuous plate; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	Cambria Steel Company, Pittsburgh PA; Illinois Steel Company, Chicago IL
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 501000.8; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	27
evaluation:	NRHP non-eligible (typical example of common structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Hickory Branch Bridge

ATCH56

GENERAL DATA

structure no.:	502000.7	city/town:	1.0 mile southwest of Dotham
county:	Atchison	feature inters.:	Hickory Branch
		cadastral grid:	S25, T64N, R39W
		highway route:	County Road 502
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, 2-angle, rigid-connected Camelback pony truss, with timber stringer approach span at each end		
substructure:	timber pile abutments and wingwalls; steel pile bent piers		
span number:	1	condition:	fair
span length:	100.0'	alterations:	none
total length:	139.0'	floor/decking:	timber deck over steel stringers
roadway width:	13.5'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator:	Inland Steel Company, East Chicago IN
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 502000.7; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	41
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Minnesota Valley Creek Bridge

ATCH57

GENERAL DATA

structure no.:	512000.0	city/town:	2.3 miles southeast of Daleview
county:	Atchison	feature inters.:	Minnesota Valley Creek
		cadastral grid:	S4/9, T63N, R38W
		highway route:	County Road 512
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, 2-angle, rigid-connected Camelback pony truss		
substructure:	steel pile bent abutments with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	80.0'	alterations:	diagonals spliced with steel plates
total length:	82.0'	floor/decking:	timber deck over steel stringers
roadway width:	15.6'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator:	Inland Steel Company, East Chicago IN
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 512000.0; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	36
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

Little Tarkio Creek Bridge

ATCH58

GENERAL DATA

structure no.:	522000.9	city/town:	1.0 mile south of Daleview
county:	Atchison	feature inters.:	East Fork of Little Tarkio Creek
		cadastral grid:	S1/12, T63N, R39W
		highway route:	County Road 522
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, 2-angle, rigid-connected Camelback pony truss		
substructure:	steel pile bent abutments with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	100.0'	alterations:	none
total length:	103.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 522000.9; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	41
evaluation:	NRHP determined non-eligible (uncommon structural type, poorly documented)
inventoried by:	Michelle Crow-Dolby 7 August 1993

East Little Tarkio Creek Bridge

ATCH59

GENERAL DATA

structure no.:	524000.5	city/town:	1.5 miles west of Daleview
county:	Atchison	feature inters.:	East Fork of Little Tarkio Creek
		cadastral grid:	S2/35, T63/64N, R39W
		highway route:	County Road 524
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 6-panel, 2-angle, rigid-connected Camelback pony truss		
substructure:	steel pile bent abutments with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	100.0'	alterations:	none
total length:	102.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 524000.5; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	41
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Little Tarkio Creek Bridge

ATCH60

GENERAL DATA

structure no.:	525000.2	city/town:	2.4 miles southwest of Daleview
county:	Atchison	feature inters.:	East Fork of Little Tarkio Creek
		cadastral grid:	S11, T63N, R39W
		highway route:	County Road 525
		highway distr.:	1
		current owner:	Atchison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, 2-angle, rigid-connected Camelback pony truss	condition:	fair
substructure:	steel pile bent abutments; timber back- and wingwalls at the south end	alterations:	none
span number:	1	floor/decking:	timber deck over steel stringers
span length:	80.0'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles
total length:	83.0'		
roadway width:	15.6'		

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator:	Inland Steel Company, Chicago IN
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 525000.2; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	36
evaluation:	NRHP determined non-eligible (uncommon structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 August 1993

Minnesota Valley Creek Bridge

ATCH62

GENERAL DATA

structure no.:	530000.5	city/town:	3.0 miles southeast of Daleview
county:	Atchison / Holt	feature inters.:	Minnesota Valley Creek
		cadastral grid:	S18/19, T63N, R38W
		highway route:	County Road 530
		highway distr.:	1
		current owner:	Atchison County / Holt County

STRUCTURAL DATA

superstructure:	steel, 4-panel, 2-angle, rigid-connected Pratt pony truss, with timber stringer approach spans at each end		
substructure:	timber pile abutments with back- and wingwalls; steel pile bent piers		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	89.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.8'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 530000.5; field inspection by Mark Hufstetler, 17 September 1990.
sign. rating:	34
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 August 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Big Tarkio River Bridge
MHTD: G 355R

ATCH01

DATE(S) OF CONSTRUCTION

1923

LOCATION

U.S. Highway 59 over Big Tarkio River; S3, T64N, R40W
6.5 miles southeast of Rock Port; Atchison County, Missouri

USE (ORIGINAL / CURRENT)

highway bridge / highway bridge

RATING NRHP possibly eligible (score: 53)

CONDITION

good

OWNER

Missouri Highway and Transportation Department

span number: 1
span length: 140.0'
total length: 348.0'
roadway wdt.: 22.0'

superstructure: steel, 6-panel, rigid-connected Pratt through truss, skewed; steel, 7-panel, rigid-connected Pratt pony truss and steel stringer approach spans
substructure: concrete abutments, wingwalls and piers
floor/decking: concrete deck over steel stringers
other features: steel guardrails

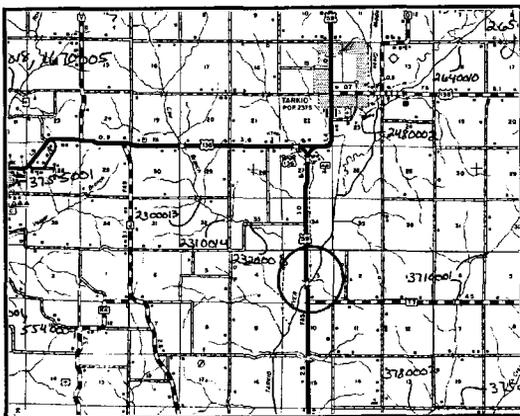
The Big Tarkio River Bridge is located on U.S. Highway 59 southeast of Rock Port. This medium-span steel truss features a 140-foot-span riveted Pratt through truss, supported on a skew by concrete piers. The structure was designed late in 1922 by engineers for the Missouri State Highway Commission and built in 1923 by W.R. Larsen for a total cost of about \$23,000. Since its completion, the Big Tarkio River Bridge has carried vehicular traffic in essentially unaltered condition.

The Missouri State Highway Department used riveted Pratt configurations for its through trusses almost from the time the agency developed its first bridge standards in the late 1910s. Structurally straightforward and versatile, these ubiquitous trusses were erected by the hundreds throughout the state in span lengths ranging from 100 to 200 feet. The Big Tarkio River Bridge is distinguished among these for its relatively early construction and its skewed configuration.

NAME(S) OF STRUCTURE

Big Tarkio River Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number G 355R; Primary System Bridges Files located at the Missouri Highway and Transportation Department, Jefferson City MO.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

15 January 1994

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Tarkio Creek Bridge
MHTD: J 176

ATCH02

DATE(S) OF CONSTRUCTION

1929-30

LOCATION

U.S. Highway 136 over East Fork of Tarkio Creek; S13/24, T65N, R39W
1.0 mile southeast of Tarkio; Atchison County, Missouri

USE (ORIGINAL / CURRENT)

highway bridge / highway bridge

RATING NRHP possibly eligible (score: 61)

CONDITION

good

OWNER

Missouri Highway and Transportation Department

span number: 5
span length: 90.0'
total length: 465.0'
roadway wdt.: 20.0'

superstructure: steel, 5-panel, rigid-connected Warren pony truss, skewed
substructure: concrete abutments, wingwalls and piers
floor/decking: asphalt on concrete over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates; vertical: 4 angles with continuous plate; diagonal: 4 angles with continuous plate or 2 angles with batten plates; lateral bracing: 1 angle; guardrail: steel pipe; bridge plate: Missouri / Highway Dept / Bridge / N° J. 176 / 1930

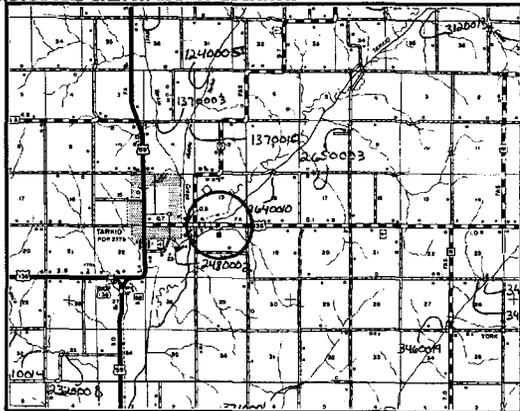
Located a mile southeast of Tarkio, this five-span pony truss carries U.S. Highway 136 over Tarkio Creek. The channel spans are rigid-connected Warren pony trusses, carried on a skew by a concrete substructure. The Tarkio Creek Bridge was designed in the summer of 1929 by engineers for the Missouri State Highway Department. On September 17th a contract to build the bridge was awarded to the George W. Condon Company. Completed the following year, the Tarkio Creek Bridge has since functioned in place with no serious alterations.

The Missouri State Highway Department used riveted Warren configurations for its pony trusses almost from the time the agency developed its first bridge standards around 1920. Structurally straightforward and versatile, these ubiquitous trusses were erected by the hundreds throughout the state in span lengths ranging from 40 to 100 feet. The Tarkio Creek Bridge is distinguished among Missouri's Warren trusses as among the few skewed examples of this mainstay structural type.

NAME(S) OF STRUCTURE

Tarkio Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number J 176; Primary System Bridge Files, located at the Missouri Highway and Transportation Department, Jefferson City MO; field inspection by Mark Hufstetler, 17 September 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

15 January 1994

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Brownville Bridge (Missouri River Bridge)

MHTD: L 98

ATCH03

DATE(S) OF CONSTRUCTION

1939

LOCATION

U.S. Highway 136 over Missouri River/Burlington Northern RR; S33, T65N, R42W
7.2 miles west of Rock Port / Brownville, Nebraska; Atchison County, Missouri

USE (ORIGINAL / CURRENT)

highway toll bridge / highway bridge

RATING NRHP listed (score: 73)

CONDITION

fair

OWNER

Missouri Highway and Transportation Department / Nebraska Department of Roads

span number: 2
span length: 420.0'
total length: 1142.0'
roadway wdt.: 22.5'

superstructure: steel, 12-panel, rigid-connected cantilevered Warren through truss; steel plate girder approach at east end; steel plate girder approach at west end
substructure: concrete abutments, wingwalls and 9 hammerhead spill-through piers
floor/decking: asphalt over concrete deck
other features: upper chord and inclined end post: 2 channels with cover plate and ventilated bottom; lower chord: 2 channels with cover plate and ventilated bottom; vertical: wide flange; diagonal: 2 ventilated channels with continuous plate; lateral bracing: cross-braced I-beam; bottom lateral: cross-braced back-to-back angle sections; strut: I-beams; guardrail: steel balustrade; bridge plate: Federal Emergency / Administration of Public Works / Franklin D. Roosevelt / President of the United States / Harold L. Ickes / Administrator of Public Works / Brownville Bridge / 1939 Constructed by / Atchison County Missouri / Under Direction of the County Court / General Contractors / C.F. Lytle Company, Substructure / Bethlehem Steel Company, Superstructure / G.H. Atkinson Paving Company, Embankments

The Atchison County Board of Supervisors appears to have initiated the planning for this bridge over the Missouri River in early 1938, by securing the prestigious firm, Ash, Howard, Needles and Tammen (AHNT) of Kansas City and New York City, for design construction supervision. AHNT modeled the proposed structure on another two-span, continuous truss bridge they had built over the Missouri River at South Omaha in 1934-35, changing details and construction methods of the previous design to improve aesthetics and reduce cost. Whereas, according to one engineer, a "hodge podge of deck trusses, girder spans and trestles" were used for the South Omaha approach spans, AHNT used only continuous deck girder approaches for the Brownville Bridge.

Twelve contractors from across the country submitted competitive construction bids to the Atchison County Court on 14 November 1938. Low-bidders awarded contracts by the county included C.W. Lytle Company of Sioux City, Iowa, for the substructure (\$241,997.00), Bethlehem Steel Company of Pennsylvania for the superstructure (\$287,547.00), and the St. Joseph-based C.W. Atkinson Company for the massive structure's approach work (\$61,777.50). Rock Port's Atchison County Mail reported on the large numbers of construction company representatives who inundated the town for the bridge's multiple contract letting. "Some idea of the numbers attracted to Rock

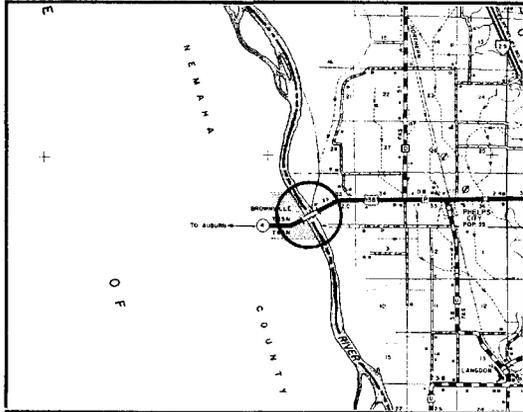
Port may be gained by the fact that on Sunday evening Hotel Opp was filled to capacity and cots had to be placed in its recreation room to accommodate some of the overflow. Others were sent to the hotel at Tarkio and many were housed in various private homes about town." Once the much sought after contracts were let, work on the substructure began early in 1939. Although delayed several weeks owing to high flood waters, the steel bridge, configured as a rigid-connected cantilevered Warren through truss, was completed by its one-year deadline in October 1939. Total project costs: \$708,878.54. The 11 December 1939 dedication, which officially opened the bridge to traffic, was punctuated by local band performances, speakers, and a festival-like atmosphere.

The Missouri Highway Commission provided \$50,000.00, and the Federal Emergency Administration of Public Works provided a grant totaling \$311,580.00. Atchison County issued bonds for the balance of the costs, and operated the bridge as a toll crossing to repay its bondholders and cover maintenance expenses. In 1945 the Nebraska Department of Roads and Irrigation reported that the bridge was in "excellent" condition, although the concrete in the piers was cracking and in need of immediate repair. No major alterations appear to have been made to the structure, which is now owned and maintained by the State of Missouri as a "free" crossing.

The Brownville Bridge is significant for its innovative pier design and association with the prestigious engineering firm of Ash, Howard, Needles and Tammen. Spanning a major river and replacing a ferry site reportedly in use since before the Civil War, the bridge also played an important role in the development of the region's economy and transportation facilities. Perhaps most significantly, the Brownville Bridge could not have been completed without grants from the Public Works Administration, which illustrates the federal government's pivotal role in developing Missouri's transportation infrastructure during the Depression Era.

NAME(S) OF STRUCTURE

Brownville Bridge (Missouri River Bridge)

PHOTOS AND SKETCH MAP OF LOCATION**LOCATION MAP**TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number L 98; files on primary system bridges located at the Missouri Highway and Transportation Department, Jefferson City MO; Nebraska Department of Roads and Irrigation, **Twenty-Third Biennial Report, 1939-1940**, p. 23; **Atchison County Mail** (newspaper): "To Get Bridge at Brownville" (26 August 1938), "Brownville Bridge to Become a Reality" (28 October 1938), 2 small articles (11 November 1938), "Brownville Bridge Bid \$591,321.50" (18 November 1938), "PWA Engineer is Here for Bridge" (2 December 1938), "Weekly Bridge Pay Roll of \$6,000" (16 June 1939), "Prepare for Steel Work on Bridge" (30 June 1939), "State Highway Dept - bids on road to bridge" (25 August 1939), "Steel on Brownville Bridge Going Up" (1 September 1939), "Here is how the Brownville Bridge Looms on the Skyline" (3 November 1939), "Brownville Bridge Open Next Week" (1 December 1939), "Brownville Bridge is Accomplished Fact," (8 December 1939), "Huge Throng at Bridge Dedication" (15 December 1939); field inspection by Mark Hofstetler, 18 September 1990.

INVENTORIED BY

Jeffrey A. Hess and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 August 1993

BUCHANAN COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
BUCH01	G 856	Garrettsburg Bridge	(replaced)
BUCH02	K 33	102 River Bridge	3- 90' 1932 riveted Warren pony truss Gerard Knutson
*BUCH03	K 590	U.S. Highway 59 Overpass	7-115' 1937 steel plate through girder Sandy Hites
BUCH04	K 673	CRI&P Railroad Overpass	1-150' 1937 riveted Parker through truss List and Clark Construc. Co.
*BUCH05	K 697R	Atchison Bridge	2-474' 1937 riveted cantilever through truss Wisconsin B&I Company et al.
BUCH06	S 787	Castile Creek Bridge	1-100' 1934 riveted Warren pony truss Martin Wunderlich
BUCH07	T 365	Platte River Bridge	1-120' 1934 riveted Pratt through truss Martin Wunderlich
*BUCH08	W 444	Bee Creek Bridge	1- 50' 1922 concrete Luten arch Topeka Bridge and Constr. Co.
BUCH09	Y 10	Brady Branch Bridge	3- 28' 1922 concrete Luten arch Topeka Bridge and Constr. Co.
BUCH10	Z 336	Bridge	1- 21' c1925 concrete arch culvert
*BUCH11	045001.6	102 River Bridge	1-150' c1925 riveted Pratt through truss
*BUCH12	045001.8	Platte River Bridge	(replaced)
*BUCH13	055003.3	Platte River Bridge	(replaced)
BUCH14	056000.5	Platte River Bridge	1-100' c1925 riveted Pratt through truss
*BUCH15	056002.1	James Branch Bridge	(replaced)
BUCH16	063000.2	Bridge	(replaced)
*BUCH17	064000.6	Bridge	(replaced)
BUCH18	083000.8	Muddy Creek Bridge	(replaced)
BUCH19	104000.1	Malden Creek Bridge	1- 60' 1902 pinned Pratt half-hip pony truss John H. Sparks, St. Joseph MO
*BUCH20	116000.4	Jenkins Creek Bridge	1- 50' 1917 concrete filled spandrel arch
*BUCH21	117001.7	Malden Creek Bridge	(replaced)
BUCH22	117002.3	Wolf Pen Creek Bridge	1- 27' c1920 concrete arch culvert
BUCH23	160001.1	Whitehead Creek Bridge	(replaced)
BUCH24	166000.0	Whitehead Creek Bridge	(replaced)
BUCH25	174000.2	Whitehead Creek Bridge	1- 73' c1910 pinned Pratt pony truss
*BUCH26	187001.4	Contrary Creek Bridge	1-133' 1888 pinned Pratt through truss Wrought Iron Bridge Co., Canton

BUCHANAN COUNTY

INCLUDED (cont.):

*BUCH27	191000.2	Bridge	1- 24'	concrete filled spandrel arch
			1916	
*BUCH28	206000.4	Contrary Creek Bridge	1-100'	pinned Pratt through truss
			c1890	
BUCH29	224001.6	Bridge		(replaced)
*BUCH30	273001.7	Contrary Creek Bridge	1- 80'	2-angle Camelback pony truss
			c1930	St. Joseph Struct. Steel (prob.)
*BUCH31	290000.8	Willis Bridge		(replaced)
*BUCH32	297000.5	Bee Creek Bridge		(replaced)
BUCH33	352000.8	Malden Creek Bridge		(replaced)
BUCH34	359000.2	Malden Creek Bridge		(replaced)
*BUCH35	386500.1	Cook Road Bridge	1- 80'	pinned Pratt through truss
			c1900	
*BUCH36	386500.2	Great Western Viaduct	3- 35'	concrete deck girder
			1927	Carrother and Huggins
*BUCH37	386500.6	Eleventh Street Underpass	1- 42'	concrete deck girder
			1927	
BUCH38	386500.8	Eighteenth Street Underpass	1- 30'	concrete deck girder
			1927	Whitham Const. Co., St. Joseph
*BUCH39	386501.4	King Hill Underpass	3- 44'	concrete deck girder
			1927	Whitham Const. Co., St. Joseph
*BUCH40	386501.9	Parker Road Bridge	1- 80'	pinned Pratt through truss
			c1885	
*BUCH41	386502.2	Bridge	1- 32'	concrete filled spandrel arch
			c1925	
*BUCH42	386502.5	Whitehead Creek Bridge	1- 30'	concrete open spandrel arch
			1927	W.A. Abrams, Unionville MO

EXCLUDED:

Pratt pony truss
074001.7

Warren pony truss
G 307

Steel stringer

F 171R1	K 681	K 794	L 246	L 351	S 115	S 445
S 785	S 786	S 866	T 233	T 861	X 598	X 599
001500.1	068000.8	099001.8	124500.1	159001.7	235000.3	270001.6
271000.8	380500.2	386501.8	386502.8			

Concrete girder

H 345R	K 475	386500.3	386501.5
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BUCHANAN COUNTY

EXCLUDED (cont.):

Concrete slab

F 172R 386500.4 218000.6 380500.1 386502.4 386502.5

Concrete box culvert

F 837R G 373R G 381 G 857 K 525 K 526 K 547R
 K 726 L 6 X 600 X 753 323000.1

Timber stringer

093000.6

SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	8	10	9	0	27
Excluded	28	11	8	0	47
	<hr/>				
	36	21	17	0	74 structures

102 River Bridge

BUCH02

GENERAL DATA

structure no.:	K 33	city/town:	5.0 miles northeast of St. Joseph
county:	Buchanan	feature inters.:	102 River
		cadastral grid:	S6, T57N, R34W
		highway route:	State Highway 6
		highway distr.:	3
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	steel, 5-panel, rigid-connected Warren pony truss		
substructure:	concrete abutments, wingwalls and piers		
span number:	3	condition:	good
span length:	90.0'	alterations:	none
total length:	279.0'	floor/decking :	concrete deck over steel stringers
roadway width:	22.0'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1932
erection cost:	\$32,004.76
designer:	Missouri State Highway Department
fabricator :	unknown
contractor :	Gerard Knutson
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number K 33; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO.
sign. rating:	46
evaluation:	NRHP non-eligible (typically configured, multiple-span example of MSHD standardized truss design)

inventoried by: Clayton B. Fraser 18 August 1992

U.S. Highway 59 Overpass

BUCH03

GENERAL DATA

structure no.: K 590	city/town: 1.5 miles northeast of Kenmoor
county: Buchanan	feature inters.: Burlington and Rock Island Railroads
	cadastral grid: S15/22, T56N, R36W
	highway route: U.S Highway 59
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel plate through girder with continuous ends	
substructure: concrete abutments, wingwalls and spill-through piers	
span number: 5; 1; 1	condition: good
span length: 115';100';80'	alterations: none
total length: 770.0'	floor/decking : concrete deck over steel stringers
roadway width: 24.0'	other features: girder: steel plate with 2-angle flanges, top and bottom, and cover plate; stringer: I-beam; bottom lateral bracing: angle; bridge plate: MISSOURI HIGHWAY DEPT BRIDGE No K-590 1936

HISTORICAL DATA

erection date: 1936-37	
erection cost: \$113,402.60	
designer: Missouri State Highway Department	
fabricator : unknown	
contractor : Sandy Hites	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number K 590; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; Tenth Biennial Report of the State Highway Commission of Missouri , 1935-36, pages 55-56, 263; field inspection by Lon Johnson, 7 August 1990.
sign. rating: 60	
evaluation:	NRHP possibly eligible (one of longest examples in state of this MSHD beam bridge type)

inventoried by: Clayton B. Fraser 26 March 1992

CRI&P Railroad Overpass

BUCH04

GENERAL DATA

structure no.:	K 673	city/town:	0.7 mile southwest of Rushville
county:	Buchanan	feature inters.:	Chicago, Rock Island and Pacific Railroad
		cadastral grid:	S15, T55N, R37W
		highway route:	U.S. Highway 59
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	steel, 8-panel, rigid-connected Parker through truss, with six concrete deck girder approach spans		
substructure:	concrete abutments, wingwalls and piers		
span number:	1	condition:	good
span length:	150.0'	alterations:	none
total length:	407.0'	floor/decking :	concrete deck over steel stringers
roadway width:	24.0'	other features:	concrete guardrails

HISTORICAL DATA

erection date:	1937
erection cost:	\$46,515.66
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	List and Clark Construction Company
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number K 673; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO.
sign. rating:	41
evaluation:	NRHP non-eligible (typically configured example of MSHD standard truss design)

inventoried by: Clayton B. Fraser 26 March 1992

Atchison Bridge

BUCH05

GENERAL DATA

structure no.: K 697R	city/town: Winthrop MO / Atchison KS
county: Buchanan	feature inters.: Missouri River
	cadastral grid: S19, T55N, R37W
	highway route: U.S. Highway 59
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 11-panel, rigid-connected cantilever through truss with 2, 7-panel, rigid-connected Warren deck trusses at each end; 3 steel stringer and 3 steel girder approach spans at west end; 4 steel stringer approach spans at east end

substructure: concrete abutments, wingwalls and piers

span number: 2	condition: excellent
span length: 474.0'	alterations: repainted
total length: 1339.0'	floor/decking : grating covered by corrugated steel and asphalt over steel stringers
roadway width: 24.0'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing or 4 built-up angles with cover and batten plates or 2 channels with double lacing; lower chord: 4 built-up angles with lacing and batten plates; vertical: 4 angles with batten plate or 2 channels with lacing or 4 built-up angles with double lacing or 2 channels with batten plates; hip vertical: 4 built up angles with lacing; diagonal: 4 angles with batten plates or 4 built up angles with lacing or 2 channels with batten plates or 4 built-up angles with batten plates; lateral bracing: 2 angles with lacing at top; 2 angles at bottom; strut: 2 angles with lacing; floor beam: I-beams; guardrail: steel balustrade

HISTORICAL DATA

erection date: 1937-38
erection cost: \$669,838.96
designer: Missouri State Highway Commission Bureau of Bridges
fabricator : unknown
contractor: Wisconsin Bridge and Iron Company, Milwaukee WI

Atchison Bridge

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number K 697R; Files on primary system bridges located at the Missouri Highway and Transportation Department, Jefferson City MO; "Free Bridge at Atchison Now in Use," **St. Joseph News-Press**, 3 July 1938; **Eleventh Biennial Report of the State Highway Commission of Missouri**, 1937-38, pp. 168 and 170; "Z.E. Jackson Led Nine-Year Battle to Obtain Bridge," **St. Joseph News-Press**, 17 September 1938; J.A.L. Waddell, **De Pontibus: A Pocket-Book for Bridge Engineers** (New York: John Wiley & Sons, 1898); field inspection by Lon Johnson, 14 September 1990.

sign. rating: 62

evaluation: NRHP possibly eligible (well-preserved example of a long-span highway bridge)

inventoried by: Clayton B. Fraser 26 March 1992

Castile Creek Bridge

BUCH06

GENERAL DATA

structure no.:	S 787	city/town:	7.1 miles southeast of Agency
county:	Buchanan	feature inters.:	Castile Creek
		cadastral grid:	S24/25, T55N, R34W
		highway route:	State Highway 116
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	steel, 5-panel, rigid-connected Warren pony truss, with steel stringer approach spans		
substructure:	concrete abutments, wingwalls and piers		
span number:	1	condition:	good
span length:	100.0'	alterations:	none
total length:	404.0'	floor/decking :	concrete deck over steel stringers
roadway width:	24.0'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1933-34
erection cost:	\$29,732.70
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	Martin Wunderlich
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number S 787; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO.
sign. rating:	43
evaluation:	NRHP non-eligible (typically configured, long-span example of MSHD standardized truss design)

Inventoried by: Clayton B. Fraser 26 March 1992

Platte River Bridge

BUCH07

GENERAL DATA

structure no.:	T 365	city/town:	7.2 miles southeast of Agency
county:	Buchanan	feature inters.:	Platte River
		cadastral grid:	S25/26, T55N, R34W
		highway route:	Missouri State Highway 116
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	steel, rigid-connected Pratt through truss, with 12 steel stringer approach spans		
substructure:	concrete abutments, wingwalls and piers		
span number:	1	condition:	good
span length:	120.0'	alterations:	none
total length:	663.0'	floor/decking :	concrete deck over steel stringers
roadway width:	20.0'	other features:	steel guardrails

HISTORICAL DATA

erection date:	1933-34
erection cost:	\$39,469.70
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	Martin Wunderlich
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number T 365; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO.
sign. rating:	41
evaluation:	NRHP non-eligible (typically configured example of MSHD standardized truss design)

Inventoried by: Clayton B. Fraser 26 March 1992

Bee Creek Bridge

BUCH08

GENERAL DATA

structure no.:	W 444	city/town:	6.0 miles southwest of Agency
county:	Buchanan	feature inters.:	Bee Creek
		cadastral grid:	S21/24, T55N, R34/35W
		highway route:	State Supplementary Route Y
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	concrete Luten arch		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	good
span length:	50.0'	alterations:	none
total length:	90.0'	floor/decking :	concrete deck over earth fill
roadway width:	18.0'	other features:	roadway cantilevered on tapered brackets from arch spandrels; guardrails: cast concrete balusters with solid concrete bulkheads

HISTORICAL DATA

erection date:	1922
erection cost:	\$12,478.00
designer:	Topeka Bridge and Construction Company, Topeka KS (based on patent by Daniel B. Luten, Indianapolis IN)
fabricator :	none
contractor:	Topeka Bridge and Construction Company, Topeka KS
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number W 444; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; Third Biennial Report of the Missouri State Highway Commission , 1921-22, page 129; field inspection by Lon Johnson, 14 September 1990.
sign. rating:	58
evaluation:	NRHP possibly eligible (well-preserved example of patented concrete bridge type)

inventoried by: Clayton B. Fraser 26 March 1992

Brady Branch Bridge

BUCH09

GENERAL DATA

structure no.: Y 10	city/town: 7.9 miles southwest of Agency
county: Buchanan	feature inters.: branch of Bee Creek
	cadastral grid: S31/36, T55N, R34/35W
	highway route: State Supplementary Route Y
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: concrete Luten arch	
substructure: concrete abutments and wingwalls	
span number: 3	condition: good
span length: 28.0'	alterations: none
total length: 97.0'	floor/decking : concrete deck over earth fill
roadway width: 18.2'	other features: roadway cantilevered on tapered brackets from arch spandrels; guardrails: cast concrete balusters with solid concrete bulkheads

HISTORICAL DATA

erection date: 1922	
erection cost: \$10,957.50	
designer: Topeka Bridge and Construction Company, Topeka KS (based on patent by Daniel B. Luten, Indianapolis IN)	
fabricator : none	
contractor: Topeka Bridge and Construction Company, Topeka KS	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number Y 10; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; Third Biennial Report of the Missouri State Highway Commission, 1921-22, page 129.	
sign. rating: 58	
evaluation: NRHP possibly eligible (well-preserved, multiple-span example of patented concrete arch design)	

inventoried by: Clayton B. Fraser 26 March 1992

Bridge

BUCH10

GENERAL DATA

structure no.:	Z 336	city/town:	0.4 mile east of Easton
county:	Buchanan	feature inters.:	unnamed stream
		cadastral grid:	S29/30, T57N, R33W
		highway route:	State Supplementary Route N
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	concrete arch culvert, skewed		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	good
span length:	21.0'	alterations:	unknown
total length:	24.0'	floor/decking :	concrete deck
roadway width:	29.6'	other features:	concrete guardrails with incised panels

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	Missouri State Highway Department
fabricator :	none
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number Z 336.
sign. rating:	18
evaluation:	NRHP non-eligible (undistinguished example of small-scale concrete culvert design)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

102 River Bridge

BUCH11

GENERAL DATA

structure no.: 045001.6	city/town: 6.0 miles east of St. Joseph
county: Buchanan	feature inters.: 102 River
	cadastral grid: S17, T57N, R34W
	highway route: County Road 45
	highway distr.: 1
	current owner: Buchanan County

STRUCTURAL DATA

superstructure: steel, 7-panel, rigid-connected Pratt through truss, skewed, with steel stringer and steel girder approach spans	
substructure: stone abutments; steel pile bent and concrete piers	
span number: 1	condition: good
span length: 150.0'	alterations: unknown
total length: 219.0'	floor/decking : timber deck over steel stringers
roadway width: 17.0'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 4 angles with batten plates; vertical: 2 channels with lacing; diagonal: 2 or 4 angles with batten plates; lateral bracing: 1 angles; strut: 2 angles, braced; portal strut: angle lattice; floor beam: I-beam, field-bolted to vertical; guardrail: steel pipe

HISTORICAL DATA

erection date: c1925	
erection cost: unknown	
designer: Missouri State Highway Department	
fabricator : Illinois Steel Company, Chicago IL	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 045001.6; field inspection by Lon Johnson, 7 September 1990.	
sign. rating: 42	
evaluation: NRHP non-eligible (uncommon skewed configuration, but inadequately documented and typically designed)	

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Platte River Bridge

BUCH14

GENERAL DATA

structure no.:	056000.5	city/town:	9.4 miles southeast of St. Joseph
county:	Buchanan	feature inters.:	Third Fork of the Platte River
		cadastral grid:	S35, T57N, R34W
		highway route:	County Road 56
		highway distr.:	1
		current owner:	Buchanan County

STRUCTURAL DATA

superstructure:	steel, 5-panel, rigid-connected Pratt through truss, with steel stringer approach spans		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	good
span length:	100.0'	alterations:	unknown
total length:	136.0'	floor/decking :	timber deck over steel stringers
roadway width:	18.2'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 056000.5.
sign. rating:	23
evaluation:	NRHP non-eligible (typically configured, inadequately documented example of MSHD standard truss design)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Malden Creek Bridge

BUCH19

GENERAL DATA

structure no.:	104000.1	city/town:	4.3 miles southeast of Easton
county:	Buchanan	feature inters.:	Malden Creek
		cadastral grid:	S16, T56N, R33W
		highway route:	County Road 104
		highway distr.:	1
		current owner:	Buchanan County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt half-hip pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	62.0'	floor/decking :	timber deck
roadway width:	16.0'	other features:	timber guardrails

HISTORICAL DATA

erection date:	1902
erection cost:	\$389.00
designer:	unknown
fabricator :	unknown
contractor:	John H. Sparks, St. Joseph MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 104000.1; Buchanan County Court Record, Book 30: n.p. (6 August 1902) - located at Buchanan County Courthouse, St. Joseph MO.

sign. rating:	40
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Jenkins Creek Bridge

BUCH20

GENERAL DATA

structure no.:	116000.4	city/town:	7.3 miles southeast of Easton
county:	Buchanan / Clinton	feature inters.:	Jenkins Creek
		cadastral grid:	S33/34, T56N, R33W
		highway route:	County Road 116
		highway distr.:	1
		current owner:	Buchanan County

STRUCTURAL DATA

superstructure:	concrete filled spandrel arch, skewed		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	good
span length:	50.0'	alterations:	none
total length:	82.0'	floor/decking :	concrete deck over earth fill
roadway width:	20.3'	other features:	concrete guardrails with incised panels

HISTORICAL DATA

erection date:	1917
erection cost:	unknown
designer:	unknown
fabricator :	none
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 116000.4; Buchanan County Court Record, Book 27: page 451 (20 May 1895); Book 28: page 309 (24 February 1899); Book 40: n.p. (10 May 1917) - located Buchanan County Courthouse, St. Joseph MO; field inspection by Lon Johnson, 7 September 1990.

sign. rating:	34
evaluation:	NRHP non-eligible (inadequately documented example of concrete arch construction)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Wolf Pen Creek Bridge

BUCH22

GENERAL DATA

structure no.:	117002.3	city/town:	7.0 miles southeast of Easton
county:	Buchanan	feature inters.:	Wolf Pen Creek
		cadastral grid:	S33, T56N, R33W
		highway route:	County Road 117
		highway distr.:	1
		current owner:	Buchanan County

STRUCTURAL DATA

superstructure:	concrete arch culvert	condition:	fair
substructure:	concrete abutments and wingwalls	alterations:	none
span number:	1	floor/decking :	concrete deck over earth fill
span length:	27.0'	other features:	guardrails: none
total length:	30.0'		
roadway width:	19.8'		

HISTORICAL DATA

erection date:	c1920
erection cost:	unknown
designer:	unknown
fabricator :	none
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 117002.3.

sign. rating:	18
evaluation:	NRHP non-eligible (poorly documented, small-scale example of concrete arch construction)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Whitehead Creek Bridge

BUCH25

GENERAL DATA

structure no.:	174000.2	city/town:	3.8 miles southeast of St. Joseph
county:	Buchanan	feature inters.:	Whitehead Creek
		cadastral grid:	S34/35, T57N, R35W
		highway route:	County Road 174
		highway distr.:	1
		current owner:	Buchanan County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	73.0'	alterations:	unknown
total length:	108.0'	floor/decking :	timber deck
roadway width:	14.0'	other features:	steel lattice guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 174000.2.

sign. rating:	28
evaluation:	NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Contrary Creek Bridge

BUCH26

GENERAL DATA

structure no.: 187001.4	city/town: 1.0 mile south of St. Joseph
county: Buchanan	feature inters.: Contrary Creek
	cadastral grid: S7, T56N, R35W
	highway route: County Road 187
	highway distr.: 1
	current owner: Buchanan County

STRUCTURAL DATA

superstructure: wrought iron, 9-panel, pin-connected Pratt through truss	
substructure: steel pile bent abutments with timber back- and wingwalls	
span number: 1	condition: fair
span length: 133.0'	alterations: truss moved to this location
total length: 135.0'	floor/decking : timber deck over timber stringers
roadway width: 14.0'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with double lacing (2 round eyerods at hip); diagonal: 2 square eyebars; counter: round rod with turnbuckle; strut: steel pipe; portal strut: lattice with decorative cast iron knee braces; lateral bracing: round rod; floor beam: tapered "fishtail" girder, U-bolted to lower chord pin; guardrail: timber

HISTORICAL DATA

erection date: 1888 (possible)	
erection cost: unknown	
designer: Wrought Iron Bridge Company, Canton OH (probable)	
fabricator : Wrought Iron Bridge Company, Canton OH (probable)	
contractor: Wrought Iron Bridge Company, Canton OH (probable)	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 187001.4; Buchanan County Court Record, Book 20: page 205 (17 October 1888), located at the Buchanan County Courthouse, St. Joseph MO; field inspection by Lon Johnson, 7 September 1990.
sign. rating: 45	
evaluation:	NRHP possibly eligible (early example of early wrought iron truss construction, moved to this location)

inventoried by: Clayton B. Fraser 26 March 1992

Bridge

BUCH27

GENERAL DATA

structure no.: 191000.2 city/town: 6.5 miles southwest of St. Joseph
county: Buchanan feature inters.: branch of Horseshoe Lake
cadastral grid: S12, T56N, R36W
highway route: County Road 191
highway distr.: 1
current owner: Buchanan County

STRUCTURAL DATA

superstructure: concrete filled spandrel arch
substructure: concrete abutments and wingwalls

span number: 1 condition: good
span length: 24.0' alterations: roadway widened on cantilevered concrete
total length: 30.0' brackets
roadway width: 24.0' floor/decking : concrete deck
other features: concrete guardrails with incised rectangular
panels

HISTORICAL DATA

erection date: 1916
erection cost: unknown
designer: unknown
fabricator : none
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory
and Appraisal: Structure Number 191000.2; field inspection by Lon
Johnson, 7 September 1990.

sign. rating: 26
evaluation: NRHP non-eligible (small-scale example of concrete arch construction,
poorly documented and substantially altered)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Contrary Creek Bridge

BUCH28

GENERAL DATA

structure no.:	206000.4	city/town:	7.0 miles southwest of St. Joseph
county:	Buchanan	feature inters.:	Contrary Creek drainage ditch
		cadastral grid:	S3/4, T56N, R36W
		highway route:	County Road 206
		highway distr.:	1
		current owner:	Buchanan County

STRUCTURAL DATA

superstructure: steel or iron, pin-connected Pratt through truss, with timber stringer approach span

substructure: timber pile bent abutments, wingwalls and pier

span number:	1	condition:	fair
span length:	100.0'	alterations:	truss moved to this location
total length:	120.0'	floor/decking :	timber deck over timber stringers
roadway width:	12.1'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 round eyerods at hip); diagonal: 2 rectangular eyebars; counter: 2 round eyerods with turnbuckles; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing, knee braced; portal strut: lattice with knee braces; floor beam: tapered "fishtail" plate girder, U-bolted to lower chord pins; guardrail: steel pipe

HISTORICAL DATA

erection date: c1890
erection cost: unknown
designer: unknown
fabricator : Oliver Iron Works
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 206000.4; field inspection by Lon Johnson, 7 September 1990.

sign. rating: 22
evaluation: NRHP non-eligible (evidently an early example of pinned truss construction, but poorly documented and moved to this location)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Contrary Creek Bridge

BUCH30

GENERAL DATA

structure no.: 273001.7	city/town: 2.0 miles northeast of DeKalb
county: Buchanan	feature inters.: Contrary Creek
	cadastral grid: S1/12, T55N, R36W
	highway route: County Road 273
	highway distr.: 1
	current owner: Buchanan County

STRUCTURAL DATA

superstructure: steel, 5-panel, rigid-connected, two-angle Camelback pony truss
substructure: concrete abutments and wingwalls

span number: 1	condition: good
span length: 80.0'	alterations: none
total length: 80.0'	floor/decking : concrete deck over steel stringers
roadway width: 18.0'	other features: upper chord and inclined end post: 2 angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: steel angle

HISTORICAL DATA

erection date: c1930
erection cost: unknown
designer: St. Joseph Structural Steel Works, St. Joseph MO (probable)
fabricator : St. Joseph Structural Steel Works, St. Joseph MO (probable);
Inland Steel Company, East Chicago IN
contractor : St. Joseph Structural Steel Works, St. Joseph MO (probable)
references: Missouri Highway and Transportation Department, Structure Inventory
and Appraisal: Structure Number 273001.7; field inspection by Lon
Johnson, 7 September 1990.
sign. rating: 34
evaluation: NRHP non-eligible (poorly documented example of relatively uncommon
truss configuration)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Cook Road Bridge

BUCH35

GENERAL DATA

structure no.:	386500.1	city/town:	east edge of St. Joseph
county:	Buchanan	feature inters.:	102 River
		cadastral grid:	S30/31, T58N, R34W
		highway route:	vacated Cook Road
		highway distr.:	1
		current owner:	City of St. Joseph

STRUCTURAL DATA

superstructure:	steel, 6-panel, pin-connected Pratt through truss, with steel and timber stringer approach spans		
substructure:	steel pile bent piers with timber wingwalls; timber pile bent piers		
span number:	1	condition:	poor
span length:	80.0'	alterations:	bridge burned; deck and stringers removed
total length:	190.0'	floor/decking :	decking and stringers removed
roadway width:	13.0'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 square eyebars at hip); diagonal: 2 rectangular eyebars; counter: 2 square eyebars with turnbuckles; lateral bracing: round rod with threaded ends; strut: I-beam; portal strut: lattice; floor beam: I-beam, U-bolted to lower chord pin; guardrail: steel angle

HISTORICAL DATA

erection date:	c1900
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 386500.1; field inspection by Lon Johnson, 13 September 1990.
sign. rating:	16
evaluation:	NRHP non-eligible (poorly preserved, poorly documented example of common structural type)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Eleventh Street Underpass

BUCH37

GENERAL DATA

structure no.:	386500.6	city/town:	St. Joseph
county:	Buchanan	feature inters.:	Maple Leaf Parkway (Corby Parkway)
		cadastral grid:	S5, T57N, R35W
		highway route:	Eleventh Street
		highway distr.:	1
		current owner:	City of St. Joseph

STRUCTURAL DATA

superstructure:	concrete deck girder, skewed		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	good
span length:	42.0'	alterations:	none
total length:	45.0'	floor/decking :	concrete deck
roadway width:	36.0'	other features:	concrete guardrails with slotted cutouts; arched spandrel girders with corbeled, faux arch rings

HISTORICAL DATA

erection date:	1927
erection cost:	unknown
designer:	W.G. Fowler
fabricator :	none
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 386500.6; **Your Parks - How They Were Developed and How They Are Operated**, typewritten manuscript, located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; Special Ordinances Book 20, located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; scrapbooks containing newspaper clippings of park-system development, located at Department of Parks and Recreation, Krug Park, St. Joseph MO; field inspection by Lon Johnson, 14 September 1990.

sign. rating:	53
evaluation:	NRHP possibly eligible (well-preserved structure built as part of citywide parks project)

Inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Eighteenth Street Underpass

BUCH38

GENERAL DATA

structure no.:	386500.8	city/town:	St. Joseph
county:	Buchanan	feature inters.:	Maple Leaf Boulevard (Corby Boulevard)
		cadastral grid:	S4, T57N, R35W
		highway route:	Eighteenth Street
		highway distr.:	1
		current owner:	City of St. Joseph

STRUCTURAL DATA

superstructure:	concrete deck girder with curved haunches		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	good
span length:	30.0'	alterations:	none
total length:	33.0'	floor/decking :	concrete deck
roadway width:	30.3'	other features:	paneled concrete parapet

HISTORICAL DATA

erection date:	1927
erection cost:	unknown
designer:	W.G. Fowler
fabricator :	none
contractor :	Whitham Construction Company, St. Joseph MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 386500.8; Your Parks - How They Were Developed and How They Are Operated , typewritten manuscript, located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; Special Ordinances Book 20, located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; scrapbooks containing newspaper clippings of park-system development, located at Department of Parks and Recreation, Krug Park, St. Joseph MO.
sign. rating:	41
evaluation:	NRHP non-eligible (well-preserved, relatively minor structure built as part of citywide parks project)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

King Hill Overpass

BUCH39

GENERAL DATA

structure no.:	386501.4	city/town:	St. Joseph
county:	Buchanan	feature inters.:	Second Street
		cadastral grid:	S29/32, T57N, R35W
		highway route:	King Hill Spur Drive / Mansfield Road
		highway distr.:	1
		current owner:	City of St. Joseph

STRUCTURAL DATA

superstructure:	concrete 3-rib deck girder		
substructure:	concrete abutments and wingwalls; concrete spill-through piers		
span number:	3	condition:	good
span length:	44.0'	alterations:	none
total length:	77.0'	floor/decking :	concrete deck
roadway width:	30.1'	other features:	concrete guardrails with square concrete balusters; arched spandrel girders; roadway cantilevered from spandrels on curved concrete brackets

HISTORICAL DATA

erection date:	1927
erection cost:	\$18,000.00
designer:	E.R. Grant
fabricator :	none
contractor:	Whitham Construction Company, St. Joseph MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 386501.4; <i>Your Parks - How They Were Developed and How They Are Operated</i> , typewritten manuscript, located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; Special Ordinances Book 20, located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; scrapbooks containing newspaper clippings of park-system development, located at Department of Parks and Recreation, Krug Park, St. Joseph MO; field inspection by Lon Johnson, 14 September 1990.
sign. rating:	51
evaluation:	NRHP possibly eligible (well-preserved structure built as part of citywide parks project)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Parker Road Bridge

BUCH40

GENERAL DATA

structure no.:	386501.9	city/town:	St. Joseph
county:	Buchanan	feature inters.:	Contrary Creek
		cadastral grid:	S1/6, T56N, R35/36W
		highway route:	Parker Road
		highway distr.:	1
		current owner:	City of St. Joseph

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt through truss
substructure: timber pile bent abutments and wingwalls

span number:	1	condition:	fair
span length:	80.0'	alterations:	truss perhaps moved
total length:	82.0'	floor/decking :	timber deck over timber stringers
roadway width:	14.0'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 round eyebars at hip); diagonal: 2 rectangular eyebars; counter: 1 square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles with kneebraces; portal strut: lattice; floor beam: plate girder, U-bolted to lower chord pin; guard-rail: timber

HISTORICAL DATA

erection date: c1885
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 386501.9; field inspection by Lon Johnson, 14 September 1990.

sign. rating: 27
evaluation: NRHP non-eligible (evidently early example of pinned truss construction, poorly documented and perhaps moved to this location)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Bridge

BUCH41

GENERAL DATA

structure no.:	386502.2	city/town:	St. Joseph
county:	Buchanan	feature inters.:	branch of Platte River
		cadastral grid:	S19, T57N, R34W
		highway route:	Riverside Terrace
		highway distr.:	1
		current owner:	City of St. Joseph

STRUCTURAL DATA

superstructure:	concrete filed spandrel arch		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	good
span length:	32.0'	alterations:	none
total length:	42.0'	floor/decking :	concrete deck over earth fill
roadway width:	18.5'	other features:	concrete guardrails with incised rectangular panels

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	none
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 386502.2.

sign. rating:	18
evaluation:	NRHP non-eligible (small-scale example of concrete bridge construction)

inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

Whitehead Creek Bridge

BUCH42

GENERAL DATA

structure no.:	386502.5	city/town:	St. Joseph
county:	Buchanan	feature inters.:	North Branch of Whitehead Creek
		cadastral grid:	S22, T57N, R35W
		highway route:	Brice Street
		highway distr.:	1
		current owner:	City of St. Joseph

STRUCTURAL DATA

superstructure:	concrete, two-rib, open spandrel arch		
substructure:	concrete abutments and wingwalls; hammerhead spill-through piers		
span number:	1	condition:	good
span length:	30.0'	alterations:	none
total length:	54.0'	floor/decking :	concrete deck
roadway width:	20.1'	other features:	decorative wrought iron guardrails with paneled concrete bulkheads

HISTORICAL DATA

erection date:	1927
erection cost:	\$7892.00
designer:	W.G. Fowler
fabricator :	none
contractor:	W.A. Abrams, Unionville MO

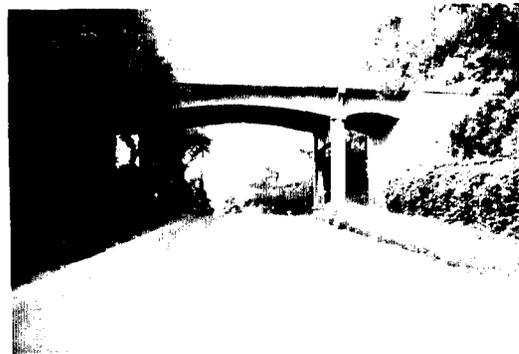
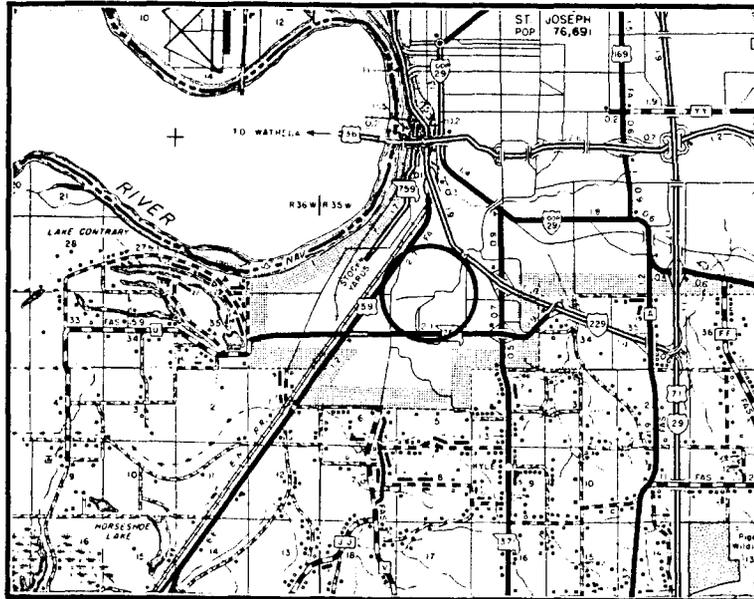
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 386502.5; **Your Parks - How They Were Developed and How They Are Operated**, typewritten manuscript located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; Special Ordinances Book 20, located at the City Clerk's Office, St. Joseph City Hall, St. Joseph MO; scrapbooks containing newspaper clippings of park-system development, located at Department of Parks and Recreation, Krug Park, St. Joseph MO; field inspection by Lon Johnson, 14 September 1990; field inspection by Lon Johnson, 14 September 1990.

sign. rating:	46
evaluation:	NRHP possibly eligible (well-preserved, unusually configured structure built as part of citywide parks project)

Inventoried by: Clayton Fraser and Lon Johnson 26 March 1992

NAME(S) OF STRUCTURE
King Hill Overpass

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 386501.4; *Your Parks - How They Were Developed and How They Are Operated*, typewritten manuscript, located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; Special Ordinances Book 20, located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; scrapbooks containing newspaper clippings of park-system development, located at Department of Parks and Recreation, Krug Park, St. Joseph MO; field inspection by Lon Johnson, 14 September 1990.

INVENTORIED BY
Clayton B. Fraser

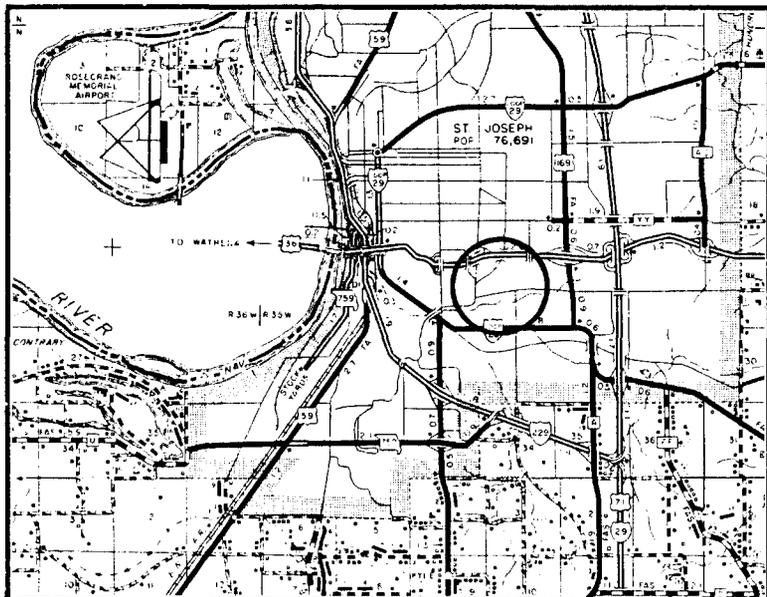
AFFILIATION
Fraserdesign, Loveland CO

DATE
26 March 1992

NAME(S) OF STRUCTURE

Whitehead Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 386502.5; **Your Parks - How They Were Developed and How They Are Operated**, typewritten manuscript located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; Special Ordinances Book 20, located at the City Clerk's Office, St. Joseph City Hall, St. Joseph MO; scrapbooks containing newspaper clippings of park-system development, located at Department of Parks and Recreation, Krug Park, St. Joseph MO; field inspection by Lon Johnson, 14 September 1990; field inspection by Lon Johnson, 14 September 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

26 March 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Whitehead Creek Bridge
MHTD: 386502.5

BUCH42

DATE(S) OF CONSTRUCTION

1927

LOCATION

Brice Street over North Branch of Whitehead Creek; S22, T57N, R35W
St. Joseph; Buchanan County, Missouri

USE (ORIGINAL / CURRENT)

city street bridge / city street bridge

RATING NRHP possibly eligible (score: 46)

CONDITION

good

OWNER

City of St. Joseph

span number: 1	superstructure: concrete, two-rib, open spandrel arch
span length: 30.0'	substructure: concrete abutments and wingwalls; hammerhead spill-through piers
total length: 54.0'	floor/decking: concrete deck
roadway wdt.: 20.1'	other features: decorative wrought iron guardrails with paneled concrete bulkheads

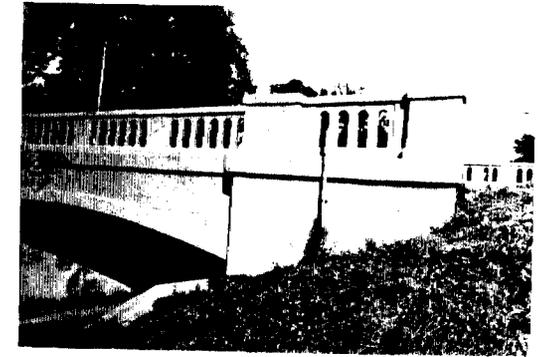
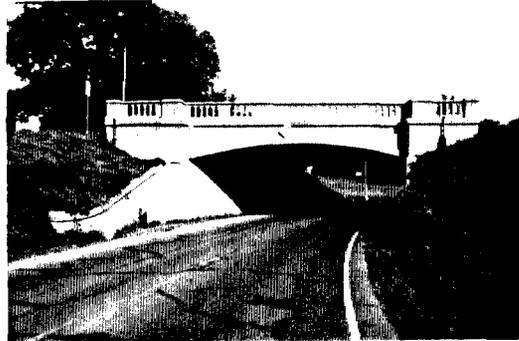
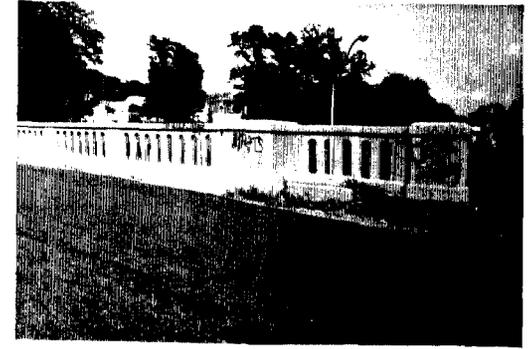
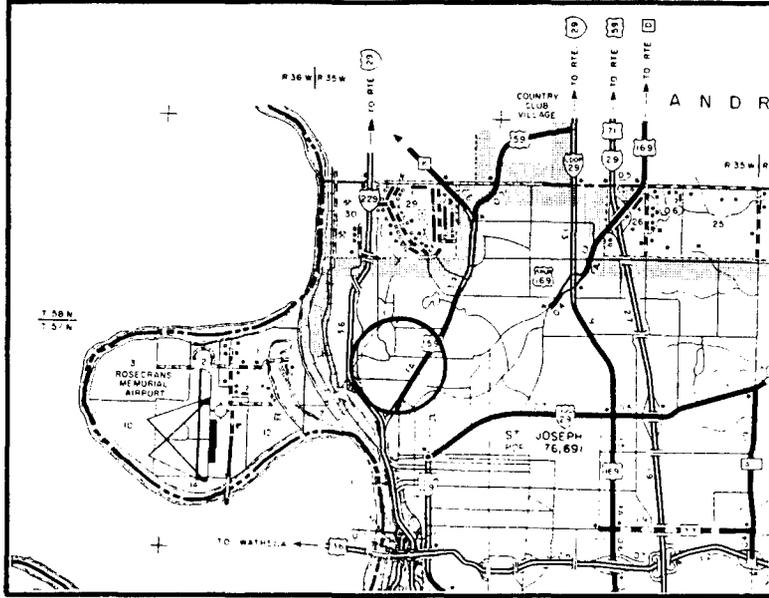
After World War I, the City of St. Joseph retained George Burnap, a landscape architect from Washington, D.C., to develop plans for an expansive urban park system. The voters gave approval to the plan, and by 1927 park areas had increased from 95 acres to almost 1200 acres. Bond issues in 1926 provided almost \$2 million for developing the park system, including an issue of \$763,000.00 for "paving, guttering, bridging, and tunneling the roadways through the park lands." This was one of two open spandrel concrete arch bridges constructed in the park system; both were located adjacent to the municipal golf course. W.A. Abrams of Unionville, Missouri, received the contract for both bridges. The shorter of the two, it presumably cost \$7892.00. The Whitehead Creek Bridge was completed in 1927 with the rest of the structures in this massive construction program.

This bridge is significant for its association with the expansive urban park system developed by the City of St. Joseph during the 1920s. The structure was one of two open spandrel arch bridges built to serve the areas adjacent to the municipal golf course. The bridge design provides an interesting contrast to the more substantial concrete bridges constructed to carry major city streets. The bridge is also significant as an outstanding example of urban park bridge design. The open concrete spandrels and thin metal balustrade provide a graceful appearance in an idyllic setting in the midst of the city.

NAME(S) OF STRUCTURE

Eleventh Street Underpass

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 386500.6; *Your Parks - How They Were Developed and How They Are Operated*, typewritten manuscript, located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; Special Ordinances Book 20, located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; scrapbooks containing newspaper clippings of park-system development, located at Department of Parks and Recreation, Krug Park, St. Joseph MO; field inspection by Lon Johnson, 14 September 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

26 March 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Eleventh Street Underpass
MHTD: 386500.6

BUCH37

DATE(S) OF CONSTRUCTION

1927

LOCATION

Eleventh Street over Maple Leaf Parkway (Corby Parkway); S5, T57N, R35W
St. Joseph; Buchanan County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP potentially eligible (score: 53)

CONDITION

good

OWNER

City of St. Joseph

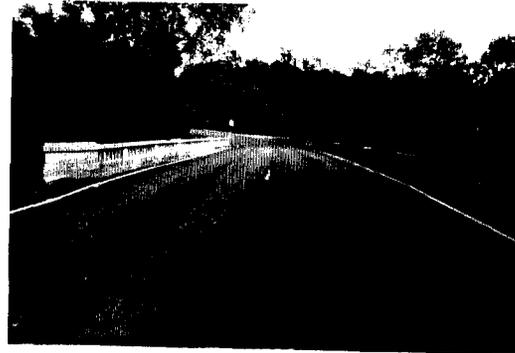
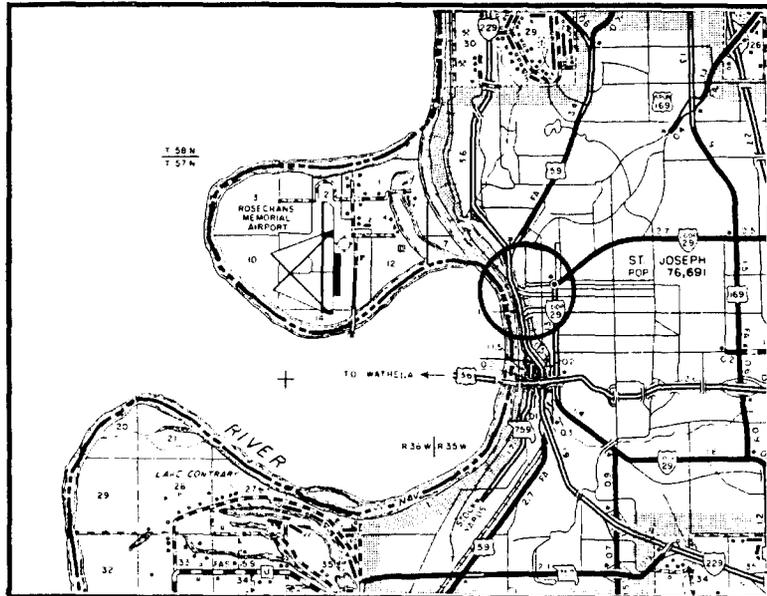
span number: 1	superstructure: concrete deck girder, skewed
span length: 42.0'	substructure: concrete abutments and wingwalls
total length: 45.0'	floor/decking: concrete deck
roadway wdt.: 36.0'	other features: concrete guardrails with slotted cutouts; arched spandrel girders with corbeled, faux arch rings

After World War I, the City of St. Joseph retained George Burnap, a landscape architect from Washington, D.C., to develop plans for an expansive urban park system. The voters gave approval to the plan, and by 1927 park areas had increased from 95 acres to almost 1200 acres. Bond issues in 1926 provided almost \$2 million for developing the park system, including an issue of \$763,000.00 for "paving, guttering, bridging, and tunneling the roadways through the park lands". The Olmstead Brothers of Brookline, Massachusetts, reviewed Burnap's plans prior to the start of construction; one major recommendation was to avoid grade on-crossings on important city streets. This street-to-street grade separation resulted from that recommendation. Built in 1927 as part of an extensive road and bridge construction effort, it typifies the small-scale, architecturally detailed overpasses built throughout the city at this time.

The Eleventh Street Underpass is significant for its association with the expansive urban park system developed by the City of St. Joseph during the 1920s. This was one of five underpasses built within twelve blocks of each other to carry major city streets over Corby Parkway. This may have been the first bridge constructed for the park system - its location is mentioned in June, 1926, and was not included with the other four underpasses constructed in 1927.

NAME(S) OF STRUCTURE
Great Western Viaduct

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 386500.2; **Your Parks - How They Were Developed and How They Are Operated**, typewritten manuscript, located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; Special Ordinances Book 20 - located at City Clerk's Office, St. Joseph City Hall, St. Joseph MO; scrapbooks containing newspaper clippings of park-system development, located at Department of Parks and Recreation, Krug Park, St. Joseph MO; field inspection by Lon Johnson, 14 September 1990.

INVENTORIED BY
Clayton B. Fraser

AFFILIATION
Fraserdesign, Loveland CO

DATE
26 March 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Great Western Viaduct
MHTD: 386500.2

BUCH36

DATE(S) OF CONSTRUCTION

1927

LOCATION

Northwest Parkway over abandoned grade of Chicago and Great Western Railroad;
St. Joseph; Buchanan County, Missouri

USE (ORIGINAL / CURRENT)

S28, T58N, R35W railroad overpass / railroad overpass

RATING NRHP potentially eligible (score: 51)

CONDITION

good

OWNER

City of St. Joseph

span number: 3

superstructure: concrete, 6-rib deck girder

span length: 35.0'

substructure: concrete abutments and wingwalls; concrete spill-through piers

total length: 108.0'

floor/decking: concrete deck

roadway wdt.: 30.0'

other features: concrete guardrails with square concrete balusters; arched haunches on spandrel girders; roadway cantilevered from spandrels on tapered concrete brackets; recessed panels on pier columns

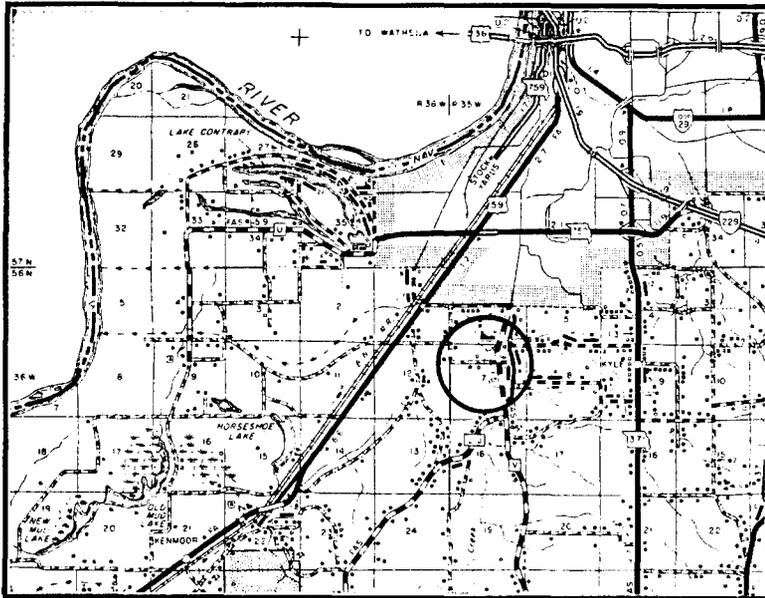
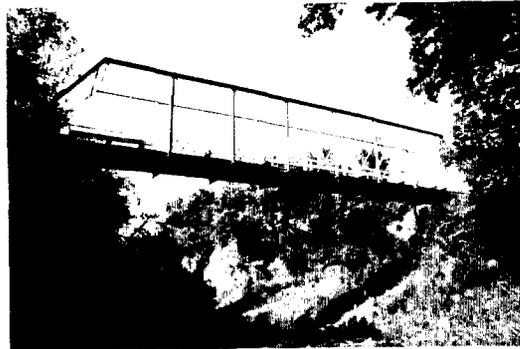
After World War I, the City of St. Joseph retained George Burnap, a landscape architect from Washington, D.C., to develop plans for an expansive urban-park system. The voters gave approval to Burnap's plan, and by 1927 park areas had increased from 95 acres to almost 1200 acres. Bond issues in 1926 provided almost \$2 million for developing the park system, including an issue of \$763,000.00 for "paving, guttering, bridging, and tunneling the roadways through the park lands." W.G. Fowler, a former draftsman for the State Highway Commission Bridge Department, was hired to design the park system's bridges. He began at once with the plans for this small-scale concrete viaduct across the Chicago - Great Western Railway tracks. Although the railway resisted paying its share of the construction costs, the Public Service Commission ordered it to pay half of the costs. Finally, in July 1927 the construction contract for the overpass was awarded to Carrother and Huggins Contractors for \$16,963.00. The St. Joseph-based builders apparently completed the structure later that year. The railroad tracks have since been removed, but the viaduct itself remains unaltered.

The Great Western Viaduct is significant for its association with the expansive urban park system developed by the City of St. Joseph during the 1920s. This was an important crossing, linking Krug Park in the northwest corner of the city with the remainder of the park system. The bridge is also significant as a representative example of urban park bridge design. The simple concrete deck girder bridge has been enhanced by the curve of the outside girders over the piers, the decorative capitals on the piers, and the cantilevered deck brackets.

NAME(S) OF STRUCTURE

Contrary Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 187001.4; Buchanan County Court Record, Book 20: page 205 (17 October 1888), located at the Buchanan County Courthouse, St. Joseph MO; field inspection by Lon Johnson, 7 September 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

26 March 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE
Contrary Creek Bridge
MHTD: 187001.4

BUCH26

DATE(S) OF CONSTRUCTION
1888 (possible)

LOCATION

County Road 187 over Contrary Creek; S7, T56N, R35W
1.0 mile south of St. Joseph; Buchanan County, Missouri

USE (ORIGINAL / CURRENT)
roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 45)

CONDITION
fair

OWNER
Buchanan County

span number: 1
span length: 133.0'
total length: 135.0'
roadway wdt.: 14.0'

superstructure: wrought iron, 9-panel, pin-connected Pratt through truss
substructure: steel pile bent abutments with timber back- and wingwalls
floor/decking: timber deck over timber stringers
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with double lacing (2 round eyerods at hip); diagonal: 2 square eyebars; counter: round rod with turnbuckle; strut: steel pipe; portal strut: lattice with decorative cast iron knee braces; lateral bracing: round rod; floor beam: tapered "fishtail" girder, U-bolted to lower chord pin; guardrail: timber

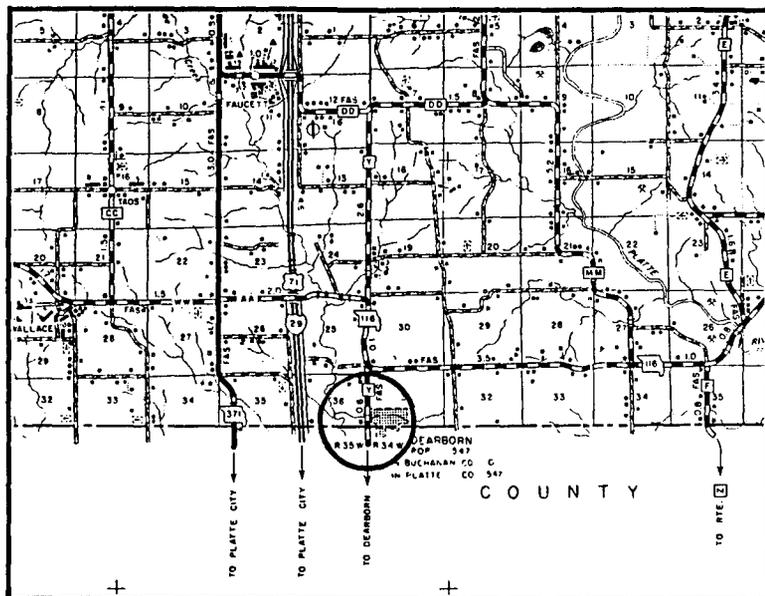
Spanning Contrary Creek just south of St. Joseph corporate limits, this medium-span through truss carries a gravel-surfaced county road in a rural Buchanan County setting. Design characteristics of the bridge itself - notably the tubular struts, double-laced verticals, cast iron knee braces and O-ringed lateral bracing - indicate that it was fabricated as a standard configuration by the Wrought Iron Bridge Company of Canton, Ohio. The truss has been evidently moved to this site and placed on steel pile bents (with the cylinder piers of an earlier bridge lying in the streambed). Its original location is unclear, however. In October 1888 Buchanan County contracted with WIBCo for a bridge across the Platte River at Agency. This may be that span, but county records do not corroborate this speculation. Although moved, the truss on the Contrary Creek Bridge remains structurally intact.

With its easy erection and extensive use of pre-fabricated standard components, the pin-connected Pratt through truss was the span of choice for all-iron bridges in the late 19th century. Industry giants such as the Wrought Iron Bridge Company and the King Iron Bridge and Manufacturing Company marketed Pratt trusses extensively through their national network of agents, and smaller, in-state contractors built Pratts extensively as well. As a result, pinned Pratt trusses constitute the largest group of through trusses in Missouri today. The Contrary Creek Bridge in Buchanan County is distinguished as an early example of iron truss construction, produced by the most prolific bridge fabricator in the country. Its physical integrity has been compromised somewhat by the subsequent move, but the truss itself remains in unaltered condition.

NAME(S) OF STRUCTURE

Brady Branch Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number Y 10; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; **Third Biennial Report of the Missouri State Highway Commission, 1921-22, page 129.**

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

26 March 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE
Brady Branch Bridge
MHTD: Y 10

BUCH09

DATE(S) OF CONSTRUCTION
1922

LOCATION

State Supplementary Route Y over branch of Bee Creek; S31/36, T55N, R34/35W
7.9 miles southwest of Agency; Buchanan County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 58)

CONDITION

good

OWNER

Missouri Highway and Transportation Department

span number: 3
span length: 28.0'
total length: 97.0'
roadway wdt.: 18.2'

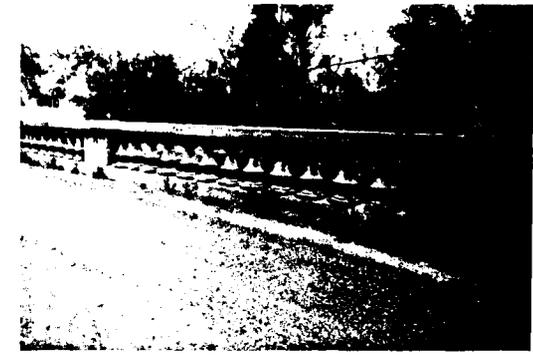
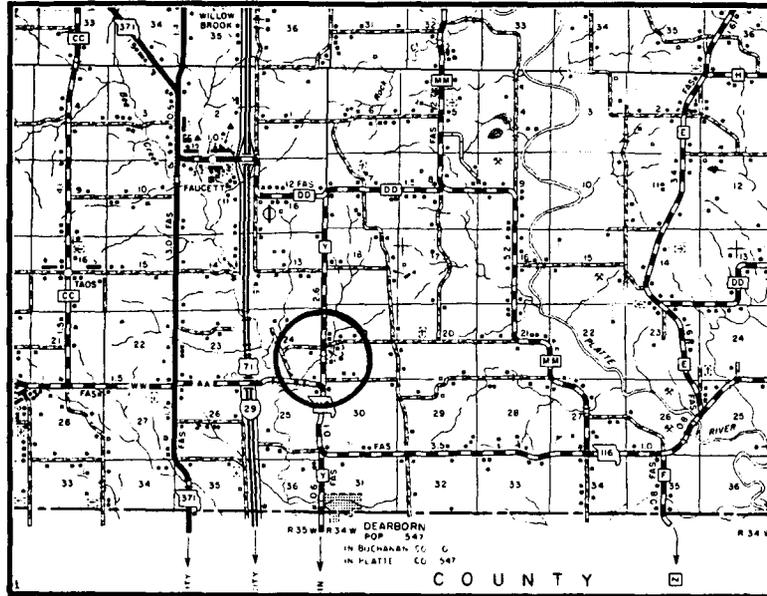
superstructure: concrete Luten arch
substructure: concrete abutments and wingwalls
floor/decking: concrete deck over earth fill
other features: roadway cantilevered on tapered brackets from arch spandrels; guardrails: cast concrete balusters with solid concrete bulkheads

This small-scale concrete arch bridge spans the Brady Branch of Bee Creek southwest of Agency. Consisting of three filled spandrel arches supported by concrete abutments and piers, the structure carries State Supplementary Route Y in this rural Buchanan County location. The Missouri State Highway Department had originally designed an 18-foot concrete span for this minor crossing in 1921, and estimated its cost at \$14,289.00. Designating this bridge, two others and the adjacent stretch of State Highway 59 as Federal Aid Project 180, the department let the construction project out for competitive bids in October. When the bids were received, however, the Topeka Bridge and Construction Company proposed to build a structure of its own design here for almost four thousand dollars less. Topeka B&C's design consisted of three short-span concrete arches that used a patent held by Indianapolis engineer Daniel B. Luten. The arches featured a roadway cantilevered from the spandrel walls on tapered concrete brackets and concrete guardrails with cast, classically shaped balusters. The highway department opted for the alternate design and awarded the contract to Topeka B&C. The Kansas contractor began work on the three structures in March 1922, completing them later that year. Total cost of the Brady Branch Bridge: \$10,957.50. Since its completion, it has functioned in place without alteration.

Indianapolis engineer Daniel B. Luten patented his namesake concrete arch in 1905. Soon thereafter, he began marketing it on a national basis through his own construction company, and through regional representatives. The Topeka Bridge and Construction Company functioned as Luten's western representative, building Luten arches and other concrete bridges in the Midwest, the mountain states and as far away as Arizona. Although these bridges were marketed extensively and Luten protected his broadly defined patent rights aggressively through the courts, his arch designs never found universal acceptance. Relatively few Luten arches have been identified by the Missouri statewide bridge inventory. The Brady Branch Bridge, Bee Creek Bridge [BUCH08] and James Branch Bridge [BUCH15], designed and built by one of Daniel Luten's major subsidiaries, are all well-preserved, small-scale representatives of early patented concrete bridge design.

NAME(S) OF STRUCTURE
Bee Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number W 444; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; Third Biennial Report of the Missouri State Highway Commission, 1921-22, page 129; field inspection by Lon Johnson, 14 September 1990.

INVENTORIED BY
Clayton B. Fraser

AFFILIATION
Fraserdesign, Loveland CO

DATE
26 March 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Bee Creek Bridge
MHTD: W 444

BUCH08

DATE(S) OF CONSTRUCTION

1922

LOCATION

State Supplementary Route Y over Bee Creek; S21/24, T55N, R34/35W
6.0 miles southwest of Agency; Buchanan County, Missouri

USE (ORIGINAL / CURRENT)

highway bridge / highway bridge

RATING NRHP possibly eligible (score: 58)

CONDITION

good

OWNER

Missouri Highway and Transportation Department

span number: 1

superstructure: concrete Luten arch

span length: 50.0'

substructure: concrete abutments and wingwalls

total length: 90.0'

floor/decking: concrete deck over earth fill

roadway wdt.: 18.0'

other features: roadway cantilevered on tapered brackets from arch spandrels; guardrails: cast concrete balusters with solid concrete bulkheads

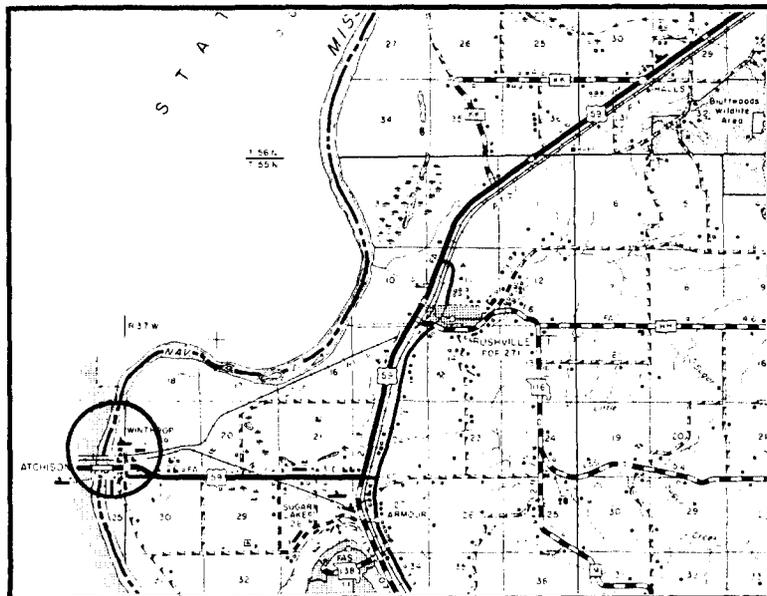
This small-scale concrete arch bridge carries State Supplementary Route Y over Bee Creek southwest of Agency. The Missouri State Highway Department had originally designed a 20-foot concrete span for this minor crossing in 1921, and estimated its cost at \$7187.00. Designating this bridge, two others and the adjacent stretch of State Highway 59 as Federal Aid Project 180, the department let the construction project out for competitive bids in October. When the bids were received, however, the Topeka Bridge and Construction Company proposed to build a structure of its own design here for almost fifteen hundred dollars less. Topeka B&C's design consisted of a medium-span concrete arch that used a patent held by Indianapolis engineer Daniel B. Luten. The arch featured a roadway cantilevered from the spandrel walls on tapered concrete brackets and concrete guardrails with cast, classically shaped balusters. The highway department opted for the alternate design and awarded the contract to Topeka B&C. The Kansas contractor began work on the three structures in March 1922, completing them later that year. Total cost of the Bee Creek Bridge was \$12,478.00. Since its completion, it has functioned in place without alteration.

Indianapolis engineer Daniel B. Luten patented his namesake concrete arch in 1905. Soon thereafter, he began marketing it on a national basis through his own construction company, and through regional representatives. The Topeka Bridge and Construction Company functioned as Luten's western representative, building Luten arches and other concrete bridges in the Midwest, the mountain states and as far away as Arizona. Although these bridges were marketed extensively and Luten protected his broadly defined patent rights aggressively through the courts, his arch designs never found universal acceptance. Relatively few Luten arches have been identified by the Missouri statewide bridge inventory. The Bee Creek Bridge, Brady Branch Bridge [BUCH09] and James Branch Bridge [BUCH15], designed and built by one of Daniel Luten's major subsidiaries, are all well-preserved, small-scale representatives of early patented concrete bridge design.

NAME(S) OF STRUCTURE

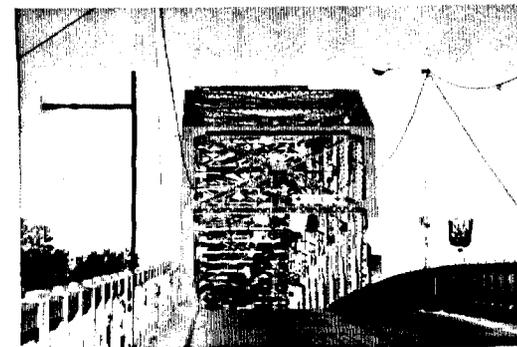
Atchison Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number K 697R; Files on primary system bridges located at the Missouri Highway and Transportation Department, Jefferson City MO; "Free Bridge at Atchison Now in Use," St. Joseph News-Press, 3 July 1938; Eleventh Biennial Report of the State Highway Commission of Missouri for the Period Ending December Thirty-first 1938, pp. 168 and 170; "Z.E. Jackson Led Nine-Year Battle to Obtain Bridge," St. Joseph News-Press, 17 September 1938; J.A.L. Waddell, De Pontibus: A Pocket-Book for Bridge Engineers (New York: John Wiley & Sons, 1898); field inspection by Lon Johnson, 14 September 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

26 March 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Atchison Bridge
MHTD: K 697R

BUCH05

DATE(S) OF CONSTRUCTION

1937-38

LOCATION

U.S. Highway 59 over Missouri River; S19, T55N, R37W
Winthrop MO / Atchison KS; Buchanan County, Missouri

USE (ORIGINAL / CURRENT)

highway bridge / highway bridge

RATING NRHP eligible (score: 61)

CONDITION

excellent

OWNER

Missouri Highway and Transportation Department

span number: 2
span length: 474.0'
total length: 1339.0'
roadway wdt.: 24.0'

superstructure: steel, 11-panel, rigid-connected cantilever through truss with 2, 7-panel, rigid-connected Warren deck trusses at each end; 3 steel stringer and 3 steel girder approach spans at west end; 4 steel stringer approach spans at east end
substructure: concrete abutments, wingwalls and piers
floor/decking: grating covered by corrugated steel and asphalt over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing or 4 built-up angles with cover and batten plates or 2 channels with double lacing; lower chord: 4 built-up angles with lacing and batten plates; vertical: 4 angles with batten plate or 2 channels with lacing or 4 built-up angles with double lacing or 2 channels with batten plates; hip vertical: 4 built up angles with lacing; diagonal: 4 angles with batten plates or 4 built up angles with lacing or 2 channels with batten plates or 4 built-up angles with batten plates; lateral bracing: 2 angles with lacing at top; 2 angles at bottom; strut: 2 angles with lacing; floor beam: I-beams; guardrail: steel balustrade

Located between Winthrop, MO and Atchison, KS, this bridge carries U.S. Highway 59 over the Missouri River north of Kansas City. Although they already had a crossing at St. Joseph, the residents of Buchanan County had since 1892 eagerly worked toward a bridge at Winthrop (also known as East Atchison) in Rush Township. In the middle 1930s their efforts sped toward fruition. The Missouri Highway Commission's Bureau of Bridges designed the new bridge, funded as Federal Aid Project 714, as a multiple-span steel truss structure. Finishing the design in early 1936, the Bureau solicited bids for the bridge's construction. On May 7 the Bushman Construction Company received the first contract, \$25,580 for the substructure of the east approach spans. Several other contracts were required to complete the remainder of the bridge: Missouri Valley Bridge and Iron Company (MoVB&I), \$12,493.20 for the pier fenders; Oscar H. Schmidt, \$52,000 for the superstructures on the approach spans; and MoVB&I and Kansas City Bridge Company, \$249,275 for the concrete piers supporting the cantilever spans.

On February 8, 1937 Wisconsin Bridge and Iron Company (WB&I) bid \$304,826.90 on construction of the main span superstructure, receiving the contract. WB&I soon began work on the eleven-span superstructure, including the three-span I-beam cantilever section (spans of 415, 474, 415 feet). After nearly a year of arduous labor, WB&I completed the bridge in late June 1938. The Atchison Bridge

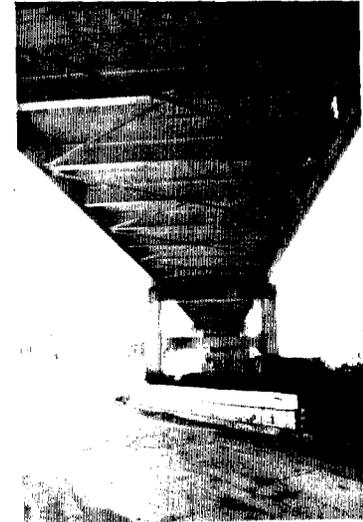
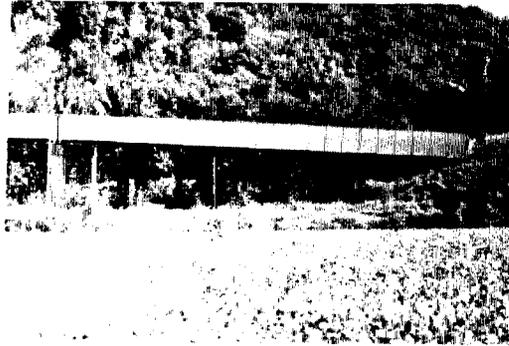
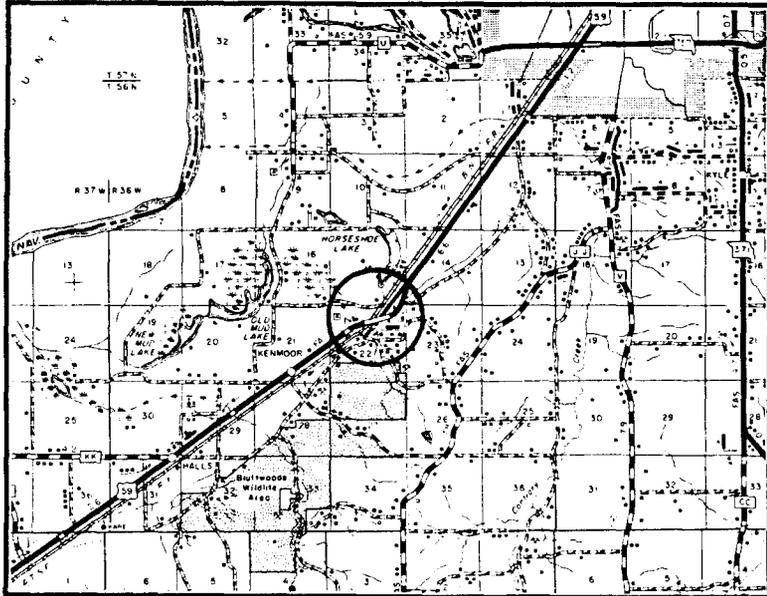
was opened and dedicated with "none too formal ceremonies" on July 2. A long automobile caravan carrying dignitaries from Atchison crossed the bridge from the western side. Within an hour, over 1,500 cars drove across the new Missouri River Bridge. Since its completion, the only alteration has been the application of paint in early 1990.

Many of Missouri's important crossings included variations on steel superstructures with concrete substructures. Cantilevers, steel I-bar, I-beams, trusses, rigid-space-frames, concrete deck girders, and arches were prominent designs. Although some cantilevers were built in the 1920s, they became more popular in the 1930s, accommodating some of the state's larger-scale structures. Cantilevering, popular among state bridge designers during the 1930s and '40s, found little support among prominent engineers of the time. J.A.L. Waddell, noted bridge engineer, condemned the design as "always inferior in rigidity to bridges of simple truss spans, and . . . always more expensive. The Atchison Bridge's \$669,838.96 price tag supports Waddell's criticism. Its design, however, placed the Atchison Bridge in the milieu of Missouri River bridge construction.

NAME(S) OF STRUCTURE

U.S. Highway 59 Overpass

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number K 590; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; Tenth Biennial Report of the State Highway Commission of Missouri, 1935-36, pages 55-56, 263; field inspection by Lon Johnson, 7 August 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

26 March 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

U.S. Highway 59 Overpass
MHTD: K 590

BUCH03

DATE(S) OF CONSTRUCTION

1936-37

LOCATION

U.S Highway 59 over Burlington and Rock Island Railroads; S15/22, T56N, R36W highway overpass / highway overpass
1.5 miles northeast of Kenmoor; Buchanan County, Missouri

USE (ORIGINAL / CURRENT)

RATING NRHP possibly eligible (score: 60)

CONDITION

good

OWNER

Missouri Highway and Transportation Department

span number: 5; 1; 1
span length: 115';100';80'
total length: 770.0'
roadway wdt.: 24.0'

superstructure: steel plate through girder with continuous ends
substructure: concrete abutments, wingwalls and spill-through piers
floor/decking: concrete deck over steel stringers
other features: girder: steel plate with 2-angle flanges, top and bottom, and cover plate; stringer: I-beam; bottom lateral bracing: angle; bridge plate: **MISSOURI HIGHWAY DEPT BRIDGE No K-590 1936**

This multiple-span overpass carries U.S. Highway 59 over the tracks of the Burlington and Rock Island railroads at a rural crossing northeast of Kenmoor. The structure consists of a series of seven long-span, riveted plate through girders (five with 115-foot spans, one with 100-foot and one with 80-foot), supported by spill-through concrete piers. The overpass was engineered by the Missouri State Highway Commission Bureau of Bridges early in 1936. Designating the project as WPGH-49-2, the agency solicited competitive proposals in May. A month later the state highway commission awarded a construction contract to Sandy Hites. Hites' crew completed the structure the following year for \$113,402.60; since that time the overpass has functioned in place without alteration.

"The past two years have been spent in completing the separation projects on the National Recovery Act Program," the state highway commission reported in 1936, "and in instituting a program under the Works Progress Administration grants to this State for grade separation purposes. Under the NRA grant, which amounted to \$6,173,740, forty grade separations were built in connection with other road work... Under the WPA appropriation, contracts have been let for 39 separations, with two others ready for letting." The Buchanan County crossing constituted a part of this massive construction effort. With its five spans, the longest of which extends 115 feet, it is distinguished as one of the longest plate girder structures on Missouri's primary road system. The U.S. Highway 59 Overpass is thus historically significant for its representation of an important statewide construction trend and technologically significant as a well-preserved, long-span example of MSHD beam design.

CALDWELL COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge	Name	Description
*CALD01	H 27R	Log Creek Bridge	1- 60' 1925	concrete filled spandrel arch Land Construction Company
*CALD02	J 340R	Log Creek Bridge	1-110' 1930	riveted Pratt through truss Chernus Construction Co.
CALD03	007000.3	Long Branch Bridge	1- 45' c1910	pinned Pratt pony truss
*CALD04	023002.0	Brushy Creek Bridge	1- 80' 1904	pinned Pratt bedstead Canton Bridge Co., Canton OH
*CALD05	032000.2	Sheep Creek Bridge	1- 70' 1904	pinned Pratt bedstead John H. Sparks, St. Joseph MO
CALD06	041001.6	Bridge	1- 30' 1905	riveted lattice bedstead J.H. Sparks, St. Joseph MO
*CALD07	075001.2	Tom Creek Bridge	1- 50' 1904	pinned Pratt bedstead Canton Bridge Co., Canton OH
*CALD08	088000.4	Lick Fork Bridge	1- 45' c1910	pinned Pratt half hip pony truss
CALD09	099000.7	Lick Fork Bridge		(replaced)
CALD10	115001.8	Otter Creek Bridge	1- 50' c1910	pinned Pratt half hip pony truss
CALD11	118001.9	Otter Creek Bridge	1- 60' c1910	pinned Pratt half-hip pony truss
*CALD12	127000.8	Cottonwood Creek Bridge		(replaced)
*CALD13	181001.9	Henkin's Ford Bridge	1-125' 1887	pinned Warren through truss King Bridge Co., Cleveland OH
*CALD14	204001.2	Mud Creek Bridge	1- 90' 1887	pinned Warren through truss King Bridge Co., Cleveland OH
*CALD15	205000.0	Mud Creek Bridge	1- 48' 1905	pinned Pratt bedstead J.H. Sparks (prob.)
*CALD16	207000.9	Bridge	1- 40' 1908	pinned Pratt half hip pony truss Standard Bridge Co, Omaha NE
CALD17	215000.3	North Mud Creek Bridge	1- 32' c1905	riveted lattice bedstead Canton Bridge Co., Canton OH
CALD18	227000.6	North Mud Creek Bridge	1- 45' 1905	pinned Pratt bedstead
CALD19	239000.1	Bridge	1- 50' c1910	pinned Pratt half-hip pony truss
*CALD20	243000.5	Mud Creek Bridge		(replaced)
CALD21	264001.7	Bridge	1- 50' c1905	pinned Pratt half-hip pony truss
CALD22	268000.9	Crooked River Bridge	1- 30' c1905	riveted lattice bedstead Canton Bridge Co. (prob.)
*CALD23	279001.5	North Mud Creek Bridge	1- 50' 1904	pinned Pratt bedstead John H. Sparks, St. Joseph MO
*CALD24	295000.8	North Mud Creek Bridge	1- 54' 1905	pinned Pratt bedstead John H. Sparks, St. Joseph MO

CALDWELL COUNTY

INCLUDED (cont.):

*CALD25	299001.0	Bridge	1- 40'	pinned Pratt bedstead
			c1905	
*CALD26	303000.4	Otter Creek Bridge	1-110	bowstring through arch-truss
			1875	Missouri Valley B&I (prob.)
CALD27	305001.2	Otter Creek Bridge	1- 50'	pinned Pratt half-hip pony truss
			1901	John H. Sparks, St. Joseph MO
CALD28	307000.2	Little Otter Creek Bridge	1- 60'	pinned Pratt pony truss
			1904	John H. Sparks, St. Joseph MO
CALD29	307002.5	Shoal Creek Bridge	1-120'	riveted Pratt through truss
			c1925	
*CALD30	316000.7	Cottonwood Creek Bridge	1- 48'	pinned Pratt bedstead
			1904	John H. Sparks, St. Joseph MO
*CALD31	324000.6	Tom Creek Bridge	1- 60'	pinned Pratt bedstead
			1904	Canton Bridge Co., Canton OH
*CALD32	324001.9	Bonanza Bridge	1-187'	pinned Whipple through truss
			1883	
CALD33	327000.1	Bridge		(replaced)
CALD34	337000.6	Tom Creek Bridge		(replaced)
*CALD35	341000.9	Shoal Creek Bridge	1-112'	pinned Pratt through truss
			c1890	King Iron Bridge Co. (prob.)
*CALD36	342002.8	Cox Ford Bridge	1-114'	pinned Pratt through truss
			1888	King Iron Bridge Co.
CALD37	349000.7	Long Creek Bridge	1- 60'	rivet 2-angle Pratt pony truss
			c1925	
CALD38	350000.8	Log Creek Bridge	1- 54'	pinned Pratt pony truss
			c1915	
CALD39	351001.3	Tub Creek Bridge	1- 24'	riveted lattice bedstead
			1904	Canton Bridge Co., Canton OH
CALD40	353000.8	Log Creek Bridge	1- 80'	riveted Pratt pony truss
			1908	John H. Sparks, St. Joseph MO
*CALD41	359000.8	Shoal Creek Bridge	1-124'	pinned Pratt through truss
			1894	Dildine Bridge Company
CALD42	359001.8	Mill Creek Bridge		(replaced)
CALD43	368001.2	Shoal Creek Bridge	1-100'	riveted Pratt pony truss
			c1930	
*CALD44	372000.5	Shoal Creek Bridge	1-120'	pinned Pratt through truss
			1901	John H. Sparks, St. Joseph MO
CALD45	383001.5	Shoal Creek Bridge	1- 50'	pinned Pratt bedstead
			c1910	
CALD46	406000.6	Long Creek Bridge	1- 60'	pinned Pratt pony truss
			c1910	
CALD47	407000.2	Long Creek Bridge	1- 60'	pinned Pratt half hip pony truss
			c1910	
CALD48	442002.4	Crooked River Bridge	1- 80'	pinned Pratt pony truss
			c1915	
CALD49	028000.8	West Sheep Creek Bridge	1- 35'	steel deck girder
			1922	Bean & Co., St. Joseph MO

CALDWELL COUNTY

EXCLUDED:

Pratt pony truss

029002.7	165000.5	230000.5	346001.2	358000.0	364001.8	366001.4445000.7
463001.2	475002.1	476001.5				

Warren pony truss

001001.8	005000.2	131000.1	242000.4	459001.5		
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Lattice bedstead

041001.6	250001.0					
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Steel stringer

S 923	T 844R	T 863	004000.1	014000.6	029001.3	031000.3080000.6
122000.3	126002.0	147000.9	151000.3	158001.3	162000.8	168000.7177000.7
196000.3	198000.7	206000.9	216000.9	217000.7	226000.6	256002.9261R01.2
264000.3	307000.8	359002.1	378000.4	378001.2	388001.9	391000.9392000.5
	403000.6	404000.5	409001.7	443000.4	449000.4	456000.6460000.3

Concrete girder

G 400	G 401	479001.3				
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Concrete slab

339000.8						
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Concrete box culvert

T 318	X 408					
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Timber stringer

259000.3						
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SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	2	41	0	0	43
Excluded	7	57	0	0	74
	<hr/>				
	9	98	0	0	117 structures

Log Creek Bridge

CALD01

GENERAL DATA

structure no.:	H 27R	city/town:	2.0 miles south of Kingston
county:	Caldwell	feature inters.:	Log Creek
		cadastral grid:	S33/34, T56N, R28W
		highway route:	State Highway 13
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	concrete filled spandrel arch		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	good
span length:	60.0'	alterations:	roadway widened and guardrails replaced, 1940
total length:	65.0	floor/decking :	concrete over earth fill
roadway width:	27.0	other features:	concrete square balustrade guardrails; corbeled arch rings

HISTORICAL DATA

erection date:	1925
erection cost:	\$7793.68
designer:	Missouri State Highway Department
fabricator :	none
contractor:	Land Construction Company
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. H 27R; Primary System Bridge files - located at the Missouri Highway and Transportation Department, Jefferson City MO; field inspection by Lon Johnson, 9 September 1990.
sign. rating:	36
evaluation:	NRHP non-eligible (typical example of MSHD concrete arch construction, substantially altered)

Inventoried by: Michelle Crow-Dolby 7 July 1993

Log Creek Bridge

CALD02

GENERAL DATA

structure no.: J 340R	city/town: 4.0 miles south of Kingston
county: Caldwell	feature inters.: Log Creek
	cadastral grid: S28/27, T56N, R28W
	highway route: State Highway 13
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 6-panel, rigid-connected Pratt through truss, with three concrete girder approach spans on the north	
substructure: concrete abutments, wingwalls and piers	
span number: 1	condition: fair
span length: 110.0'	alterations: guardrails replaced, 1940
total length: 256.0'	floor/decking : concrete deck over steel stringers
roadway width: 20.0'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates; vertical: 4 angles with lacing; diagonal: 2 channels with batten plates; floor beam: I-beam; guardrail: 2 channels; curved approach guardrails at each end; endpost-mounted bridge plate: Missouri Highway Department / Bridge N ^o J340 / 1930

HISTORICAL DATA

erection date: 1930	
erection cost: \$34,441.63	
designer: Missouri State Highway Department	
fabricator : Inland Steel Company, Pittsburgh PA	
contractor: Chernus Construction Company	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. J 340R; Primary System Bridge files - located at the Missouri Highway and Transportation Department, Jefferson City MO; field inspection by Lon Johnson, 8 September 1990.	
sign. rating: 34	
evaluation: NRHP non-eligible (typically configured MSHD highway truss)	

Inventoried by: Michelle Crow-Dolby 7 July 1993

Long Branch Bridge

CALD03

GENERAL DATA

structure no.:	007000.3	city/town:	4.0 miles southwest of Kidder
county:	Caldwell	feature inters.:	Long Branch
		cadastral grid:	S4/9, T57N, R28W
		highway route:	County Road 7
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	45.0'	alterations:	unknown
total length:	45.0'	floor/decking :	timber deck
roadway width:	12.4'	other features:	timber guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 007000.3.

sign. rating:	18
evaluation:	NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby 7 July 1993

Brushy Creek Bridge

CALD04

GENERAL DATA

structure no.: 023002.0	city/town: 5.5 miles northeast of Kidder
county: Caldwell	feature inters.: Brushy Creek
	cadastral grid: S28/33, T57N, R29W
	highway route: County Road 23
	highway distr.: 1
	current owner: Caldwell County

STRUCTURAL DATA

superstructure: steel, 6-panel, pin-connected Pratt bedstead
substructure: concrete abutments and wingwalls

span number: 1	condition: fair
span length: 80.0'	alterations: substructure replaced
total length: 80.0'	floor/decking : timber deck
roadway width: 11.4'	other features: upper chord and upright end post: 2 channels with cover plate and lacing; lower chord: 2 channels with cover plate, 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 1 looped square eyebar with turn-buckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice

HISTORICAL DATA

erection date: 1904
erection cost: \$7900.00 (22-bridge contract)
designer: Canton Bridge Company, Canton OH
fabricator : Canton Bridge Company, Canton OH;
Cambria Steel Company, Pittsburgh PA
contractor: Canton Bridge Company, Canton OH

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 023002.0; Caldwell County Court Record, Book O: page 181 (1 May 1903), page 344 (4 November 1903) - located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.

sign. rating: 42
evaluation: NRHP non-eligible (typically configured, long-span example of common structural type, altered)

Inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

Sheep Creek Bridge

CALD05

GENERAL DATA

structure no.:	032000.2	city/town:	5.5 miles southeast of Hamilton
county:	Caldwell	feature inters.:	Sheep Creek
		cadastral grid:	S36/1, T57/56N, R29W
		highway route:	County Road 32
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected, Pratt bedstead		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	70.0'	alterations:	substructure replaced
total length:	72.0'	floor/decking :	timber deck
roadway width:	14.0'	other features:	upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plate at outriders, forged eye-bars; vertical: 4 angles with lacing; diagonal: square rods, round rods with threaded ends at center panel; lateral bracing: round rods with threaded ends; stringer and floor beam: I-beam; guardrail: steel lattice

HISTORICAL DATA

erection date:	1904
erection cost:	\$940.00
designer:	unknown
fabricator :	Cambria Steel Company, Pittsburgh PA
contractor :	John H. Sparks, St. Joseph MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 032000.2; Caldwell County Court Record, Book O: page 592 (6 September 1904); Caldwell County Bridge Record, n.p. - both located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.
sign. rating:	44
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby 7 July 1993

Bridge

CALD06

GENERAL DATA

structure no.:	041001.6	city/town:	2.1 miles east of Kidder
county:	Caldwell	feature inters.:	tributary of Marrowbone Creek
		cadastral grid:	S5, T57N, R28W
		highway route:	County Road 41
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 2-panel, rigid-connected lattice bedstead with continuous endposts		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	30.0'	alterations:	mid-span pier added
total length:	30.0'	floor/decking :	timber deck
roadway width:	13.5'	other features:	none

HISTORICAL DATA

erection date:	1905
erection cost:	\$255.00
designer:	unknown
fabricator :	unknown
contractor:	J.H. Sparks, St. Joseph MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 075001.2; Caldwell County Bridge Record, n.p. - located at the Caldwell County Courthouse, Kingston MO.
sign. rating:	44
evaluation:	NRHP non-eligible (small-scale example of common structural type, altered)

inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

Tom Creek Bridge

CALD07

GENERAL DATA

structure no.: 075001.2	city/town: 2.4 miles southwest of Hamilton
county: Caldwell	feature inters.: Tom Creek
	cadastral grid: S22/27, T57N, R28W
	highway route: County Road 75
	highway distr.: 1
	current owner: Caldwell County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt bedstead	
substructure: steel pile bent piers with timber wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: substructure replaced
total length: 50.0'	floor/decking : timber deck over steel stringers
roadway width: 11.6'	other features: upper chord and upright end post: 2 channels with cover plate and lacing; lower chord: 2 angles with lacing; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: one angle

HISTORICAL DATA

erection date: 1904	
erection cost: \$7900.00 (22-bridge contract)	
designer: Canton Bridge Company, Canton OH	
fabricator : Canton Bridge Company, Canton OH	
contractor: Canton Bridge Company, Canton OH	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 075001.2; Caldwell County Court Record, Book O: page 181 (1 May 1903), pages 341-2 (4 November 1903) - located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.
sign. rating: 40	
evaluation:	NRHP non-eligible (typically configured example of common structural type, altered)

inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

Lick Fork Bridge

CALD08

GENERAL DATA

structure no.:	088000.4	city/town:	3.0 miles northeast of Hamilton
county:	Caldwell	feature inters.:	Lick Fork
		cadastral grid:	S5/8, T57N, R27W
		highway route:	County Road 88
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	45.0'	alterations:	none
total length:	45.0'	floor/decking :	timber deck
roadway width:	12.1'	other features:	upper chord and inclined end post: 2 channels with lacing; lower chord: 2 looped eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pins; guardrail: 2 channels

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 088000.4; field inspection by Lon Johnson, 8 September 1990.

sign. rating: 21
evaluation: NRHP non-eligible (atypically configured example of common structural type, inadequately documented)

inventoried by: Michelle Crow-Dolby 7 July 1993

Otter Creek Bridge

CALD10

GENERAL DATA

structure no.:	115001.8	city/town:	6.0 miles southwest of Breckenridge
county:	Caldwell	feature inters.:	Otter Creek
		cadastral grid:	S26/35, T57N, R27W
		highway route:	County Road 115
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	76.0'	floor/decking :	timber deck
roadway width:	13.4'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 115001.8.

sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 July 1993

Otter Creek Bridge

CALD11

GENERAL DATA

structure no.:	118001.9	city/town:	4.9 miles southeast of Hamilton
county:	Caldwell	feature inters.:	Otter Creek
		cadastral grid:	S27/22, T57N, R27W
		highway route:	County Road 118
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss
substructure: steel pile abutments with timber wingwalls

span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.4'	other features:	steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 118001.9.

sign. rating: 28
evaluation: NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 July 1993

Henkin's Ford Bridge

CALD13

GENERAL DATA

structure no.:	181001.9	city/town:	3.0 miles east of Proctorville
county:	Caldwell	feature inters.:	Shoal Creek
		cadastral grid:	S8/9, T56N, R26W
		highway route:	County Road 181
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	wrought iron, 10-panel, pin-connected Warren through truss, with steel stringer approach spans		
substructure:	concrete abutments and wingwalls; concrete-filled iron cylinder piers; pile bent piers at approach spans		
span number:	1	condition:	fair
span length:	125.0'	alterations:	replacement of abutments and approach spans
total length:	195.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.0'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 looped round eyerods; diagonal: 2 channels with cover and batten plates, 22 looped square eyebars; strut: I-beam; floor beam: riveted plate girder, U-bolted from lower chord pins; guardrail: lattice

HISTORICAL DATA

erection date:	1887
erection cost:	\$2698.00
designer:	King Bridge Company, Cleveland OH
fabricator :	King Bridge Company, Cleveland OH
contractor:	King Bridge Company, Cleveland OH
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 181001.9; <i>The Hamiltonian</i> , 13 May 1887, 22 July 1887, 13 January 1888; <i>History of Caldwell and Livingston Counties, Missouri</i> (St. Louis: National Historical Company, 1886), page 254; field inspection by Clayton Fraser and John Roberts, 14 October 1993.
sign. rating:	72
evaluation:	NRHP eligible (well-preserved example of early, rare truss type)

inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

Mud Creek Bridge

CALD14

GENERAL DATA

structure no.:	204001.2	city/town:	2.0 miles northeast of Braymer
county:	Caldwell	feature inters.:	Mud Creek
		cadastral grid:	S36, T56/55N, R26W
		highway route:	County Road 204
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure: wrought iron, 6-panel, pin-connected Warren through truss
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	90.0'	alterations:	substructure replaced
total length:	90.0'	floor/decking :	timber deck
roadway width:	13.6'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; diagonal: 2 channels with cover and batten plates, 2 looped square eyebars; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, U-bolted to lower chord pins; guardrail: 2 angles

HISTORICAL DATA

erection date: 1887
erection cost: unknown
designer: King Bridge Company, Cleveland OH
fabricator : King Bridge Company, Cleveland OH
contractor : King Bridge Company, Cleveland OH

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 204001.2; *The Hamiltonian*, 13 May 1887, 22 July 1887, 13 January 1888; *History of Caldwell and Livingston Counties, Missouri* (St. Louis: National Historical Company, 1886), page 254; Caldwell County Court Record, Book O: page 181 (1 May 1903) - located at the Caldwell County Courthouse, Kingston MO; field inspection by Clayton Fraser and John Roberts, 14 October 1993.

sign. rating: 67
evaluation: NRHP possibly eligible (well-preserved example of early, rare truss type)

inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

Mud Creek Bridge

CALD15

GENERAL DATA

structure no.: 205000.0	city/town: 3.0 miles northeast of Braymer
county: Caldwell/ Livingston	feature inters.: Mud Creek
	cadastral grid: S25/36, T56N, R26W
	highway route: County Road 205
	highway distr.: 1
	current owner: Caldwell County and Livingston Counties

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead	
substructure: truss-leg abutments with timber back- and wingwalls	
span number: 1	condition: fair
span length: 48.0'	alterations: I-beam reinforcement added to one upper chord
total length: 50.0'	floor/decking : timber deck over steel stringers
roadway width: 13.6'	other features: upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angle with batten plates (outer panels), 2 looped rectangular eyebars (inner panel); vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pins; guardrail: 2 angle

HISTORICAL DATA

erection date: 1905	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: J.H. Sparks, St. Joseph MO (probable)	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 205000.0; Livingston County Court Record, Book O: page 309 (6 July 1905) - located at Livingston County Courthouse, Chillicothe MO; field inspection by Lon Johnson, 8 September 1990.
sign. rating: 39	
evaluation:	NRHP non-eligible (typically configured, partially documented example of common structural type)

inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

Bridge

CALD16

GENERAL DATA

structure no.:	207000.9	city/town:	2.6 miles northwest of Braymer
county:	Caldwell	feature inters.:	unnamed stream
		cadastral grid:	S28/33, T56N, R26W
		highway route:	County Road 207
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	40.0'	alterations:	none
total length:	40.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord and inclined end post: 2 channels with lacing; lower chord: 2 looped square eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pins; guardrail: 1 angle

HISTORICAL DATA

erection date: 1908
erection cost: unknown
designer: Standard Bridge Company, Omaha NE
fabricator : Standard Bridge Company, Omaha NE
contractor : Standard Bridge Company, Omaha NE

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 207000.9; Caldwell County Court Record, Book Q: page 182 (5 May 1908) - located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.

sign. rating: 38
evaluation: NRHP non-eligible (atypically configured, small-scale example of common structural type)

inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

North Mud Creek Bridge

CALD17

GENERAL DATA

structure no.:	215000.3	city/town:	4.3 miles northwest of Braymer
county:	Caldwell	feature inters.:	North Mud Creek
		cadastral grid:	S1/6, T55N, R27/26W
		highway route:	County Road 215
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure: steel, 2-panel, rigid-connected lattice bedstead with continuous ends
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	32.0'	alterations:	unknown
total length:	32.0'	floor/decking :	timber deck
roadway width:	13.8'	other features:	unknown

HISTORICAL DATA

erection date: c1905
erection cost: unknown
designer: Canton Bridge Company, Canton OH (probable)
fabricator : Canton Bridge Company, Canton OH (probable)
contractor: Canton Bridge Company, Canton OH (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 215000.3.

sign. rating: 43
evaluation: NRHP non-eligible (small-scale example of uncommon structural type, partially documented)

inventoried by: Michelle Crow-Dolby 7 July 1993

North Mud Creek Bridge

CALD18

GENERAL DATA

structure no.:	227000.6	city/town:	2.4 miles southwest of Braymer
county:	Caldwell	feature inters.:	North Mud Creek
		cadastral grid:	S16/17, T55N, R26W
		highway route:	County Road 227
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt bedstead		
substructure:	steel pile abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	45.0'	alterations:	unknown
total length:	65.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	lattice guardrails

HISTORICAL DATA

erection date:	1905
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 227000.6.

sign. rating:	26
evaluation:	NRHP non-eligible (typically configured, small-scale example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 July 1993

Bridge

CALD19

GENERAL DATA

structure no.: 239000.1	city/town: 2.8 miles southeast of Braymer
county: Caldwell	feature inters.: tributary of Willow Creek
	cadastral grid: S26/25, T55N, R26W
	highway route: County Road 239
	highway distr.: 1
	current owner: Caldwell County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss	
substructure: concrete abutments and wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: unknown
total length: 50.0'	floor/decking : timber deck
roadway width: 13.4'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 239000.1; field inspection by Lon Johnson, 8 September 1990.

sign. rating: 26
evaluation: NRHP non-eligible (typically configured, small-scale example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 July 1993

Bridge

CALD21

GENERAL DATA

structure no.:	264001.7	city/town:	3.0 miles southwest of Cowgill
county:	Caldwell	feature inters.:	branch of East Fork of Crooked River
		cadastral grid:	S32, T55N, R27W
		highway route:	County Road 264
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	concrete abutments and wingwalls with steel cylinder piers		
span number:	1	condition:	fair
span length:	50.0'	alterations:	none
total length:	51.0'	floor/decking :	timber deck
roadway width:	13.4'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 264001.7.

sign. rating:	26
evaluation:	NRHP non-eligible (undistinguished example of common structural type)

inventoried by: Michelle Crow-Dolby 7 July 1993

Bridge

CALD22

GENERAL DATA

structure no.:	268000.9	city/town:	2.4 miles southwest of Cowgill
county:	Caldwell	feature inters.:	branch of East Fork of Crooked River
		cadastral grid:	S29, T55N, R27W
		highway route:	County Road 268
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 2-panel, rigid-connected lattice bedstead with continuous endposts		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	30.0'	alterations:	none
total length:	30.0'	floor/decking :	concrete deck
roadway width:	12.0'	other features:	unknown

HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	Canton Bridge Company, Canton OH (probable)
fabricator :	Canton Bridge Company, Canton OH (probable)
contractor:	Canton Bridge Company, Canton OH (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 268000.9; field inspection by Lon Johnson, 8 September 1990.
sign. rating:	43
evaluation:	NRHP non-eligible (small-scale example of uncommon structural type, partially documented)

inventoried by: Michelle Crow-Dolby 7 July 1993

North Mud Creek Bridge

CALD23

GENERAL DATA

structure no.: 279001.5	city/town: 2.5 miles north of Cowgill
county: Caldwell	feature inters.: North Mud Creek
	cadastral grid: S4/9, T55N, R27W
	highway route: County Road 279
	highway distr.: 1
	current owner: Caldwell County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt bedstead
substructure: concrete abutments and wingwalls

span number: 1	condition: fair
span length: 50.0'	alterations: none
total length: 50.0'	floor/decking : timber deck over steel stringers
roadway width: 13.3'	other features: upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plate (outer panels), 2 looped rectangular eyebars (middle panel); vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; floor beam: I-beam, U-bolted to lower chord pins; guardrail: lattice

HISTORICAL DATA

erection date: 1904
erection cost: \$580.00 (contract amount)
designer: unknown
fabricator : Cambria Steel Company, Pittsburgh PA
contractor : John H. Sparks, St. Joseph MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 279001.5; Caldwell County Court Record, Book O: n.p. (12 June 1902), page 591 (6 September 1904) - located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.

sign. rating: 38
evaluation: NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby 7 July 1993

North Mud Creek Bridge

CALD24

GENERAL DATA

structure no.: 295000.8	city/town: 4.5 miles northeast of Cowgill
county: Caldwell	feature inters.: North Mud Creek
	cadastral grid: S35/36, T56N, R27W
	highway route: County Road 295
	highway distr.: 1
	current owner: Caldwell County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt bedstead	
substructure: bedstead legs with stone backwall; concrete abutments and wingwalls	
span number: 1	condition: fair
span length: 54.0'	alterations: none
total length: 54.0'	floor/decking : timber deck over steel stringers
roadway width: 13.8'	other features: upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plates (outer panes), 2 looped rectangular eyebars (inner panel); vertical: 4 angles with lacing; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pins; guard-rail: lattice

HISTORICAL DATA

erection date: 1904-05	
erection cost: \$644.00	
designer: unknown	
fabricator : unknown	
contractor: John H. Sparks, St. Joseph MO	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 295000.8; Caldwell County Court Record, Book O: page 592 (6 September 1904); Caldwell County Bridge Record, n.p. - both located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.
sign. rating: 47	
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 7 July 1993

Bridge

CALD25

GENERAL DATA

structure no.:	299001.0	city/town:	5.5 miles northeast of Proctorville
county:	Caldwell	feature inters.:	branch of Shoal Creek
		cadastral grid:	S13/24, T56N, R27W
		highway route:	County Road 299
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt bedstead
substructure: truss-legs with timber back- and wingwalls

span number:	1	condition:	fair
span length:	40.0'	alterations:	none
total length:	40.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.8'	other features:	upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 4 angles with cover plate (outer panels), 2 looped rectangular eyebars (inner panel); vertical: 4 angles with lacing; diagonal: 2 angles with batten plates, 2 looped rectangular eyebars; lateral: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pins; guardrail: 1 angle

HISTORICAL DATA

erection date: c1905
erection cost: unknown
designer: unknown
fabricator : Cambria Steel Company, Pittsburgh PA
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 299001.0; field inspection by Lon Johnson, 8 September 1990.

sign. rating: 28
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 7 July 1993

Otter Creek Bridge

CALD26

GENERAL DATA

structure no.: 303000.4	city/town: 6.0 miles east of Proctorville
county: Caldwell	feature inters.: Otter Creek
	cadastral grid: S11, T56N, R27W
	highway route: County Road 303
	highway distr.: 1
	current owner: Caldwell County

STRUCTURAL DATA

superstructure: wrought iron, 10-panel, bolted Bowstring through arch-truss	
substructure: timber pile bent abutments and wingwalls	
span number: 1	condition: fair
span length: 120.0'	alterations: bridge closed to traffic
total length: unknown'	floor/decking : timber deck over steel or iron stringers
roadway width: unknown'	other features: upper chord: 2 channels with cover plate and double lacing; lower chord: 2 rectangular eyebars, spliced; vertical: star iron; diagonal: round rod with threaded ends; lateral bracing: round rod with threaded ends (top and bottom); strut: pipe; floor beam: riveted plate girder, U-bolted to lower chords; outrider: star iron

HISTORICAL DATA

erection date: 1875	
erection cost: \$4000.00 (approximate cost)	
designer: Missouri Valley Bridge and Iron Works, Leavenworth KS (probable)	
fabricator : Missouri Valley Bridge and Iron Works, Leavenworth KS (probable)	
contractor : Missouri Valley Bridge and Iron Works, Leavenworth KS (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 303000.4; History of Caldwell and Livingston Counties, Missouri (St. Louis: National Historical Company, 1886), page 254; field inspection by Lon Johnson, 8 September 1990.	
sign. rating: 76	
evaluation: NRHP eligible (excellent example of now-rare structural type)	

Inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

Otter Creek Bridge

CALD27

GENERAL DATA

structure no.:	305001.2	city/town:	4.0 miles south of Leton
county:	Caldwell	feature inters.:	Otter Creek
		cadastral grid:	S35/2, T57N, R27W
		highway route:	County Road 305
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent pier and abutments with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	66.0'	floor/decking :	timber deck
roadway width:	11.4'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1901
erection cost:	\$570.00 (contract amount)
designer:	unknown
fabricator :	unknown
contractor:	John H. Sparks, St. Joseph Mo
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 305001.2; Caldwell County Court Record, Book N: page 403 (3 April 1901), page 415 (22 April 1901) - located at the Caldwell County Courthouse, Kingston MO.
sign. rating:	43
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 7 July 1993

Little Otter Creek Bridge

CALD28

GENERAL DATA

structure no.:	307000.2	city/town:	5.8 miles southeast of Hamilton
county:	Caldwell	feature inters.:	Little Otter Creek
		cadastral grid:	S9/10, T56N, R27W
		highway route:	County Road 307
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.5'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1904
erection cost:	\$1120.00 (3-bridge contract)
designer:	unknown
fabricator :	unknown
contractor:	John H. Sparks, St. Joseph MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 307000.2; Caldwell County Court Record, Book O: page 530 (4 August 1904) - located at the Caldwell County Courthouse, Kingston MO.
sign. rating:	44
evaluation:	NRHP non-eligible (typically configured example of common structural type)

Inventoried by: Michelle Crow-Dolby 7 July 1993

Shoal Creek Bridge

CALD29

GENERAL DATA

structure no.:	307002.5	city/town:	6.5 miles southeast of Hamilton
county:	Caldwell	feature inters.:	Shoal Creek
		cadastral grid:	S9/10, T56N, R27W
		highway route:	County Road 307
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Pratt through truss, with steel stringer approach spans		
substructure:	concrete abutments and wingwalls; steel pile bent piers		
span number:	1	condition:	fair
span length:	120.0'	alterations:	none
total length:	360.0'	floor/decking :	concrete deck over steel stringers
roadway width:	14.5'	other features:	steel guardrails

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 307002.5.
sign. rating:	26
evaluation:	NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 July 1993

Cottonwood Creek Bridge

CALD30

GENERAL DATA

structure no.:	316000.7	city/town:	2.5 miles northeast of Bonanza
county:	Caldwell	feature inters.:	Cottonwood Creek
		cadastral grid:	S8/9, T56N, R27W
		highway route:	County Road 316
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt bedstead		
substructure:	truss leg abutments with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	48.0'	alterations:	none
total length:	48.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.8'	other features:	upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plate (outer panels), 2 looped rectangular eyebars (inner panel); vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pins; guardrail: 2 angles

HISTORICAL DATA

erection date:	1904
erection cost:	\$579.00
designer:	unknown
fabricator :	Cambria Steel Company, Pittsburgh PA
contractor:	John H. Sparks, St. Joseph MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 316000.7; Caldwell County Court Record, Book O: n.p. (12 June 1902), page 592 (6 September 1904); Caldwell County Bridge Record, n.p. - both located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.
sign. rating:	38
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 7 July 1993

Tom Creek Bridge

CALD31

GENERAL DATA

structure no.: 324000.6	city/town: 1.0 mile north of Bonanza
county: Caldwell	feature inters.: Tom Creek
	cadastral grid: S14, T56N, R27W
	highway route: County Road 324
	highway distr.: 1
	current owner: Caldwell County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt bedstead
substructure: concrete abutments and wingwalls

span number: 1	condition: fair
span length: 60.0'	alterations: none
total length: 61.0'	floor/decking : timber deck over steel stringers
roadway width: 11.7'	other features: upper chord and upright end post: 2 channels with cover plate and lacing; lower chord: 2 angles with lacing (outer panels), 2 looped rectangular eyebars (inner panels); vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pins; guardrail: single channel

HISTORICAL DATA

erection date: 1904
erection cost: \$7900.00 (22-bridge contract)
designer: Canton Bridge Company, Canton OH
fabricator : Canton Bridge Company, Canton OH
contractor: Canton Bridge Company, Canton OH

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 324000.6; Caldwell County Court Record, Book O: page 181 (1 May 1903), page 343 (4 November 1903) - located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.

sign. rating: 44
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 7 July 1993

Bonanza Bridge

CALD32

GENERAL DATA

structure no.: 324001.9 city/town: Bonanza
county: Caldwell feature inters.: Shoal Creek
cadastral grid: S19, T56N, R27W
highway route: County Road 324
highway distr.: 1
current owner: Caldwell County

STRUCTURAL DATA

superstructure: wrought iron, 11-panel, pin-connected Whipple through truss
substructure: concrete-filled iron cylinder pier abutment with concrete backwalls (north); stone masonry abutment (south)

span number: 1 condition: fair
span length: 187.0' alterations: partial replacement of substructure
total length: 187.0' floor/decking : timber/concrete deck over iron or steel stringers
roadway width: 14.0' other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with double lacing, 2 looped square rods at hip; diagonal: 2 punched rectangular eyebars; lateral bracing: round rod with threaded ends (top and bottom); floor beam: riveted plate girder, U-bolted to lower chord pins; guardrail: 2 channels

HISTORICAL DATA

erection date: 1883
erection cost: \$6000.00 (approximate cost)
designer: unknown
fabricator : Carnegie Iron Company, Pittsburgh PA
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 324001.9; *The Hamiltonian*, 21 September 1883; *History of Caldwell and Livingston Counties, Missouri* (St. Louis: National Historical Company, 1886), page 254; Fraserdesign, "Bonanza Bridge: Preliminary Determination of NRHP Eligibility for the Missouri Historic Bridge Inventory," 25 September 1991; letter and enclosures from Richard Heisler, P.E., to Michael W. Weichman, dated 28 August 1991; field inspection by Lon Johnson, 8 September 1990.

sign. rating: 66
evaluation: NRHP eligible (exceptional, long-span example of now-rare structural type)

inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

Shoal Creek Bridge

CALD35

GENERAL DATA

structure no.:	341000.9	city/town:	1.6 miles northeast of Kingston
county:	Caldwell	feature inters.:	Shoal Creek
		cadastral grid:	S23, T56N, R28W
		highway route:	County Road 341
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure: steel or wrought iron, 7-panel, pin-connected Pratt through truss, with steel stringer approach spans

substructure: stone masonry abutments; concrete-filled iron cylinder piers; steel pile bent piers at approach spans

span number:	1	condition:	fair
span length:	112.0'	alterations:	none
total length:	168.0'	floor/decking :	timber deck over iron or steel stringers
roadway width:	13.6'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with double lacing, looped round eyerod at hips; diagonal: 2 looped rectangular or square eyebars; lateral bracing: round rod with threaded ends; strut: one angle; portal strut: lattice, with knee braces; floor beam: variable depth "fishtail" plate girder, U-bolted to lower chord pins; guard-rail: lattice

HISTORICAL DATA

erection date: c1890

erection cost: unknown

designer: unknown

fabricator : Carnegie Iron Company, Pittsburgh PA

contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 341000.9; field inspection by Lon Johnson, 8 September 1990.

sign. rating: 34

evaluation: NRHP non-eligible (well-preserved, inadequately documented example of mainstay structural type)

inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

Cox Ford Bridge

CALD36

GENERAL DATA

structure no.: 342002.8 city/town: 3.5 miles east of Kingston
county: Caldwell feature inters.: Shoal Creek
cadastral grid: S24, T56N, R28W
highway route: County Road 342
highway distr.: 1
current owner: Caldwell County

STRUCTURAL DATA

superstructure: wrought iron, 6-panel, pin-connected Pratt through truss, with steel stringer approach spans
substructure: concrete abutments and wingwalls; concrete-filled iron cylinder piers; steel pile bents piers at approach spans

span number: 1 condition: fair
span length: 114.0' alterations: none
total length: 194.0' floor/decking : timber deck over iron or steel stringers
roadway width: 14.5' other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with double lacing, 2 looped round eyerods at hip; diagonal: 2 punched rectangular eyebars; counter: looped round eyerod with turnbuckle; lateral bracing: round rod with threaded ends (top and bottom); strut: 4 angles with lacing; floor beam: variable depth, "fishtail" plate girder; guardrail: 2 channels; portal builder's plate: 1888 / King Iron Bridge Company / Cleveland, O

HISTORICAL DATA

erection date: 1888
erection cost: \$4095.00 (contract amount)
designer: King Iron Bridge Company, Cleveland OH
fabricator : King Iron Bridge Company, Cleveland OH
contractor: King Iron Bridge Company, Cleveland OH

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 342002.8; *The Hamiltonian*, 17 April 1885, 23 April 1886, 17 February 1888, 6 April 1888, 25 May 1888, 22 June 1888; *History of Caldwell and Livingston Counties, Missouri* (St. Louis: National Historical Company, 1886), page 254; field inspection by Lon Johnson, 8 September 1990.

sign. rating: 53
evaluation: NRHP possibly eligible (well-preserved, well-documented example of mainstay structural type)

inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

Long Creek Bridge

CALD37

GENERAL DATA

structure no.:	349000.7	city/town:	2.1 miles south of Kingston
county:	Caldwell	feature inters.:	branch of Long Creek
		cadastral grid:	S33/34, T55/56N, R28W
		highway route:	County Road 349
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 4-panel, rigid-connected 2-angle Pratt pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	60.0'	floor/decking :	concrete deck over steel stringers
roadway width:	12.8'	other features:	no guardrails

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 349000.7.

sign. rating:	30
evaluation:	NRHP non-eligible (undistinguished, undocumented example of uncommon structural type)

inventoried by: Michelle Crow-Dolby 7 July 1993

Log Creek Bridge

CALD38

GENERAL DATA

structure no.:	350000.8	city/town:	3.0 miles southwest of Kingston
county:	Caldwell	feature inters.:	Log Creek
		cadastral grid:	S31/6, T56/55N, R28W
		highway route:	County Road 350
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	concrete abutments, wingwalls and piers		
span number:	1	condition:	fair
span length:	54.0'	alterations:	none
total length:	112.0'	floor/decking :	timber deck
roadway width:	13.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 350000.8.
sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 July 1993

Tub Creek Bridge

CALD39

GENERAL DATA

structure no.: 351001.3	city/town: 2.7 miles southwest of Kingston
county: Caldwell	feature inters.: Tub Creek
	cadastral grid: S31, T56N, R28W
	highway route: County Road 351
	highway distr.: 1
	current owner: Caldwell County

STRUCTURAL DATA

superstructure: steel, 2-panel, rigid-connected lattice bedstead, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber back- and wingwalls	
span number: 1	condition: fair
span length: 24.0'	alterations: none
total length: 56.0'	floor/decking : timber deck
roadway width: 12.2'	other features: unknown

HISTORICAL DATA

erection date: 1904	
erection cost: \$7900.00 (22-bridge contract)	
designer: Canton Bridge Company, Canton OH	
fabricator : Canton Bridge Company, Canton OH	
contractor: Canton Bridge Company, Canton OH	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 351001.3; Caldwell County Court Record, Book M: page 187 (7 June 1897); Book O: page 344 (4 November 1903) - located at the Caldwell County Courthouse, Kingston MO.	
sign. rating: 47	
evaluation: NRHP non-eligible (small-scale example of uncommon structural type)	

Inventoried by: Michelle Crow-Dolby 7 July 1993

Log Creek Bridge

CALD40

GENERAL DATA

structure no.: 353000.8	city/town: 1.4 miles southwest of Kingston
county: Caldwell	feature inters.: Log Creek
	cadastral grid: S28/29, T56N, R28W
	highway route: County Road 353
	highway distr.: 1
	current owner: Caldwell County

STRUCTURAL DATA

superstructure: steel, 5-panel, rigid-connected Pratt pony truss	
substructure: concrete abutments and wingwalls	
span number: 1	condition: fair
span length: 80.0'	alterations: unknown
total length: 80.0'	floor/decking : timber deck
roadway width: 13.7'	other features: steel angle guardrails

HISTORICAL DATA

erection date: 1908
erection cost: unknown
designer: Dildine Bridge Company, Cameron MO
fabricator : unknown
contractor : Dildine Bridge Company, Cameron MO
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 353000.8; Caldwell County Court Record, Book O: page 530 (4 August 1904); Book Q: page 182 (5 May 1908) - located at the Caldwell County Courthouse, Kingston MO.
sign. rating: 53
evaluation: NRHP possibly eligible (earliest dateable example in state of mainstay structural type)

inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

Shoal Creek Bridge

CALD41

GENERAL DATA

structure no.:	359000.8	city/town:	2.9 miles northwest of Kingston
county:	Caldwell	feature inters.:	Shoal Creek
		cadastral grid:	S17/18, T56N, R28W
		highway route:	County Road 359
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure: steel, 7-panel, pin-connected Pratt through truss, with steel stringer approach spans

substructure: concrete abutments and wingwalls; steel pile bent piers

span number:	1	condition:	fair
span length:	124.0'	alterations:	none
total length:	204.0'	floor/decking :	timber/concrete deck over steel stringers
roadway width:	12.3'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing, 2 looped square eyebars at hip; diagonal: 2 looped rectangular eyebars; counter: looped square eyebar with turnbuckle; lateral bracing: round rod with threaded ends (top and bottom); strut: 4 angles with double lacing; floor beam: I-beam, U-bolted to lower chord pins; guardrail: 2 channels; portal builder's plate: 1894 / John Dildine / and Co / Builders / Wmure / L.L. Frost / JM Loomis / CO / Judges

HISTORICAL DATA

erection date: 1894
erection cost: unknown
designer: John Dildine and Company, Cameron MO
fabricator : unknown
contractor: John Dildine and Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 359000.8; field inspection by Lon Johnson, 8 September 1990.

sign. rating: 52
evaluation: NRHP possibly eligible (well-preserved, relatively early example of main-stay structural type)

inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

Shoal Creek Bridge

CALD43

GENERAL DATA

structure no.:	368001.2	city/town:	4.8 miles northwest of Kingston
county:	Caldwell	feature inters.:	Shoal Creek
		cadastral grid:	S11/12, T56N, R28W
		highway route:	County Road 368
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 5-panel, rigid-connected Pratt pony truss, with steel stringer approach spans		
substructure:	concrete abutments, wingwalls and piers		
span number:	1	condition:	fair
span length:	100.0'	alterations:	unknown
total length:	131.0'	floor/decking :	unknown
roadway width:	13.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1930
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 368001.2.

sign. rating:	28
evaluation:	NRHP non-eligible (typically configured, poorly documented example of common structural type)

inventoried by: Michelle Crow-Dolby 7 July 1993

Shoal Creek Bridge

CALD44

GENERAL DATA

structure no.:	372000.5	city/town:	3.9 miles of northwest of Kingston
county:	Caldwell	feature inters.:	Shoal Creek
		cadastral grid:	S12/7, T56N, R28W
		highway route:	County Road 372
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 6-panel, pin-connected Pratt through truss, with steel stringer approach spans		
substructure:	concrete abutments and wingwalls with solid concrete piers		
span number:	1	condition:	fair
span length:	120.0'	alterations:	none
total length:	180.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.7'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with batten plates; diagonal: 2 punched rectangular eyebars; counter: 2 looped round eyerods with turnbuckles; lateral bracing: round rod with threaded ends at top and bottom; strut and floor beam: I-beam; guardrail: 2 angles

HISTORICAL DATA

erection date:	1901
erection cost:	\$1710.00 (contract amount)
designer:	unknown
fabricator :	Illinois Steel Company, Chicago IL
contractor:	John H. Sparks, St. Joseph MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 372000.5; Caldwell County Court Record, Book N: page 403 (3 April 1901), page 415 (22 April 1901) - located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.
sign. rating:	46
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Clayton Fraser and Michelle Crow-Dolby 7 July 1993

Shoal Creek Bridge

CALD45

GENERAL DATA

structure no.: 383001.5	city/town: 9.0 miles southwest of Kidder
county: Caldwell	feature inters.: Shoal Creek
	cadastral grid: S7, T56N, R29W
	highway route: County Road 383
	highway distr.: 1
	current owner: Caldwell County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt bedstead	
substructure: concrete abutments and wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: none
total length: 51.0'	floor/decking : timber deck
roadway width: 13.6'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 383001.5.

sign. rating: 29
evaluation: NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Michelle Crow-Dolby 7 July 1993

Long Creek Bridge

CALD46

GENERAL DATA

structure no.: 406000.6	city/town: 2.2 miles northwest of Polo
county: Caldwell	feature inters.: Long Creek
	cadastral grid: S16, T55N, R28W
	highway route: County Road 406
	highway distr.: 1
	current owner: Caldwell County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss	
substructure: concrete abutments and wingwalls	
span number: 1	condition: fair
span length: 60.0'	alterations: none
total length: 60.0'	floor/decking : concrete deck over steel stringers
roadway width: 13.2'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 406000.6.

sign. rating: 28
evaluation: NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 July 1993

Long Creek Bridge

CALD47

GENERAL DATA

structure no.:	407000.2	city/town:	1.0 mile northwest of Polo
county:	Caldwell	feature inters.:	Long Creek
		cadastral grid:	S17/20, T55N, R28W
		highway route:	County Road 407
		highway dist.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	60.0'	floor/decking :	concrete deck over steel stringers
roadway width:	11.6'	other features:	lattice guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 407000.2.
sign. rating:	28
evaluation:	NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 7 July 1993

Crooked River Bridge

CALD48

GENERAL DATA

structure no.: 442002.4	city/town: 5.7 miles southwest of Polo
county: Caldwell	feature inters.: Crooked River
	cadastral grid: S34, T55N, R29W
	highway route: County Road 442
	highway distr.: 1
	current owner: Caldwell County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss	
substructure: unknown	
span number: 1	condition: fair
span length: 80.0'	alterations: none
total length: 118.0'	floor/decking : timber deck
roadway width: 13.8'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1915
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 442002.4.

sign. rating: 30
evaluation: NRHP non-eligible (typical example of common structural type, poorly documented)

Inventoried by: Michelle Crow-Dolby 7 July 1993

West Sheep Creek Bridge

CALD49

GENERAL DATA

structure no.:	028000.8	city/town:	0.5 mile east of Barwick
county:	Caldwell	feature inters.:	West Sheep Creek
		cadastral grid:	S26/35, T57N, R29W
		highway route:	County Road 28
		highway distr.:	1
		current owner:	Caldwell County

STRUCTURAL DATA

superstructure:	steel deck girder	condition:	fair
substructure:	concrete abutments and wingwalls	alterations:	none
span number:	1	floor/decking :	concrete deck
span length:	35.0'	other features:	steel angle guardrails
total length:	36.0'		
roadway width:	15.8'		

HISTORICAL DATA

erection date:	1922
erection cost:	\$7,057.86 (five-bridge contract)
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	Bean and Company, St. Joseph MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 442002.4; Missouri State Highway Board, Third Biennial Report : 1921-1922, page 137.
sign. rating:	36
evaluation:	NRHP possibly eligible (earliest dateable example in state of this MSHD beam-bridge design)

inventoried by: Michelle Crow-Dolby 7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Brushy Creek Bridge
MHTD: 023002.0

CALD04

DATE(S) OF CONSTRUCTION

1904

LOCATION

County Road 23 over Brushy Creek; S28/33, T57N, R29W
5.5 miles northeast of Kidder; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 42)

CONDITION

fair

OWNER

Caldwell County

span number: 1
span length: 80.0'
total length: 80.0'
roadway wdt.: 11.4'

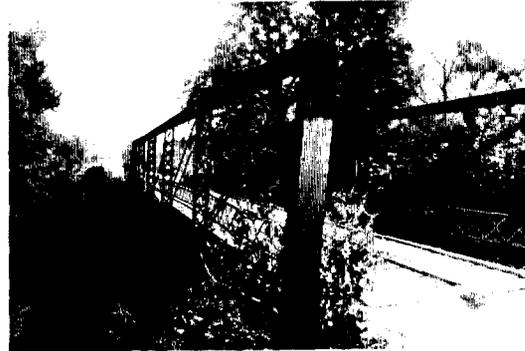
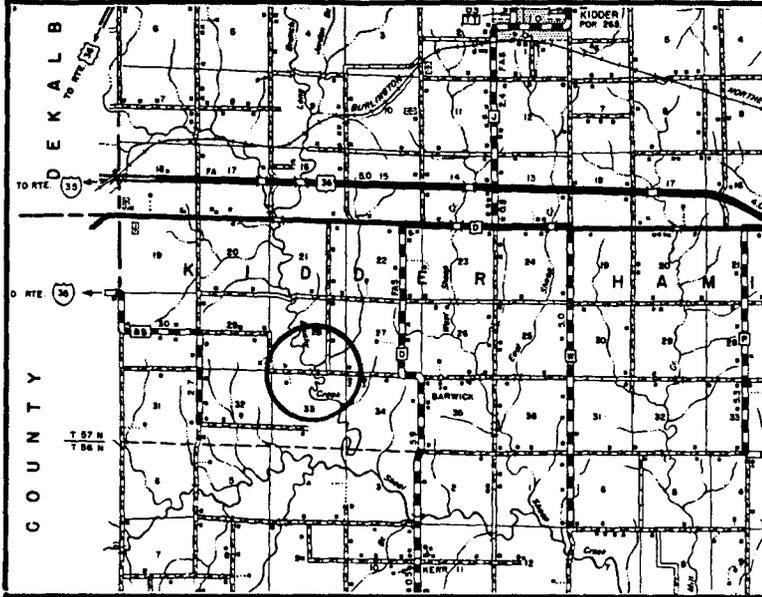
superstructure: steel, 6-panel, pin-connected Pratt bedstead
substructure: concrete abutments and wingwalls
floor/decking: timber deck
other features: upper chord and upright end post: 2 channels with cover plate and lacing; lower chord: 2 channels with cover plate, 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 1 looped square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice

Located in Kidder Township, this long-span steel truss carries a gravel-surfaced county road over Brushy Creek. The structure is comprised of a pinned bedstead, supported by concrete abutments. The Brushy Creek Bridge was one of 22 bridges erected in Caldwell County from a single contract let in November 1903. The bridges had been surveyed by the county road and bridge commissioner that spring and a contract let to the Canton Bridge Company for \$8990.00 to fabricate and build all 22 spans. For some reason this contract was never executed, however, and a second contract was signed with the Canton, Ohio, bridge firm in November for a thousand dollars less. Canton used steel components rolled in Pittsburgh by the Cambria steel works to fabricate this 80-foot bedstead, erecting it over Brushy Creek northeast of Kidder in 1904. The Brushy Creek Bridge has functioned in place since that time, with no major alterations and minimal maintenance.

In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the 1890s and early 1900s. Hundreds remain in place today; in fact Missouri has probably more bedsteads than any other state. The Brushy Creek Bridge is a long-span, though not particularly distinguished, example of this statewide bridge construction trend.

NAME(S) OF STRUCTURE
Brushy Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 023002.0; Caldwell County Court Record, Book O: page 181 (1 May 1903), page 344 (4 November 1903) - located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Sheep Creek Bridge
MHTD: 032000.2

CALD05

DATE(S) OF CONSTRUCTION

1904

LOCATION

County Road 32 over Sheep Creek; S36/1, T57/56N, R29W
5.5 miles southeast of Hamilton; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 44)

CONDITION

fair

OWNER

Caldwell County

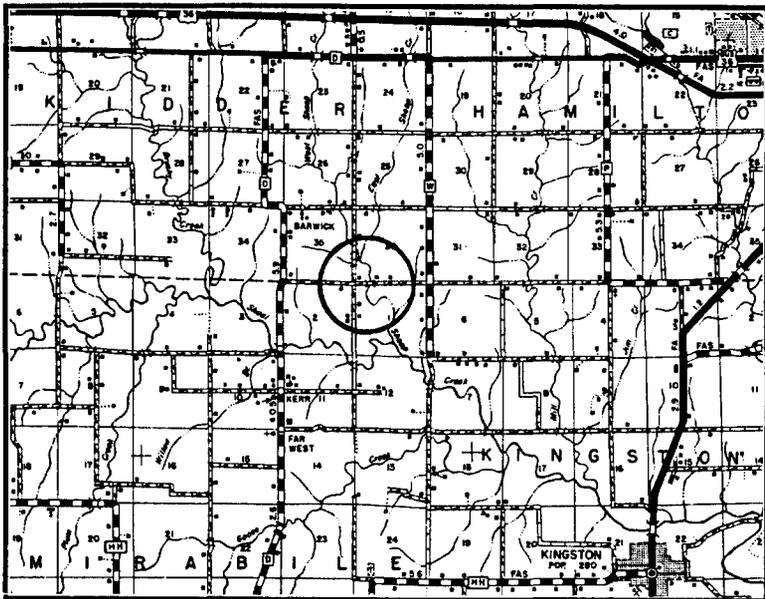
span number: 1	superstructure: steel, 5-panel, pin-connected, Pratt bedstead
span length: 70.0'	substructure: steel pile bent piers with timber wingwalls
total length: 72.0'	floor/decking: timber deck
roadway wdt.: 14.0'	other features: upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plate at outriders, forged eye-bars; vertical: 4 angles with lacing; diagonal: square rods, round rods with threaded ends at center panel; lateral bracing: round rods with threaded ends; stringer and floor beam: I-beam; guardrail: steel lattice

On September 6, 1904, the Caldwell County Court executed a contract with J.H. Sparks of St. Joseph, Missouri, for the erection of a 70-foot truss between Kidder and Mirabile Townships in the northwestern part of the county. The truss - a pin-connected bedstead on 18-foot legs - would span Sheep Creek southeast of Hamilton. Sparks utilized steel components rolled in Pittsburgh by the Cambria Steel Company for the span, erecting it later that year for \$940.00. The Sheep Creek Bridge has carried vehicular traffic since, with the subsequent replacement of its truss legs with concrete abutments as the most serious alteration.

In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the late 1890s and early 1900s. Hundreds remain in place today -in fact, Missouri has probably more bedsteads than any other state. The Sheep Creek Bridge is a typically configured example of this statewide bridge construction trend.

NAME(S) OF STRUCTURE
Sheep Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 032000.2; Caldwell County Court Record, Book O: page 592 (6 September 1904); Caldwell County Bridge Record, n.p. - both located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Tom Creek Bridge
MHTD: 075001.2

CALD07

DATE(S) OF CONSTRUCTION

1904

LOCATION

County Road 75 over Tom Creek; S22/27, T57N, R28W
2.4 miles southwest of Hamilton; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 40)

CONDITION

fair

OWNER

Caldwell County

span number: 1

superstructure: steel, 3-panel, pin-connected Pratt bedstead

span length: 50.0'

substructure: steel pile bent piers with timber wingwalls

total length: 50.0'

floor/decking: timber deck over steel stringers

roadway wdt.: 11.6'

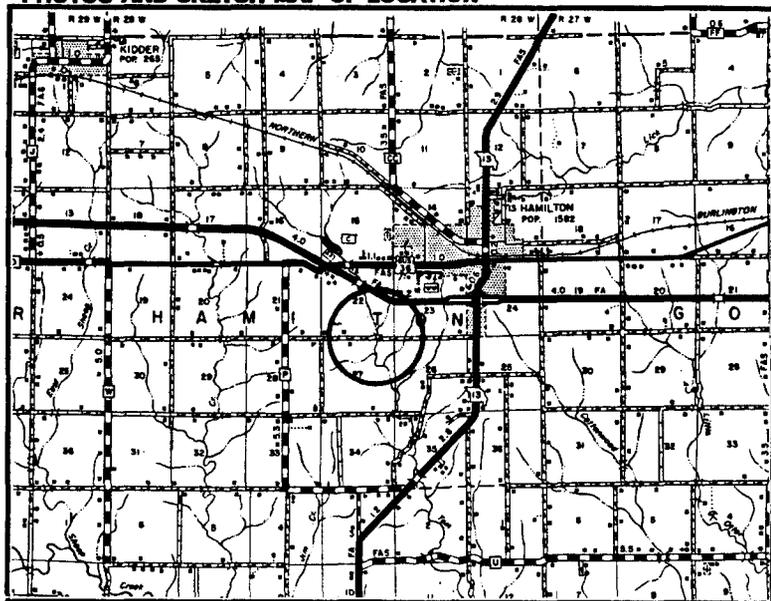
other features: upper chord and upright end post: 2 channels with cover plate and lacing; lower chord: 2 angles with lacing; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: one angle

Located in Hamilton Township, this medium-span steel truss carries a gravel-surfaced county road over Tom Creek. The structure is comprised of a pinned bedstead, supported by concrete abutments. The Tom Creek Bridge was one of 22 bridges erected in Caldwell County from a single contract let in November 1903. The bridges had been surveyed by the county road and bridge commissioner that spring and a contract let to the Canton Bridge Company for \$8990.00 to fabricate and build all 22 spans. For some reason this contract was never executed, however, and a second contract was signed with the Canton, Ohio, bridge firm in November for a thousand dollars less. Canton used steel components rolled in Pittsburgh by the Cambria steel works to fabricate this 80-foot bedstead, erecting it over Tom Creek east of Kidder in 1904. The Brushy Creek Branch Bridge has functioned in place since that time, with no major alterations and minimal maintenance.

The Canton Bridge Company, fabricator for this medium-span bestead, maintained an extensive list of standard truss types and was extensively involved in Missouri bridge construction after the turn of the century. The bedstead truss, as built by Canton and others incorporated a single "leg" at each of the four corners, which acted as both end post and substructural support. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous bedsteads were erected throughout Missouri in the 1890s and early 1900s. Hundreds remain in place today -in fact, Missouri has probably more bedsteads than any other state. The Tom Creek Bridge is a typically configured example of this statewide bridge construction trend.

NAME(S) OF STRUCTURE
Tom Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 075001.2; Caldwell County Court Record, Book O: page 181 (1 May 1903), pages 341-2 (4 November 1903) - located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Henkin's Ford Bridge
MHTD: 181001.9

CALD13

DATE(S) OF CONSTRUCTION

1887

LOCATION

County Road 181 over Shoal Creek; S8/9, T56N, R26W
3.0 miles east of Proctorville; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP eligible (score: 72)

CONDITION

fair

OWNER

Caldwell County

span number: 1
span length: 125.0'
total length: 195.0'
roadway wdt.: 14.0'

superstructure: wrought iron, 10-panel, pin-connected Warren through truss, with steel stringer approach spans
substructure: concrete abutments and wingwalls; concrete-filled iron cylinder piers; pile bent piers at approach spans
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 looped round eyerods; diagonal: 2 channels with cover and batten plates, 22 looped square eyebars; strut: I-beam; floor beam: riveted plate girder, U-bolted from lower chord pins; guardrail: lattice

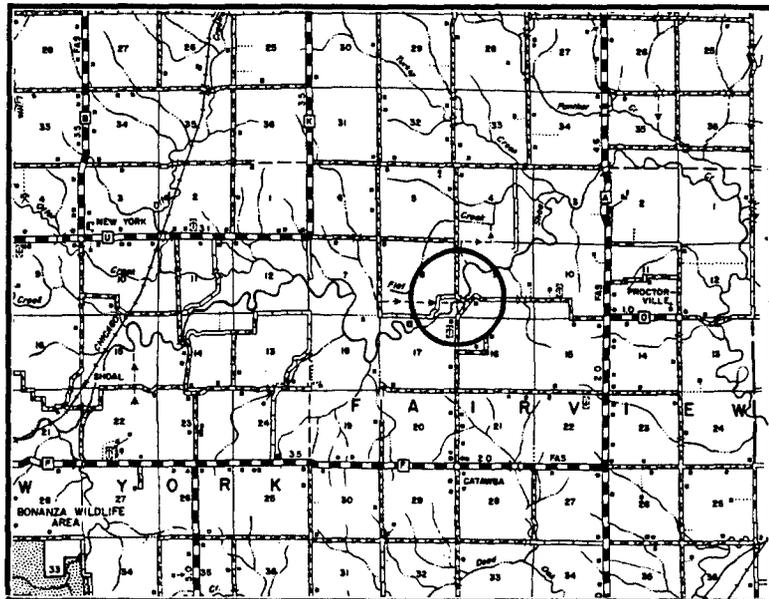
One of Caldwell County's earliest county road crossing sites was Henkin's Ford, located some three miles west of Proctorville in Fairview Township. In 1870 a 140-foot timber truss was built here - only the second major span built in the county. By the late 1880s, however, the timber span was showing signs of wear, and the county court moved to replace it. At their request, County Surveyor W.J. Boyd inspected the site and made estimates for a new structure in May 1887. Boyd solicited competitive proposals for the new span the following month, and in July a contract for its fabrication and erection was awarded to the King Bridge Company for \$2698.00. The Cleveland-based bridge giant used a pin-connected Warren truss configuration for the bridge, erecting the 125-foot span on iron tubular piers in 1887. In January 1888, King was paid for the completion of the Henkin's Ford Bridge across Shoal Creek and a shorter span over Mud Creek near Braymer [CALD14]. Since its completion, the Henkin's Ford Bridge has functioned in place, with the replacement of its abutments and approach spans as the most serious alterations to date.

The Pratt and Warren truss configurations were both developed in the 1840s, but it was the Pratt that received the most widespread use in the late 19th century. The reasons for this probably relate to the versatility of the pin-connected Pratt for different spans lengths and its easier erection using timber falseworks. Relatively few pinned Warren trusses were ever built in Missouri and only a couple remain in use today - both in Caldwell County - with the Henkin's Ford Bridge being the longer. Thus, this structure is distinguished as a well-preserved example of a rare 19th century truss type.

NAME(S) OF STRUCTURE

Henkin's Ford Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 181001.9; *The Hamiltonian*, 13 May 1887, 22 July 1887, 13 January 1888; *History of Caldwell and Livingston Counties, Missouri* (St. Louis: National Historical Company, 1886), page 254; field inspection by Clayton Fraser and John Roberts, 14 October 1993.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Mud Creek Bridge
MHTD: 204001.2

CALD14

DATE(S) OF CONSTRUCTION

1887

LOCATION

County Road 204 over Mud Creek; S36, T56/55N, R26W
2.0 miles northeast of Braymer; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 67)

CONDITION

fair

OWNER

Caldwell County

span number: 1
span length: 90.0'
total length: 90.0'
roadway wdt.: 13.6'

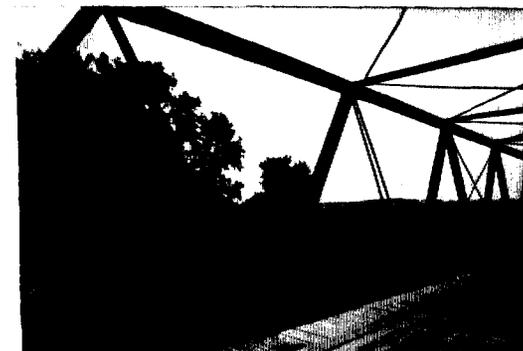
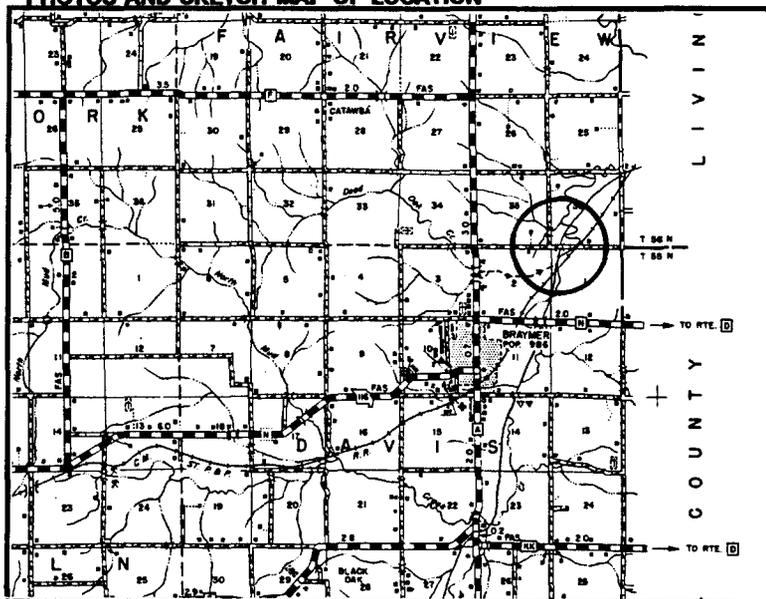
superstructure: wrought iron, 6-panel, pin-connected Warren through truss
substructure: concrete abutments and wingwalls
floor/decking: timber deck
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; diagonal: 2 channels with cover and batten plates, 2 looped square eyebars; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, U-bolted to lower chord pins; guardrail: 2 angles

Located two miles northeast of Braymer, this short-span through carries an unpaved county road across Mud Creek in Davis Township. The structure consists of a single pin-connected Warren truss, supported by concrete abutments. The Mud Creek Bridge is one of two such pinned Warren spans erected for the county in 1887 by the King Bridge Company of Cleveland, Ohio [other: CALD13]. Completed by January 1888, the Mud Creek Bridge has since carried vehicular traffic to the present. Its original substructure has been replaced with concrete, possibly as a part of an extensive bridge construction program undertaken in 1903. Other than this alteration, the structure remains physically intact.

The Mud Creek Bridge employs a classic Warren truss configuration, with a web that relies on simple triangulation for its rigidity. "The term Warren truss or Warren girder was originally applied only to the particular use of the Triangular truss in which the web triangles are all equilateral; but later writers generally use the name for any triangular truss," noted bridge engineer J.A.L. Waddell wrote in his 1916 treatise **Bridge Engineering**. "As there is no special advantage in making the web triangles equilateral, there does not appear to be any good *raison d'être* for the use of the true Warren type." In a state dominated by bowstrings and Pratt trusses in the 1870s and early 1880s, relatively few Warren trusses were built. Only a couple of such bridges survive in Missouri to the present, both erected in Caldwell County in 1887. Built by the one of the nation's most prolific bridge builders, it is an important early transportation-related resource.

NAME(S) OF STRUCTURE

Mud Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION**LOCATION MAP**TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 204001.2; *The Hamiltonian*, 13 May 1887, 22 July 1887, 13 January 1888; *History of Caldwell and Livingston Counties, Missouri* (St. Louis: National Historical Company, 1886), page 254; Caldwell County Court Record, Book O: page 181 (1 May 1903) - located at the Caldwell County Courthouse, Kingston MO; field inspection by Clayton Fraser and John Roberts, 14 October 1993.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

North Mud Creek Bridge
MHTD: 279001.5

CALD23

DATE(S) OF CONSTRUCTION

1904

LOCATION

County Road 279 over North Mud Creek; S4/9, T55N, R27W
2.5 miles north of Cowgill; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 38)

CONDITION

fair

OWNER

Caldwell County

span number: 1	superstructure: steel, 3-panel, pin-connected Pratt bedstead
span length: 48.0'	substructure: concrete abutments and wingwalls
total length: 48.0'	floor/decking: timber deck over steel stringers
roadway wdt.: 13.3'	other features: upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plate (outer panels), 2 looped rectangular eyebars (middle panel); vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; floor beam: I-beam, U-bolted to lower chord pins; guardrail: lattice

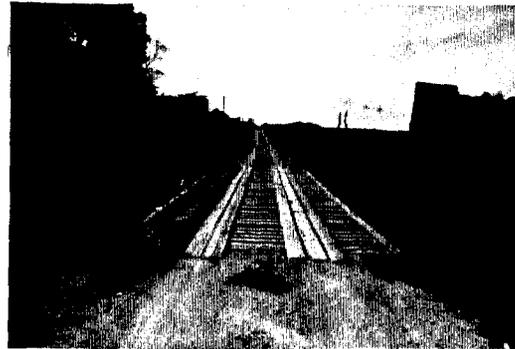
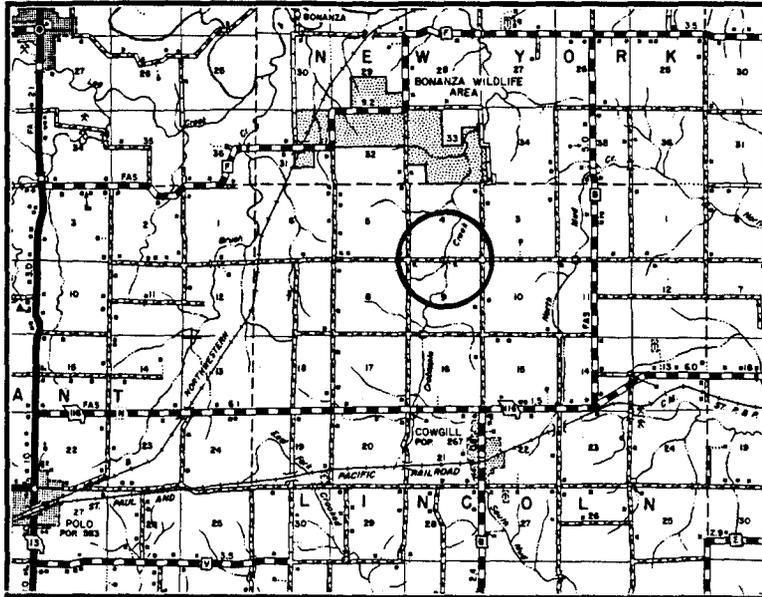
Carrying County Road 279 in south-central Caldwell County north of Cowgill, this bridge spans North Mud Creek. Configured as a single-span pin-connected Pratt bedstead, the truss is supported by a concrete substructure. Preceding this truss was a crossing which dates to 1902. In June of that year county officials entered into a contract with Dildine Bridge Company of Cameron to erect a bridge at this location. The bridge, costing \$329.00, however was short-lived. Only two years later, in September of 1904, county officials were letting a contract to St. Joseph-based John H. Sparks for a 48-foot bedstead crossing North Mud Creek. The cost of the bridge was \$580.00. Since its completion in 1904, the North Mud Creek Bridge has carried intermittent traffic with no major alterations of note.

In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the late 1890s and early 1900s. Hundreds remain in place today -in fact, Missouri has probably more bedsteads than any other state. The Tom Creek Bridge is a well-preserved, though not particularly distinguished, example of this statewide bridge construction trend.

NAME(S) OF STRUCTURE

North Mud Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 279001.5; Caldwell County Court Record, Book O: n.p. (12 June 1902), page 591 (6 September 1904) - located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

North Mud Creek Bridge
MHTD: 295000.8

CALD24

DATE(S) OF CONSTRUCTION

1904-05

LOCATION

County Road 295 over North Mud Creek; S35/36, T56N, R27W
4.5 miles northeast of Cowgill; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 47)

CONDITION

fair

OWNER

Caldwell County

span number: 1	superstructure: steel, 3-panel, pin-connected Pratt bedstead
span length: 54.0'	substructure: bedstead legs with stone backwall; concrete abutments and wingwalls
total length: 54.0'	floor/decking: timber deck over steel stringers
roadway wdt.: 13.8'	other features: upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plates (outer panes), 2 looped rectangular eyebars (inner panel); vertical: 4 angles with lacing; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pins; guardrail: lattice

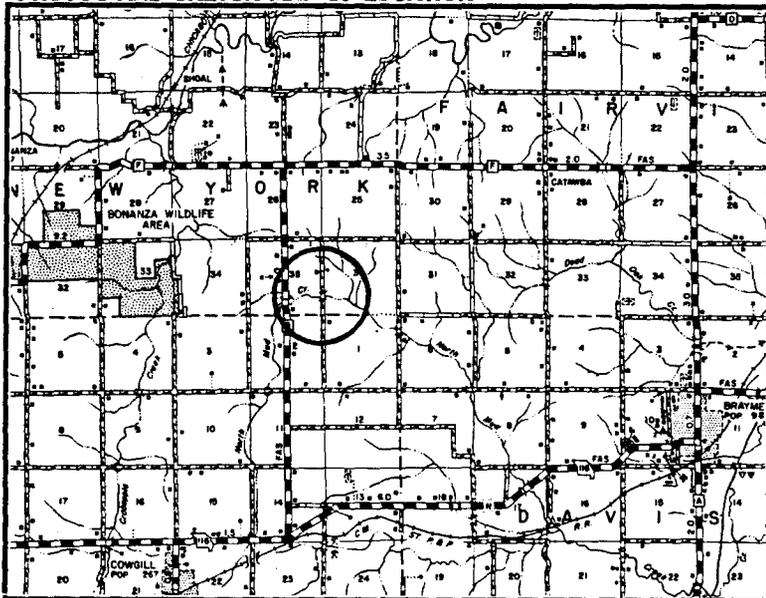
Carrying County Road 295 in central Caldwell County northeast of Cowgill, this bridge spans North Mud Creek. Configured as a single-span pin-connected Pratt bedstead, the truss is supported by a stone and concrete substructure. The history of this truss dates to 1904. In September of that year, county court officials let a contract to John H. Sparks of St. Joseph for the erection of a bridge at this location. The contract amount for the project was \$644.00. The North Mud Creek Bridge, since its construction in 1905, has carried local traffic with no major structural alterations for almost ninety years.

In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the late 1890s and early 1900s. Hundreds remain in place today -in fact, Missouri has probably more bedsteads than any other state. The North Mud Creek Bridge is a well-preserved, though not particularly distinguished, example of this statewide bridge construction trend.

NAME(S) OF STRUCTURE

North Mud Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 295000.8; Caldwell County Court Record, Book O: page 592 (6 September 1904); Caldwell County Bridge Record, n.p. - both located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Otter Creek Bridge
MHTD: 303000.4

CALD26

DATE(S) OF CONSTRUCTION

1875

LOCATION

County Road 303 over Otter Creek; S11, T56N, R27W
6.0 miles east of Proctorville; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / closed

RATING NRHP eligible (score: 76)

CONDITION

fair

OWNER

Caldwell County

span number: 1

span length: 120.0'

total length: unknown

roadway wdt.: unknown

superstructure: wrought iron, 10-panel, bolted Bowstring through arch-truss

substructure: timber pile bent abutments and wingwalls

floor/decking: timber deck over steel or iron stringers

other features: upper chord: 2 channels with cover plate and double lacing; lower chord: 2 rectangular eyebars, spliced; vertical: star iron; diagonal: round rod with threaded ends; lateral bracing: round rod with threaded ends (top and bottom); strut: pipe; floor beam: riveted plate girder, U-bolted to lower chords; outrider: star iron

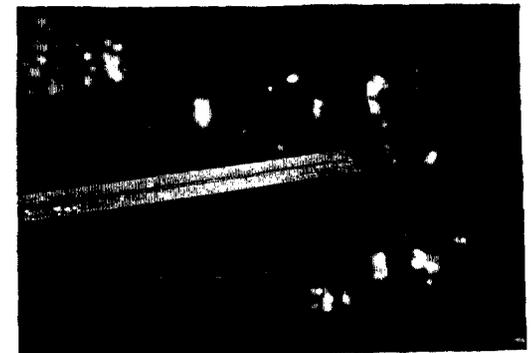
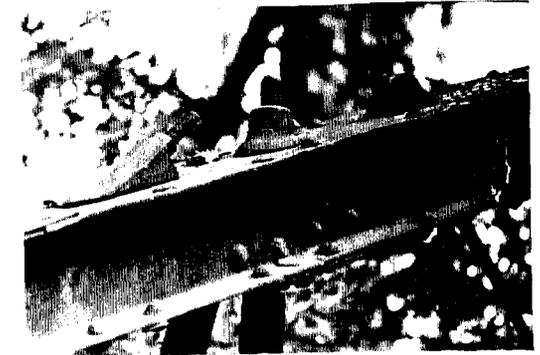
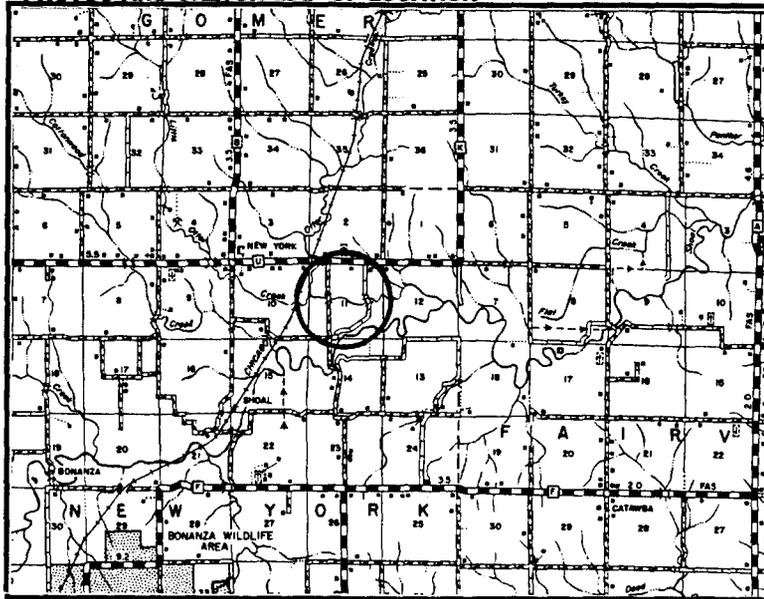
Like virtually all of Missouri's counties, Caldwell County followed a definite progression in its bridge construction in the 19th century, in response to evolving transportation needs and technological development in the bridge industry. The first simple spans, built as the county was undergoing its initial settlement, were rudimentary timber structures. These were cheap and easy to build but lacking in durability and limited in span length. With greater revenues from increased settlement, the county could undertake more ambitious timber/iron combination trusses in the 1860s and 1870s. These, in turn, were superseded in the mid-1870s by all-iron spans, made readily available by mass production. Although the county court barely noticed the transition from iron to steel in the 1890s, this evolution marked a watershed that would continue into the 20th century for bridge fabricators and the rolling mills that supplied them. Only one of the earliest iron spans remains in place in the county: this medium-span bowstring arch-truss that spans Otter Creek in New York Township. According to an 1886 history of Caldwell County, the Otter Creek Bridge was erected at this rural crossing east of Proctorville in 1875 for an approximate cost of \$4000.00. The fabricator is not reported, but physical attributes of the arch itself suggest that it was built by the Missouri Valley Bridge and Iron Works of Leavenworth, Kansas. The Otter Creek carried traffic for over a hundred years, with only maintenance-related repairs, until its more recent closure to traffic. Today it stands at the end of a vacated county road.

The bowstring arch-truss was the iron span of choice for Missouri counties in the late 1860s and 1870s. Marketed extensively throughout the Midwest by such industry giants as the King Iron Bridge and Manufacturing Company, the Wrought Iron Bridge Company and the Missouri Valley Bridge and Iron Works, these often-patented bridge forms featured a wide range of span lengths, economical fabrication costs and relatively quick erection. The proliferation of the bowstring corresponded with the initial development of Missouri's road system; as a result, perhaps thousands of these prototypical iron spans were erected throughout the state. The bowstring had some rather severe structural flaws, however, relating primarily to lateral stability of the arches, and it was largely superseded by the pin-connected

truss in the early 1880s. Despite this, some bowstrings were still erected in Missouri in the 1880s, although the number dwindled precipitously by the decade's end. Through subsequent attrition, almost all of Missouri's bowstrings have since been demolished and replaced. Now only a handful remains in place. Caldwell County's first all-iron span, the Otter Creek Bridge is thus technologically significant as a well-preserved, now-rare example in the state of what was once a mainstay structural type.

NAME(S) OF STRUCTURE
Otter Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 303000.4; History of Caldwell and Livingston Counties, Missouri (St. Louis: National Historical Company, 1886), page 254; field inspection by Lon Johnson, 8 September 1990.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Cottonwood Creek Bridge
MHTD: 316000.7

CALD30

DATE(S) OF CONSTRUCTION

1904

LOCATION

County Road 316 over Cottonwood Creek; S8/9, T56N, R27W
2.5 miles northeast of Bonanza; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 38)

CONDITION

fair

OWNER

Caldwell County

span number: 1

span length: 48.0'

total length: 48.0'

roadway wdt.: 13.8'

superstructure: steel, 3-panel, pin-connected Pratt bedstead

substructure: truss leg abutments with timber back- and wingwalls

floor/decking: timber deck over steel stringers

other features: upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plate (outer panels), 2 looped rectangular eyebars (inner panel); vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pins; guardrail: 2 angles

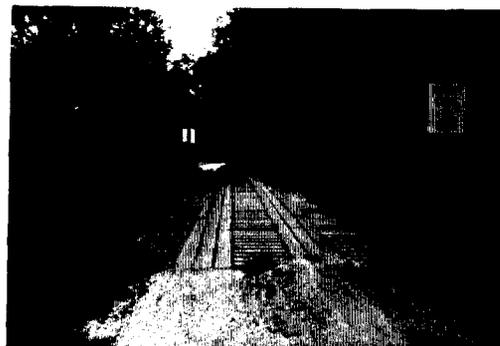
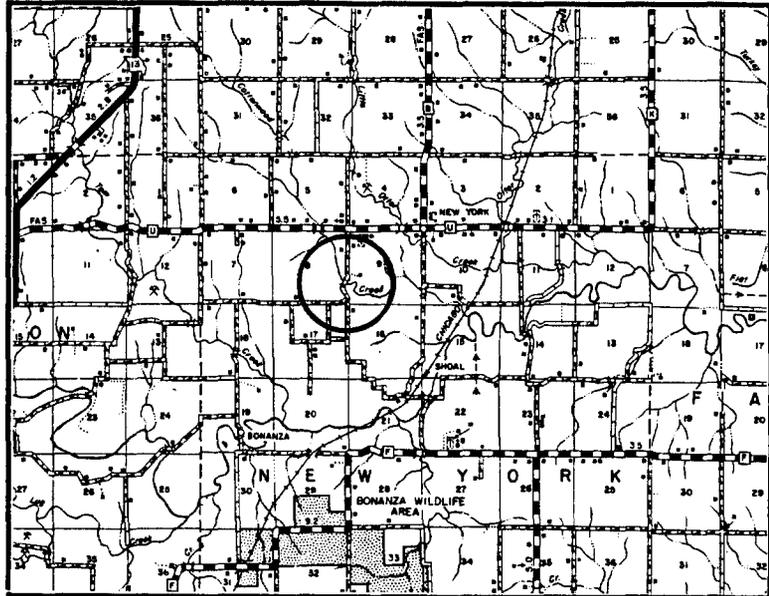
Carrying County Road 316 in central Caldwell County northeast of Bonanza, this bridge spans Cottonwood Creek. Configured as a single-span pin-connected Pratt bedstead, the truss is supported by a steel and timber substructure. Preceding this truss was a crossing which dates to 1902. In June of that year county officials entered into a contract with John H. Sparks to erect a bridge at this location. The bridge, costing \$125.00, however, was short-lived. Only two years later, in September of 1904, county officials were again contracting with Sparks for a 48-foot bedstead crossing Cottonwood Creek. The cost of the bridge had increased to \$579.00. Since its completion in 1904, the Cottonwood Creek has carried intermittent traffic with no major alterations of note.

In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the late 1890s and early 1900s. Hundreds remain in place today -in fact, Missouri has probably more bedsteads than any other state. The Cottonwood Creek Bridge is a well-preserved, though not particularly distinguished, example of this statewide bridge construction trend.

NAME(S) OF STRUCTURE

Cottonwood Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 316000.7; Caldwell County Court Record, Book O: n.p. (12 June 1902), page 592 (6 September 1904); Caldwell County Bridge Record, n.p. - both located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Tom Creek Bridge
MHTD: 324000.6

CALD31

DATE(S) OF CONSTRUCTION

1904

LOCATION

County Road 324 over Tom Creek; S14, T56N, R27W
1.0 mile north of Bonanza; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 44)

CONDITION

fair

OWNER

Caldwell County

span number: 1
span length: 60.0'
total length: 61.0'
roadway wdt.: 11.7'

superstructure: steel, 4-panel, pin-connected Pratt bedstead
substructure: concrete abutments and wingwalls
floor/decking: timber deck over steel stringers
other features: upper chord and upright end post: 2 channels with cover plate and lacing; lower chord: 2 angles with lacing (outer panels), 2 looped rectangular eyebars (inner panels); vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pins; guardrail: single channel

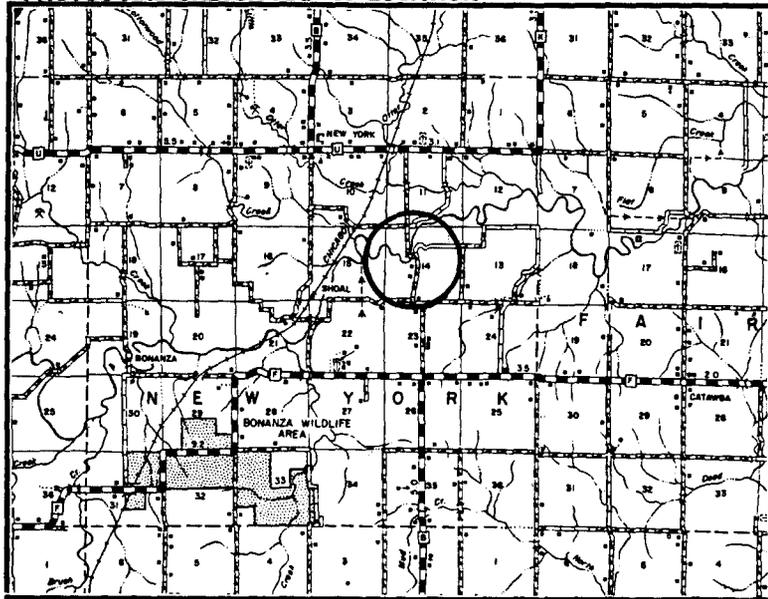
Located in New York Township, this medium-span steel truss carries a gravel-surfaced county road over Tom Creek. The structure is comprised of a pinned bedstead, supported by concrete abutments. The Tom Creek Bridge was one of 22 bridges erected in Caldwell County from a single contract let in November 1903. The bridges had been surveyed by the county road and bridge commissioner that spring and a contract let to the Canton Bridge Company for \$8990.00 to fabricate and build all 22 spans. For some reason this contract was never executed, however, and a second contract was signed with the Canton, Ohio, bridge firm in November for a thousand dollars less. Canton used steel components rolled in Pittsburgh by the Cambria steel works to fabricate this 60-foot bedstead, erecting it over Tom Creek a mile north of Bonanza in 1904. The Tom Creek Bridge has functioned in place since that time, with no major alterations and minimal maintenance.

In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the late 1890s and early 1900s. Hundreds remain in place today -in fact, Missouri has probably more bedsteads than any other state. The Tom Creek Bridge is a well-preserved, though not particularly distinguished, example of this statewide bridge construction trend.

NAME(S) OF STRUCTURE

Tom Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 324000.6; Caldwell County Court Record, Book O: page 181 (1 May 1903), page 343 (4 November 1903) - located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Bonanza Bridge
MHTD: 324001.9

CALD32

DATE(S) OF CONSTRUCTION

1883

LOCATION

County Road 324 over Shoal Creek; S19, T56N, R27W
Bonanza; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP eligible (score: 66)

CONDITION

fair

OWNER

Caldwell County

span number: 1
span length: 187.0'
total length: 187.0'
roadway wdt.: 14.0'

superstructure: wrought iron, 11-panel, pin-connected Whipple through truss
substructure: concrete-filled iron cylinder pier abutment with concrete backwalls (north); stone masonry abutment (south)
floor/decking: timber/concrete deck over iron or steel stringers
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with double lacing, 2 looped square rods at hip; diagonal: 2 punched rectangular eyebars; lateral bracing: round rod with threaded ends (top and bottom); floor beam: riveted plate girder, U-bolted to lower chord pins; guardrail: 2 channels

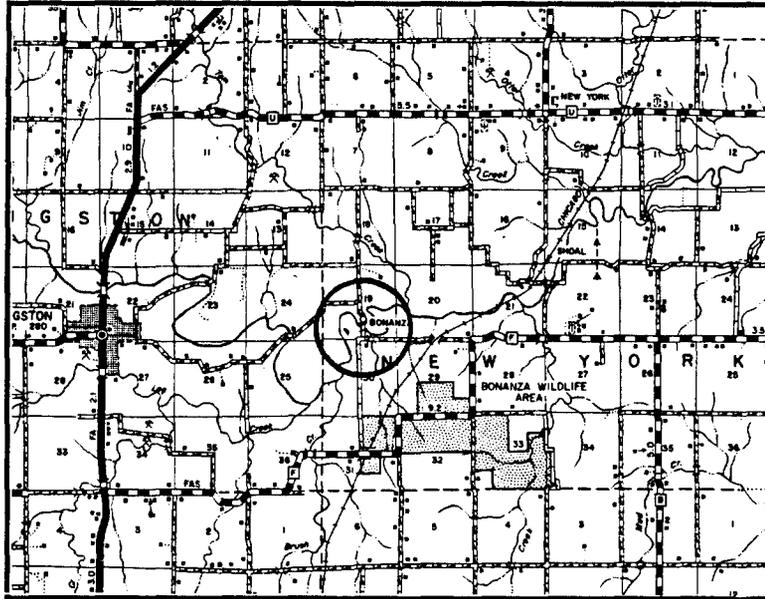
One of the more significant bridges included in Missouri's statewide historic bridge inventory is this long-span truss located just south of Bonanza, in central Caldwell County. Configured as a pin-connected Whipple (or double-intersection Pratt) through truss, the Bonanza Bridge is one of less than ten such structures known to exist in Missouri. First patented by Squire Whipple, bridge engineer and builder from New York, the Whipple truss was a popular choice for longer span crossings - generally in excess of 150 feet - between 1850 and 1890. Whipple trusses were used on most of the railroad bridges built across the Missouri River in the 1880s, due largely to engineer George S. Morison. But they were never very common for wagon trusses. The Whipple truss differed from the more common Pratt in that its diagonal members extended across, not one, but two panels. Although more costly, this variation provided greater lateral support for the diagonals, a critical consideration on deep, long-span trusses. By the turn of the century, Parker and Camelback trusses (Pratt variants with polygonal upper chords) had supplanted the Whipple as the truss of choice for longer span crossings. Accordingly, all of Missouri's extant Whipples date from before that time.

With its Whipple web configuration, rolled, wrought iron components, U-bolted, plate girder floor beams, and unslotted turnbuckles, the Bonanza Bridge typifies wagon truss construction of the early 1880s. According to an 1886 history of the county, the structure was erected here in 1883 for an approximate cost of \$6000.00. Unfortunately, county records that would have provided details of the span's construction history were evidently lost when the Caldwell County Courthouse burned shortly before the turn of the century. It is known

that the Caldwell County Court was still considering construction of the Bonanza Bridge late in September 1883. If the county history is correct, a contract to fabricate and build the long-span truss must have been awarded soon thereafter, but the contractor is not known at this time. Despite this gap in the bridge's documentation, it still remains an important transportation-related resource: an exceptional early wrought iron truss located at an important county road crossing.

NAME(S) OF STRUCTURE

Bonanza Bridge

PHOTOS AND SKETCH MAP OF LOCATION**LOCATION MAP**TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 324001.9; *The Hamiltonian*, 21 September 1883; *History of Caldwell and Livingston Counties, Missouri* (St. Louis: National Historical Company, 1886), page 254; Fraserdesign, "Bonanza Bridge: Preliminary Determination of NRHP Eligibility for the Missouri Historic Bridge Inventory," 25 September 1991; letter and enclosures from Richard Heisler, P.E., to Michael W. Weichman, dated 28 August 1991; field inspection by Lon Johnson, 8 September 1990.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Shoal Creek Bridge
MHTD: 341000.9

CALD35

DATE(S) OF CONSTRUCTION

c1890

LOCATION

County Road 341 over Shoal Creek; S23, T56N, R28W
1.6 miles northeast of Kingston; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / abandoned

RATING NRHP non-eligible (score: 34)

CONDITION

fair

OWNER

Caldwell County

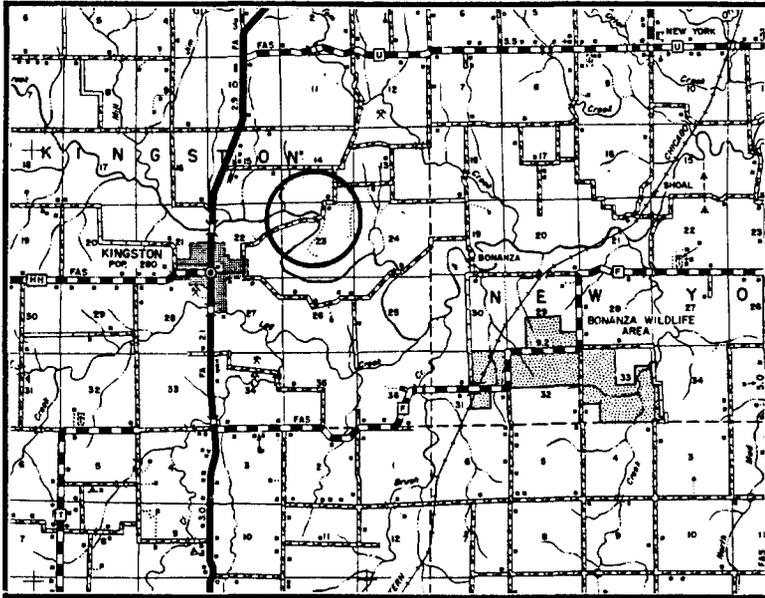
span number: 1
span length: 112.0'
total length: 168.0'
roadway wdt.: 13.6'

superstructure: steel or wrought iron, 7-panel, pin-connected Pratt through truss, with steel stringer approach spans
substructure: stone masonry abutments; concrete-filled iron cylinder piers; steel pile bent piers at approach spans
floor/decking: timber deck over iron or steel stringers
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with double lacing, looped round eyerod at hips; diagonal: 2 looped rectangular or square eyebars; lateral bracing: round rod with threaded ends; strut: one angle; portal strut: lattice, with knee braces; floor beam: variable depth "fishtail" plate girder, U-bolted to lower chord pins; guardrail: lattice

This short-span crossing of Shoal Creek is located some two miles northeast of Kingston, in rural Kingston Township. Comprised of a pin-connected Pratt through truss with steel stringer approach spans, the superstructure is supported by stone masonry abutments and iron tubular piers. Physical attributes of the truss suggest that it was fabricated around 1890, and the stone/iron substructure suggests that it remains in its original location. County records are inconclusive regarding the bridge's construction, however, largely due to a courthouse fire in the late 1890s. The Shoal Creek Bridge thus remains a well-preserved, but inadequately documented, example of what was once a mainstay structural type in Missouri.

NAME(S) OF STRUCTURE
Shoal Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 341000.9; field inspection by Lon Johnson, 8 September 1990.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Cox Ford Bridge
MHTD: 342002.8

CALD36

DATE(S) OF CONSTRUCTION

1888

LOCATION

County Road 342 over Shoal Creek; S24, T56N, R28W
3.5 miles east of Kingston; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 53)

CONDITION

fair

OWNER

Caldwell County

span number: 1
span length: 114.0'
total length: 194.0'
roadway wdt.: 14.5'

superstructure: wrought iron, 6-panel, pin-connected Pratt through truss, with steel stringer approach spans
substructure: concrete abutments and wingwalls; concrete-filled iron cylinder piers; steel pile bents piers at approach spans
floor/decking: timber deck over iron or steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with double lacing, 2 looped round eyerods at hip; diagonal: 2 punched rectangular eyebars; counter: looped round eyerod with turnbuckle; lateral bracing: round rod with threaded ends (top and bottom); strut: 4 angles with lacing; floor beam: variable depth, "fishtail" plate girder; guardrail: 2 channels; portal builder's plate: 1888 / King Iron Bridge Company / Cleveland, O

This short-span crossing of Shoal Creek is located some 3½ miles east of Kingston, in rural Kingston Township. Comprised of a pin-connected Pratt through truss with steel stringer approach spans, the superstructure is supported by stone masonry abutments and iron tubular piers. In April 1885 the Caldwell County Court received a citizens' petition for a bridge at the Cox Ford of Shoal Creek to carry the road between Kingston and Bonanza. That and subsequent petitions were dismissed by the court for three years. It was not until April 1888, in fact, that the judges granted the petitioners their wish and ordered county road and bridge commissioner W.J. Boyd to view the site and make estimates for the proposed bridge. Boyd advertised for competitive proposals for the Cox Ford and four other bridges in May, awarding a contract to build the Cox Ford span to the King Iron Bridge Company for \$4095.00. King fabricated the 114-foot truss in its huge Cleveland, Ohio, shops, and assembled the span over iron tubular piers later that year. Since its completion, the Cox Ford Bridge has carried vehicular traffic, with only maintenance-related repairs.

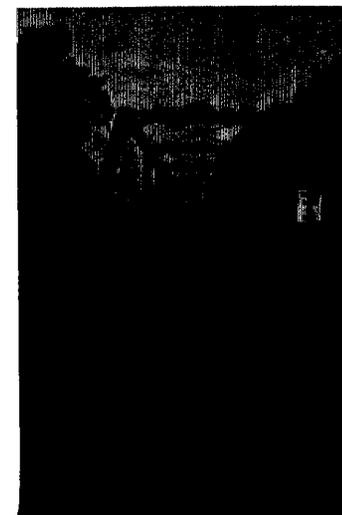
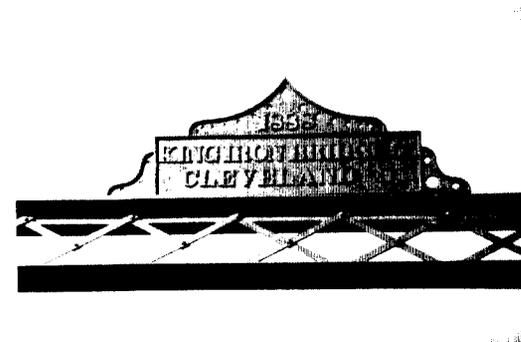
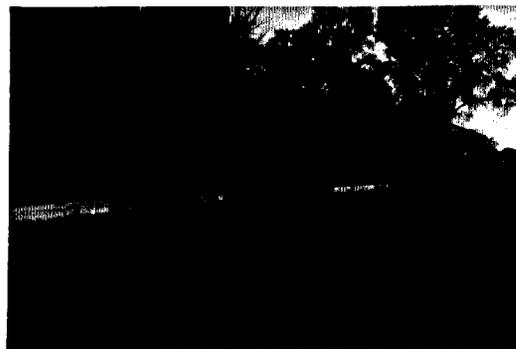
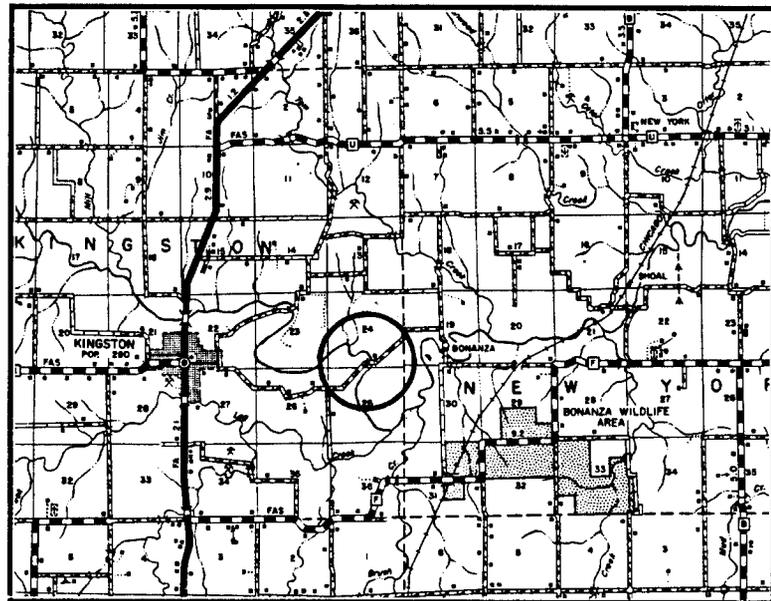
As one of America's most prolific bridge fabricators, the King Iron Bridge Company maintained an extensive catalogue of truss types, ranging from the exotic to the commonplace. King, like most of the region's bridge builders of the time, relied heavily on pin-connected Pratt truss variants for its standard truss types. Patented in 1844 by Thomas and Caleb Pratt, the Pratt design was characterized by upper chords and vertical members acting in compression and lower chords and diagonals that acted in tension. Its parallel chords and

equal panel lengths resulted in standardized sizes for the verticals, diagonals and chord members, making fabrication and assembly relatively easy. In the highly competitive bridge manufacturing industry, in which efficiency equated with profit, Pratt trusses received almost universal use. "The Pratt truss is the type most commonly used in America for spans under two hundred and fifty feet in length," noted bridge engineer J.A.L. Waddell wrote in 1916. "Its advantages are simplicity, economy of metal, and suitability for connecting to the floor and lateral systems." Virtually all of the major regional fabricators manufactured Pratt trusses and marketed them extensively to Missouri's counties in the late 19th and early 20th centuries. As a result, thousands were built during this period of extensive bridge construction. Hundreds remain in use today, of which the Cox Ford Bridge in Caldwell County is a relatively early, well-preserved example.

NAME(S) OF STRUCTURE

Cox Ford Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 342002.8; *The Hamiltonian*, 17 April 1885, 23 April 1886, 17 February 1888, 6 April 1888, 25 May 1888, 22 June 1888; *History of Caldwell and Livingston Counties, Missouri* (St. Louis: National Historical Company, 1886), page 254; field inspection by Lon Johnson, 8 September 1990.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Log Creek Bridge
MHTD: 353000.8

CALD40

DATE(S) OF CONSTRUCTION

1908

LOCATION

County Road 353 over Log Creek; S28/29, T56N, R28W
1.4 miles southwest of Kingston; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 53)

CONDITION

fair

OWNER

Caldwell County

span number: 1
span length: 80.0'
total length: 80.0'
roadway wdt.: 13.7'

superstructure: steel, 5-panel, rigid-connected Pratt pony truss
substructure: concrete abutments and wingwalls
floor/decking: timber deck
other features: steel angle guardrails

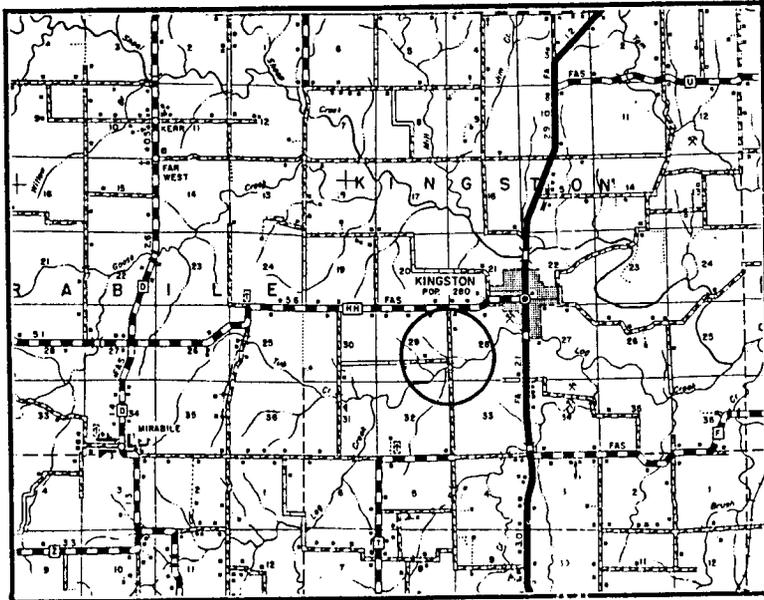
Located 1½ miles southwest of Kingston, this long-span pony truss carries a gravel-surfaced county road over Log Creek. The Log Creek Bridge is comprised of a single rigid-connected Pratt truss, supported by concrete abutments. The Log Creek Bridge dates to 1908. In May of that year the Caldwell County Engineer contracted for six steel bridges: four to the Standard Bridge Company of Omaha, and two—including this span—to the Dildine Bridge Company of Cameron. Apparently completed later in 1908, the Log Creek Bridge has functioned in place since, with only maintenance-related repairs.

Thousands of Pratt trusses were erected throughout Missouri in the late 19th and early 20th centuries. Marketed by virtually all of the in-state and regional bridge companies, this versatile structural type was used overwhelmingly by the counties for short- and medium-span applications. These all featured what was termed the American method of construction, with chord members joined by pins. After the turn of the century, however, riveted connections began to supersede pinned, and eventually the riveted Pratt pony displaced its pinned predecessor. The Log Creek Bridge in Caldwell County is distinguished as one of the earliest riveted Pratt pony truss—a harbinger of what would soon become a mainstay structural type in Missouri.

NAME(S) OF STRUCTURE

Log Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 353000.8; Caldwell County Court Record, Book O: page 530 (4 August 1904); Book Q: page 182 (5 May 1908) - located at the Caldwell County Courthouse, Kingston MO.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

12 May 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Shoal Creek Bridge
MHTD: 359000.8

CALD41

DATE(S) OF CONSTRUCTION

1894

LOCATION

County Road 359 over Shoal Creek; S17/18, T56N, R28W
2.9 miles northwest of Kingston; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 52)

CONDITION

fair

OWNER

Caldwell County

span number: 1
span length: 124.0'
total length: 204.0'
roadway wdt.: 12.3'

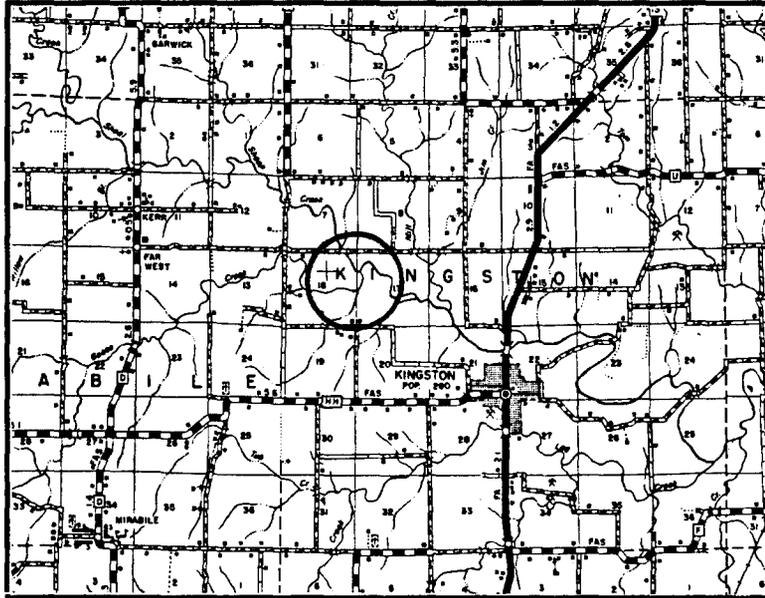
superstructure: steel, 7-panel, pin-connected Pratt through truss, with steel stringer approach spans
substructure: concrete abutments and wingwalls; steel pile bent piers
floor/decking: timber/concrete deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing, 2 looped square eyebars at hip; diagonal: 2 looped rectangular eyebars; counter: looped square eyebar with turnbuckle; lateral bracing: round rod with threaded ends (top and bottom); strut: 4 angles with double lacing; floor beam: I-beam, U-bolted to lower chord pins; guardrail: 2 channels; portal builder's plate: 1894 / John Dildine / and Co / Builders / Wmure / L.L. Frost / JM Loomis / CO / Judges

According to its builder's plate, the Shoal Creek Bridge was erected in 1894 by John Dildine and Company of Cameron, Missouri. Caldwell County Court records provide no additional construction documentation, however, due largely to a courthouse fire in the late 1890s. Configured as a single-span, pin-connected Pratt through truss, the structure is supported by a concrete and steel substructure. Located in rural west-central Caldwell County northwest of Kingston, the seven-panel bridge functions in place today, with no substantial alterations. The Shoal Creek Bridge is a well-preserved, although rather poorly documented, early example of a mainstay structural type - the pin-connected Pratt through truss.

Like most of the region's bridge builders of the time, the Dildine Bridge Company relied heavily on pin-connected Pratt truss variants for its standard truss types. Patented in 1844 by Thomas and Caleb Pratt, the Pratt design was characterized by upper chords and vertical members acting in compression and lower chords and diagonals that acted in tension. Its parallel chords and equal panel lengths resulted in standardized sizes for the verticals, diagonals and chord members, making fabrication and assembly relatively easy. In the highly competitive bridge manufacturing industry, in which efficiency equated with profit, Pratt trusses received almost universal use. "The Pratt truss is the type most commonly used in America for spans under two hundred and fifty feet in length," noted bridge engineer J.A.L. Waddell wrote in 1916. "Its advantages are simplicity, economy of metal, and suitability for connecting to the floor and lateral systems." Virtually all of the major regional fabricators manufactured Pratt trusses and marketed them extensively to Missouri's counties in the late 19th and early 20th centuries. As a result, thousands were built during this period of extensive bridge construction. Hundreds remain in use today, of which the Shoal Creek Bridge in Caldwell County is a relatively early, well-preserved example.

NAME(S) OF STRUCTURE
Shoal Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 359000.8; field inspection by Lon Johnson, 8 September 1990.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Shoal Creek Bridge
MHTD: 372000.5

CALD44

DATE(S) OF CONSTRUCTION

1901

LOCATION

County Road 372 over Shoal Creek; S12/7, T56N, R28W
3.9 miles of northwest of Kingston; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 46)

CONDITION

fair

OWNER

Caldwell County

span number: 1

span length: 120.0'

total length: 180.0'

roadway wdt.: 13.7'

superstructure: steel, 6-panel, pin-connected Pratt through truss, with steel stringer approach spans

substructure: concrete abutments and wingwalls with solid concrete piers

floor/decking: timber deck over steel stringers

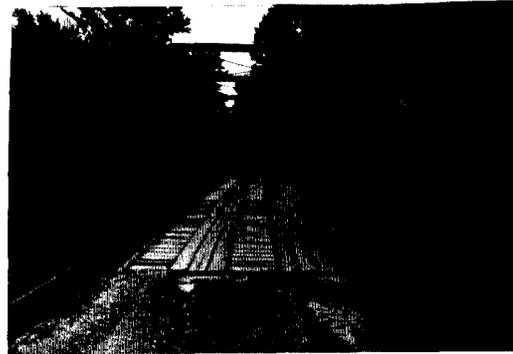
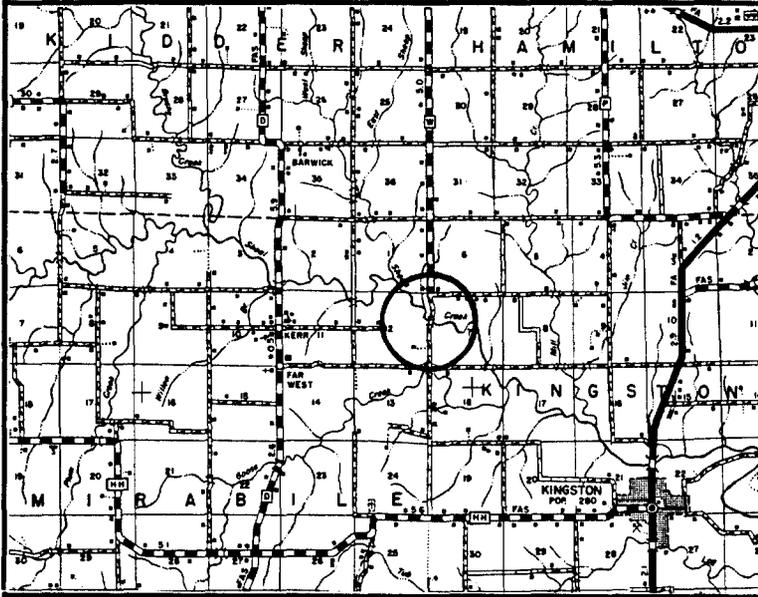
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with batten plates; diagonal: 2 punched rectangular eyebars; counter: 2 looped round eyerods with turnbuckles; lateral bracing: round rod with threaded ends at top and bottom; strut and floor beam: I-beam; guardrail: 2 angles

This medium-scale Pratt through truss spans Shoal Creek almost four miles northwest of Kingston, the county seat. The single-span 6-panel structure is approached by two steel stringers on the north and one steel stringer on the south. Carrying County Road 372, the bridge features a timber deck and concrete substructure. According to county court records, on April 3, 1901, Caldwell County officials advertised for competitive bids for a bridge at this location. By the end of the month, a contract had been let to the St. Joseph-based John H. Sparks for the aggregate sum of \$1710.00. Completed by the contractor later the same year using steel components rolled by the Illinois Steel Company, the Shoal Creek Bridge functions in place today. The bridge retains a high degree of structural integrity while being used by local travelers in west-central Caldwell County.

Marketed exclusively by virtually all of the in-state and regional bridge contractors and promoted in the form of standardized designs, the pinned Pratt through truss was widely used by Missouri's counties to carry roads over the state's myriad watercourses. Thousands of such trusses were erected across the state in the late 19th and early 20th centuries, and many remain in place today. A typically configured and well-preserved example of a mainstay bridge type in Missouri.

NAME(S) OF STRUCTURE
Shoal Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 372000.5; Caldwell County Court Record, Book N: page 403 (3 April 1901), page 415 (22 April 1901) - located at the Caldwell County Courthouse, Kingston MO; field inspection by Lon Johnson, 8 September 1990.

INVENTORIED BY

Clayton Fraser and Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

West Sheep Creek Bridge
MHTD: 028000.8

CALD49

DATE(S) OF CONSTRUCTION

1922

LOCATION

County Road 28 over West Sheep Creek; S26/35, T57N, R29W
0.5 mile east of Barwick; Caldwell County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 36)

CONDITION

fair

OWNER

Caldwell County

span number: 1
span length: 35.0'
total length: 36.0'
roadway wdt.: 15.8'

superstructure: steel deck girder
substructure: concrete abutments and wingwalls
floor/decking: concrete deck
other features: steel angle guardrails

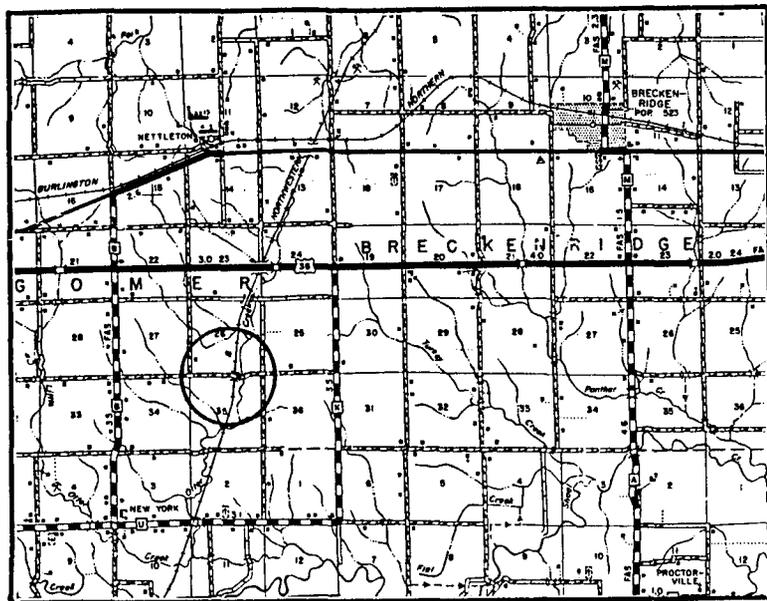
Spanning West Sheep Creek a half mile east of Barwick, this small-scale structure was designed by the Missouri State Highway Department in 1922 as part of construction on the East-West Highway through Caldwell County. As delineated by the agency's bridge department, the structure consisted of a single steel deck girder supported by concrete abutments. That year the highway department contracted with Bean and Company of St. Joseph, Missouri, to build this bridge, four others and a mile of adjacent highway. The contractors completed the structures and road for a total cost of \$7,057.86. The other spans have since been replaced, but the West Sheep Creek Bridge remains in place on this secondary county road.

In the 1921-22 biennium, the highway department prepared special designs for 293 structures, for an aggregate length of some 20,000 feet and a cost in excess of \$2.3 million. With a single 35-foot span, the West Sheep Creek Bridge numbered among the technologically less noteworthy of these. Despite this, the bridge is today important as the earliest dateable example of MSHD beam bridge design - an important resource from the state highway department's formative period.

NAME(S) OF STRUCTURE

West Sheep Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 442002.4; Missouri State Highway Board, Third Biennial Report: 1921-1922, page 137.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

7 July 1993

CLINTON COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
*CLIN01	H 653R	Plattsburg Bridge	1- 64' concrete open spandrel arch 1927 Mike Haas (replaced)
*CLIN02	J 245	Shoal Creek Bridge	1- 36' riveted kingpost pony truss c1905 Dildine Bridge Company (prob.)
*CLIN03	018001.2	McGuire Branch Bridge	1- 45' pinned Pratt pony truss c1910 Dildine Bridge Company (prob.)
CLIN04	029001.7	Castile Creek Bridge	1- 60' pinned Pratt pony truss c1910 Dildine Bridge Company (prob.) (replaced)
CLIN05	031000.9	Castile Creek Bridge	1- 25' concrete filled spandrel arch c1920
CLIN06	032000.2	Castile Creek Bridge	(replaced)
CLIN07	058000.7	Grindstone Creek Culvert	1- 50' pinned Pratt pony truss 1909 Dildine Bridge Company
*CLIN08	072002.8	Shoal Creek Bridge	1- 27' steel stringer 1918 county work force
CLIN09	089000.4	Deer Creek Bridge	1- 50' pinned Pratt pony truss c1910 Dildine Bridge Company (prob.) (replaced)
CLIN10	104000.3	Little Platte River Bridge	1- 36' riveted Pratt bedstead c1910 Dildine Bridge Company (prob.) (replaced)
CLIN11	115001.6	Little Platte River Bridge	1- 42' pinned Pratt pony truss c1910 Dildine Bridge Company (prob.)
CLIN12	118001.5	Little Platte River Bridge	1-100' riveted Camelback pony truss c1915
CLIN13	123001.3	Shoal Creek Bridge	1- 90' riveted Baltimore bedstead 1906 John Dildine Bridge Company
CLIN14	124001.7	Shoal Creek Bridge	1-108' rivet 2-angle Warren pony truss c1920
CLIN15	126000.4	Little Shoal Creek Bridge	1- 40' pinned Pratt half-hip pony truss 1898 Dildine Bridge Company
CLIN16	129000.5	Little Shoal Creek Bridge	1-100' pinned Pratt through truss 1902 Dildine Bridge Company
CLIN17	152000.6	Little Platte River Bridge	
*CLIN18	255000.5	Little Platte River Bridge	
*CLIN19	256000.9	Little Platte River Bridge	
CLIN20	291001.5	Roberts Branch Bridge	
*CLIN21	325001.4	Castile Creek Bridge	

EXCLUDED:

Warren pony truss
 H 655R1 T 45

CLINTON COUNTY

EXCLUDED (cont.):

Steel stringer

T 128	X 444	X 565	001000.2	003000.5	029000.1	030000.1
075000.3	079000.9	094000.4	102000.6	135000.7	143000.1	145001.5
177001.9	188000.0	232000.1	267000.9	270000.2	291000.9	292003.7
306000.8	311001.8	352001.2	354000.1	355001.1	356R00.4	357000.3
363001.3						

Steel multi-plate culvert

032001.1

Concrete girder

J 244	J 257	J 434	X 608
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Concrete slab

J 246	333001.3
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Concrete box culvert

G 906	J 264	J 265	K 261	K 277	L 7	T 46
T 127						

SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	1	15	0	0	16
Excluded	18	28	0	0	46
	<hr/>				
	19	43	0	0	62 structures

Plattsburg Bridge

CLIN01

GENERAL DATA

structure no.:	H 653R	city/town:	Plattsburg
county:	Clinton	feature inters.:	Platte River Fork
		cadastral grid:	S24, T55N, R32W
		highway route:	State Highway 116
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	concrete open spandrel arch, with concrete slab approach spans		
substructure:	concrete abutments, wingwalls and piers		
span number:	1	condition:	good
span length:	64.0'	alterations:	concrete curbs and guardrails replaced
total length:	137.0'	floor/decking :	concrete deck
roadway width:	22.0'	other features:	Armco guardrails

HISTORICAL DATA

erection date:	1927
erection cost:	\$12,462.64
designer:	Missouri State Highway Department
fabricator :	none
contractor:	Mike Haas

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number H 653R; Missouri Primary System Bridge Record, located at Missouri Highway and Transportation Department, Jefferson City MO; Clinton County Court Record 13: page 94 (8 May 1894) - located at Clinton County Courthouse, Plattsburg MO; field inspection by Lon Johnson, 19 September 1990.

sign. rating:	34
evaluation:	NRHP non-eligible (single-span representative of Missouri State Highway Department concrete design of the 1920s, altered)

inventoried by: Lon Johnson 9 September 1992

McGuire Branch Bridge

CLIN03

GENERAL DATA

structure no.:	018001.2	city/town:	7.2 miles northeast of Gower
county:	Clinton	feature inters.:	McGuire Branch
		cadastral grid:	S6/31, T56/57N, R32W
		highway route:	County Road 18
		highway distr.:	1
		current owner:	Clinton County

STRUCTURAL DATA

superstructure:	steel, rigid-connected kingpost pony truss		
substructure:	timber pile bent abutments and wingwalls		
span number:	1	condition:	fair
span length:	36.0'	alterations:	vertical and end post stiffeners on one web replaced
total length:	37.0'	floor/decking:	timber deck over steel stringers
roadway width:	15.2'	other features:	inclined end post: I-beam; lower chord: 2 channels with batten plates; vertical: I-beam (north web), 4 angles with lacing (south web); end post stiffener: I-beam (north web), 2 angles with batten plates (south web); lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels

HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator:	Dildine Bridge Company, Cameron MO (probable); Cambria Steel Company, Pittsburgh PA
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 018001.2; field inspection by Lon Johnson, 15 September 1990.
sign. rating:	44
evaluation:	NRHP non-eligible (undocumented example of now-rare structural type, significantly altered)

inventoried by: Lon Johnson 9 September 1992

Castile Creek Bridge

CLIN04

GENERAL DATA

structure no.:	029001.7	city/town:	9.2 miles northwest of Plattsburg
county:	Clinton	feature inters.:	branch of Castile Creek
		cadastral grid:	S2/3, T56N, R32W
		highway route:	County Road 29
		highway distr.:	1
		current owner:	Clinton County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	45.0'	alterations:	unknown
total length:	45.0'	floor/decking :	concrete deck over steel stringers
roadway width:	13.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	Dildine Bridge Company, Cameron MO (probable)
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 029001.7; Clinton County Court Record, Book 13: page 589 (2 June 1896) - located at Clinton County Courthouse, Plattsburg MO.
sign. rating:	30
evaluation:	NRHP non-eligible (typically configured example of common structural type, with incomplete documentation)

inventoried by: Lon Johnson 9 September 1992

Castile Creek Bridge

CLIN05

GENERAL DATA

structure no.:	031000.9	city/town:	9.7 miles northwest of Plattsburg
county:	Clinton	feature inters.:	Castile Creek
		cadastral grid:	S4, T56N, R32W
		highway route:	County Road 31
		highway distr.:	1
		current owner:	Clinton County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers, with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	61.0'	floor/decking :	timber deck
roadway width:	13.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	Dildine Bridge Company, Cameron MO (probable)
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 031000.9.
sign. rating:	38
evaluation:	NRHP non-eligible (partially documented, undistinguished example of common structural type)

Inventoried by: Lon Johnson 9 September 1992

Grindstone Creek Bridge

CLIN07

GENERAL DATA

structure no.:	058000.7	city/town:	4.7 miles southwest of Cameron
county:	Clinton	feature inters.:	Grindstone Creek
		cadastral grid:	S24/25, T57N, R31W
		highway route:	County Road 58
		highway distr.:	1
		current owner:	Clinton County

STRUCTURAL DATA

superstructure:	concrete filled spandrel arch		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	25.0'	alterations:	unknown
total length:	57.0'	floor/decking :	concrete over earth fill
roadway width:	18.0'	other features:	concrete parapets with incised rectangular panels

HISTORICAL DATA

erection date:	c1920
erection cost:	unknown
designer:	unknown
fabricator :	none
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 058000.7.
sign. rating:	18
evaluation:	NRHP non-eligible (technologically undistinguished example of small-scale concrete arch construction)

Inventoried by: Lon Johnson 9 September 1992

Deer Creek Bridge

CLIN09

GENERAL DATA

structure no.:	089000.4	city/town:	6.7 miles south of Cameron
county:	Clinton	feature inters.:	Deer Creek
		cadastral grid:	S23/26, T56N, R30W
		highway route:	County Road 89
		highway distr.:	1
		current owner:	Clinton County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	51.0'	floor/decking :	timber deck
roadway width:	11.9'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1909
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO
fabricator :	Dildine Bridge Company, Cameron MO
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 089000.4; Clinton County Court Record, Book 19: page 77 (7 April 1909) - located at Clinton County Courthouse, Plattsburg MO.
sign. rating:	41
evaluation:	NRHP non-eligible (well-documented, undistinguished example of a mainstay structural type)

inventoried by: Lon Johnson 9 September 1992

Little Platte River Bridge

CLIN10

GENERAL DATA

structure no.:	104000.3	city/town:	9.3 miles southwest of Cameron
county:	Clinton	feature inters.:	Little Platte River
		cadastral grid:	S9, T56N, R31W
		highway route:	County Road 104
		highway distr.:	1
		current owner:	Clinton County

STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	27.0'	other features:	unknown
total length:	28.0'		
roadway width:	17.0'		

HISTORICAL DATA

erection date: 1918
erection cost: unknown
designer: unknown
fabricator : St. Joseph Structural Steel Company, St. Joseph MO
contractor: county work force

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 104000.3; Clinton County Court Record, Book 22: page 130 (5 August 1918) - located at Clinton County Courthouse, Plattsburg MO.

sign. rating: 30
evaluation: NRHP non-eligible (undistinguished example of an exceedingly common structural type, used at short-span crossings)

inventoried by: Lon Johnson 9 September 1992

Little Platte River Bridge

CLIN11

GENERAL DATA

structure no.:	115001.6	city/town:	5.6 miles northeast of Plattsburg
county:	Clinton	feature inters.:	Little Platte River
		cadastral grid:	S3/34, T55/56N, R31W
		highway route:	County Road 115
		highway distr.:	1
		current owner:	Clinton County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	timber pile bent abutments and wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	52.0'	floor/decking:	timber deck
roadway width:	11.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator:	Dildine Bridge Company, Cameron MO (probable)
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 115001.6.
sign. rating:	30
evaluation:	NRHP non-eligible (typically configured and modestly scaled example of common structural type)

inventoried by: Lon Johnson 9 September 1992

Shoal Creek Bridge

CLIN13

GENERAL DATA

structure no.:	123001.3	city/town:	8.7 miles northeast of Plattsburg
county:	Clinton	feature inters.:	Shoal Creek
		cadastral grid:	S5, T55N, R30W
		highway route:	County Road 123
		highway distr.:	1
		current owner:	Clinton County

STRUCTURAL DATA

superstructure:	steel, 3-panel, rigid-connected Pratt truss-leg bedstead, with steel stringer approach spans		
substructure:	steel truss legs with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	36.0'	alterations:	unknown
total length:	60.0'	floor/decking :	timber deck
roadway width:	13.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	Dildine Bridge Company, Cameron MO (probable)
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 123001.3; Clinton County Court Record, Book 15: page 297 (5 February 1900) - located at Clinton County Courthouse, Plattsburg MO.
sign. rating:	35
evaluation:	NRHP non-eligible (typical, short-span example of a relatively common Pratt truss sub-type)

inventoried by: Lon Johnson 9 September 1992

Little Shoal Creek Bridge

CLIN16

GENERAL DATA

structure no.:	129000.5	city/town:	5.3 miles northeast of Lathrop
county:	Clinton	feature inters.:	Little Shoal Creek
		cadastral grid:	S33, T56N, R30W
		highway route:	County Road 129
		highway distr.:	1
		current owner:	Clinton County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	timber pile bent abutments and wingwalls		
span number:	1	condition:	fair
span length:	42.0'	alterations:	unknown
total length:	42.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	Dildine Bridge Company, Cameron MO (probable)
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 129000.5.
sign. rating:	30
evaluation:	NRHP non-eligible (typically configured, short-span example of common structural type)

inventoried by: Lon Johnson 9 September 1992

Little Platte River Bridge

CLIN17

GENERAL DATA

structure no.: 152000.6	city/town: 2.7 miles northeast of Plattsburg
county: Clinton	feature inters.: Little Platte River
	cadastral grid: S17, T55N, R31W
	highway route: County Road 152
	highway distr.: 1
	current owner: Clinton County

STRUCTURAL DATA

superstructure: steel, 6-panel, rigid-connected Camelback pony truss, with steel stringer approach spans	
substructure: concrete-filled steel cylinder piers	
span number: 1	condition: fair
span length: 100.0'	alterations: unknown
total length: 139.0'	floor/decking : concrete deck over steel stringers
roadway width: 14.0'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1920
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 152000.6; Clinton County Court Record, Book 17: page 38 (3 March 1903) - located at Clinton County Courthouse, Plattsburg MO.

sign. rating: 38
evaluation: NRHP non-eligible (a long-span example of an uncommon structural type, but lacking in documentation)

inventoried by: Lon Johnson 9 September 1992

Little Platte River Bridge

CLIN18

GENERAL DATA

structure no.:	255000.5	city/town:	Plattsburg
county:	Clinton	feature inters.:	Little Platte River
		cadastral grid:	S25, T55N, R32W
		highway route:	County Road 255
		highway distr.:	1
		current owner:	Clinton County

STRUCTURAL DATA

superstructure: steel, 5-panel, rigid-connected Baltimore bedstead truss, with steel stringer approach span at each end

substructure: concrete abutments and wingwalls; truss-leg piers

span number:	1	condition:	fair
span length:	90.0'	alterations:	bridge closed; deck removed
total length:	139.0'	floor/decking :	timber deck (removed) over steel stringers
roadway width:	11.7'	other features:	upper chord, upright end post and lower chord: 2 channels; vertical: 1 channel; diagonal: 2 angles; lateral bracing: 1 angle; strut: 1 channel; floor beam: I-beam, field-bolted to vertical

HISTORICAL DATA

erection date: 1906

erection cost: \$2425.00

designer: unknown

fabricator : Dildine Bridge Company, Cameron MO;
Cambria Steel Company, Pittsburgh PA

contractor: Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 255000.5; Clinton County Court Record, Book 17: page 160 (5 June 1906), page 396 (1 July 1907), page 433 (12 October 1907) - located at Clinton County Courthouse, Plattsburg MO; field inspection by Lon Johnson, 15 September 1990.

sign. rating: 76

evaluation: NRHP eligible (unique example of esoteric truss type)

inventoried by: Lon Johnson 9 September 1992

Little Platte River Bridge

CLIN19

GENERAL DATA

structure no.:	256000.9	city/town:	1.5 miles southwest of Plattsburg
county:	Clinton	feature inters.:	Little Platte River
		cadastral grid:	S26, T55N, R32W
		highway route:	County Road 256
		highway distr.:	1
		current owner:	Clinton County

STRUCTURAL DATA

superstructure:	steel, 12-panel, rigid-connected Warren pony truss, with steel stringer approach spans		
substructure:	concrete abutments; concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	108.0'	alterations:	none
total length:	136.0'	floor/decking :	concrete deck over steel stringers
roadway width:	12.6'	other features:	upper chord, inclined end post, lower chord, vertical, diagonal and strut: 2 angles; floor beam: I-beam, field-bolted to vertical; guard-rail: 2 channels

HISTORICAL DATA

erection date:	c1920
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 256000.9; field inspection by Lon Johnson, 15 September 1990.
sign. rating:	44
evaluation:	NRHP non-eligible (a long-span example of an uncommon structural type, lacking in documentation)

inventoried by: Lon Johnson 9 September 1992

Roberts Branch Bridge

CLIN20

GENERAL DATA

structure no.:	291001.5	city/town:	3.5 miles southwest of Plattsburg
county:	Clinton	feature inters.:	Roberts Branch
		cadastral grid:	S33, T55N, R32W
		highway route:	County Road 291
		highway distr.:	1
		current owner:	Clinton County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss, with steel stringer approach span		
substructure:	concrete abutments, wingwalls and piers		
span number:	1	condition:	fair
span length:	40.0'	alterations:	none
total length:	61.0'	floor/decking :	timber deck
roadway width:	11.7'	other features:	timber guardrails

HISTORICAL DATA

erection date:	1898
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO
fabricator :	Dildine Bridge Company, Cameron MO
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 291001.5; Clinton County Court Record, Book 14: page 510 (2 June 1898) - located at Clinton County Courthouse, Plattsburg MO.
sign. rating:	43
evaluation:	NRHP non-eligible (typically configured, early example of a mainstay structural type)

inventoried by: Lon Johnson 9 September 1992

Castile Creek Bridge

CLIN21

GENERAL DATA

structure no.:	325001.4	city/town:	6.2 miles northwest of Plattsburg
county:	Clinton	feature inters.:	Castile Creek
		cadastral grid:	S12, T55N, R33W
		highway route:	County Road 325
		highway distr.:	1
		current owner:	Clinton County

STRUCTURAL DATA

superstructure: steel, 6-panel, pin-connected Pratt through truss, with timber and steel stringer approach spans

substructure: concrete abutments and wingwalls; steel pile bent piers

span number:	1	condition:	fair
span length:	100.0'	alterations:	approach spans replaced
total length:	166.0'	floor/decking :	asphalt on timber deck, over steel stringers
roadway width:	11.5'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 square eyebars at hip); diagonal: 2 punched rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; portal strut: steel lattice; floor beam: I-beam, U-bolted to lower chord pins; guardrail: steel pipe; portal builder's plate: 1902 / BUILT BY DILDINE BRIDGE AND / CONSTRUCTION CO.

HISTORICAL DATA

erection date: 1902

erection cost: \$1750.00

designer: Dildine Bridge and Construction Company, Cameron MO

fabricator : Dildine Bridge and Construction Company, Cameron MO;
Carnegie Steel Company, Pittsburgh PA

contractor: Dildine Bridge and Construction Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 325001.4; Clinton County Court Record, Book E: page 128 (3 March 1902); Book 17: page 121 (6 July 1903) - located at Clinton County Courthouse, Plattsburg MO; field inspection by Lon Johnson, 15 September 1990.

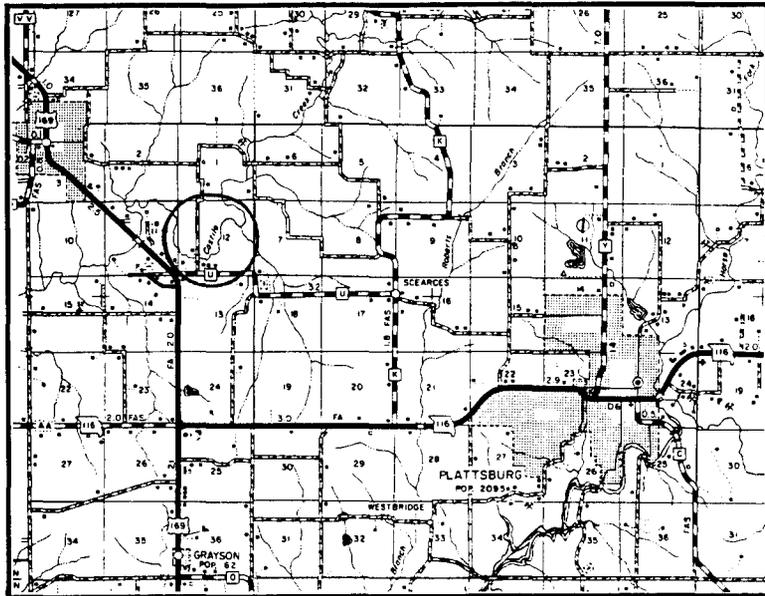
sign. rating: 45

evaluation: NRHP non-eligible (typical example of mainstay structural type)

inventoried by: Lon Johnson 9 September 1992

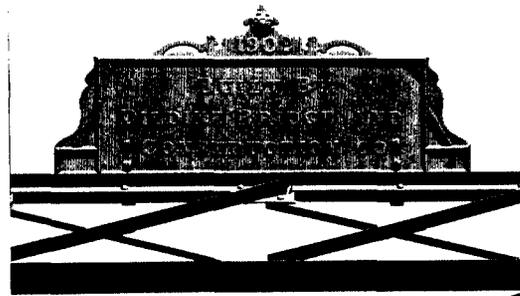
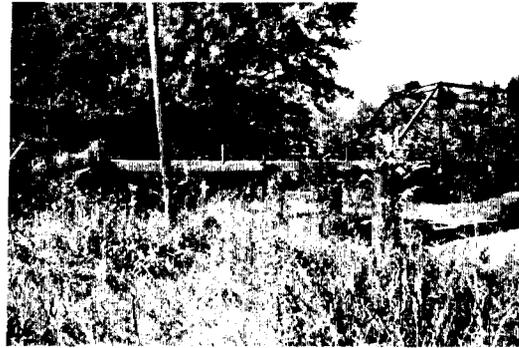
NAME(S) OF STRUCTURE
Castile Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 325001.4; Clinton County Court Record, Book E: page 128 (3 March 1902); Book 17: page 121 (6 July 1903) - located at Clinton County Courthouse, Plattsburg MO; field inspection by Lon Johnson, 15 September 1990.

INVENTORIED BY
Clayton Fraser and Lon Johnson

AFFILIATION
Fraserdesign, Loveland CO

DATE
9 September 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE
 Castile Creek Bridge
 MHTD: 325001.4

CLIN21

DATE(S) OF CONSTRUCTION
 1902

LOCATION

County Road 325 over Castile Creek; S12, T55N, R33W
 6.2 miles northwest of Plattsburg; Clinton County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 45)

CONDITION

fair

OWNER

Clinton County

span number: 1
 span length: 100.0'
 total length: 166.0'
 roadway wdt.: 11.5'

superstructure: steel, 6-panel, pin-connected Pratt through truss, with timber and steel stringer approach spans
 substructure: concrete abutments and wingwalls; steel pile bent piers
 floor/decking: asphalt on timber deck, over steel stringers
 other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 square eyebars at hip); diagonal: 2 punched rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; portal strut: steel lattice; floor beam: I-beam, U-bolted to lower chord pins; guardrail: steel pipe; portal builder's plate: 1902 / BUILT BY DILDINE BRIDGE AND / CONSTRUCTION CO.

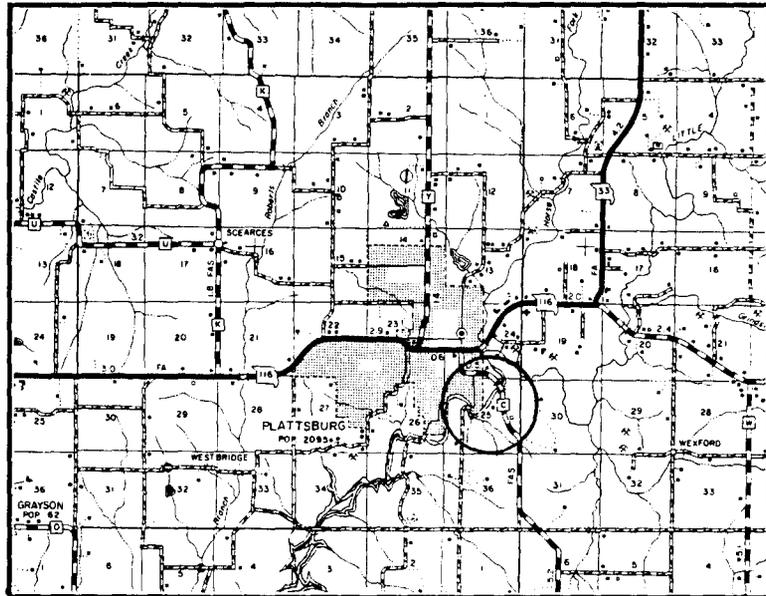
On February 3, 1902, the Clinton County Court directed the county's road and bridge commissioner to advertise for bids for a bridge to be built in the southwest $\frac{1}{4}$ of Township 55 North, Range 33 West. Acting quickly, the commissioner immediately solicited bids, presenting them to the court a month later. On March 3rd the court awarded a \$1750.00 contract to the Dildine Bridge and Construction Company for the structure's erection. The Cameron, Missouri, firm completed the structure early that summer, and on July 6th was paid \$1750.00 for work on the bridge. A pin-connected Pratt through truss, the Castile Creek Bridge continues to carry local traffic in rural Atchison Township.

The Castile Creek Bridge is a structurally intact, well-documented example of pinned Pratt truss construction. Significant for its association with the prolific Dildine Bridge Company, the structure is technologically representative - a Missouri's mainstay structural type for medium-span crossings.

NAME(S) OF STRUCTURE

Little Platte River Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 255000.5; Clinton County Court Record, Book 17: page 160 (5 June 1906), page 396 (1 July 1907), page 433 (12 October 1907) - located at Clinton County Courthouse, Plattsburg MO; field inspection by Lon Johnson, 15 September 1990.

INVENTORIED BY

Clayton Fraser and Lon Johnson

AFFILIATION

Fraserdesign, Loveland CO

DATE

9 September 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Little Platte River Bridge
MHTD: 255000.5

CLIN18

DATE(S) OF CONSTRUCTION

1906

LOCATION

County Road 255 over Little Platte River; S25, T55N, R32W
Plattsburg; Clinton County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / abandoned

RATING NRHP eligible (score: 76)

CONDITION

fair

OWNER

Clinton County

span number: 1
span length: 90.0'
total length: 139.0'
roadway wdt.: 11.7'

superstructure: steel, 5-panel, rigid-connected Baltimore bedstead truss, with steel stringer approach span at each end
substructure: concrete abutments and wingwalls; truss-leg piers
floor/decking: timber deck (removed) over steel stringers
other features: upper chord, upright end post and lower chord: 2 channels; vertical: 1 channel; diagonal: 2 angles; lateral bracing: 1 angle; strut: 1 channel; floor beam: I-beam, field-bolted to vertical

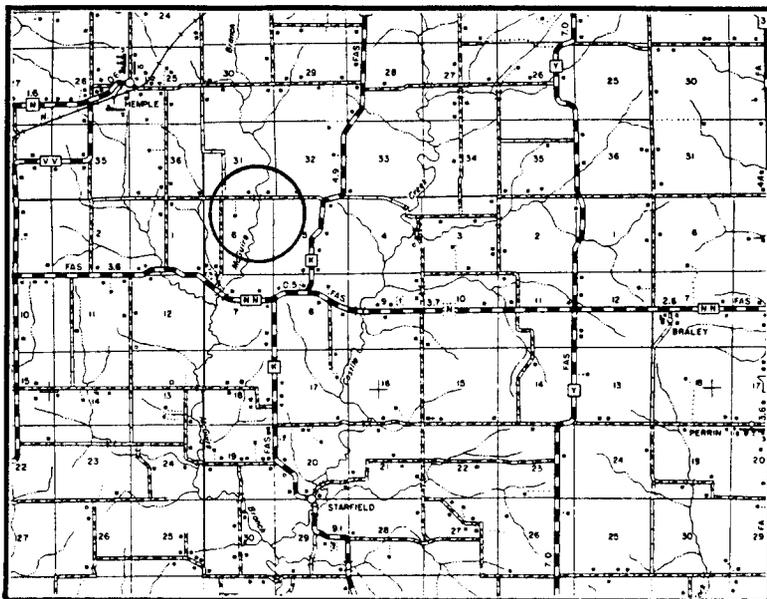
On June 5, 1906, the Clinton County Court received bids to build three bridges, including a 90-foot span across the Little Platte River just southeast of Plattsburg. Ranging from \$2025.00 to \$2305.00, bids were received from the Dildine Bridge Company, Illinois Steel Bridge Company, Midland Bridge Company, Kansas City Bridge Company and Canton Bridge Company. Dildine's bid was low, and for an additional \$400.00 the firm offered to build the bridge on steel tubes - a proposal that the county evidently rejected. Based in nearby Cameron, Missouri, Dildine began work on this bridge and another smaller crossing later that year, but progress was initially slow. It was not until October 1907 that both bridges had been accepted and warrants issued for their construction. For unknown reasons, the Little Platte River Bridge Dildine opted not to build a traditional Pratt truss, but instead erected this long-span truss-leg bedstead with an uncommon Baltimore truss configuration. Now abandoned, this esoteric truss is the only one of its kind identified by Missouri's statewide historic bridge inventory. Although its timber deck has been removed, the bridge is otherwise structurally intact.

Pin-connected Pratt trusses were built by the thousands throughout Missouri in the years surrounding the turn of the century. Through trusses were usually built at crossings more than 80-feet in length, but pony trusses of up to 100 feet were occasionally built. Other truss types - including Parker, Camelback and Pennsylvania trusses - were built less commonly, usually at long-span crossings. But Baltimore configured trusses were rarely built in Missouri; only six existing bridges use this peculiar configuration. Among these, the Little Platte River Bridge is unique as the only non-through truss, and the only such structure with bedstead legs. As a well-documented and evidently unique truss type, the Little Platte River Bridge is a significant structural anomaly.

NAME(S) OF STRUCTURE

McGuire Branch Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 018001.2; field inspection by Lon Johnson, 15 September 1990.

INVENTORIED BY

Clayton Fraser and Lon Johnson

AFFILIATION

Fraserdesign, Loveland CO

DATE

9 September 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

McGuire Branch Bridge
MHTD: 018001.2

CLIN03

DATE(S) OF CONSTRUCTION

c1905

LOCATION

County Road 18 over McGuire Branch; S6/31, T56/57N, R32W
7.2 miles northeast of Gower; Clinton County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 44)

CONDITION

fair

OWNER

Clinton County

span number: 1
span length: 36.0'
total length: 37.0'
roadway wdt.: 15.2'

superstructure: steel, rigid-connected kingpost pony truss
substructure: timber pile bent abutments and wingwalls
floor/decking: timber deck over steel stringers
other features: inclined end post: I-beam; lower chord: 2 channels with batten plates; vertical: I-beam (north web), 4 angles with lacing (south web); end post stiffener: I-beam (north web), 2 angles with batten plates (south web); lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels

This short span crossing of McGuire Branch is located some seven miles northeast of Gower, in Lafayette Township. A rigid-connected kingpost pony truss, the bridge is supported by a timber substructure. Minutes of the Clinton County Court do not refer specifically to the small structure, but it appears to have been built circa 1905. The Dildine Bridge Company of nearby Cameron, Missouri, built similar kingposts elsewhere and was the dominant bridge builder in Clinton County in the years following the turn of the century. A firm that was active throughout northern Missouri, Dildine Bridge was probably responsible for the erection of the McGuire Branch Bridge. Since its completion, the crossing has been utilized primarily by local citizens in rural northwestern Clinton County. The vertical and end post stiffeners on the north web have been replaced with I-beams, perhaps as the result of collision damage.

The simplest of truss types, the kingpost pony truss was utilized throughout Missouri during the state's settlement and early development periods in the 19th century. Earlier examples were simple wood trusses, while later kingposts were built of iron or steel. By the turn of the century, though, Pratt truss configurations - including truss-leg bedsteads and half-hip pony trusses - had become the designs of choice for short-span crossings. Consequently, few kingposts were built after 1900, and only a dozen are now known to remain in use on Missouri's roadways. One of only four Missouri kingposts with riveted connections, the McGuire Branch Bridge is technologically representative of kingpost pony truss construction - a once popular but now rare truss design.

DAVISS COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
DAVI01	H 752	Big Creek Ditch Bridge	1-140' riveted Pratt through truss 1928 Pioneer Construction Company
*DAVI02	J 146R	Grand River Bridge	3-140' riveted Pratt through truss 1930 Pioneer Construction Company
*DAVI03	017002.0	Cypress Creek Bridge	1- 40' riveted kingpost pony truss c1910 John Dildine Bridge Co. (prob.)
DAVI04	025002.8	Hickory Creek Bridge	1- 25' pinned Pratt bedstead c1910 John Dildine Bridge Co. (prob.)
DAVI05	044002.8	Little Creek Bridge	1- 40' steel stringer 1919
DAVI06	044003.6	Little Creek Bridge	1- 40' pinned Pratt pony truss 1909 John Dildine Bridge Company
DAVI07	052000.2	Tombstone Creek Bridge	1- 40' pinned Pratt bedstead c1915 John Dildine Bridge Co. (prob.)
DAVI08	065001.1	Hickory Creek Bridge	1- 32 riveted Pratt half-hip pony truss c1915 John Dildine Bridge Co. (prob.)
DAVI09	110R01.1	Muddy Creek Bridge	(replaced)
DAVI10	114000.5	Clear Creek Bridge	1- 40' pinned Pratt bedstead 1900 John Dildine Bridge Company
DAVI11	121001.8	Clear Creek Bridge	1- 60' pinned Pratt pony truss c1910 John Dildine Bridge Co. (prob.)
*DAVI12	135000.2	Grand River Bridge	(replaced)
*DAVI13	154000.3	Lick Fork Bridge	1- 95' bowstring through arch-truss 1876 Missouri Valley B&I Company
DAVI14	172000.6	Bear Branch Bridge	1- 54' pinned Pratt pony truss c1910 John Dildine Bridge Co. (prob.)
DAVI15	198001.7	Marrowbone Creek Bridge	1- 60' pinned Pratt bedstead 1900 John Dildine Bridge Company
DAVI16	207000.7	Dog Creek Bridge	1- 40' pinned Pratt pony truss c1910 John Dildine Bridge Co. (prob.)
DAVI17	208001.1	Dog Creek Bridge	1- 50' pinned Pratt pony truss c1910 John Dildine Bridge Co. (prob.)
DAVI18	216000.6	Marrowbone Creek Bridge	(replaced)
*DAVI19	245000.5	Smith Branch Bridge	1- 30' pinned Pratt half-hip pony truss c1900 John Dildine Bridge Co. (prob.)
DAVI20	282000.7	Owl Creek Bridge	1- 40' pinned Pratt bedstead c1910 John Dildine Bridge Co. (prob.)
DAVI21	289001.8	Little Creek Bridge	1- 40' pinned Pratt bedstead 1907 John Dildine Bridge Company
DAVI22	304001.0	Muddy Creek Bridge	1- 40' pinned Pratt bedstead c1910 John Dildine Bridge Co. (prob.)
*DAVI23	315003.4	Grindstone Creek Bridge	1-120' pinned Pratt through truss 1886 Missouri Valley B&I Works
*DAVI24	317000.8	Grand River Bridge	1-180' pinned Whipple through truss 1883 Kansas City B&I Company

DAVISS COUNTY

INCLUDED (cont.):

*DAVI25	321002.5	Grand River Bridge	1-178' 1892	pinned Pratt through truss John Dildine Bridge Company
*DAVI26	329001.1	Sampson Creek Bridge	1-140' c1910	pinned Camelback through truss John Dildine Bridge Co. (prob.)
*DAVI27	330000.9	Sampson Creek Bridge	1-100' 1893	pinned Pratt through truss John Dildine Bridge Company
*DAVI28	349003.1	Big Creek Bridge	1-102' c1910	pinned Pratt through truss John Dildine Bridge Co. (prob.)
*DAVI29	350001.2	Big Goose Creek Bridge	2- 60' c1910	pinned Pratt pony truss
*DAVI30	355002.4	Big Creek Bridge	1- 98' c1910	pinned Pratt through truss John Dildine Bridge Co. (prob.)
*DAVI31	359000.6	Big Creek Bridge	1-160' c1910	pinned Camelback through truss John Dildine Bridge Co. (prob.)
DAVI32	364000.9	Cypress Creek Bridge	1- 80' c1910	pinned Pratt pony truss John Dildine Bridge Co. (prob.)
DAVI33	380000.7	Hickory Creek Bridge	1- 50' 1916	pinned Pratt bedstead
DAVI34	396000.7	Pilot Grove Creek Bridge	1- 40' c1910	pinned Pratt pony truss John Dildine Bridge Company
DAVI35	420001.1	Big Muddy Creek Bridge	1- 40' c1910	pinned Pratt pony truss John Dildine Bridge Company
*DAVI36	422R01.0	Pilot Grove Creek Bridge	1- 60' 1947	2-angle Pratt pony truss Chillicothe Iron Works
DAVI37	435000.1	Bushy Creek Bridge	1- 23' 1918	steel stringer
DAVI38	455000.7	Big Muddy Creek Bridge	1-150' c1930	riveted Pratt through truss
DAVI39	458000.1	Grand River Bridge		(destroyed)
DAVI40	465000.6	Honey Creek Bridge	1- 40' 1907	pinned Pratt pony truss John Dildine Bridge Co. (prob.)
DAVI41	557001.0	Cypress Creek Bridge	1-102' c1930	riveted Pratt pony truss
DAVI42	574000.8	Big Muddy Creek Bridge		(replaced)
DAVI43	586000.0	Grand River Bridge		(destroyed)

EXCLUDED:

Pratt pony truss

033000.4 139001.1 283001.0 439001.2

Warren pony truss

H 751 J 860 006002.0 053000.1 220001.2 262001.5 269000.7
288001.9 384000.2 427000.9 565002.0

Warren bedstead

019000.2 157000.8 159000.8 197000.7 203000.3 217000.7 283002.9
366001.1 446001.0 566000.8

DAVISS COUNTY

EXCLUDED:

Steel stringer

F 100R1	J 986	K 169	K 170	K 184	S 182	S 791
S 792	S 793	T 895	T 896	X 117	X 733	X 968
002001.1	002001.5	007000.2	020R00.9	022000.7	022002.7	032000.5
045002.5	045004.1	049R00.3	052001.9	070001.4	092001.4	095001.7
097000.4	110002.8	116000.9	141000.0	146000.3	148001.0	157000.2
170001.2	197000.5	213R00.8	213000.5	215001.4	216002.2	232R01.2
251001.1	254001.6	256000.6	259R00.2	259R00.7	261000.7	266000.4
278000.4	283000.5	288001.2	289001.1	292000.6	293001.1	311R00.5
334001.2	350001.6	355000.3	370000.8	378002.0	384002.1	385001.5
393R00.6	395R00.5	401001.9	402001.0	408000.4	410000.6	414001.6
422000.8	424R00.3	431002.4	434000.1	434001.7	444000.1	446000.1
450001.0	459001.0	464001.5	475000.0	475001.8	477001.4	490R03.2
506000.3	514000.0	519R00.4	519000.6	525001.8	530000.1	555001.4
568000.4	592000.8	603001.0	615R01.0	615000.2	616000.4	

Steel girder

F1136R1	011001.3	375001.0	563001.3
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Concrete girder

F 102R1	S 442	364002.5
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Concrete slab

018002.4

Concrete box culvert

J 131	K 171	K 183	K 224	S 794	X 130	X 574
X 734	X 967					

Timber stringer

431000.8

SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	2	35	0	0	37
Excluded	28	112	0	0	140
	30	147	0	0	177 structures

Big Creek Ditch Bridge

DAVI01

GENERAL DATA

structure no.: H 752	city/town: Pattonsburg
county: Daviess	feature inters.: Big Creek Ditch
	cadastral grid: S26, T61N, R29W
	highway route: U.S. Highway 69
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 7-panel, rigid-connected Pratt through truss, with rigid-connected Warren pony truss approach span

substructure: concrete abutments, wingwalls and piers

span number: 1	condition: good
span length: 140.0'	alterations: none
total length: 248.0'	floor/decking : concrete deck over steel stringers
roadway width: 20.0'	other features: steel pipe guardrails

HISTORICAL DATA

erection date: 1928

erection cost: \$38,869.17

designer: Missouri State Highway Department

fabricator : unknown

contractor: Pioneer Construction Company, Kansas City MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number H 752; Files on Primary System Bridges - located at the Missouri Highway and Transportation Department, Jefferson City MO.

sign. rating: 41

evaluation: NRHP non-eligible (typically configured example of highway truss construction)

inventoried by: Michelle Crow-Dolby 2 April 1993

Grand River Bridge

DAVI02

GENERAL DATA

structure no.:	J 146R	city/town:	1.8 miles south of Pattonsburg
county:	Daviess	feature inters.:	Grand River
		cadastral grid:	S11, T60N, R29W
		highway route:	U.S. Highway 69
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 7-panel, rigid-connected Pratt through truss, with concrete girder approach spans

substructure: concrete abutments, wingwalls and piers

span number:	3	condition:	good
span length:	140.0'	alterations:	deck repaired and guardrails replaced, 1990
total length:	617.0'	floor/decking :	asphalt / concrete deck over steel stringers
roadway width:	20.4'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates, top and bottom; vertical: 4 angles with lacing; diagonal: 2 angles with batten plates; lateral bracing: 1 angle; strut: 2 angles with lacing; portal strut: 2 angles with lacing; guardrail: steel angle; bridge plate: Missouri Highway Dept. / Bridge N* J.146 / 1930

HISTORICAL DATA

erection date: 1930

erection cost: \$77,658.25

designer: Missouri State Highway Department

fabricator : Inland Steel Company, East Chicago IN

contractor: Pioneer Construction Company, Kansas City MO (1930);
Chester Brothers Construction Company (1990)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number J 146R; Files on Primary System Bridges - located Ta the Missouri Highway and Transportation Department, Jefferson City MO; field inspection by Mitzi Rossillon, 13 September 1990.

sign. rating: 44

evaluation: NRHP non-eligible (typically configured example of highway truss construction, somewhat notable for its multiple-span configuration)

inventoried by: Michelle Crow-Dolby 2 April 1993

Cypress Creek Bridge

DAVI03

GENERAL DATA

structure no.: 017002.0	city/town: 3.1 miles northwest of Salem
county: Daviess	feature inters.: Cypress Creek
	cadastral grid: S3, T61N, R28W
	highway route: County Road 17
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 2-panel, rigid-connected kingpost pony truss	
substructure: steel pile bent piers with timber back- and wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: bridge closed
total length: 41.0'	floor/decking : timber deck over steel stringers
roadway width: 11.6'	other features: inclined end post: I-beam; lower chord: 2 channels with batten plates; vertical: 4 angles with lacing; lateral bracing: round rod with threaded ends; guardrail: 2 angles

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: John Dildine Bridge Company, Cameron MO (probable)	
fabricator : Cambria Steel Company, Pittsburgh PA	
contractor: John Dildine Bridge Company, Cameron MO (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 017002.0; field inspection by Mitzi Rossillon, 13 September 1990.	
sign. rating: 53	
evaluation: NRHP possibly eligible (well-preserved atypically configured example of once-mainstay, now rare structural type)	

inventoried by: Michelle Crow-Dolby 2 April 1993

Hickory Creek Bridge

DAVI04

GENERAL DATA

structure no.:	025002.8	city/town:	4.1 miles northeast of Salem
county:	Daviess	feature inters.:	Hickory Creek
		cadastral grid:	S33, T62N, R27W
		highway route:	County Road 25
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt truss-leg bedstead		
substructure:	steel pile bent piers with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	25.0'	alterations:	unknown
total length:	25.0'	floor/decking :	timber deck
roadway width:	12.1'	other features:	unknown

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO (probable)
fabricator :	John Dildine Bridge Company, Cameron MO (probable)
contractor:	John Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 025002.8.
sign. rating:	30
evaluation:	NRHP non-eligible (typically configured, inadequately documented example of common structural type)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Little Creek Bridge

DAVI05

GENERAL DATA

structure no.:	044002.8	city/town:	9.4 miles east of Salem
county:	Daviess	feature inters.:	branch of Little Creek
		cadastral grid:	S4/33, T61/62N, R26W
		highway route:	County Road 44
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	concrete abutments and wingwalls	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	40.0'	other features:	steel angle guardrails
total length:	41.0'		
roadway width:	13.5'		

HISTORICAL DATA

erection date:	1919
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 044002.8.

sign. rating:	24
evaluation:	NRHP non-eligible (typical example of exceedingly common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Little Creek Bridge

DAVI06

GENERAL DATA

structure no.:	044003.6	city/town:	9.8 miles north of Jamesport
county:	Daviess	feature inters.:	Little Creek
		cadastral grid:	S3, T61N, R26W
		highway route:	County Road 44
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	53.0'	floor/decking :	timber deck
roadway width:	12.3'	other features:	unknown

HISTORICAL DATA

erection date:	1909
erection cost:	\$1677.00 (multiple-bridge contract amount)
designer:	John Dildine Bridge Company, Cameron MO
fabricator :	John Dildine Bridge Company, Cameron MO
contractor:	John Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 044003.6; Daviess County Court Record, Book N: page 286 (1 February 1909) -located at Daviess County Courthouse, Gallatin MO.
sign. rating:	41
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Tombstone Creek Bridge

DAVI07

GENERAL DATA

structure no.: 052000.2	city/town: 10.6 miles north of Jamesport
county: Daviess	feature inters.: Tombstone Creek
	cadastral grid: S34, T62N, R26W
	highway route: County Road 52
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead	
substructure: steel pile bent piers with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 48.0'	floor/decking : timber deck
roadway width: 11.4'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1915
erection cost: unknown
designer: John Dildine Bridge Company, Cameron MO (probable)
fabricator : John Dildine Bridge Company, Cameron MO (probable)
contractor: John Dildine Bridge Company, Cameron MO (probable)
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 052000.2.
sign. rating: 35
evaluation: NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Hickory Creek Bridge

DAVI08

GENERAL DATA

structure no.:	065001.1	city/town:	4.5 miles north of Jamesport
county:	Daviess	feature inters.:	Hickory Creek
		cadastral grid:	S36, T61N, R26W
		highway route:	County Road 65
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure: steel, 2-panel, rigid-connected Pratt half-hip pony truss
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	32.0'	alterations:	unknown
total length:	32.0'	floor/decking :	timber deck
roadway width:	11.9'	other features:	unknown

HISTORICAL DATA

erection date: c1915
erection cost: unknown
designer: John Dildine Bridge Company, Cameron MO (probable)
fabricator : John Dildine Bridge Company, Cameron MO (probable)
contractor: John Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 065001.1.

sign. rating: 35
evaluation: NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Clear Creek Bridge

DAVI10

GENERAL DATA

structure no.: 114000.5	city/town: 3.2 miles north of Lock Springs
county: Daviess	feature inters.: Clear Creek
	cadastral grid: S25, T59N, R26W
	highway route: County Road 114
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber back- and wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 122.0'	floor/decking : timber deck
roadway width: 11.5'	other features: steel angle guardrails

HISTORICAL DATA

erection date: 1900	
erection cost: unknown	
designer: John Dildine Bridge Company, Cameron MO	
fabricator : John Dildine Bridge Company, Cameron MO	
contractor: John Dildine Bridge Company, Cameron MO	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 114000.5; Daviess County Court Record, Book L: page 251 (5 August 1900) - located at Daviess County Courthouse, Gallatin MO.	
sign. rating: 41	
evaluation: NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 2 April 1993

Clear Creek Bridge

DAVI11

GENERAL DATA

structure no.: 121001.8	city/town: 1.0 mile northwest of Lock Springs
county: Daviess	feature inters.: Clear Creek
	cadastral grid: S2/11, T58N, R26W
	highway route: County Road 121
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: concrete-filled steel cylinder piers	
span number: 1	condition: fair
span length: 60.0'	alterations: unknown
total length: 91.0'	floor/decking : timber deck
roadway width: 11.6'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: John Dildine Bridge Company, Cameron MO (probable)	
fabricator : John Dildine Bridge Company, Cameron MO (probable)	
contractor: John Dildine Bridge Company, Cameron MO (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 121001.8.	
sign. rating: 38	
evaluation: NRHP non-eligible (typically configured, inadequately documented example of common structural type)	

inventoried by: Michelle Crow-Dolby 2 April 1993

Lick Fork Bridge

DAVI13

GENERAL DATA

structure no.:	154000.3	city/town:	4.6 miles southwest of Lock Springs
county:	Daviess	feature inters.:	Lick Fork
		cadastral grid:	S29, T58N, R26W
		highway route:	County Road 154
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure: wrought iron, 10-panel, bowstring through arch-truss, with steel stringer approach span

substructure: concrete abutments and iron pile pier

span number:	1	condition:	fair
span length:	95.0'	alterations:	none
total length:	110.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.5'	other features:	upper chord: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: star iron with star iron outrider with threaded ends; diagonal: round rods with threaded ends; strut: 1 channel; guardrail: 2 channels on main truss, 2 angles on approach span

HISTORICAL DATA

erection date: 1876

erection cost: \$2500.00 (probable)

designer: Missouri Valley Bridge and Iron Company, Leavenworth KS (probable)

fabricator : Missouri Valley Bridge and Iron Company, Leavenworth KS (probable)

contractor: Missouri Valley Bridge and Iron Company, Leavenworth KS (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 154000.3; Daviess County Court Record, Book F: page 382 (18 September 1876); Book J: page 191 (6 February 1893)- located at Daviess County Courthouse, Gallatin MO.

sign. rating: 76

evaluation: NRHP eligible (well-preserved example of once-mainstay, now rare structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Bear Branch Bridge

DAVI14

GENERAL DATA

structure no.:	172000.6	city/town:	7.6 miles south of Gallatin
county:	Daviess	feature inters.:	Bear Branch
		cadastral grid:	S27/28, T58N, R27W
		highway route:	County Road 172
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled steel cylinder piers with timber backwalls		
span number:	1	condition:	fair
span length:	54.0'	alterations:	unknown
total length:	54.0'	floor/decking :	timber deck
roadway width:	11.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO (probable)
fabricator :	John Dildine Bridge Company, Cameron MO (probable)
contractor :	John Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 172000.6.
sign. rating:	35
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Marrowbone Creek Bridge

DAVI15

GENERAL DATA

structure no.:	198001.7	city/town:	6.6 miles south of Altamont
county:	Daviess	feature inters.:	Marrowbone Creek
		cadastral grid:	S33, T58N, R28W
		highway route:	County Road 198
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt truss-leg bedstead		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.5'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1900
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO
fabricator :	John Dildine Bridge Company, Cameron MO
contractor:	John Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 198001.7; Daviess County Court Record, Book L: page 168 (4 June 1900) - located at Daviess County Courthouse, Gallatin MO.
sign. rating:	44
evaluation:	NRHP non-eligible (typical example of relatively common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Dog Creek Bridge

DAVI16

GENERAL DATA

structure no.: 207000.7	city/town: 3.2 miles south of Altamont
county: Daviess	feature inters.: Dog Creek
	cadastral grid: S8, T58N, R28W
	highway route: County Road 207
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach span	
substructure: concrete-filled steel cylinder piers	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 69.0'	floor/decking : timber deck
roadway width: 13.8'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: John Dildine Bridge Company, Cameron MO (probable)	
fabricator : John Dildine Bridge Company, Cameron MO (probable)	
contractor: John Dildine Bridge Company, Cameron MO (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 207000.7.	
sign. rating: 35	
evaluation: NRHP non-eligible (typical example of common structural type)	

Inventoried by: Michelle Crow-Dolby 2 April 1993

Dog Creek Bridge

DAVI17

GENERAL DATA

structure no.:	208001.1	city/town:	3.3 miles south of Altamont
county:	Daviess	feature inters.:	Dog Creek
		cadastral grid:	S8/17, T58N, R28W
		highway route:	County Road 208
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	50.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO (probable)
fabricator :	John Dildine Bridge Company, Cameron MO (probable)
contractor:	John Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 208001.1.
sign. rating:	35
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Smith Branch Bridge

DAVI19

GENERAL DATA

structure no.: 245000.5	city/town: 4.7 miles southwest of Altamont
county: Daviess	feature inters.: branch of Smith Branch
	cadastral grid: S5, T58N, R29W
	highway route: County Road 245
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss
substructure: concrete abutments and wingwalls

span number: 1	condition: fair
span length: 30.0'	alterations: substructure repaired and beginning to collapse
total length: 31.0'	floor/decking : timber deck over steel and reinforcing timber stringers
roadway width: 11.4	other features: upper chord and inclined end post: 2 channels with lacing; lower chord: 2 looped square eyebars; vertical: 4 angles with lacing; diagonal: 2 looped round eyerods; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

HISTORICAL DATA

erection date: c1900
erection cost: unknown
designer: John Dildine Bridge Company, Cameron MO (probable)
fabricator : John Dildine Bridge Company, Cameron MO (probable)
contractor : John Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 245000.5; field inspection by Mitzi Rossillon, 9 September 1990.

sign. rating: 31
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Owl Creek Bridge

DAVI20

GENERAL DATA

structure no.: 282000.7	city/town: 5.7 miles northwest of Altamont
county: Daviess	feature inters.: Owl Creek
	cadastral grid: S18/19, T59N, R29W
	highway route: County Road 282
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead	
substructure: steel pile bent piers with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 56.0'	floor/decking : timber deck
roadway width: 11.6'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: John Dildine Bridge Company, Cameron MO (probable)
fabricator : John Dildine Bridge Company, Cameron MO (probable)
contractor: John Dildine Bridge Company, Cameron MO (probable)
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 282000.7.
sign. rating: 35
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Little Creek Bridge

DAVI21

GENERAL DATA

structure no.:	289001.8	city/town:	5.9 miles south of Pattonsburg
county:	Daviess	feature inters.:	Little Creek
		cadastral grid:	S4, T59N, R29W
		highway route:	County Road 289
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	40.0'	floor/decking :	timber deck
roadway width:	12.0'	other features:	unknown

HISTORICAL DATA

erection date: 1907
erection cost: unknown
designer: John Dildine Bridge Company, Cameron MO
fabricator : John Dildine Bridge Company, Cameron MO
contractor : John Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 289001.8; Daviess County Court Record, Book N: page 98 (7 October 1907) - located at Daviess County Courthouse, Gallatin MO.

sign. rating: 36
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Muddy Creek Bridge

DAVI22

GENERAL DATA

structure no.: 304001.0	city/town: 3.9 miles southwest of Pattonsburg
county: Daviess	feature inters.: Muddy Creek
	cadastral grid: S20, T60N, R29W
	highway route: County Road 304
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 68.0'	floor/decking : timber deck
roadway width: 12.0'	other features: unknown

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: John Dildine Bridge Company, Cameron MO (probable)	
fabricator : John Dildine Bridge Company, Cameron MO (probable)	
contractor: John Dildine Bridge Company, Cameron MO (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 304001.0.	
sign. rating: 35	
evaluation: NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 2 April 1993

Grindstone Creek Bridge

DAVI23

GENERAL DATA

structure no.: 315003.4	city/town: 2.9 miles south of Pattonsburg
county: Daviess	feature inters.: Grindstone Creek
	cadastral grid: S15, T60N, R29W
	highway route: County Road 315
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: wrought iron, 7-panel, pin-connected Pratt through truss, with steel stringer approach span at each end
substructure: concrete-filled iron cylinder piers; timber pile bent abutments

span number: 1	condition: fair
span length: 120.0'	alterations: none
total length: 143.0'	floor/decking : timber deck over steel stringers
roadway width: 13.7'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 round eyerods at the hip); diagonal: 2 punched rectangular eyebars; counter: 2 round eyerods with turnbuckles; lateral bracing: round rod with threaded ends; strut: I-beam; floor beam: I-beam, U-bolted to vertical; guardrail: 1 channel; builder's plate: MO. VALLEY BRIDGE / & / IRON WORKS / 1886

HISTORICAL DATA

erection date: 1886
erection cost: \$2050.00 (contract amount)
designer: Missouri Valley Bridge and Iron Works, Leavenworth KS
fabricator : Missouri Valley Bridge and Iron Works, Leavenworth KS;
Carnegie Iron Works, Pittsburgh PA;
Illinois Steel Company, Chicago IL
contractor: Missouri Valley Bridge and Iron Works, Leavenworth KS

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 315003.4; Daviess County Court Record, Book H: page 453 (2 November 1885)- located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 9 September 1990.

sign. rating: 56
evaluation: NRHP possibly eligible (well-preserved, relatively early example of main-stay structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Grand River Bridge

DAVI24

GENERAL DATA

structure no.: 317000.8	city/town: 1.8 miles south of Pattonsburg
county: Daviess	feature inters.: Grand River
	cadastral grid: S10, T60N, R29W
	highway route: County Road 317
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: wrought iron, 11-panel, pin-connected Whipple through truss; pin-connected Pratt through truss; 2 steel stringer approach spans

substructure: concrete abutments and wingwalls; concrete-filled iron cylinder piers; steel pile bent piers

span number: 1	condition: fair
span length: 180.0'	alterations: collision damage to guardrails
total length: 331.0'	floor/decking : timber deck over steel stringers
roadway width: 11.8'	other features: Whipple through truss: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 square rods at the hip); diagonal: 2 punched rectangular eyerods; counter: round rods with turnbuckles; lateral bracing: round rods with turnbuckles; strut: 4 angles with lacing; portal strut: 4 angles with lacing; guardrail: 2 angles; bridge plate: KANSAS CITY / BRIDGE & IRON CO./ 1883; Pratt through truss: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 round rods at the hip); diagonal: 2 punched rectangular eyerods; counter: round rods with turnbuckles; lateral bracing: round rods with turnbuckles; strut: 2 channels with spacers; portal strut: 4 angles with lacing; guardrail: 2 angles

HISTORICAL DATA

erection date: 1883

erection cost: unknown

designer: Kansas City Bridge and Iron Company, Kansas City MO

fabricator : Kansas City Bridge and Iron Company, Kansas City MO;
Carnegie Iron Works, Pittsburgh PA;
Cambria Iron Company, Pittsburgh PA

contractor : Kansas City Bridge and Iron Company, Kansas City MO

Grand River Bridge

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 317000.8; field inspection by Mitzi Rossillon, September 1990.

sign. rating: 78

evaluation: NRHP eligible (well-preserved, long-span example of rare early truss type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Grand River Bridge

DAVI25

GENERAL DATA

structure no.: 321002.5	city/town: 1.9 miles southwest of Pattonsburg
county: Daviess	feature inters.: Grand River
	cadastral grid: S9, T60N, R29W
	highway route: County Road 321
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: wrought iron, 9-panel, pin-connected Pratt through truss; 6 steel stringer approach spans at the west end and 2 steel stringer approach spans at the east end	
substructure: steel pile bent piers with timber wingwalls; concrete-filled iron cylinder piers	
span number: 1	condition: fair
span length: 178.0'	alterations: two approach spans, including abutments and piers, replaced
total length: 395.0'	floor/decking : timber deck over steel stringers
roadway width: 15.5'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars (4 rectangular eyebars on panels 4, 5 and 6); vertical: 2 channels with lacing (2 square eyebars at the hip); diagonal: 2 rectangular eyebars; counter: round rods with turnbuckles; lateral bracing: round rods with threaded ends; strut: 4 angles with lacing; portal strut: 4 angles with lacing; guardrail: 2 channels (cable on east approach span)

HISTORICAL DATA

erection date: 1892	
erection cost: \$4970.00	
designer: John Dildine Bridge Company, Cameron MO	
fabricator : John Dildine Bridge Company, Cameron MO; Carnegie Iron Works, Pittsburgh PA	
contractor : John Dildine Bridge Company, Cameron MO	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 321002.5; Daviess County Court Record, Book J: page 204 (8 February 1893), page 81 (6 June 1892); Daviess County Court Record, Book N: pages 157-159 (4 February 1908), page 265 (7 December 1908) - located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 12 September 1990.	

Grand River Bridge

sign. rating: 53
evaluation: NRHP possibly eligible (relatively early, long-span example of mainstay structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Sampson Creek Bridge

DAVI26

GENERAL DATA

structure no.:	329001.1	city/town:	Pattonsburg
county:	Daviess	feature inters.:	Sampson Creek
		cadastral grid:	S27, T61N, R29W
		highway route:	County Road 329
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure: steel, 7-panel, pin-connected Camelback through truss
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	140.0'	alterations:	abutments replaced
total length:	140.0'	floor/decking :	concrete deck over steel stringers
roadway width:	14.7'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars, and 4 angles with batten plates; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: angles; portal strut: channels and angles; guardrail: 2 channels

HISTORICAL DATA

erection date: c1910
erection cost: John Dildine Bridge Company, Cameron MO (probable)
designer: John Dildine Bridge Company, Cameron MO (probable)
fabricator : Illinois Steel Company, Chicago IL
contractor: John Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 329001.1; field inspection by Mitzi Rossillon 9 September 1990.

sign. rating: 47
evaluation: NRHP possibly eligible (typically configured, though altered, example of uncommon structural type, only partially documented)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Sampson Creek Bridge

DAVI27

GENERAL DATA

structure no.:	330000.9	city/town:	1.0 mile northwest of Pattonsburg
county:	Daviess	feature inters.:	Sampson Creek
		cadastral grid:	S22, T61N, R29W
		highway route:	County Road 330
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure: wrought iron, 7-panel, pin-connected Pratt through truss, with steel stringer approach span at each end
substructure: steel pile bent piers and abutments

span number:	1	condition:	fair
span length:	100.0'	alterations:	truss moved
total length:	132.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.7'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (2 square eyerods at the hip); diagonal: 2 looped rectangular eyerods; counter: round rods with turnbuckles; lateral bracing: round rods with turnbuckles - top, round rods - bottom; strut: 4 angles with lacing; portal strut: 2 angles; floor beam: I-beam, U-bolted to vertical; guardrail: 2 channels; builders plate: JOHN DILDINE & CO / BRIDGE BUILDERS / CAMERON MO / 1893

HISTORICAL DATA

erection date: 1893
erection cost: \$950.00
designer: John Dildine Bridge Company, Cameron MO
fabricator : John Dildine Bridge Company, Cameron MO
contractor: John Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 330000.9; Daviess County Court Record, Book J: page 276 (7 August 1893), page 303 (5 September 1893); Daviess County Court Record, Book M: page 188 (5 July 1904) - located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 13 September 1990.

sign. rating: 39
evaluation: NRHP non-eligible (typically configured example of mainstay structural type, moved to this location)

inventoried by: Michelle Crow-Dolby 2 April 1993

Big Creek Bridge

DAVI28

GENERAL DATA

structure no.: 349003.1	city/town: 3.8 miles north of Pattonsburg
county: Daviess	feature inters.: Big Creek
	cadastral grid: S11, T61N, R29W
	highway route: County Road 349
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 6-panel, pin-connected Pratt through truss, with steel stringer approach spans at each end	
substructure: concrete abutments; steel pile bent piers	
span number: 1	condition: fair
span length: 102.0'	alterations: substructure replaced
total length: 142.0'	floor/decking : timber deck over steel stringers; concrete deck on approach spans
roadway width: 15.8'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 rectangular eyebars; vertical: 2 channels with lacing (2 square eyebars at the hip); diagonal: 2 punched rectangular eyebars; counter: round rods with turnbuckles; lateral bracing: round rods with threaded ends; strut: 2 angles; portal strut: A-frame; guardrail: 2 channels

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: John Dildine Bridge Company, Cameron MO (probable)	
fabricator : John Dildine Bridge Company, Cameron MO (probable)	
contractor: John Dildine Bridge Company, Cameron MO (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 349003.1; field inspection by Mitzi Rossillon, 13 September 1990.	
sign. rating: 33	
evaluation: NRHP non-eligible (typical example of common structural type, altered and poorly documented)	

inventoried by: Michelle Crow-Dolby 2 April 1993

Big Goose Creek Bridge

DAVI29

GENERAL DATA

structure no.:	350001.2	city/town:	4.5 miles north of Pattonsburg
county:	Daviess	feature inters.:	Big Goose Creek
		cadastral grid:	S3, T61N, R29W
		highway route:	County Road 350
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure: steel, 5-panel, rigid-connected Pratt pony truss, with pin-connected Pratt half-hip pony approach span at the east end and steel stringer approach span at the west end

substructure: concrete abutments; timber wingwalls; steel pile bent piers with concrete caps

span number:	2	condition:	fair
span length:	60.0'	alterations:	trusses moved to this location
total length:	104.0'	floor/decking :	concrete deck over steel stringers
roadway width:	13.0'	other features:	rigid-connected Pratt pony: upper chord and inclined end post: I-beam; lower chord: 2 angles with batten plates; vertical: 4 angles with batten plates; diagonal: 2 angles with batten plates; counter: 2 angles with batten plates; lateral bracing: round rod with threaded ends; pin- and rigid-connected Pratt pony: upper chord and inclined end post: 2 channels with lacing; lower chord: 2 angles; vertical: 4 angles with lacing; diagonal: 2 angles; lateral bracing: round rod with threaded ends; guardrail: 2 channels

HISTORICAL DATA

erection date: c1920
erection cost: unknown
designer: unknown
fabricator : Illinois Steel Company, Chicago IL;
Jones and Laughlin Steel Company, Pittsburgh PA
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 350001.2; field inspection by Mitzi Rossillon, 13 September 1990.

sign. rating: 24
evaluation: NRHP non-eligible (poorly preserved, poorly documented example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Big Creek Bridge

DAVI30

GENERAL DATA

structure no.: 355002.4	city/town: 1.6 miles north of Pattonsburg
county: Daviess	feature inters.: Big Creek
	cadastral grid: S23, T61N, 29RW
	highway route: County Road 355
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt through truss, with steel stringer approach span at each end

substructure: concrete abutments and steel pile bent piers

span number: 1	condition: fair
span length: 98.0'	alterations: 1 set of hip verticals repaired; bottom lateral brace broken
total length: 138.0'	floor/decking : timber deck over steel stringers
roadway width: 11.6'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (2 square rods at the hip); diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beam, U-bolted to vertical; guardrail: 2 channels

HISTORICAL DATA

erection date: c1910

erection cost: unknown

designer: John Dildine Bridge Company, Cameron MO (probable)

fabricator : John Dildine Bridge Company, Cameron MO (probable); Illinois Steel Company, Chicago IL

contractor: John Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 355002.4; Daviess County Court Record, Book J: page 276 (7 August 1893), page 303 (5 September 1893) - located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 9 September 1990.

sign. rating: 35

evaluation: NRHP non-eligible (typically configured, poorly documented example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Big Creek Bridge

DAVI31

GENERAL DATA

structure no.: 359000.6	city/town: immediately east of Pattonsburg
county: Daviess	feature inters.: Big Creek
	cadastral grid: S36, T61N, R29W
	highway route: County Road 359
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 8-panel, pin-connected Camelback through truss, with steel stringer approach span at the west end

substructure: concrete abutments, wingwalls and piers

span number: 1	condition: fair
span length: 160.0'	alterations: substructure replaced
total length: 180.0'	floor/decking : concrete deck over steel stringers
roadway width: 15.7'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (4 angles with batten plates at the hip); diagonal: 2 looped rectangular eyebars; counter: round eyerods with turnbuckles; lateral bracing: round rod with threaded ends; strut: 2 angles; portal strut: 2 angles; guardrail: 2 angles (south side of west approach span is cable)

HISTORICAL DATA

erection date: c1910

erection cost: unknown

designer: John Dildine Bridge Company, Cameron MO (probable)

fabricator : John Dildine Bridge Company, Cameron MO (probable);
Inland Steel Company, East Chicago IN

contractor: John Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 359000.6; field inspection by Mitzi Rossillon, 9 September 1990.

sign. rating: 50

evaluation: NRHP possibly eligible (typically configured, though altered, example of uncommon structural type, only partially documented)

inventoried by: Michelle Crow-Dolby 2 April 1993

Cypress Creek Bridge

DAVI32

GENERAL DATA

structure no.:	364000.9	city/town:	3.9 miles northeast of Pattonsburg
county:	Daviess	feature inters.:	Cypress Creek
		cadastral grid:	S20, T61N, R28W
		highway route:	County Road 364
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	80.0'	alterations:	unknown
total length:	80.0'	floor/decking :	timber deck
roadway width:	11.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO (probable)
fabricator :	John Dildine Bridge Company, Cameron MO (probable)
contractor:	John Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 364000.9.
sign. rating:	40
evaluation:	NRHP non-eligible (typical example of common truss type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Hickory Creek Bridge

DAVI33

GENERAL DATA

structure no.:	380000.7	city/town:	1.4 miles southeast of Salem
county:	Daviess	feature inters.:	Hickory Creek
		cadastral grid:	S7, T61N, R27W
		highway route:	County Road 380
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt truss-leg bedstead		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	69.0'	floor/decking :	timber deck
roadway width:	11.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1916
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 380000.7.

sign. rating:	32
evaluation:	NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 2 April 1993

Pilot Grove Creek Bridge

DAVI34

GENERAL DATA

structure no.:	396000.7	city/town:	7.2 miles southeast of Salem
county:	Daviess	feature inters.:	Pilot Grove Creek
		cadastral grid:	S25, T61N, R27W
		highway route:	County Road 396
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	84.0'	floor/decking :	timber deck
roadway width:	11.9'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO (probable)
fabricator :	John Dildine Bridge Company, Cameron MO (probable)
contractor:	John Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 396000.7.
sign. rating:	35
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Big Muddy Creek Bridge

DAVI35

GENERAL DATA

structure no.:	420001.1	city/town:	1.9 miles southeast of Jameson
county:	Daviess	feature inters.:	Big Muddy Creek
		cadastral grid:	S29, T60N, R27W
		highway route:	County Road 420
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	60.0'	floor/decking :	timber deck
roadway width:	15.4'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO (probable)
fabricator :	John Dildine Bridge Company, Cameron MO (probable)
contractor :	John Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 420001.1.
sign. rating:	35
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Pilot Grove Creek Bridge

DAVI36

GENERAL DATA

structure no.: 422R01.0	city/town: 3.5 miles southeast of Jameson
county: Daviess	feature inters.: Pilot Grove Creek
	cadastral grid: S28/33, T60N, R27W
	highway route: County Road 422
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 4-panel, rigid-connected, 2-angle Pratt pony truss, with steel stringer approach spans	
substructure: timber abutments; steel pile bent piers	
span number: 1	condition: fair
span length: 60.0'	alterations: abutments replaced
total length: 114.0'	floor/decking : timber deck over alternating steel and timber stringers
roadway width: 15.5	other features: upper chord and inclined end post: 2 angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; lateral bracing: round rod with threaded ends; guardrail: channel and angle

HISTORICAL DATA

erection date: 1947 (probable)	
erection cost: unknown	
designer: Chillicothe Iron Works, Chillicothe MO (probable)	
fabricator : Chillicothe Iron Works, Chillicothe MO (probable)	
contractor : county crew	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 422R01.0; field inspection by Mitzi Rossillon, 13 September 1990.	
sign. rating: 37	
evaluation: NRHP non-eligible (undistinguished example of uncommon truss type)	

inventoried by: Michelle Crow-Dolby 2 April 1993

Brushy Creek Bridge

DAVI37

GENERAL DATA

structure no.:	435000.1	city/town:	4.0 miles southwest of Jamesport
county:	Daviess	feature inters.:	branch of Brushy Creek
		cadastral grid:	S1/6, T59N, R26/27W
		highway route:	County Road 435
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	24.0'	other features:	unknown
total length:	24.0'		
roadway width:	16.5'		

HISTORICAL DATA

erection date:	1918
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 435000.1.
sign. rating:	24
evaluation:	NRHP non-eligible (typical, small-scale example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Big Muddy Creek Bridge

DAVI38

GENERAL DATA

structure no.:	455000.7	city/town:	2.3 miles west of Carlow
county:	Daviess	feature inters.:	Big Muddy Creek
		cadastral grid:	S36, T59N, R27W
		highway route:	County Road 455
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 9-panel, rigid-connected Pratt through truss, with steel stringer approach spans		
substructure:	unknown		
span number:	1	condition:	fair
span length:	150.0'	alterations:	unknown
total length:	196.0'	floor/decking :	timber deck
roadway width:	15.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1930
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 455000.7.

sign. rating:	28
evaluation:	NRHP non-eligible (typical example of common truss type, poorly documented)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Honey Creek Bridge

DAVI40

GENERAL DATA

structure no.:	465000.6	city/town:	2.7 miles south of Gallatin
county:	Daviess	feature inters.:	Honey Creek
		cadastral grid:	S31, T59N, R27W
		highway route:	County Road 465
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	40.0'	alterations:	none
total length:	80.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails; laced endposts

HISTORICAL DATA

erection date:	1907
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO
fabricator :	John Dildine Bridge Company, Cameron MO
contractor:	John Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 465000.6; Daviess County Court Record, Book N: page 10 (5 February 1907) - located at Daviess County Courthouse, Gallatin MO.
sign. rating:	43
evaluation:	NRHP non-eligible (typical example of common truss type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Cypress Creek Bridge

DAVI41

GENERAL DATA

structure no.:	557001.0	city/town:	3.4 miles east of Pattonsburg
county:	Daviess	feature inters.:	Cypress Creek
		cadastral grid:	S29, T61N, R28W
		highway route:	County Road 557
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 5-panel, rigid-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	102.0'	alterations:	unknown
total length:	103.0'	floor/decking :	timber deck
roadway width:	16.3'	other features:	unknown

HISTORICAL DATA

erection date:	c1930
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 557001.0.

sign. rating:	28
evaluation:	NRHP non-eligible (long-span example of common truss type, poorly documented)

inventoried by: Michelle Crow-Dolby 2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Cypress Creek Bridge
MHTD: 017002.0

DAVI03

DATE(S) OF CONSTRUCTION

c1910

LOCATION

County Road 17 over Cypress Creek; S3, T61N, R28W
3.1 miles northwest of Salem; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / abandoned

RATING NRHP possibly eligible (score: 53)

CONDITION

fair

OWNER

Daviess County

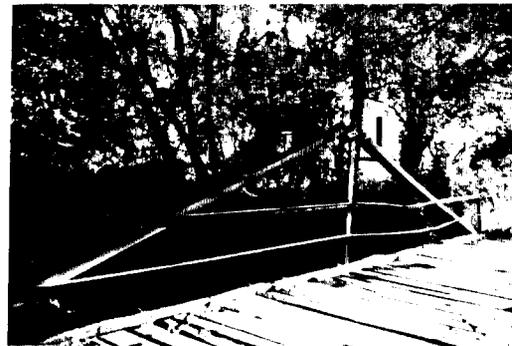
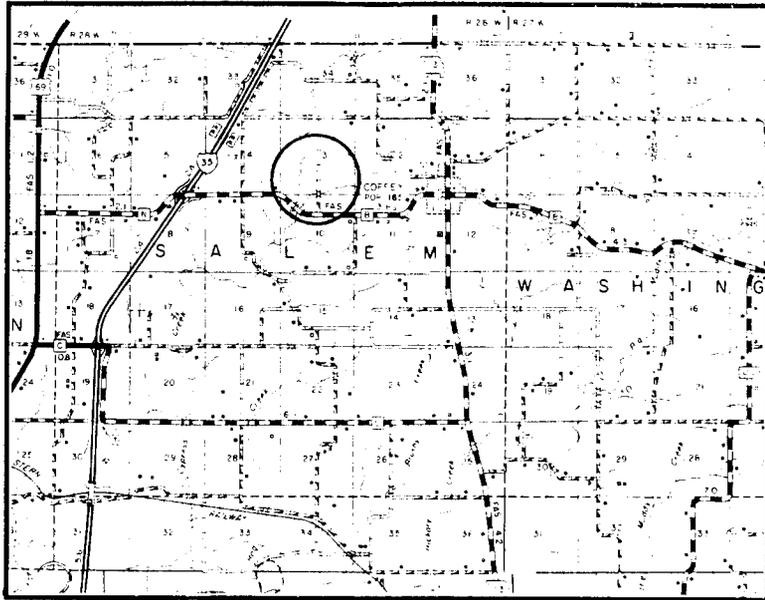
span number: 1
span length: 40.0'
total length: 41.0'
roadway wdt.: 11.6'

superstructure: steel, 2-panel, rigid-connected kingpost pony truss
substructure: steel pile bent piers with timber back- and wingwalls
floor/decking: timber deck over steel stringers
other features: inclined end post: I-beam; lower chord: 2 channels with batten plates; vertical: 4 angles with lacing; lateral bracing: round rod with threaded ends; guardrail: 2 angles

Located about 3.1 miles northwest of Salem, the small-scale truss bridge spans Cypress Creek on an abandoned section of ungraded county road. The structure is comprised of a single rigid-connected kingpost pony truss supported by steel pile bent abutments with timber backwalls. Made up of steel components rolled in Pittsburgh by Cambria, the truss is atypically configured, with steel I-beams used in lieu of built-up box beams for the inclined endposts. No specific reference to this bridge has been found in Daviess County records. It was most likely fabricated and built by the John Dildine Bridge Company of Cameron, Missouri, Daviess County's perennial contractor in the early 20th century. Other than the bridge's closure to vehicular traffic, it appears in essentially unaltered condition today.

NAME(S) OF STRUCTURE
Cypress Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 017002.0; field inspection by Mitzi Rossillon, 13 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

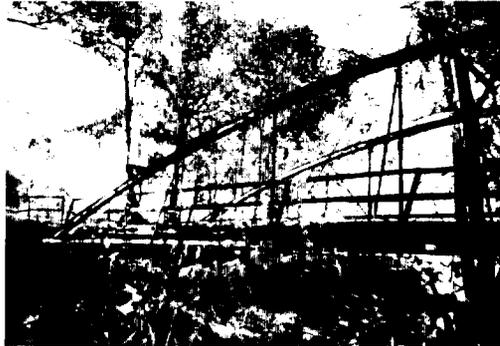
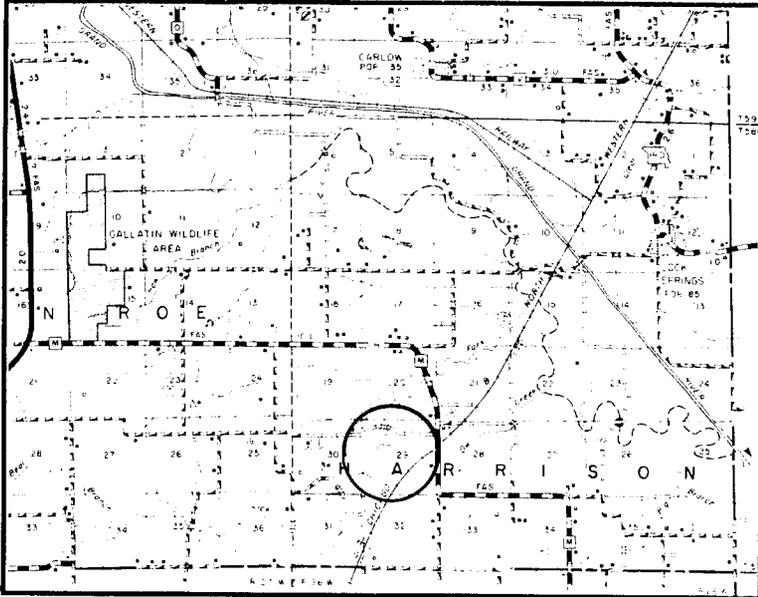
DATE

2 April 1993

NAME(S) OF STRUCTURE

Lick Fork Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 154000.3; Daviess County Court Record, Book F: page 382 (18 September 1876); Book J: page 191 (6 February 1893)- located at Daviess County Courthouse, Gallatin MO.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Lick Fork Bridge
MHTD: 154000.3

DAVI13

DATE(S) OF CONSTRUCTION

1876

LOCATION

County Road 154 over Lick Fork; S29, T58N, R26W
4.6 miles southwest of Lock Springs; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP eligible (score: 76)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 95.0'
total length: 110.0'
roadway wdt.: 13.5'

superstructure: wrought iron, 10-panel, bowstring through arch-truss, with steel stringer approach span
substructure: concrete abutments and iron pile pier
floor/decking: timber deck over steel stringers
other features: upper chord: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: star iron with star iron outrider with threaded ends; diagonal: round rods with threaded ends; strut: 1 channel; guardrail: 2 channels on main truss, 2 angles on approach span

Designed and built by the Missouri Valley Bridge and Iron Company of Leavenworth, Kansas, the Lick Fork Creek Bridge features a timber deck over steel stringers and is supported by a steel stringer approach span on its north end. In September of 1876 the Daviess County Court let a contract to design and build a bridge in Section 29, Township 58, Range 26. Total estimated cost of the bridge: \$2500.00. County records indicate that seventeen years later, in 1893, James Burns was paid \$160.00 for "replacing the bridge's chords, needle beams, five or more joists, one piling and the flooring." The bowstring through arch truss is situated in southeastern Daviess County approximately five miles southwest of Lock Springs. Since its 1876 completion the bridge has carried County Road 154 across the creek in this sparsely populated setting.

The bowstring arch-truss was the iron span of choice for Missouri counties in the late 1860s and 1870s. Marketed exclusively throughout the Midwest by such industry giants as the King Iron Bridge and Manufacturing Company and the Wrought Iron Bridge Company, these often-patented bridge forms featured a wide range of span lengths, economical fabrication cost and relatively quick erection. The proliferation of the bowstring corresponded with the initial development of Missouri's road system; as a result, perhaps thousands of these prototypical iron spans erected throughout the state. The bowstring had some rather severe structural flaws, however, relating primarily to lateral stability of the arches, and it was largely superseded by the pin-connected truss in the early 1880s. Despite this, some bowstrings were still erected in Missouri in the 1880s, although the number dwindled precipitously by the decade's end. Through subsequent attrition, almost all of Missouri's bowstrings have been demolished and replaced. Now less than twenty remain in place. The Lick Fork Creek Bridge is thus historically significant as one of the last remaining examples in the state of what was once a mainstay structural type.

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Grindstone Creek Bridge
MHTD: 315003.4

DAVI23

DATE(S) OF CONSTRUCTION

1886

LOCATION

County Road 315 over Grindstone Creek; S15, T60N, R29W
2.9 miles south of Pattonsburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 56)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 120.0'
total length: 143.0'
roadway wdt.: 13.7'

superstructure: wrought iron, 7-panel, pin-connected Pratt through truss, with steel stringer approach span at each end
substructure: concrete-filled iron cylinder piers; timber pile bent abutments
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 round eyerods at the hip); diagonal: 2 punched rectangular eyebars; counter: 2 round eyerods with turnbuckles; lateral bracing: round rod with threaded ends; strut: I-beam; floor beam: I-beam, U-bolted to vertical; guardrail: 1 channel; builder's plate: **MO. VALLEY BRIDGE / & / IRON WORKS / 1886**

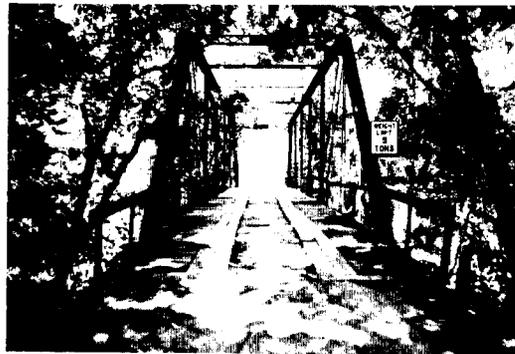
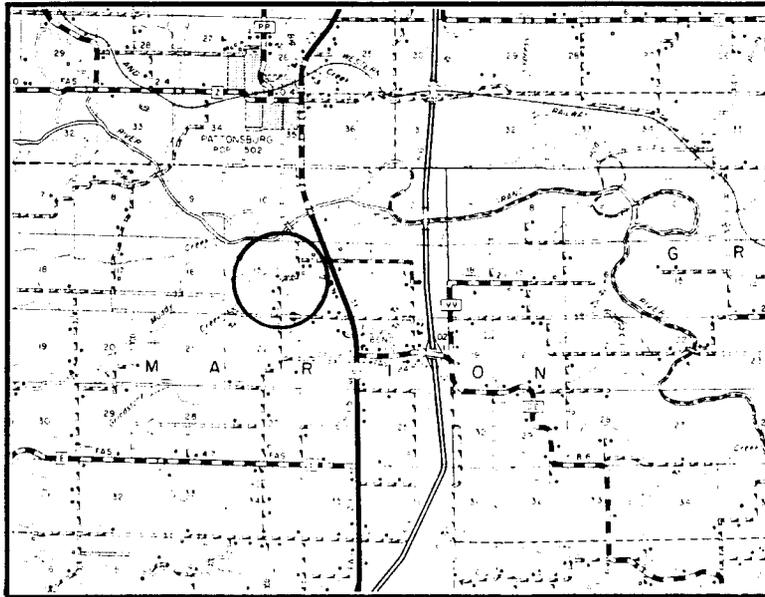
This medium-span truss carries an unpaved county road over Grindstone Creek, south of Pattonsburg in western Daviess County. The structure consists of a single pin-connected Pratt through truss, supported by iron cylinder piers and approached by steel stringers on both sides. County records indicate that a construction contract was let to the Missouri Valley Bridge Company of Leavenworth, Kansas, for the inclusive amount of \$2050.00. The Carnegie Iron Company, based out of Pittsburgh, furnished the wrought iron components for the truss. The bridge, according to contractual agreement, was to be completed on or before February 1886. The Grindstone Creek Bridge has carried inter-county traffic since, with the modification of its guardrails as the only alteration of note.

From the 1870s through the 1910s, thousands of pin-connected Pratt through trusses were erected on Missouri's roadways. Although several hundred of these structures remain in use today, relatively few date from before the turn of the century. And fewer yet of the 19th century survivors remain structurally intact. The Grindstone Creek Bridge stands out among Missouri's pin-connected trusses as an early, fairly well-documented example that has retained a high degree of structural integrity.

NAME(S) OF STRUCTURE

Grindstone Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 315003.4; Daviess County Court Record, Book H: page 453 (2 November 1885)- located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 9 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Grand River Bridge
MHTD: 317000.8

DAVI24

DATE(S) OF CONSTRUCTION

1883

LOCATION

County Road 317 over Grand River; S10, T60N, R29W
1.8 miles south of Pattonsburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP eligible (score: 78)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 180.0'
total length: 331.0'
roadway wdt.: 11.8'

superstructure: wrought iron, 11-panel, pin-connected Whipple through truss; pin-connected Pratt through truss; 2 steel stringer approach spans
substructure: concrete abutments and wingwalls; concrete-filled iron cylinder piers; steel pile bent piers
floor/decking: timber deck over steel stringers
other features: Whipple through truss: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 square rods at the hip); diagonal: 2 punched rectangular eyerods; counter: round rods with turnbuckles; lateral bracing: round rods with turnbuckles; strut: 4 angles with lacing; portal strut: 4 angles with lacing; guardrail: 2 angles; bridge plate: **KANSAS CITY / BRIDGE & IRON CO./ 1883**; Pratt through truss: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 round rods at the hip); diagonal: 2 punched rectangular eyerods; counter: round rods with turnbuckles; lateral bracing: round rods with turnbuckles; strut: 2 channels with spacers; portal strut: 4 angles with lacing; guardrail: 2 angles

This long-span combination Whipple and Pratt through truss carries a gravel-surfaced county road across the Grand River about two miles south of Pattonsburg, in northwestern Daviess County. The Grand River Bridge consists of a pin-connected Whipple through truss and a Pratt through truss supported by steel pile bent piers and concrete-filled steel cylinder piers. The bridge was fabricated and erected by the Kansas City Bridge and Iron Company, using wrought iron components rolled by Carnegie Iron Works Company and Cambria Iron Company, both of Pittsburgh, Pennsylvania. Built at an unknown cost, the bridge was completed in 1883. Barring minor collision damage, the truss retains its historical and structural integrity in its original heavily wooded setting.

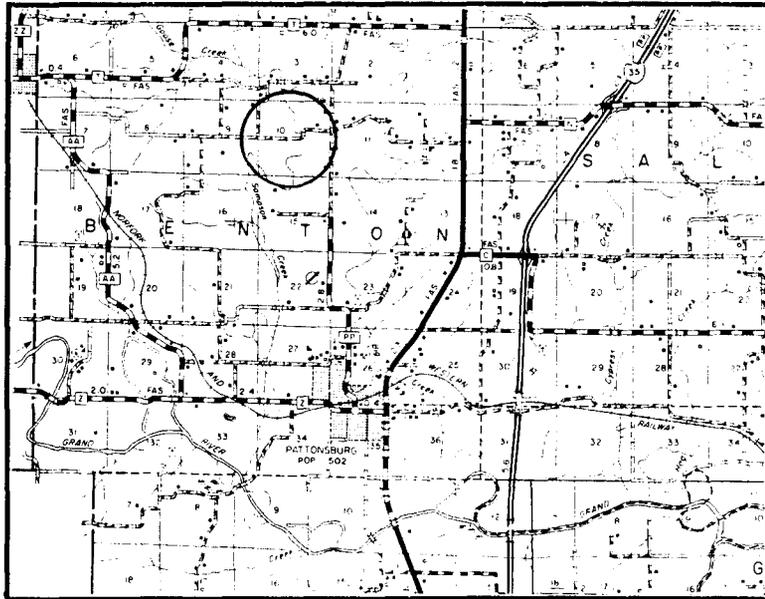
Configured as a pin-connected Whipple (or double-intersection Pratt) through truss, the Grand River Bridge is one of less than ten such structures known to exist in Missouri. First patented by Squire Whipple, bridge engineer and builder from New York, the Whipple truss was a popular choice for long-span crossings between 1850 and 1890. The Whipple truss differed from the more common Pratt in that its diagonal members extended across, not one, but two panels. Although more costly, this variation provided greater lateral support

connection to the floor and lateral systems." Virtually all of the regional bridge fabricators manufactured Pratt trusses and marketed them extensively to Missouri's counties. The John Dildine Bridge Company was a major player in Missouri during the late 19th century, and this structure represents that company's penchant for pinned truss construction. With an erection date of 1892, the Grand River Bridge is distinguished as one of the oldest originally placed Pratt trusses in Missouri. It is thus technologically and historically significant, well-preserved, transportation-related resource, one of the most important of the state's early spans.

NAME(S) OF STRUCTURE

Grand River Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 317000.8; field inspection by Mitzi Rossillon, September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Grand River Bridge
MHTD: 321002.5

DAVI25

DATE(S) OF CONSTRUCTION

1892

LOCATION

County Road 321 over Grand River; S9, T60N, R29W
1.9 miles southwest of Pattonsburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 53)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 178.0'
total length: 395.0'
roadway wdt.: 15.5'

superstructure: wrought iron, 9-panel, pin-connected Pratt through truss; 6 steel stringer approach spans at the west end and 2 steel stringer approach spans at the east end
substructure: steel pile bent piers with timber wingwalls; concrete-filled iron cylinder piers
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars (4 rectangular eyebars on panels 4, 5 and 6); vertical: 2 channels with lacing (2 square eyebars at the hip); diagonal: 2 rectangular eyebars; counter: round rods with turnbuckles; lateral bracing: round rods with threaded ends; strut: 4 angles with lacing; portal strut: 4 angles with lacing; guardrail: 2 channels (cable on east approach span)

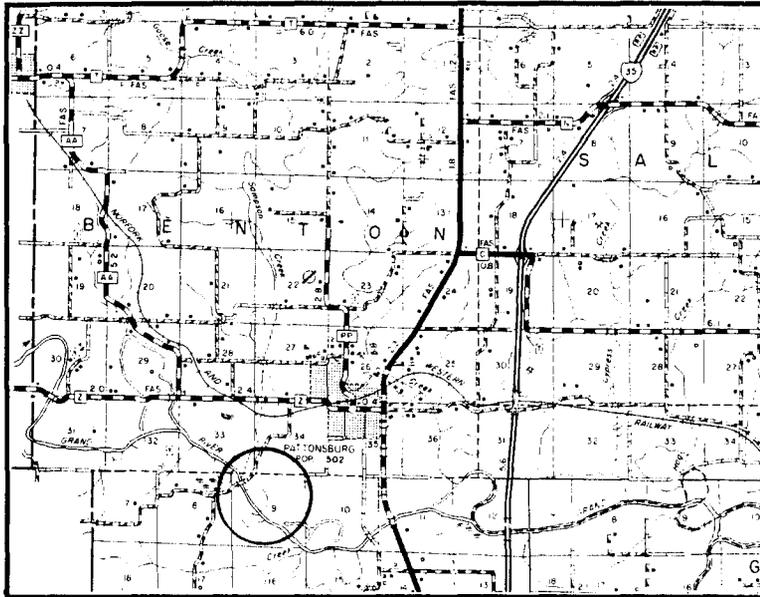
Located near Pattonsburg in western Daviess County, the Grand River Bridge features a pinned Pratt through truss with six steel stringer approach spans on its west end and two steel stringer approach spans on its east end. Supported by steel pile bent and tubular piers, this long-span bridge dates to 1892. The Daviess County Court that year awarded a contract to the John Dildine Bridge Company of Cameron, Missouri, to fabricate and erect the bridge for \$4970.00, which was to be shared equally between subscribers and the county. In 1908, several years after its completion, the county again contracted Dildine to do repair work on the bridge's main span and approaches. One of several pinned Pratt through trusses included in the county's historic bridge inventory, the Grand River Bridge is one of the longest. Its substructure and approach spans have apparently been replaced, but the truss itself remains intact.

In the early 1880s, the pin-connected Pratt truss superseded the bowstring arch-truss as the iron bridge of choice for medium-span wagon crossings. Patented in 1844 by Thomas and Caleb Pratt, the Pratt design is distinguished by vertical members acting in compression and diagonals that act in tension. "The Pratt truss is the type most commonly used in America for spans under two hundred and fifty (250) feet in length," noted bridge engineer J.A.L. Waddell in 1916. "Its advantages are simplicity, economy of metal, and suitability for connection to the floor and lateral systems." Virtually all of the regional bridge fabricators manufactured Pratt trusses and marketed them extensively to Missouri's counties. The John Dildine Bridge Company was a major player in Missouri during the late 19th century, and this structure represents that company's penchant for pinned truss construction. With an erection date of 1892, the Grand River Bridge is distinguished as one of the oldest originally placed Pratt trusses in Missouri. It is thus a technologically and historically significant, transportation-related resource.

NAME(S) OF STRUCTURE

Grand River Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 321002.5; Daviess County Court Record, Book J: page 204 (8 February 1893), page 81 (6 June 1892); Daviess County Court Record, Book N: pages 157-159 (4 February 1908), page 265 (7 December 1908) - located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 12 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Sampson Creek Bridge
MHTD: 329001.1

DAVI26

DATE(S) OF CONSTRUCTION

c1910

LOCATION

County Road 329 over Sampson Creek; S27, T61N, R29W
Pattonsburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 47)

CONDITION

fair

OWNER

Daviess County

span number: 1

span length: 140.0'

total length: 140.0'

roadway wdt.: 14.7'

superstructure: steel, 7-panel, pin-connected Camelback through truss

substructure: concrete abutments and wingwalls

floor/decking: concrete deck over steel stringers

other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars, and 4 angles with batten plates; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: angles; portal strut: channels and angles; guardrail: 2 channels

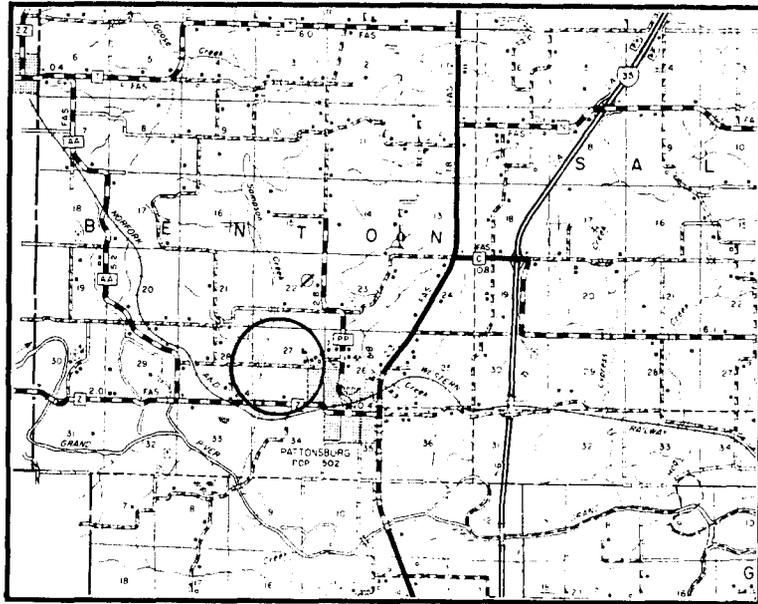
This single-span Camelback through truss over Sampson Creek features a 140-foot span length. Located in northwestern Daviess County in the town of Pattonburg, the Sampson Creek Bridge features a concrete deck over steel stringers and concrete abutments. The bridge construction company of choice in Daviess County, John Dildine Bridge Company of Cameron, Missouri, was probably contracted around 1910 to fabricate and erect the bridge. At some later time the original abutments were removed and new concrete abutments were installed. Other than this minor alteration, the structure remains structurally intact and continues to carry intermittent vehicular traffic.

Straight-chorded Pratt through trusses were used extensively throughout Missouri for medium-span crossings in the late 19th and early 20th centuries. For longer crossings after about the turn of the century, however, bridge companies could develop greater efficiency with polygonal-chorded Pratt variants - primarily Parker, Pennsylvania and Camelback trusses. With its distinctive five-faceted upper chords, the Camelback configuration was disdained by some engineers (including the redoubtable J.A.L. Waddell, who called it "uncompromisingly ugly") for its tendency under certain conditions to reverse compressive and tensile forces acting on their individual members. As a result, Camelback trusses never received widespread acceptance. Relatively few were ever built on Missouri's roads, and less than ten have been identified as extant by the bridge inventory. The Sampson Creek Bridge is thus technologically significant as a well-preserved example of this uncommon structural type.

NAME(S) OF STRUCTURE

Sampson Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 329001.1; field inspection by Mitzi Rossillon 9 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Sampson Creek Bridge
MHTD: 330000.9

DAVI27

DATE(S) OF CONSTRUCTION

1893

LOCATION

County Road 330 over Sampson Creek; S22, T61N, R29W
1.0 mile northwest of Pattonsburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 39)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 100.0'
total length: 132.0'
roadway wdt.: 13.7'

superstructure: wrought iron, 7-panel, pin-connected Pratt through truss, with steel stringer approach span at each end
substructure: steel pile bent piers and abutments
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (2 square eyerods at the hip); diagonal: 2 looped rectangular eyerods; counter: round rods with turnbuckles; lateral bracing: round rods with turnbuckles - top, round rods - bottom; strut: 4 angles with lacing; portal strut: 2 angles; floor beam: I-beam, U-bolted to vertical; guardrail: 2 channels; builders plate: JOHN DILDINE & CO / BRIDGE BUILDERS / CAMERON MO / 1893

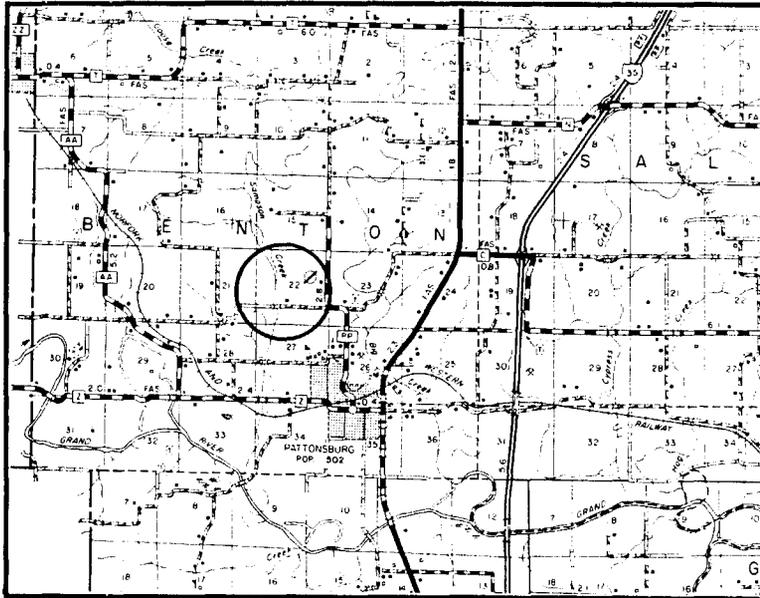
Competitive bids were solicited by the Daviess County Court for a bridge across Sampson Creek in August 1893. After reviewing all the bids, the judges signed a contract with John Dildine Bridge Company of Cameron, Missouri, on September 5, 1893, to fabricate and erect the Pratt through truss. Total cost: \$950.00. Completed in 1893, the Sampson Creek Bridge continues to carry vehicular traffic in northwestern Daviess County. Both county records along with the bridge's contemporary bent piers and approach spans indicate that the truss is not in its original locale. Although documentation does not refer to this relocation specifically, the truss might have been moved from a site over Big Creek near Pattonsburg. The bridge, despite its probable transfer and modern features, retains an average degree of physical integrity. It currently carries daily traffic on County Road 330.

Having long since acquired a sense of time and place at its current location northwest of Pattonsburg, the Sampson Creek Bridge is a well-preserved and fairly well-documented example of a mainstay structural type - the Pratt through truss.

NAME(S) OF STRUCTURE

Sampson Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 330000.9; Daviess County Court Record, Book J: page 276 (7 August 1893), page 303 (5 September 1893); Daviess County Court Record, Book M: page 188 (5 July 1904) - located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 13 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Big Creek Bridge
MHTD: 349003.1

DAVI28

DATE(S) OF CONSTRUCTION

c1910

LOCATION

County Road 349 over Big Creek; S11, T61N, R29W
3.8 miles north of Pattonsburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 33)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 102.0'
total length: 142.0'
roadway wdt.: 15.8'

superstructure: steel, 6-panel, pin-connected Pratt through truss, with steel stringer approach spans at each end
substructure: concrete abutments; steel pile bent piers
floor/decking: timber deck over steel stringers; concrete deck on approach spans
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 rectangular eyebars; vertical: 2 channels with lacing (2 square eyebars at the hip); diagonal: 2 punched rectangular eyebars; counter: round rods with turnbuckles; lateral bracing: round rods with threaded ends; strut: 2 angles; portal strut: A-frame; guardrail: 2 channels

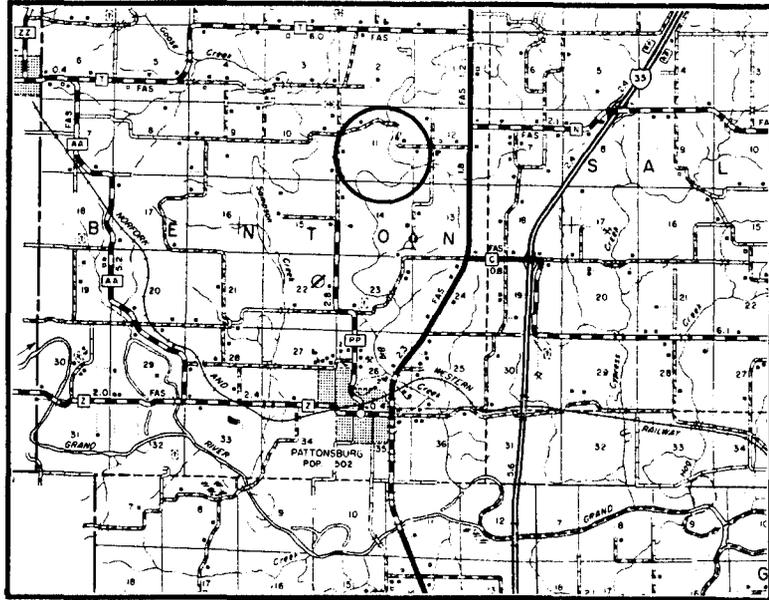
Among the Daviess County bridges in Missouri's historic bridge inventory is this short-span through truss. Located about four miles north of Pattonsburg, the structure carries County Road 349 over Big Creek. The bridge consists of a pinned Pratt through truss with portal A-frame bracing and a timber deck over steel stringers. Two steel stringer spans, supported by steel pile bents, form approaches on each end. Around 1910 the locally popular contractor, John Dildine Bridge Company of Cameron, Missouri, was probably awarded a contract to erect a bridge at this location. The Big Creek Bridge has carried traffic in north-western Daviess County since, with the replacement of its approach spans as well as its pile bents the only alterations of note.

In Missouri the pinned Pratt through truss was the bridge of choice for short- and medium-span applications in the late 19th and early 20th centuries. Most of the structures erected during this period were based on standard plans developed by either the state highway department (after 1917) or by the individual bridge companies, such as the prolific John Dildine Bridge Company, based out of Cameron. As a result, thousands of Pratts were built across the state, all very much alike in detailing, and today the Pratt truss constitutes the most populous group of through trusses. The Big Creek Bridge is a typically configured pin-connected Pratt through truss.

NAME(S) OF STRUCTURE

Big Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 349003.1; field inspection by Mitzi Rossillon, 13 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Big Creek Bridge
MHTD: 359000.6

DAVI31

DATE(S) OF CONSTRUCTION

c1910

LOCATION

County Road 359 over Big Creek; S36, T61N, R29W
immediately east of Pattonsburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 50)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 160.0'
total length: 180.0'
roadway wdt.: 15.7'

superstructure: steel, 8-panel, pin-connected Camelback through truss, with steel stringer approach span at the west end
substructure: concrete abutments, wingwalls and piers
floor/decking: concrete deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (4 angles with batten plates at the hip); diagonal: 2 looped rectangular eyebars; counter: round eyerods with turnbuckles; lateral bracing: round rod with threaded ends; strut: 2 angles; portal strut: 2 angles; guardrail: 2 angles (south side of west approach span is cable)

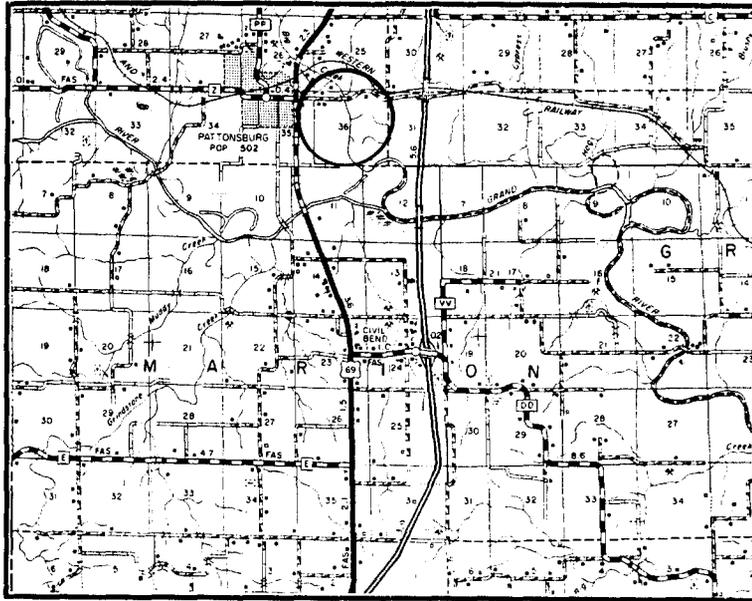
This single-span Camelback through truss over Big Creek features a 160-foot span length. Located in northwestern Daviess County on the edge of Pattonsburg, the Big Creek Bridge features a concrete deck over steel stringers and concrete abutments. The bridge construction company of choice in Daviess County, John Dildine Bridge Company of Cameron, Missouri, was probably contracted around 1910 to fabricate and erect the bridge. At some later time the original abutments were removed and new concrete abutments were installed. Other than this minor alteration, the structure remains structurally intact and continues to carry intermittent vehicular traffic.

Straight-chorded Pratt through trusses were used extensively throughout Missouri for medium-span crossings in the late 19th and early 20th centuries. For longer crossings after about the turn of the century, however, bridge companies could develop greater efficiency with polygonal-chorded Pratt variants - primarily Parker, Pennsylvania and Camelback trusses. With its distinctive five-faceted upper chords, the Camelback configuration was disdained by some engineers (including the redoubtable J.A.L. Waddell, who called it "uncompromisingly ugly") for its tendency under certain conditions to reverse compressive and tensile forces acting on their individual members. As a result, Camelback trusses never received widespread acceptance. Relatively few were ever built on Missouri's roads, and less than ten have been identified as extant by the bridge inventory. The Big Creek Bridge is thus technologically significant as a well-preserved example of this uncommon structural type.

NAME(S) OF STRUCTURE

Big Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES
Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 359000.6; field inspection by Mitzi Rossillon, 9 September 1990.

INVENTORIED BY
Michelle Crow-Dolby

AFFILIATION
Fraserdesign, Loveland CO

DATE
2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Pilot Grove Creek Bridge
MHTD: 422R01.0

DAVI36

DATE(S) OF CONSTRUCTION

1947 (probable)

LOCATION

County Road 422 over Pilot Grove Creek; S28/33, T60N, R27W
3.5 miles southeast of Jameson; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 37)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 60.0'
total length: 114.0'
roadway wdt.: 15.5'

superstructure: steel, 4-panel, rigid-connected, 2-angle Pratt pony truss, with steel stringer approach spans
substructure: timber abutments; steel pile bent piers
floor/decking: timber deck over alternating steel and timber stringers
other features: upper chord and inclined end post: 2 angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; lateral bracing: round rod with threaded ends; guardrail: channel and angle

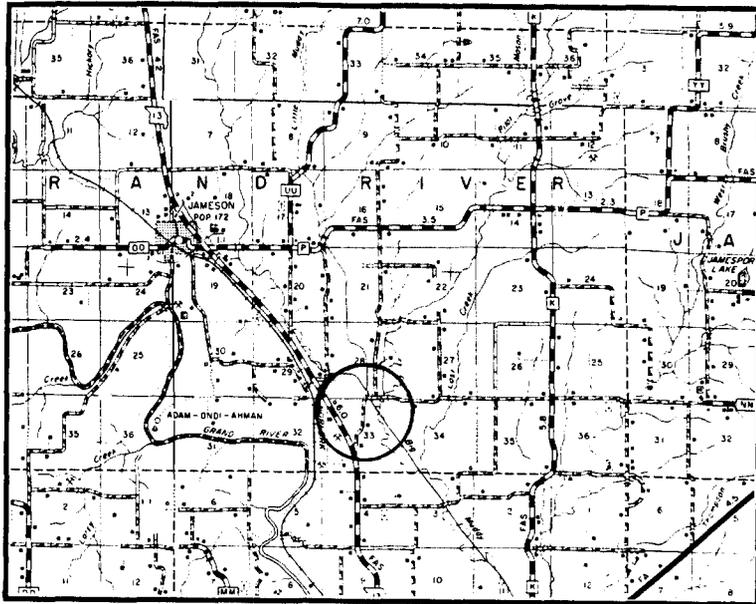
Designed by the Chillicothe Iron Works in Missouri and built by a county crew, this rigid-connected 2-angle Pratt pony truss dates to approximately 1947. The Pilot Grove Creek Bridge is situated in east-central Daviess County southeast of Jameson. The structure, which spans the Pilot Grove Creek, consists of a riveted Pratt pony truss, atypically configured with 2-angle upper chords. The bridge's abutments are not original, but the truss remains otherwise intact. Today, the bridge continues to carry intermittent traffic in its rural setting.

The oldest of the less than two dozen two-angle riveted Pratt pony trusses included in the historic bridge inventory, the poorly documented Pilot Grove Creek Bridge typifies this bridge building trend.

NAME(S) OF STRUCTURE

Pilot Grove Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES
Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 422R01.0; field inspection by Mitzi Rossillon, 13 September 1990.

INVENTORIED BY
Michelle Crow-Dolby

AFFILIATION
Fraserdesign, Loveland CO

DATE
2 April 1993

DEKALB COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
DEKA01	J 777	Grindstone Creek Bridge	12-20' steel stringer 1931 H.C. Botsford
DEKA02	014000.3	Third Fork Bridge	1- 60' pinned Pratt half-hip pony truss c1905 Dildine Bridge Company [prob.]
DEKA03	026001.0	Bridge	1- 43' pinned Pratt bedstead 1901 Dildine Bridge Company
DEKA04	034001.5	Little Third Fork Bridge	1- 30' pinned Pratt half-hip pony truss c1905 Dildine Bridge Company [prob.]
DEKA05	036000.4	Little Third Fork Bridge	1- 60' pinned Pratt half-hip pony truss c1905 Dildine Bridge Company [prob.]
DEKA06	043000.5	Bridge	1- 30' pinned Pratt half-hip pony truss 1902 Dildine Bridge Company
DEKA07	044000.6	Third Fork Bridge	(replaced)
DEKA08	069001.6	Lost Creek Bridge	1- 40' pinned Pratt half-hip pony truss 1907 Dildine Bridge Company
DEKA09	074000.2	Lost Creek Bridge	1- 40' pinned Pratt pony truss 1895 Dildine Bridge Company [prob.]
DEKA10	078000.5	Lost Creek Bridge	(replaced)
DEKA11	081000.6	Lost Creek Bridge	1- 60' pinned Pratt half-hip pony truss 1907 Dildine Bridge Company
DEKA12	082000.1	Lost Creek Bridge	1- 60' pinned Pratt pony truss 1893 Dildine Bridge Company
DEKA13	083000.3	Lost Creek Bridge	1- 40' pinned Pratt pony truss c1900 Dildine Bridge Company [prob.]
DEKA14	108001.1	Lost Creek Bridge	(replaced)
*DEKA15	115000.1	Lost Creek Bridge	1- 40' pinned Pratt half-hip pony truss c1900 Dildine Bridge Company [prob.]
DEKA16	124000.9	Lost Creek Bridge	1- 60' pinned Pratt half-hip pony truss 1904 Dildine Bridge Company
DEKA17	129000.0	Lost Creek Bridge	(replaced)
*DEKA18	131002.5	Grindstone Creek Bridge	1- 25' pinned kingpost pony truss 1901 Dildine Bridge Company
*DEKA19	139000.3	Lost Creek Bridge	1- 60' pinned Pratt half-hip pony truss 1894 Dildine Bridge Company
DEKA20	160000.7	Big Muddy Creek Bridge	1- 50' pinned Pratt pony truss 1904 Dildine Bridge Company
DEKA21	160002.5	Big Muddy Creek Bridge	1- 37' pinned Pratt half-hip pony truss c1910 Dildine Bridge Company [prob.]
*DEKA22	177000.5	Grindstone Creek Bridge	1- 60' 2-angle Pratt pony truss c1925
*DEKA23	177000.8	Grindstone Creek Bridge	1-100' pinned Pratt through truss 1899 Dildine Bridge Company
*DEKA24	193001.1	Lost Creek Bridge	1- 80' pinned Pratt through truss 1894 Dildine Bridge Company

DEKALB COUNTY

INCLUDED (cont.):

DEKA25	195001.5	Branscombe Bridge	1- 60'	pinned Pratt pony truss
			1903	Dildine Bridge Company
DEKA26	196000.1	Lost Creek Bridge	1- 40'	pinned Pratt half-hip pony truss
			1898	Dildine Bridge Company
DEKA27	211001.1	Grindstone Creek Bridge	1- 64'	pinned Pratt pony truss
			c1910	Dildine Bridge Company [prob.]
DEKA28	215003.7	Grindstone Creek Bridge		(replaced)
DEKA29	215004.2	Grindstone Creek Bridge		(replaced)
DEKA30	220001.1	Lost Creek Bridge	1- 70'	pinned Pratt pony truss
			1900	Dildine Bridge Company
DEKA31	227000.7	Grindstone Creek Bridge		(replaced)
DEKA32	241000.9	Lost Creek Bridge	1- 60'	pinned Pratt pony truss
			1911	Dildine Bridge Company
DEKA33	260000.9	Bridge		(replaced)
DEKA34	267000.2	Castile Creek Bridge	1- 60'	pinned Pratt pony truss
			c1910	Dildine Bridge Company [prob.]
*DEKA35	278000.8	Lost Creek Bridge		(replaced)
*DEKA36	296000.8	Bridge		(replaced)
DEKA37	298000.1	Little Third Fork Bridge	1- 40'	pinned Pratt half-hip pony truss
			1895	Dildine Bridge Company
DEKA38	299000.3	Little Third Fork Bridge	1- 60'	pinned Pratt half-hip pony truss
			1894	Dildine Bridge Company
DEKA39	304000.8	Little Third Fork Bridge	1- 60'	pinned Pratt half-hip pony truss
			1901	Dildine Bridge Company
DEKA40	305000.6	Little Third Fork Bridge	1- 60'	pinned Pratt half-hip pony truss
			1898	Dildine Bridge Company
DEKA41	312000.4	Third Fork Bridge		(replaced)
DEKA42	322001.1	Morgan Branch Bridge		(replaced)
DEKA43	322002.2	Little Third Fork Bridge		(replaced)
DEKA44	328001.3	Morgan Branch Bridge	1- 40'	pinned Pratt half-hip pony truss
			c1905	Dildine Bridge Company [prob.]
DEKA45	328001.9	Little Third Fork Bridge	1- 64'	pinned Pratt pony truss
			c1910	Dildine Bridge Company [prob.]
*DEKA46	330000.4	Bridge		(replaced)
DEKA47	336000.8	Third Fork Bridge	1- 60'	pinned Pratt half-hip pony truss
			1899	Dildine Bridge Company
DEKA48	338001.3	Bridge		(replaced)
DEKA49	342000.4	Third Fork Bridge		(replaced)
DEKA50	347000.5	Third Fork Bridge		(replaced)
DEKA51	356000.2	Little Third Fork Bridge	1- 70'	pinned Pratt pony truss
			c1910	Dildine Bridge Company [prob.]
DEKA52	376000.4	Jordan Creek Bridge	1- 35'	pinned Pratt half-hip pony truss
			1893	Dildine Bridge Company
DEKA53	380000.2	Bridge	1- 30'	pinned Pratt half-hip pony truss
			c1905	Dildine Bridge Company [prob.]

DEKALB COUNTY

INCLUDED (cont.):

DEKA54	391000.9	Castile Creek Bridge	1- 40'	pinned Pratt half-hip pony truss
			1895	Dildine Bridge Company
DEKA55	391001.1	Bover Bridge	1- 40'	pinned Pratt half-hip pony truss
			c1900	Dildine Bridge Company [prob.]
DEKA56	391001.7	Castile Creek Bridge	1- 30'	pinned Pratt half-hip pony truss
			1893	Dildine Bridge Company
DEKA57	422000.4	Lost Creek Bridge		(replaced)
DEKA58	453000.9	Wamsley Creek Bridge		(replaced)
*DEKA59	458001.6	Grindstone Creek Bridge		(replaced)
DEKA60	463000.7	Wamsley Creek Bridge		(replaced)
DEKA61	464000.1	Wamsley Creek Bridge		(replaced)
DEKA62	479000.2	Bridge		(replaced)
DEKA63	480001.2	Brushy Creek Bridge		(replaced)

EXCLUDED:

Warren bedstead

005001.3	008000.9	008001.2	043000.3	052000.4	092000.6	124000.2
199000.6	248000.6	275000.5	354000.7	373000.2	423003.3	

Lattice bedstead

056003.0

Steel stringer

G 538R	J 674	J 675	J 776	J 777	L 165	S 419
T 641	X 138	X 139	X 140	X 473	013001.2	027001.8
028001.0	056001.5	059000.1	060001.1	060002.7	060002.8	064000.1
082000.5	085000.8	085001.9	088001.5	092R01.9	094000.9	099000.5
109000.3	112001.9	120002.3	131002.3	141001.8	156001.8	162000.4
163001.0	167000.7	168000.7	176000.2	177001.4	184000.1	205000.4
216000.3	226000.1	236000.3	255000.9	257000.5	259000.9	278001.1
280000.0	295002.1	316000.4	316002.0	317000.8	322000.3	331002.1
345001.8	346000.9	364001.0	364001.1	367001.3	375001.0	377001.5
397001.7	405000.9	436000.7	449000.2	451001.1	458003.6	460000.1
487000.1						

Steel girder

018000.6	071000.1	093000.4	101003.2	140000.1	178001.3	181000.1
239000.8						

Concrete girder

H 91R	J 605	J 700	443001.5
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Concrete box culvert

G 690R	J 247	P 25	328002.6
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DEKALB COUNTY

SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	1	39	0	0	40
Excluded	19	84	0	0	103
<hr/>					
Total	20	123	0	0	143

Grindstone Creek Bridge

DEKA01

GENERAL DATA

structure no.:	J 777	city/town:	1.5 miles east of Weatherby
county:	DeKalb	feature inters.:	Grindstone Creek
		cadastral grid:	S24, T59N, R30W
		highway route:	State Highway 6
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	steel stringer		
substructure:	timber pile bent abutments, wingwalls and piers		
span number:	12	condition:	fair
span length:	20.0'	alterations:	none
total length:	243.0'	floor/decking :	concrete deck
roadway width:	20.0'	other features:	steel pipe guardrails

HISTORICAL DATA

erection date:	1931
erection cost:	\$8483.96
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	H.C. Botsford
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number J 777; files on Primary System Bridges - located at the Missouri Highway and Transportation Department, Jefferson City MO.
sign. rating:	42
evaluation:	NRHP non-eligible (typical example of common MSHD bridge configuration, distinguished somewhat by its multiplicity of spans)

Inventoried by: Michelle Crow-Dolby 6 April 1993

Third Fork Bridge

DEKA02

GENERAL DATA

structure no.:	014000.3	city/town:	1.8 miles north of Union Star
county:	DeKalb	feature inters.:	Third Fork of Platte River
		cadastral grid:	S23, T60N, R33W
		highway route:	County Road 14
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.0'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 014000.3.
sign. rating:	35
evaluation:	NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby 6 April 1993

Bridge

DEKA03

GENERAL DATA

structure no.: 026001.0 city/town: 6.3 miles west of Fairport
county: DeKalb feature inters.: branch of Lost Creek
cadastral grid: S14, T60N, R32W
highway route: County Road 26
highway distr.: 1
current owner: DeKalb County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt bedstead
substructure: concrete abutments and wingwalls

span number: 1 condition: fair
span length: 43.0' alterations: substructure replaced
total length: 43.0' floor/decking : concrete deck
roadway width: 13.7' other features: steel angle guardrails

HISTORICAL DATA

erection date: 1901
erection cost: \$300.00 (contract amount)
designer: Dildine Bridge Company, Cameron MO (probable)
fabricator : unknown
contractor: Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 026001.0; DeKalb County Court Record, Book 5: page 166 (4 February 1901), page 173 (7 February 1901) - located at the DeKalb County Courthouse, Maysville MO.

sign. rating: 32
evaluation: NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby 6 April 1993

Little Third Fork Bridge

DEKA04

GENERAL DATA

structure no.: 034001.5 city/town: 5.3 miles east of Union Star
county: DeKalb feature inters.: Little Third Fork
cadastral grid: S21/28, T60N, R32W
highway route: County Road 34
highway distr.: 1
current owner: DeKalb County

STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss
substructure: concrete abutments and wingwalls

span number: 1 condition: fair
span length: 30.0' alterations: substructure replaced
total length: 30.0' floor/decking : timber deck
roadway width: 13.7' other features: steel angle guardrails

HISTORICAL DATA

erection date: c1905
erection cost: unknown
designer: Dildine Bridge Company, Cameron MO (probable)
fabricator : unknown
contractor: Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 034001.5.

sign. rating: 27
evaluation: NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby 6 April 1993

Little Third Fork Bridge

DEKA05

GENERAL DATA

structure no.:	036000.4	city/town:	5.6 miles east of Union Star
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S27/28, T60N, R32W
		highway route:	County Road 36
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss
substructure: unknown

span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date: c1905
erection cost: unknown
designer: Dildine Bridge Company, Cameron MO (probable)
fabricator : unknown
contractor: Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 036000.4.

sign. rating: 35
evaluation: NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby 6 April 1993

Bridge

DEKA06

GENERAL DATA

structure no.:	043000.5	city/town:	2.1 miles south of Union Star
county:	DeKalb	feature inters.:	branch of Third Fork of Platte River
		cadastral grid:	S3/10, T59N, R33W
		highway route:	County Road 43
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	30.0'	alterations:	none
total length:	30.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1902
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 043000.5; DeKalb County Warrant Register, Bridges: Dildine (1902) - located at the DeKalb County Court-house, Maysville MO.
sign. rating:	32
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby 6 April 1993

Lost Creek Bridge

DEKA08

GENERAL DATA

structure no.: 069001.6	city/town: 4.9 miles west of Fairport
county: DeKalb	feature inters.: Lost Creek
	cadastral grid: S25, T60N, R32W
	highway route: County Road 69
	highway distr.: 1
	current owner: DeKalb County

STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss	
substructure: steel pile bent piers with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: substructure replaced
total length: 54.0'	floor/decking : timber deck over timber stringers
roadway width: 11.9'	other features: steel angle guardrails

HISTORICAL DATA

erection date: 1907	
erection cost: unknown	
designer: Dildine Bridge Company, Cameron MO (probable)	
fabricator : unknown	
contractor: Dildine Bridge Company, Cameron MO	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 069001.6; DeKalb County Court Record, Book 5: page 603 (9 April 1908) - located at the DeKalb County Courthouse, Maysville MO.	
sign. rating: 37	
evaluation: NRHP non-eligible (typically configured example of common structural type)	

inventoried by: Michelle Crow-Dolby 6 April 1993

Lost Creek Bridge

DEKA09

GENERAL DATA

structure no.: 074000.2 city/town: 2.2 miles north of Winslow
county: DeKalb / Gentry feature inters.: Lost Creek
cadastral grid: S1/12, T60/61N, R32W
highway route: County Road 74
highway distr.: 1
current owner: DeKalb County / Gentry County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss
substructure: steel pile bent piers with timber wingwalls

span number: 1 condition: fair
span length: 40.0' alterations: substructure replaced
total length: 43.0' floor/decking : timber deck
roadway width: 15.8' other features: steel angle guardrails

HISTORICAL DATA

erection date: 1895
erection cost: \$300.00 (contract amount, divided equally between DeKalb and Gentry Counties)
designer: Dildine Bridge Company, Cameron MO (probable)
fabricator : unknown
contractor: Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 074000.2; Gentry County Court Record, Book I, page 233 (3 September 1895) - located at Gentry County Courthouse, Albany MO.

sign. rating: 34
evaluation: NRHP non-eligible (typically configured example of common structural type)

inventoried by: Clayton B. Fraser 6 April 1993

Lost Creek Bridge

DEKA11

GENERAL DATA

structure no.:	081000.6	city/town:	5.8 miles north of Amity
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S1/12, T59N, R32W
		highway route:	County Road 81
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.0'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1907
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 081000.6; DeKalb County Court Record, Book 5: page 580 (7 October 1907), page 581 (8 October 1907) - located at the Dekalb County Courthouse, Maysville MO.
sign. rating:	40
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby 6 April 1993

Lost Creek Bridge

DEKA12

GENERAL DATA

structure no.:	082000.1	city/town:	5.1 miles north of Amity
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S7/12, T59N, R31/32W
		highway route:	County Road 82
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	wrought iron or steel, 3-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled iron cylinder piers		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	69.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1892-93
erection cost:	\$885.00
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 082000.1; DeKalb County Court Record, Book 4: page 165 (26 September 1892), pages 177-78 (6 December 1892) - located at the Dekalb County Courthouse, Maysville MO.
sign. rating:	48
evaluation:	NRHP possibly eligible (typically configured, well-preserved example of common structural type)

inventoried by: Clayton B. Fraser 6 April 1993

Lost Creek Bridge

DEKA13

GENERAL DATA

structure no.:	083000.3	city/town:	5.0 miles north of Amity
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S7, T59N, R31W
		highway route:	County Road 83
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss with outriders
substructure: unknown

span number:	1	condition:	fair
span length:	40.0'	alterations:	none
total length:	67.0'	floor/decking :	timber deck
roadway width:	13.5'	other features:	steel angle guardrails

HISTORICAL DATA

erection date: c1900
erection cost: unknown
designer: Dildine Bridge Company, Cameron MO (probable)
fabricator : unknown
contractor: Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 083000.3; Warrant Register, Bridges: Dildine (1900) - located at the DeKalb County Courthouse, Maysville MO.

sign. rating: 36
evaluation: NRHP non-eligible (largely undocumented example of common structural type)

Inventoried by: Michelle Crow-Dolby 6 April 1993

Lost Creek Bridge

DEKA15

GENERAL DATA

structure no.: 115000.1	city/town: 2.4 miles north of Fairport
county: DeKalb	feature inters.: North Fork of Lost Creek
	cadastral grid: S13/14, T60N, R31W
	highway route: County Road 115
	highway distr.: 1
	current owner: DeKalb County

STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt pony truss with laced endposts
substructure: concrete abutments and wingwalls

span number: 1	condition: fair
span length: 40.0'	alterations: substructure replaced
total length: 40.0'	floor/decking : timber deck over steel stringers
roadway width: 13.4'	other features: upper chord and inclined end post: 2 channels with top and bottom lacing; lower chord: 2 looped square eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

HISTORICAL DATA

erection date: c1900
erection cost: unknown
designer: Dildine Bridge Company, Cameron MO (probable)
fabricator : unknown
contractor: Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 115000.1; field inspection by Mitzi Rossillon, 8 September 1990.

sign. rating: 29
evaluation: NRHP non-eligible (atypically configured example of common structural type, poorly documented and possibly moved to this location)

inventoried by: Michelle Crow-Dolby 6 April 1993

Lost Creek Bridge

DEKA16

GENERAL DATA

structure no.:	124000.9	city/town:	2.7 miles south of Fairport
county:	DeKalb	feature inters.:	North Fork of Lost Creek
		cadastral grid:	S1/12, T59N, R31W
		highway route:	County Road 124
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1904
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 124000.9; DeKalb County Court Record, Book 5: page 328 (2 March 1903); Warrant Register, Bridges: Dildine (1904) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	40
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby 6 April 1993

Grindstone Creek Bridge

DEKA18

GENERAL DATA

structure no.:	131002.5	city/town:	3.2 miles northwest of Weatherby
county:	DeKalb	feature inters.:	branch of Grindstone Creek
		cadastral grid:	S8/9, T59N, R30W
		highway route:	County Road 131
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, pin-connected kingpost pony truss		
substructure:	steel pile bent abutments embedded in concrete		
span number:	1	condition:	fair
span length:	25.0'	alterations:	none
total length:	25.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.3'	other features:	inclined end post: I-beam; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with batten plates; lateral bracing: round rod with threaded ends; guardrail: 2 angles

HISTORICAL DATA

erection date:	1900-01
erection cost:	\$300.00 (contract amount)
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	Carnegie Steel Company, Pittsburgh PA
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 131002.5; DeKalb County Court Record, Book 5: page 125 (9 May 1900), page 166 (4 February 1901), page 197 (3 April 1901) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.
sign. rating:	57
evaluation:	NRHP possibly eligible (well-preserved example of once-mainstay, now rare structural type)

Inventoried by: Clayton B. Fraser 6 April 1993

Lost Creek Bridge

DEKA19

GENERAL DATA

structure no.:	139000.3	city/town:	1.8 miles east of Fairport
county:	DeKalb	feature inters.:	North Fork of Lost Creek
		cadastral grid:	S19, T60N, R30W
		highway route:	County Road 139
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss with laced ends; steel stringer approach span at east end		
substructure:	concrete abutments; steel pile bent pier (braced with angles)		
span number:	1	condition:	fair
span length:	60.0'	alterations:	original tubular-pier substructure replaced
total length:	76.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.7'	other features:	upper chord: 2 channels with top and bottom lacing, continuous and batten plates; inclined end post: 2 channels with top and bottom lacing; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

HISTORICAL DATA

erection date:	1894
erection cost:	\$871.00
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 139000.3; DeKalb County Court Record, Book 4: page 228 (8 August 1893), page 240 (6 November 1893) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.
sign. rating:	51
evaluation:	NRHP possibly eligible (atypically configured, relatively early example of mainstay structural type)

inventoried by: Clayton B. Fraser 6 April 1993

Big Muddy Creek Bridge

DEKA20

GENERAL DATA

structure no.:	160000.7	city/town:	2.3 miles west of Santa Rosa
county:	DeKalb	feature inters.:	Big Muddy Creek
		cadastral grid:	S22/27, T60N, R30W
		highway route:	County Road 160
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	substructure replaced
total length:	50.0'	floor/decking :	timber deck
roadway width:	11.5'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1904
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 160000.7; DeKalb County Court Record, Book 5: page 397 (4 May 1904) -located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	37
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby 6 April 1993

Big Muddy Creek Bridge

DEKA21

GENERAL DATA

structure no.:	160002.5	city/town:	1.0 mile north of Santa Rosa
county:	DeKalb	feature inters.:	Big Muddy Creek
		cadastral grid:	S24/25, T60N, R30W
		highway route:	County Road 160
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent abutments with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	37.0'	alterations:	substructure replaced
total length:	37.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 160002.5.
sign. rating:	30
evaluation:	NRHP non-eligible (typically configured, small-scale example of common structural type)

Inventoried by: Michelle Crow-Dolby 6 April 1993

Grindstone Creek Bridge

DEKA22

GENERAL DATA

structure no.:	177000.5	city/town:	2.7 miles south of Santa Rosa
county:	DeKalb	feature inters.:	Grindstone Creek
		cadastral grid:	S12, T59N, R30W
		highway route:	County Road 177
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure: steel, 4-panel, rigid-connected, 2-angle Pratt pony truss
substructure: concrete abutments with timber wingwalls

span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	60.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.7'	other features:	upper chord and inclined end post: 2 angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 1 angle; lateral bracing: round rod with threaded ends; guardrail: 2 angles

HISTORICAL DATA

erection date: c1925
erection cost: unknown
designer: unknown
fabricator : Inland Steel Company, East Chicago IN
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 177000.5; DeKalb County Court Record, Book 5: page 49 (5 June 1899); Warrant Register, Bridges: Dildine (1899) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.

sign. rating: 34
evaluation: NRHP non-eligible (uncommon structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 6 April 1993

Grindstone Creek Bridge

DEKA23

GENERAL DATA

structure no.:	177000.8	city/town:	2.7 miles south of Santa Rosa
county:	DeKalb	feature inters.:	Grindstone Creek
		cadastral grid:	S12, T59N, R30W
		highway route:	County Road 177
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt through truss, with steel stringer approach span at each end		
substructure:	concrete abutments and concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	100.0'	alterations:	none
total length:	170.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.5'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: round rod with turn-buckle; lateral bracing: round rod with threaded ends; strut: lattice; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

HISTORICAL DATA

erection date:	1899
erection cost:	\$1750.00 (contract amount)
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 177000.8; DeKalb County Court Record, Book 5: page 49 (5 June 1899); Warrant Register, Bridges: Dildine (1899) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.
sign. rating:	45
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Clayton B. Fraser and Michelle Crow-Dolby 6 April 1993

Lost Creek Bridge

DEKA24

GENERAL DATA

structure no.: 193001.1	city/town: 2.3 miles west of Weatherby
county: DeKalb	feature inters.: Lost Creek
	cadastral grid: S28/29, T59N, R30W
	highway route: County Road 193
	highway distr.: 1
	current owner: DeKalb County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt through truss, with laced ends; steel stringer approach span	
substructure: concrete-filled steel cylinder piers; timber pile bent abutments with timber back- and wingwalls	
span number: 1	condition: fair
span length: 80.0'	alterations: none
total length: 98.0'	floor/decking : timber deck over steel stringers
roadway width: 11.7'	other features: upper chord and inclined end post: 2 channels with top and bottom lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 square eyebars at the hip); diagonal: 2 looped square eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with turnbuckles (top) and round rods with threaded ends (bottom); strut: angles with lacing; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

HISTORICAL DATA

erection date: 1894	
erection cost: \$1487.00 (contract amount)	
designer: Dildine Bridge Company, Cameron MO (probable)	
fabricator : unknown	
contractor: Dildine Bridge Company, Cameron MO	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 193001.1; DeKalb County Courthouse Record, Book 4: page 264 (8 March 1894), page 275 (8 May 1894) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.	
sign. rating: 52	
evaluation: NRHP possibly eligible (well-preserved, relatively early example of main-stay structural type)	

inventoried by: Clayton B. Fraser and Michelle Crow-Dolby 6 April 1993

Branscombe Bridge

DEKA25

GENERAL DATA

structure no.:	195001.5	city/town:	3.7 miles northeast of Maysville
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S19/20, T59N, R30W
		highway route:	County Road 195
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers with timber abutments and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	187.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1903
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor :	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 195001.5; DeKalb County Court Record, Book 5: page 150 (9 October 1900), page 306 (6 November 1902), page 320 (3 February 1903), page 328 (2 March 1903) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	40
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Clayton B. Fraser and Michelle Crow-Dolby 6 April 1993

Lost Creek Bridge

DEKA26

GENERAL DATA

structure no.:	196000.1	city/town:	3.2 miles northeast of Maysville
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S19, T59N, R30W
		highway route:	County Road 196
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	substructure replaced
total length:	78.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1898
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 196000.1; DeKalb County Court Record, Book 4: page 429 (17 August 1897), page 434 (6 September 1897), page 467 (2 May 1898) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	39
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Clayton B. Fraser and Michelle Crow-Dolby 6 April 1993

Grindstone Creek Bridge

DEKA27

GENERAL DATA

structure no.: 211001.1	city/town: 4.3 miles south of Weatherby
county: DeKalb	feature inters.: Grindstone Creek
	cadastral grid: S10, T58N, R30W
	highway route: County Road 211
	highway distr.: 1
	current owner: DeKalb County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 64.0'	alterations: substructure replaced
total length: 110.0'	floor/decking : timber deck
roadway width: 11.8'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: Dildine Bridge Company, Cameron MO (probable)	
fabricator : unknown	
contractor: Dildine Bridge Company, Cameron MO (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 211001.1.	
sign. rating: 34	
evaluation: NRHP non-eligible (typically configured example of common structural type, largely undocumented)	

inventoried by: Michelle Crow-Dolby 6 April 1993

Lost Creek Bridge

DEKA30

GENERAL DATA

structure no.:	220001.1	city/town:	5.2 miles east of Maysville
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S27/34, T59N, R30W
		highway route:	County Road 220
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	70.0'	alterations:	substructure replaced
total length:	137.0'	floor/decking :	timber deck
roadway width:	11.5'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1900
erection cost:	\$1080.00 (contract amount)
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 220001.1; DeKalb County Court Record, Book 5: page 74 (10 October 1899), page 110 (2 April 1900); Warrant Register, Bridges: Dildine (1900) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	40
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby 6 April 1993

Lost Creek Bridge

DEKA32

GENERAL DATA

structure no.:	241000.9	city/town:	1.4 miles southeast of Maysville
county:	DeKalb	feature inters.:	West Fork of Lost Creek
		cadastral grid:	S2, T58N, R31W
		highway route:	County Road 241
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1911
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor :	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 241000.9; DeKalb County Court Record, Book 6: page 99 (7 November 1910) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	34
evaluation:	NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Clayton B. Fraser and Michelle Crow-Dolby 6 April 1993

Castile Creek Bridge

DEKA34

GENERAL DATA

structure no.:	267000.2	city/town:	2.3 miles south of Amity
county:	DeKalb	feature inters.:	Castile Creek
		cadastral grid:	S13/24, T58N, R32W
		highway route:	County Road 267
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.4'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 267000.2.
sign. rating:	38
evaluation:	NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby 6 April 1993

Little Third Fork Bridge

DEKA37

GENERAL DATA

structure no.: 298000.1	city/town: 5.2 miles northwest of Amity
county: DeKalb	feature inters.: Little Third Fork
	cadastral grid: S15, T59N, R32W
	highway route: County Road 298
	highway distr.: 1
	current owner: DeKalb County

STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss	
substructure: steel pile bent piers with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: substructure replaced
total length: 40.0'	floor/decking : timber deck
roadway width: 11.2'	other features: steel angle guardrails

HISTORICAL DATA

erection date: 1895
erection cost: unknown
designer: Dildine Bridge Company, Cameron MO (probable)
fabricator : unknown
contractor: Dildine Bridge Company, Cameron MO
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 298000.1; DeKalb County Court Record, Book 4: page 332 (6 May 1895) - located at the DeKalb County Courthouse, Maysville MO..
sign. rating: 46
evaluation: NRHP possibly eligible (typically configured, small-scale example of mainstay structural type)

inventoried by: Clayton B. Fraser and Michelle Crow-Dolby 6 April 1993

Little Third Fork Bridge

DEKA38

GENERAL DATA

structure no.:	299000.3	city/town:	5.2 miles northwest of Amity
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S15/16, T59N, R32W
		highway route:	County Road 299
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1893-94
erection cost:	\$784.00
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 299000.3; DeKalb County Court Record, Book 4: page 240 (6 November 1893) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	49
evaluation:	NRHP possibly eligible (relatively early example of mainstay structural type)

Inventoried by: Clayton B. Fraser 6 April 1993

Little Third Fork Bridge

DEKA39

GENERAL DATA

structure no.:	304000.8	city/town:	4.2 miles northwest of Amity
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S21/28, T59N, R32W
		highway route:	County Road 304
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss
substructure: steel pile bent piers with timber wingwalls

span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date: 1901
erection cost: \$700.00 (contract amount)
designer: Dildine Bridge Company, Cameron MO (possible)
fabricator : unknown
contractor: Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 304000.8; DeKalb County Court Record, Book 5: page 166 (4 February 1901), page 173 (7 February 1901) - located at the DeKalb County Courthouse, Maysville MO.

sign. rating: 40
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 6 April 1993

Little Third Fork Bridge

DEKA40

GENERAL DATA

structure no.:	305000.6	city/town:	3.8 miles northwest of Amity
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S28, T59N, R32W
		highway route:	County Road 305
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.5'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1898
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 305000.6; DeKalb County Court Record, Book 4: page 429 (17 August 1897), page 434 (6 September 1897), page 467 (2 May 1898) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	42
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Clayton B. Fraser and Michelle Crow-Dolby 6 April 1993

Morgan Branch Bridge

DEKA44

GENERAL DATA

structure no.:	328001.3	city/town:	3.0 miles northeast of Clarksdale
county:	DeKalb	feature inters.:	Morgan Branch
		cadastral grid:	S17, T58N, R32W
		highway route:	County Road 328
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent pier and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	substructure replaced
total length:	62.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 328001.3.
sign. rating:	32
evaluation:	NRHP non-eligible (typical, small-scale example of common structural type, largely undocumented)

Inventoried by: Michelle Crow-Dolby 6 April 1993

Little Third Fork Bridge

DEKA45

GENERAL DATA

structure no.:	328001.9	city/town:	4.2 miles west of Amity
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S8/17, T58N, R32W
		highway route:	County Road 328
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	64.0'	alterations:	substructure replaced
total length:	64.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 328001.9.
sign. rating:	34
evaluation:	NRHP non-eligible (typically configured example of common structural type, largely undocumented)

Inventoried by: Michelle Crow-Dolby 6 April 1993

Third Fork Bridge

DEKA47

GENERAL DATA

structure no.:	336000.8	city/town:	4.8 miles northwest of Clarksdale
county:	DeKalb	feature inters.:	Third Fork of Platte River
		cadastral grid:	S3, T58N, R33W
		highway route:	County Road 336
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced; bridge closed
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1899
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 336000.8; DeKalb County Court Record, Book 5: page 8 (11 November 1898) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	42
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Clayton B. Fraser and Michelle Crow-Dolby 6 April 1993

Little Third Fork Bridge

DEKA51

GENERAL DATA

structure no.:	356000.2	city/town:	immediately southeast of Clarksdale
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S25, T58N, R33W
		highway route:	County Road 356
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	70.0'	alterations:	unknown
total length:	70.0'	floor/decking :	concrete deck
roadway width:	15.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 356000.2.
sign. rating:	38
evaluation:	NRHP non-eligible (typical example of common structural type, inadequately documented)

inventoried by: Michelle Crow-Dolby 6 April 1993

Jordan Creek Bridge

DEKA52

GENERAL DATA

structure no.:	376000.4	city/town:	4.0 miles southwest of Clarksdale
county:	DeKalb	feature inters.:	branch of Jordan Creek
		cadastral grid:	S10, T57N, R33W
		highway route:	County Road 376
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss with laced end-posts		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	35.0'	alterations:	substructure replaced
total length:	35.0'	floor/decking :	timber deck
roadway width:	13.4'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1893
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 376000.4; DeKalb County Court Record, Book 4: page 165 (26 September 1892), pages 177-78 (6 December 1892), page 227 (7 August 1893) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	52
evaluation:	NRHP possibly eligible (relatively early, atypically configured example of mainstay structural type)

inventoried by: Clayton B. Fraser 6 April 1993

Bridge

DEKA53

GENERAL DATA

structure no.:	380000.2	city/town:	4.0 miles southwest of Clarksdale
county:	DeKalb	feature inters.:	branch of Jordan Creek
		cadastral grid:	S10, T57N, R33W
		highway route:	County Road 380
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	30.0'	alterations:	substructure replaced
total length:	30.0'	floor/decking :	timber deck
roadway width:	11.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 380000.2.
sign. rating:	27
evaluation:	NRHP non-eligible (typically configured, small-scale poorly documented example of common structural type)

inventoried by: Michelle Crow-Dolby 6 April 1993

Castile Creek Bridge

DEKA54

GENERAL DATA

structure no.: 391000.9	city/town: 3.0 miles northeast of Stewartville
county: DeKalb	feature inters.: Castile Creek
	cadastral grid: S3, T57N, R32W
	highway route: County Road 391
	highway distr.: 1
	current owner: DeKalb County

STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss, with steel stringer approach spans	
substructure: steel pile bent pier and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: substructure replaced
total length: 65.0'	floor/decking : timber deck
roadway width: 11.4'	other features: steel angle guardrails

HISTORICAL DATA

erection date: 1895	
erection cost: \$357.00 (contract amount)	
designer: Dildine Bridge Company, Cameron MO (probable)	
fabricator : unknown	
contractor: Dildine Bridge Company, Cameron MO	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 391000.9; DeKalb County Court Record, Book 4: page 354 (14 October 1895) - located at the DeKalb County Courthouse, Maysville MO.	
sign. rating: 46	
evaluation: NRHP possibly eligible (relatively early example of mainstay structural type)	

inventoried by: Clayton B. Fraser 6 April 1993

Bover Bridge

DEKA55

GENERAL DATA

structure no.:	391001.1	city/town:	3.0 miles northeast of Stewartsville
county:	DeKalb	feature inters.:	branch of Castile Creek
		cadastral grid:	S2/35, T57N, R32W
		highway route:	County Road 391
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	substructure replaced, 1910
total length:	40.0'	floor/decking :	timber deck
roadway width:	11.4'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1900
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor :	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 391001.1; DeKalb County Bridge Foreman's Reports (1910 - 1920): Bridge No. 1923, 30 March 1910 - 2 April 1910.
sign. rating:	27
evaluation:	NRHP non-eligible (typically configured example of common structural type, inadequately documented)

inventoried by: Michelle Crow-Dolby 6 April 1993

Castille Creek Bridge

DEKA56

GENERAL DATA

structure no.:	391001.7	city/town:	3.4 miles northeast of Stewartville
county:	DeKalb	feature inters.:	branch of Castile Creek
		cadastral grid:	S2/35, T57N, R32W
		highway route:	County Road 391
		highway distr.:	1
		current owner:	DeKalb County

STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	30.0'	alterations:	substructure replaced
total length:	30.0'	floor/decking :	timber deck
roadway width:	11.3'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1893
erection cost:	\$230.00 (contract amount)
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 391001.7; DeKalb County Court Record, Book 4: page 165 (26 September 1892), page 178 (6 December 1892) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	46
evaluation:	NRHP possibly eligible (typically configured example of common structural type)

Inventoried by: Clayton B. Fraser 6 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Lost Creek Bridge
MHTD: 082000.1

DEKA12

DATE(S) OF CONSTRUCTION

1892-93

LOCATION

County Road 82 over Lost Creek; S7/12, T59N, R31/32W
5.1 miles north of Amity; DeKalb County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 48)

CONDITION

fair

OWNER

DeKalb County

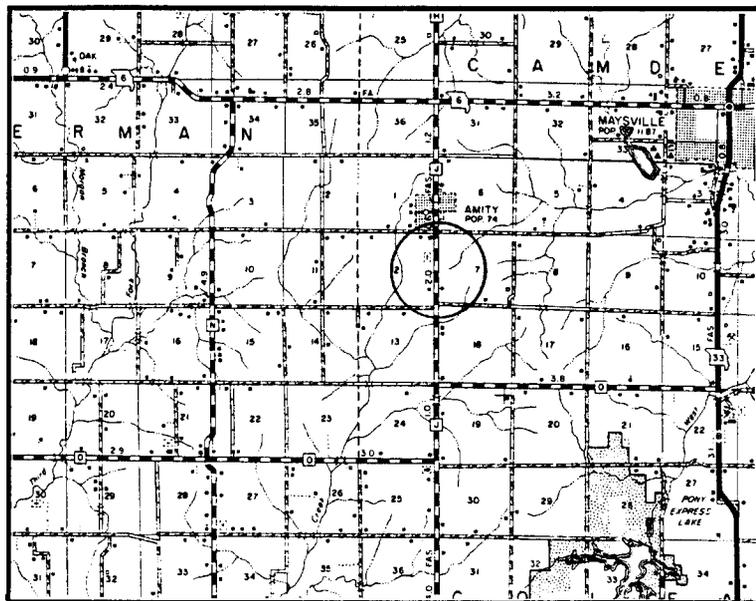
span number: 1 superstructure: wrought iron or steel, 3-panel, pin-connected Pratt pony truss
span length: 60.0' substructure: concrete-filled iron cylinder piers
total length: 69.0' floor/decking: timber deck
roadway wdt.: 13.7' other features: steel angle guardrails

This medium-span truss spans Lost Creek on an unsurfaced county road some five miles north of Amity. The structure is comprised of a single pin-connected Pratt pony truss that rests on tubular piers. The Lost Creek Bridge was erected in 1892-93 by the Dildine Bridge Company of Cameron, Missouri for \$885.00. The structure is today distinguished by its relatively early construction date and its well-preserved condition. It typifies pinned truss construction in Missouri from the late 19th century.

NAME(S) OF STRUCTURE

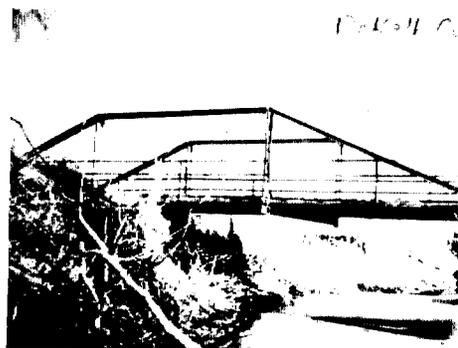
Lost Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 082000.1; DeKalb County Court Record, Book 4: page 165 (26 September 1892), pages 177-78 (6 December 1892) - located at the Dekalb County Courthouse, Maysville MO.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

6 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Grindstone Creek Bridge
MHTD: 131002.5

DEKA18

DATE(S) OF CONSTRUCTION

1900-01

LOCATION

County Road 131 over branch of Grindstone Creek; S8/9, T59N, R30W
3.2 miles northwest of Weatherby; DeKalb County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 57)

CONDITION

fair

OWNER

DeKalb County

span number: 1	superstructure: steel, pin-connected kingpost pony truss
span length: 25.0'	substructure: steel pile bent abutments embedded in concrete
total length: 25.0'	floor/decking: timber deck over steel stringers
roadway wdt.: 13.3'	other features: inclined end post: I-beam; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with batten plates; lateral bracing: round rod with threaded ends; guardrail: 2 angles

This small-scale truss bridge spans a branch of Grindstone Creek northwest of Weatherby, in east-central Dekalb County. Configured as a pin-connected kingpost pony truss, the single-span structure is supported by steel pile bent abutments embedded in concrete backwalls. The Grindstone Creek Bridge dates to May 1900, when the DeKalb County Court ordered the preparation of plans and specifications for a bridge at this location. After approving the engineer's drawings, court officials advertised for competitive construction bids. The Dildine Bridge Company of Cameron, Missouri, originally submitted a bid of \$336.00 but lowered the amount to \$300.00, and was subsequently awarded the contract. Dildine was contracted by the county almost exclusively for the erection of all its bridges between 1890 and the 1910s. Since its completion in 1901, the Grindstone Creek Bridge has continued to carry intermittent rural traffic with no major alterations of note.

As one of northwestern Missouri's most prolific bridge builders, the Dildine Bridge Company maintained a roster of standard truss types, with a range of span lengths. Like most of Missouri's bridge builders of the time, Dildine relied heavily on pin-connected Pratt truss variants as his standard truss types. The short-span bridge that he erected for the Grindstone Creek crossing in 1901 was a kingpost pony truss, the most fundamental of the truss configurations. With its inclined endposts, straight lower chord and single vertical at mid-span, the kingpost formed a simple triangular web comprised of two equal panels. When the roadway was carried beneath the truss in a through configuration, the endposts acted in compression and the vertical and lower chord in tension.

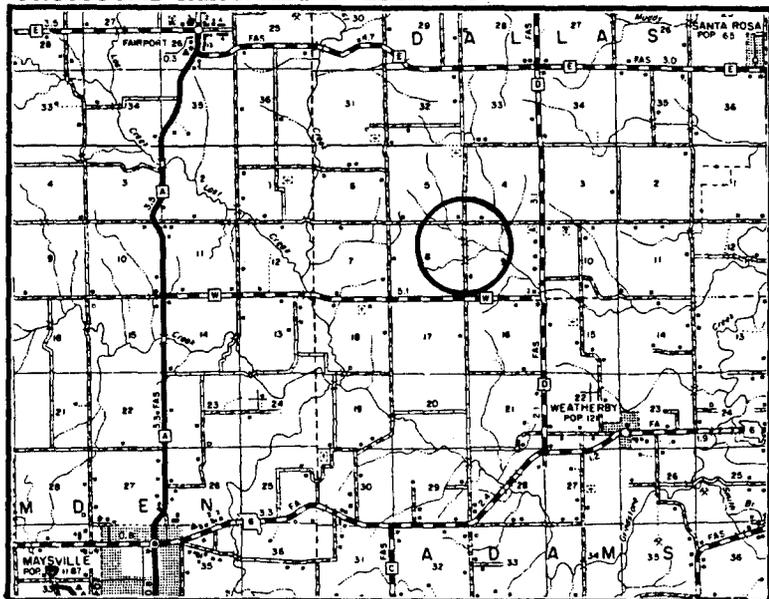
The kingpost's origins are ancient and obscure. Its symmetrical triangular form lent itself naturally to timber roof framing, where the truss was first used in the Middle Ages. In 1570 Italian architect Andrea Palladio described a simple kingpost truss bridge in his general treatise on architecture, *I quattro libri della architettura*; he attributed its source to well-established antecedents. Using Palladio's work (translated into English in 1742) and their own empirical designs, early American carpenters constructed kingpost bridges at minor crossings throughout the East. The technology spread westward to Missouri with the pioneers in the late 18th and early 19th centuries. As a result, uncounted timber kingposts were built on the region's early roads. The truss form remained the same as its construction

evolved from the vernacular to the industrial in the 19th century, with the principle changes involving the materials used: timber, timber/iron, iron, steel. The kingpost as a structural type was generally limited to relatively short-span applications, however, and as steel beam bridges received widespread acceptance after the turn of the century, erection of kingpost trusses declined rapidly. The Grindstone Creek Bridge in DeKalb County is a well-preserved, rare example of this once-mainstay structural type.

NAME(S) OF STRUCTURE

Grindstone Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 131002.5; DeKalb County Court Record, Book 5: page 125 (9 May 1900), page 166 (4 February 1901), page 197 (3 April 1901) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

6 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Lost Creek Bridge
MHTD: 139000.3

DEKA19

DATE(S) OF CONSTRUCTION

1894

LOCATION

County Road 139 over North Fork of Lost Creek; S19, T60N, R30W
1.8 miles east of Fairport; DeKalb County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 51)

CONDITION

fair

OWNER

DeKalb County

span number: 1
span length: 60.0'
total length: 76.0'
roadway wdt.: 11.7'

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss with laced ends; steel stringer approach span at east end
substructure: concrete abutments; steel pile bent pier (braced with angles)
floor/decking: timber deck over steel stringers
other features: upper chord: 2 channels with top and bottom lacing, continuous and batten plates; inclined end post: 2 channels with top and bottom lacing; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

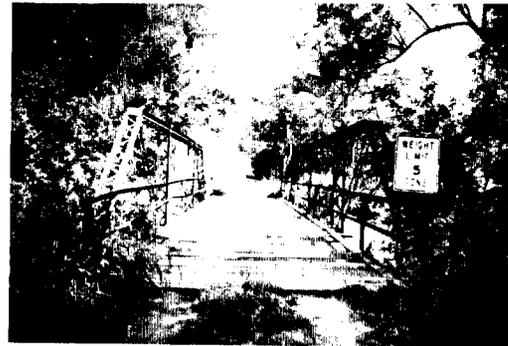
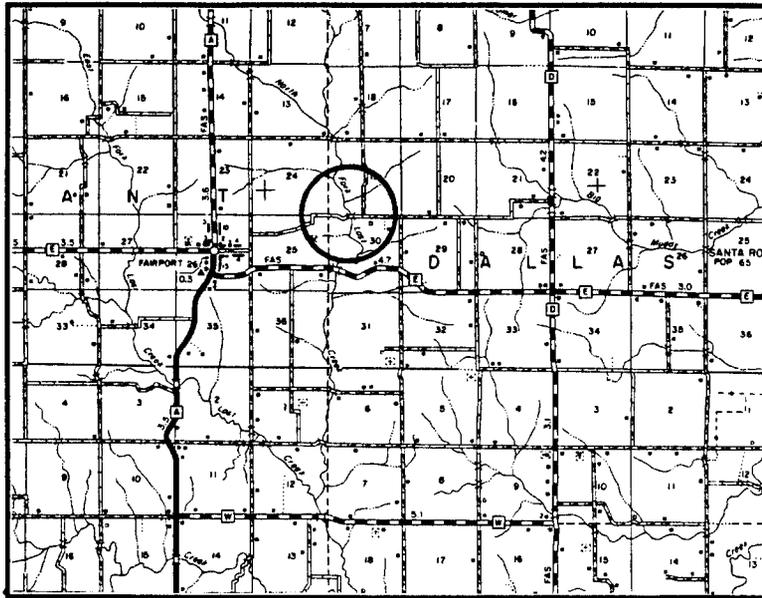
Crossing the North Fork of Lost Creek, this medium-span pony truss is located east of Fairport, in northeastern DeKalb County. The structure is comprised of a pinned Pratt half-hip pony truss, supported by steel pile bents and approached by a steel stringer span on its east end. The Lost Creek Bridge dates to August 1893. That month the DeKalb County Court instructed the county engineer to delineate plans for a bridge here. After approving the plans and specifications, the county court solicited competitive bids for the bridge's construction. Three months later the court hired the Dildine Bridge Company to fabricate and build the bridge for \$871.00. Dildine was contracted by the county almost exclusively for the erection of all its bridges between 1890 and the 1910s. The Lost Fork Bridge was originally erected on tubular iron piers; at some subsequent date a new substructure was built and the truss moved onto the new substructure. Since the move, the bridge continues to function in place today.

A subtype of the venerable Pratt pony truss, the Pratt half-hip configuration was marketed extensively by regional bridge companies in the early 1900s. Its primary advantage as a structural type was that, by eliminating the vertical members at the hip connection, it was more materially conservant than the standard Pratt. Its disadvantage was that it was generally limited to short-span applications: typically 30-60 feet. But Missouri's myriad small streams lent themselves to this range, and, as a result, thousands of half-hip trusses were erected across the state in the late 19th century and early 20th century. The Lost Creek Bridge in DeKalb County is a typically configured example of this mainstay truss type. Built in 1894 by one of the state's most prolific bridge manufacturers, it is a well-preserved transportation-related resource.

NAME(S) OF STRUCTURE

Lost Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 139000.3; DeKalb County Court Record, Book 4: page 228 (8 August 1893), page 240 (6 November 1893) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

6 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Grindstone Creek Bridge
MHTD: 177000.8

DEKA23

DATE(S) OF CONSTRUCTION

1899

LOCATION

County Road 177 over Grindstone Creek; S12, T59N, R30W
2.7 miles south of Santa Rosa; DeKalb County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 45)

CONDITION

fair

OWNER

DeKalb County

span number: 1
span length: 100.0'
total length: 170.0'
roadway wdt.: 11.5'

superstructure: steel, 5-panel, pin-connected Pratt through truss, with steel stringer approach span at each end
substructure: concrete abutments and concrete-filled steel cylinder piers
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; strut: lattice; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

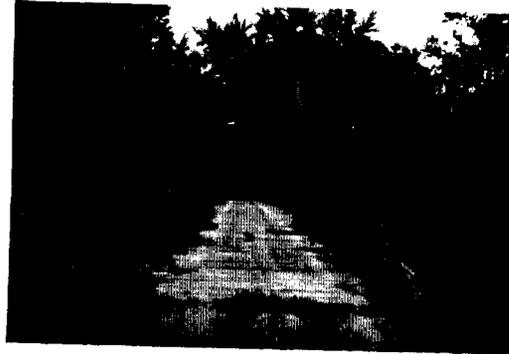
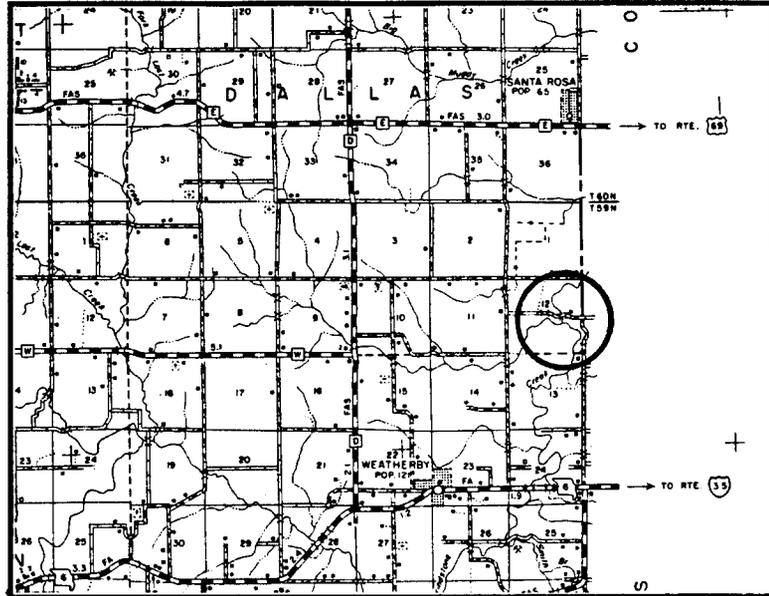
This structure carries County Road 177 across Grindstone Creek on the eastern edge of DeKalb County. Configured as a single-span, pinned Pratt through truss and approached on either side by steel stringers, the bridge rests on a concrete/steel substructure. The Grindstone Creek Bridge dates to 1898. In February of that year, the DeKalb County Court instructed the county engineer to delineate plans for a bridge at this crossing. After accepting the specifications, county officials let out the project for competitive bid. In June 1899, the Dildine Bridge Company of Cameron, Missouri, contracted with the county to provide and erect a 100-foot truss across the creek for the sum of \$1750.00. Dildine held a virtual bridge-building monopoly in the county from the 1890s to the 1910s. Continuing to function in place, the Grindstone Creek Bridge maintains a high degree of both historical and structural integrity.

In Missouri the pinned Pratt through truss was the bridge of choice for short- and medium-span applications in the late 19th and early 20th centuries. Most of the structures erected during this period were based on standard plans developed either by the state highway department (after 1917) or by the individual bridge companies, such as the Dildine Bridge Company in Cameron. As a result, thousands of Pratts were built across the state, all very much alike in detailing, and today the Pratt truss constitutes the most populous group of through trusses. The Grindstone Creek Bridge is a typically configured example of the pin-connected Pratt through truss.

NAME(S) OF STRUCTURE

Grindstone Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 177000.8; DeKalb County Court Record, Book 5: page 49 (5 June 1899); Warrant Register, Bridges: Dildine (1899) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

6 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Lost Creek Bridge
MHTD: 193001.1

DEKA24

DATE(S) OF CONSTRUCTION

1894

LOCATION

County Road 193 over Lost Creek; S28/29, T59N, R30W
2.3 miles west of Weatherby; DeKalb County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 52)

CONDITION

fair

OWNER

DeKalb County

span number: 1
span length: 80.0'
total length: 98.0'
roadway wdt.: 11.7'

superstructure: steel, 4-panel, pin-connected Pratt through truss, with laced ends; steel stringer approach span
substructure: concrete-filled steel cylinder piers; timber pile bent abutments with timber back- and wingwalls
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with top and bottom lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 square eyebars at the hip); diagonal: 2 looped square eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with turnbuckles (top) and round rods with threaded ends (bottom); strut: angles with lacing; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

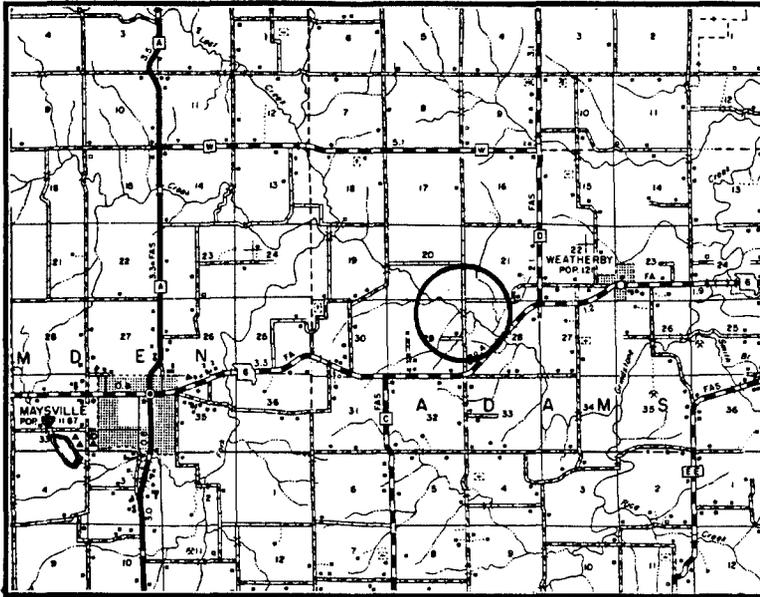
Located west of Weatherby in east-central DeKalb County, this pinned Pratt truss carries County Road 193 across Lost Creek. The structure is comprised of an 80-foot Pratt through truss, supported by concrete-filled cylinder piers and approached by a single steel stringer span on one side. The bridge dates to March 1894, when county officials instructed the county engineer to prepare plans and specifications for a permanent structure at this crossing. Two months later a contract to fabricate and erect the truss was let to the Dildine Bridge Company of Cameron, Missouri, for \$1487.00. Dildine held a virtual bridge-building monopoly in the county spanning from the 1890s to the 1910s. Since its completion, the Lost Creek Bridge continues to carry local traffic in its heavily wooded setting. The bridge retains a high degree structural integrity, with no major alterations of note.

In Missouri the pinned Pratt through truss was the bridge of choice for short- and medium-span applications in the late 19th and early 20th centuries. Most of the structures erected during this period were based on standard plans developed either by the state highway department (after 1917) or by the individual bridge companies, such as the Dildine Bridge Company in Cameron. As a result, thousands of Pratts were built across the state, all very much alike in detailing, and today the Pratt truss constitutes the most populous group of through trusses. The Lost Creek Bridge is a typically configured, early example of the pin-connected Pratt through truss.

NAME(S) OF STRUCTURE

Lost Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 193001.1; DeKalb County Courthouse Record, Book 4: page 264 (8 March 1894), page 275 (8 May 1894) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

6 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Little Third Fork Bridge
MHTD: 298000.1

DEKA37

DATE(S) OF CONSTRUCTION

1895

LOCATION

County Road 298 over Little Third Fork; S15, T59N, R32W
5.2 miles northwest of Amity; DeKalb County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 46)

CONDITION

fair

OWNER

DeKalb County

span number:	1	superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss
span length:	40.0'	substructure:	steel pile bent piers with timber wingwalls
total length:	40.0'	floor/decking:	timber deck
roadway wdt.:	11.2'	other features:	steel angle guardrails

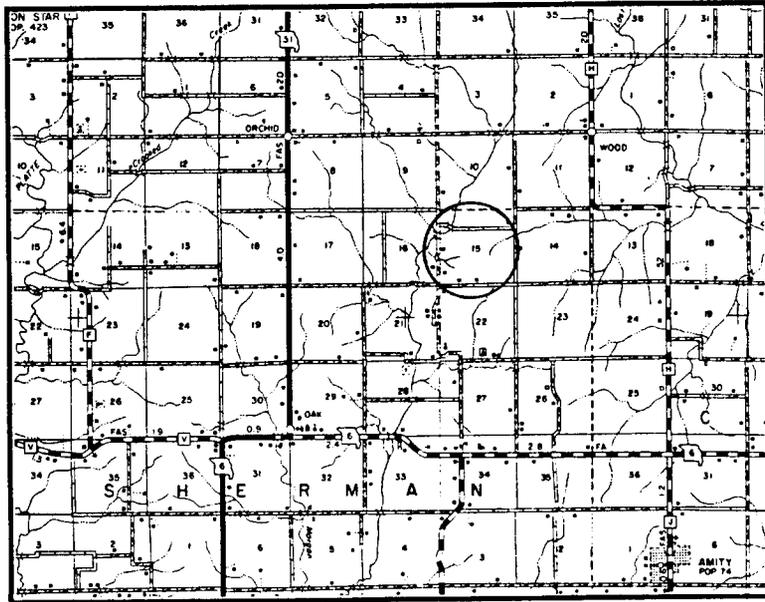
This short-span truss crosses the Little Third Fork on an unsurfaced county road northwest of Amity. The structure is comprised of a single pin-connected Pratt half-hip pony truss that rests on a steel pile bent substructure. The Little Third Fork Bridge was erected in 1895 by the Dildine Bridge Company of Cameron, Missouri. The structure today carries vehicular traffic with its superstructure intact and its substructure replaced.

A subtype of the venerable Pratt pony truss, the Pratt half-hip configuration was marketed extensively by regional bridge companies in the early 1900s. Its primary advantage as a structural type was that, by eliminating the vertical members at the hip connection, it was more materially conservant that the standard Pratt. Its disadvantage was that it was generally limited to short-span applications: typically 30-60 feet. But Missouri's myriad small streams lent themselves to this range, and, as a result, thousands of half-hip trusses were erected across the state in the late 19th century and early 20th century. The Little Third Fork Bridge in DeKalb County is a typically configured example of this mainstay truss type.

NAME(S) OF STRUCTURE

Little Third Fork Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 298000.1; DeKalb County Court Record, Book 4: page 332 (6 May 1895) - located at the DeKalb County Courthouse, Maysville MO..

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

6 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Little Third Fork Bridge
MHTD: 299000.3

DEKA38

DATE(S) OF CONSTRUCTION

1893-94

LOCATION

County Road 299 over Little Third Fork; S15/16, T59N, R32W
5.2 miles northwest of Amity; DeKalb County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 49)

CONDITION

fair

OWNER

DeKalb County

span number: 1	superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss
span length: 60.0'	substructure: steel pile bent abutments with timber wingwalls
total length: 60.0'	floor/decking: timber deck
roadway wdt.: 11.8'	other features: steel angle guardrails

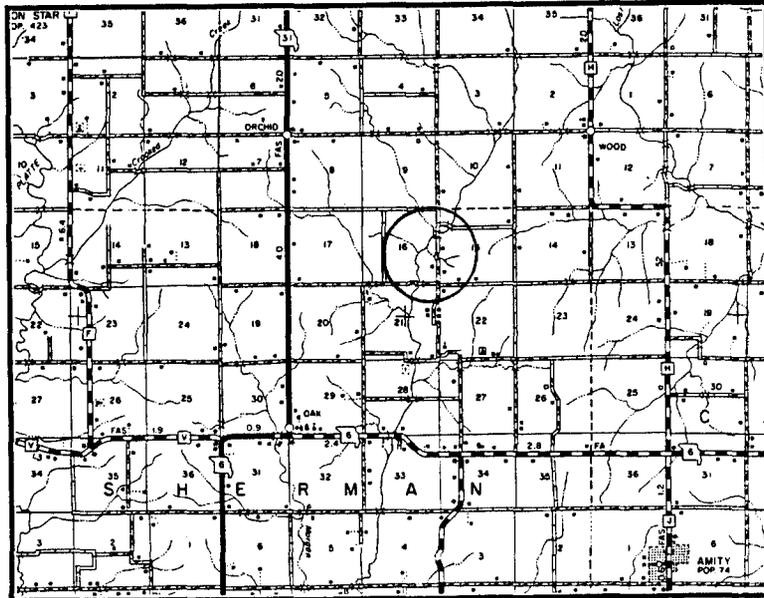
This medium-span truss spans the Little Third Fork on an unsurfaced county road northwest of Amity. The structure is comprised of a single pin-connected Pratt half-hip pony truss that rests on a steel pile bent substructure. The Little Third Fork Bridge was erected in 1893-94 by the Dildine Bridge Company of Cameron, Missouri. The structure today carries vehicular traffic with its superstructure intact and its substructure replaced.

A subtype of the venerable Pratt pony truss, the Pratt half-hip configuration was marketed extensively by regional bridge companies in the early 1900s. Its primary advantage as a structural type was that, by eliminating the vertical members at the hip connection, it was more materially conservant that the standard Pratt. Its disadvantage was that it was generally limited to short-span applications: typically 30-60 feet. But Missouri's myriad small streams lent themselves to this range, and, as a result, thousands of half-hip trusses were erected across the state in the late 19th century and early 20th century. The Little Third Fork Bridge in DeKalb County is a typically configured example of this mainstay truss type. Built in 1893-94 by one of the state's most prolific bridge manufacturers, it is a noteworthy transportation-related resource.

NAME(S) OF STRUCTURE

Little Third Fork Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 299000.3; DeKalb County Court Record, Book 4: page 240 (6 November 1893) - located at the DeKalb County Courthouse, Maysville MO.

INVENTORIED BY
Clayton Fraser

AFFILIATION
Fraserdesign, Loveland CO

DATE
24 September 1994

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Jordan Creek Bridge
MHTD: 376000.4

DEKA52

DATE(S) OF CONSTRUCTION

1893

LOCATION

County Road 376 over branch of Jordan Creek; S10, T57N, R33W
4.0 miles southwest of Clarksdale; DeKalb County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 52)

CONDITION

fair

OWNER

DeKalb County

span number: 1 superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss with laced endposts
span length: 35.0' substructure: concrete abutments and wingwalls
total length: 35.0' floor/decking: timber deck
roadway wdt.: 13.4' other features: steel angle guardrails

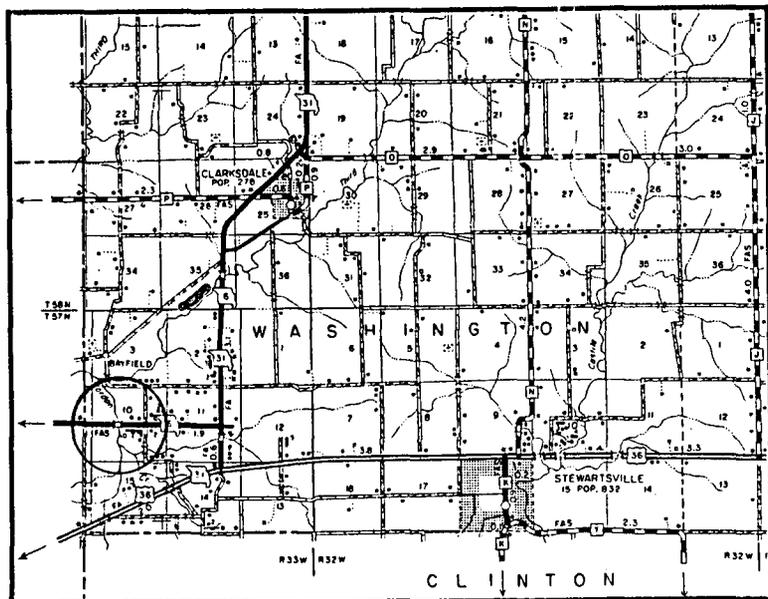
This short-span truss spans a branch of Jordan Creek on an unsurfaced county road some four miles southwest of Clarksdale. The structure is comprised of a single pin-connected Pratt half-hip pony truss that rests on a concrete substructure. The Jordan Creek Bridge was erected in 1893 by the Dildine Bridge Company of Cameron, Missouri. The structure today carries vehicular traffic with its superstructure intact and its substructure replaced.

A subtype of the venerable Pratt pony truss, the Pratt half-hip configuration was marketed extensively by regional bridge companies in the early 1900s. Its primary advantage as a structural type was that, by eliminating the vertical members at the hip connection, it was more materially conservant than the standard Pratt. Its disadvantage was that it was generally limited to short-span applications: typically 30-60 feet. But Missouri's myriad small streams lent themselves to this range, and, as a result, thousands of half-hip trusses were erected across the state in the late 19th century and early 20th century. With its laced end posts, the Jordan Creek Bridge in DeKalb County is an atypically configured example of this mainstay truss type. Built in 1893 by one of the state's most prolific bridge manufacturers, it is a noteworthy transportation-related resource.

NAME(S) OF STRUCTURE

Jordan Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 376000.4; DeKalb County Court Record, Book 4: page 165 (26 September 1892), pages 177-78 (6 December 1892), page 227 (7 August 1893) - located at the DeKalb County Courthouse, Maysville MO.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

6 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Castile Creek Bridge
MHTD: 391000.9

DEKA54

DATE(S) OF CONSTRUCTION

1895

LOCATION

County Road 391 over Castile Creek; S3, T57N, R32W
3.0 miles northeast of Stewartsville; DeKalb County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 46)

CONDITION

fair

OWNER

DeKalb County

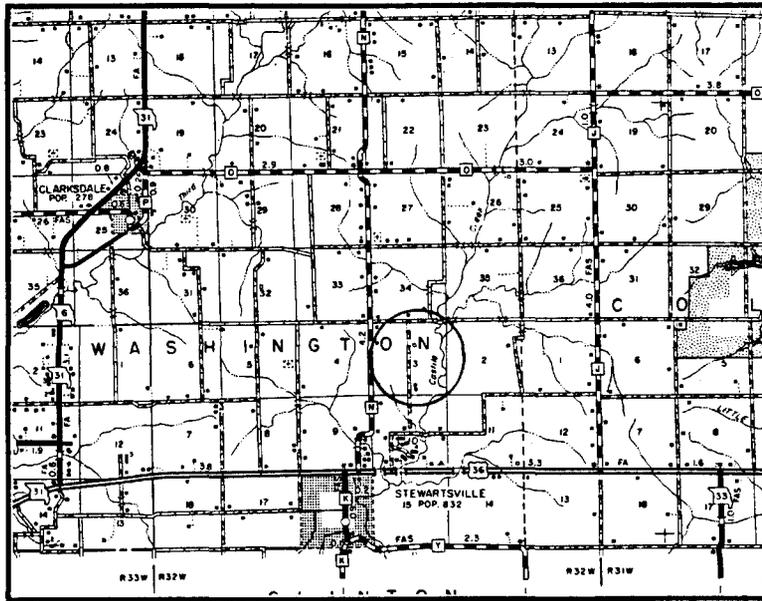
span number: 1 superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss with approach spans
span length: 40.0' substructure: steel pile bent pier and abutments with timber wingwalls
total length: 65.0' floor/decking: timber deck
roadway wdt.: 11.4' other features: steel angle guardrails

This short-span truss spans a branch of Castile Creek on an unsurfaced county road northeast of Stewartsville. The structure is comprised of a single pin-connected Pratt half-hip pony truss that rests on steel pile bents. The Castile Creek Bridge was erected in 1895 by the Dildine Bridge Company of Cameron, Missouri, for \$357.00. The structure today carries vehicular traffic with its superstructure intact and its substructure replaced.

A subtype of the venerable Pratt pony truss, the Pratt half-hip configuration was marketed extensively by regional bridge companies in the early 1900s. Its primary advantage as a structural type was that, by eliminating the vertical members at the hip connection, it was more materially conservant than the standard Pratt. Its disadvantage was that it was generally limited to short-span applications: typically 30-60 feet. But Missouri's myriad small streams lent themselves to this range, and, as a result, thousands of half-hip trusses were erected across the state in the late 19th century and early 20th century. The Castile Creek Bridge in DeKalb County is a typically configured example of this mainstay truss type.

NAME(S) OF STRUCTURE
Castile Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 391001.7; DeKalb County Court Record, Book 4: page 165 (26 September 1892), page 178 (6 December 1892) - located at the DeKalb County Courthouse, Maysville MO.

INVENTORIED BY
Clayton Fraser

AFFILIATION
Fraserdesign, Loveland CO

DATE
6 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Castile Creek Bridge
MHTD: 391001.7

DEKA56

DATE(S) OF CONSTRUCTION

1893

LOCATION

County Road 391 over branch of Castile Creek; S2/35, T57N, R32W
3.4 miles northeast of Stewartville; DeKalb County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 46)

CONDITION

fair

OWNER

DeKalb County

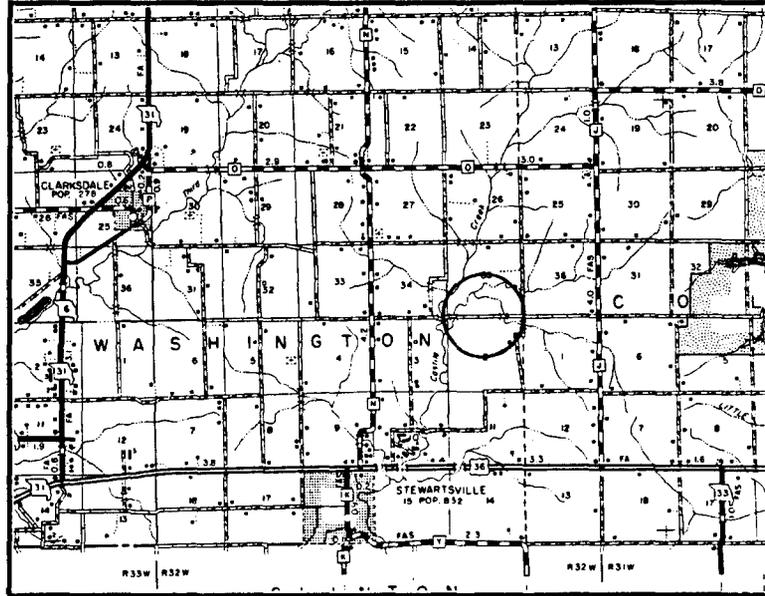
span number:	1	superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss
span length:	30.0'	substructure:	steel pile bent abutments with timber wingwalls
total length:	30.0'	floor/decking:	timber deck
roadway wdt.:	11.3'	other features:	steel angle guardrails

This short-span truss spans a branch of Castile Creek on an unsurfaced county road northeast of Stewartville. The structure is comprised of a single pin-connected Pratt half-hip pony truss that rests on a concrete substructure. The Castile Creek Bridge was erected in 1893 by the Dildine Bridge Company of Cameron, Missouri, for \$230.00. The structure today carries vehicular traffic with its superstructure intact and its substructure replaced.

A subtype of the venerable Pratt pony truss, the Pratt half-hip configuration was marketed extensively by regional bridge companies in the early 1900s. Its primary advantage as a structural type was that, by eliminating the vertical members at the hip connection, it was more materially conservant than the standard Pratt. Its disadvantage was that it was generally limited to short-span applications: typically 30-60 feet. But Missouri's myriad small streams lent themselves to this range, and, as a result, thousands of half-hip trusses were erected across the state in the late 19th century and early 20th century. The Castile Creek Bridge in DeKalb County is a typically configured example of this mainstay truss type. Built in 1893 by one of the state's most prolific bridge manufacturers, it is a noteworthy transportation-related resource.

NAME(S) OF STRUCTURE
Castille Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 391001.7; DeKalb County Court Record, Book 4: page 165 (26 September 1892), page 178 (6 December 1892) - located at the DeKalb County Courthouse, Maysville MO.

INVENTORIED BY
Clayton B. Fraser

AFFILIATION
Fraserdesign, Loveland CO

DATE
6 April 1993

GENTRY COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
GENT01	F 324R	Middle Fork Bridge	1-100' riveted Pratt through truss 1923 Illinois Steel Bridge Company
GENT02	F 331R	East Fork Bridge	1-110' riveted Pratt through truss 1937 C.H. Atkinson Paving Co.
*GENT03	026000.5	West Fork Bridge	1-140' riveted Pratt through truss c1920
GENT04	061000.5	Middle Fork Bridge	1-120' pinned Pratt through truss c1910
*GENT05	066000.5	Middle Fork Bridge	1-100' 2-angle Camelback pony truss c1930
GENT06	105000.3	Martin Branch Bridge	1- 60' pinned Pratt pony truss c1910
*GENT07	123000.2	Big Muddy Creek Bridge	1-100' pinned Pratt through truss c1905
*GENT08	182R01.5	Grand River Bridge	1-160' riveted Camelback through truss c1950
*GENT09	220000.6	East Fork Bridge	(replaced)
*GENT10	283000.3	Wildcat Creek Bridge	(replaced)
*GENT11	308000.3	Wildcat Creek Bridge	(replaced)
GENT12	337000.3	Island City Bridge	(replaced)
GENT13	412004.2	Lost Creek Bridge	1- 40' pinned Pratt pony truss 1895 Dildine Bridge Co. (prob.)
GENT14	465001.0	Bridge	1- 50' pinned Pratt pony truss c1915
*GENT15	469001.0	Elam Bend Bridge	1-160' pinned Camelback through truss 1895 St. Joseph Bridge and Iron Co.

EXCLUDED:

Pratt pony truss

003000.3 016002.4 046000.8 050000.6 129001.5 155000.3

Warren pony truss

H 551 201000.4

Bailey truss

003001.0

Steel stringer / girder

S 95	S 385	S 386	T 195	T 332	X 725	X 848
003000.1	003000.2	003000.4	007R00.4	008000.9	017001.7	022000.2
031000.2	031001.6	034000.0	038R01.1	047000.6	057000.1	057002.2
059001.0	070000.8	071000.8	073001.7	075R00.5	078R01.3	081002.5
081002.8	106000.2	106001.1	108000.2	109000.0	113R00.2	114001.4

GENTRY COUNTY

EXCLUDED (cont.):

Steel stringer / girder

115000.3	117000.4	123000.7	126000.4	133000.2	133000.3	135R01.1
135R03.0	135000.4	135002.3	144000.4	144000.7	144001.7	152000.0
153000.6	165002.5	170002.5	185000.3	193000.1	204000.6	210000.1
215000.4	216R00.1	224000.8	226000.0	239002.2	243000.1	251000.8
254000.2	258000.7	265000.4	266000.1	275000.2	275000.7	278R00.7
288000.7	288002.0	289000.9	289002.0	308000.8	308001.5	310000.4
316001.4	322R01.3	322000.3	324000.5	324001.0	326001.0	327001.3
334R00.3	347000.4	347000.6	349001.0	355R01.0	355001.8	355002.4
358001.3	369000.0	372000.4	373000.8	374000.3	377000.7	378001.2
383000.2	386001.1	387000.2	389000.3	396000.9	406000.4	409000.2
413000.2	417001.5	422000.2	422001.0	423000.6	425000.1	430001.1
446R00.6	448000.5	450R00.9	450000.6	458R00.7	471000.2	474000.4
486000.2	488001.3	489R02.5	489001.5	493002.8	509000.7	517000.5

Concrete girder

H 223	H 248	J 83	J 84	J 203
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Concrete slab

F 302R	F 327R1	F 336R1	J 204
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Concrete box culvert

H 618R	H 642	K 138	K 612	K 731	P 34	S 387
T 178	T 194	T 197	X 136	X 726	X 969	

Timber stringer

K 150	290000.1	291000.1
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SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	2	9	0	0	11
Excluded	31	129	0	0	160
	33	138	0	0	174 structures

Middle Fork Bridge

GENT01

GENERAL DATA

structure no.: F 324R	city/town: 3.8 miles southwest of Albany
county: Gentry	feature inters.: Middle Fork of Grand River
	cadastral grid: S21/28, T63N, R31W
	highway route: U.S. Highway 136
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 5-panel, rigid-connected Pratt through truss; 3-panel, rigid-connected Warren pony truss approaches	
substructure: concrete abutments, wingwalls and piers	
span number: 1	condition: good
span length: 100.0'	alterations: none
total length: 208.0'	floor/decking : concrete deck over steel stringers
roadway width: 20.0'	other features: steel pipe guardrails

HISTORICAL DATA

erection date: 1923
erection cost: \$23,882.10
designer: Missouri State Highway Department
fabricator : unknown
contractor: Illinois Steel Bridge Company, Jacksonville IL
references: Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number F 324R; Files on primary system bridges, located at Missouri Highway and Transportation Department, Jefferson City MO.
sign. rating: 34
evaluation: NRHP non-eligible (typically configured example of MSHD truss design, lacking technological or historical importance)

Inventoried by: Clayton B. Fraser 10 March 1994

East Fork Bridge

GENT02

GENERAL DATA

structure no.:	F 331R	city/town:	1.4 miles west of Albany
county:	Gentry	feature inters.:	East Fork of Grand River
		cadastral grid:	S23, T63N, R31W
		highway route:	U.S. Highway 136
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Pratt through truss; steel plate through girder approaches		
substructure:	concrete abutments, wingwalls and piers		
span number:	1	condition:	good
span length:	110.0'	alterations:	salvaged truss moved to this location, 1922; truss replaced with new span, 1937
total length:	237.0'	floor/decking :	concrete deck over steel stringers
roadway width:	22.0'	other features:	steel guardrails

HISTORICAL DATA

erection date:	1922; 1937
erection cost:	\$37,140.34
designer:	Missouri State highway Department
fabricator :	unknown
contractor:	C.H. Atkinson Paving Company
references:	Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number F331R; Files on primary system bridges, located at Missouri Highway and Transportation Department, Jefferson City MO.
sign. rating:	36
evaluation:	NRHP non-eligible (typically configured example of MSHD truss design, lacking technological or historical importance)

inventoried by: Clayton B. Fraser 10 March 1994

West Fork Bridge

GENT03

GENERAL DATA

structure no.:	026000.5	city/town:	6.1 miles north of Stanberry
county:	Gentry	feature inters.:	West Fork of Grand River
		cadastral grid:	S32, T64N, R32W
		highway route:	County Road 26
		highway distr.:	1
		current owner:	Gentry County

STRUCTURAL DATA

superstructure:	steel, 8-panel, rigid-connected Pratt through truss; two, 2-angle, rigid-connected Camelback pony truss approaches		
substructure:	steel pile bent piers and abutments, with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	140.0'	alterations:	none
total length:	365.0'	floor/decking :	timber deck over steel stringers
roadway width:	15.1'	other features:	main span: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 angles with batten plate; vertical: 4 angles with batten plate; diagonal: 2 angles with batten plate; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, field-bolted to vertical; guardrail: 2 angles

HISTORICAL DATA

erection date:	c1920
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number 026000.5; field inspection by Mitzi Rossillon, 15 September 1990.
sign. rating:	23
evaluation:	NRHP non-eligible (undistinguished, undocumented example of common structural type)

inventoried by: Clayton B. Fraser 10 March 1994

Middle Fork Bridge

GENT04

GENERAL DATA

structure no.:	061000.5	city/town:	9.8 miles northwest of Albany
county:	Gentry	feature inters.:	Middle Fork of Grand River
		cadastral grid:	S6/7, T64N, R31W
		highway route:	County Road 61
		highway distr.:	1
		current owner:	Gentry County

STRUCTURAL DATA

superstructure:	steel, 6-panel, pin-connected Pratt through truss		
substructure:	steel pile bent piers and abutments, with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	120.0'	alterations:	none
total length:	179.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.5'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number 061000.5.
sign. rating:	26
evaluation:	NRHP non-eligible (undistinguished, undocumented example of common structural type)

inventoried by: Clayton B. Fraser 10 March 1994

Middle Fork Bridge

GENT05

GENERAL DATA

structure no.:	066000.5	city/town:	9.0 miles northwest of Albany
county:	Gentry	feature inters.:	Middle Fork of Grand River
		cadastral grid:	S8/17, T64N, R31W
		highway route:	County Road 66
		highway distr.:	1
		current owner:	Gentry County

STRUCTURAL DATA

superstructure: steel, 6-panel, 2-angle Camelback pony truss, with steel stringer approach spans

substructure: steel pile bent piers and abutments, with timber back- and wingwalls

span number:	1	condition:	fair
span length:	100.0'	alterations:	none
total length:	134.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.3'	other features:	upper chord, inclined end post, lower chord and vertical: 2 angles; diagonal: 1 angle; lateral bracing: round rods with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles on right-angle posts

HISTORICAL DATA

erection date: c1930
erection cost: unknown
designer: unknown
fabricator : Inland Steel Company, East Chicago IN
contractor : unknown

references: Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number 066000.5; field inspection by Mitzi Rossillon, 15 September 1990.

sign. rating: 37
evaluation: NRHP non-eligible (example of uncommon structural type, undocumented)

inventoried by: Clayton B. Fraser 10 March 1994

Martin Branch Bridge

GENT06

GENERAL DATA

structure no.:	105000.3	city/town:	9.2 miles northeast of Albany
county:	Gentry	feature inters.:	Martin Branch
		cadastral grid:	S6, T64N, R30W
		highway route:	County Road 105
		highway distr.:	1
		current owner:	Gentry County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and abutments, with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	88.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number 105000.3.

sign. rating:	28
evaluation:	NRHP non-eligible (typically configured, technologically undistinguished example of common structural type, largely undocumented)

inventoried by: Clayton B. Fraser 10 March 1994

Big Muddy Creek Bridge

GENT07

GENERAL DATA

structure no.:	123000.2	city/town:	6.8 miles north of Albany
county:	Gentry	feature inters.:	Big Muddy Creek
		cadastral grid:	S13, T64N, R31W
		highway route:	County Road 123
		highway distr.:	1
		current owner:	Gentry County

STRUCTURAL DATA

superstructure:	steel, 6-panel, pin-connected Pratt through truss		
substructure:	concrete-filled steel cylinder piers, with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	100.0'	alterations:	none
total length:	100.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.6'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, field-bolted to verticals; guardrail: none

HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	unknown
fabricator :	Lackawanna Steel Company, Pittsburgh PA
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number 123000.2; field inspection by Mitzi Rossillon, 15 September 1990.
sign. rating:	28
evaluation:	NRHP non-eligible (typically configured, technologically undistinguished example of common structural type, largely undocumented)

inventoried by: Clayton B. Fraser 10 March 1994

Grand River Bridge

GENT08

GENERAL DATA

structure no.: 182R01.5	city/town: 3.2 miles southwest of Albany
county: Gentry	feature inters.: Grand River
	cadastral grid: S34/33, T63N, R31W
	highway route: County Road 182
	highway distr.: 1
	current owner: Gentry County

STRUCTURAL DATA

superstructure: steel, 9-panel, rigid-connected Camelback through truss, with steel stringer approach span	
substructure: steel pile bent piers and abutments, with timber back- and wingwalls	
span number: 1	condition: fair
span length: 160.0'	alterations: south approach span added, 1979
total length: 200.0'	floor/decking : timber deck over steel stringers
roadway width: 16.0'	other features: upper chord and inclined end post: 2 channels with cover plates and lacing; lower chord: 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 angles with batten plates; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date: c1950	
erection cost: unknown	
designer: unknown	
fabricator : Skullen Steel Company	
contractor: unknown	
references:	Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number 182001.5; field inspection by Mitzi Rossillon, 16 September 1990.
sign. rating: 44	
evaluation:	NRHP non-eligible (uncommon structural type, poorly documented)

inventoried by: Clayton Fraser and Michelle Crow-Dolby 10 March 1994

Lost Creek Bridge

GENT13

GENERAL DATA

structure no.:	412004.2	city/town:	4.2 miles southeast of King City
county:	Gentry	feature inters.:	Lost Creek
		cadastral grid:	S36, T61N, R32W
		highway route:	County Road 412
		highway distr.:	1
		current owner:	Gentry County / DeKalb County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments, with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	none
total length:	43.0'	floor/decking :	timber deck over steel stringers
roadway width:	15.0'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1895
erection cost:	\$300.00
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	Dildine Bridge Company, Cameron MO (probable)
contractor :	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number 412004.2; Gentry County Court Record, Book I, page 233 (3 September 1895) - located at the Gentry County Courthouse, Albany MO.
sign. rating:	48
evaluation:	NRHP possibly eligible (relatively early example of mainstay structural type)

inventoried by: Clayton B. Fraser 10 March 1994

Bridge

GENT14

GENERAL DATA

structure no.:	465001.0	city/town:	1.6 miles southwest of McFall
county:	Gentry	feature inters.:	Branch of Grand River
		cadastral grid:	S11, T61N, R30W
		highway route:	County Road 465
		highway distr.:	1
		current owner:	Gentry County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	none
total length:	50.0'	floor/decking :	timber deck over steel stringers
roadway width:	15.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1920
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number 465001.0.

sign. rating:	23
evaluation:	NRHP non-eligible (typical, small-scale example of common structural type, largely undocumented)

inventoried by: Clayton B. Fraser 10 March 1994

Elam Bend Bridge

GENT15

GENERAL DATA

structure no.: 469001.0	city/town: 3.1 miles southwest of McFall
county: Gentry	feature inters.: Grand River
	cadastral grid: S22/23, T61N, R30W
	highway route: County Road 469
	highway distr.: 1
	current owner: Gentry County

STRUCTURAL DATA

superstructure: steel, 11-panel, pin-connected Camelback through truss, with steel stringer approach spans at either end	
substructure: concrete-filled steel cylinder piers, timber pile bent abutments with timber back- and wingwalls	
span number: 1	condition: fair
span length: 160.0'	alterations: modified, 1982; non-original approaches
total length: 231.0'	floor/decking : timber deck over steel stringers
roadway width: 14.9'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing; hip vertical: 2 looped square eyebars; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beam, U-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

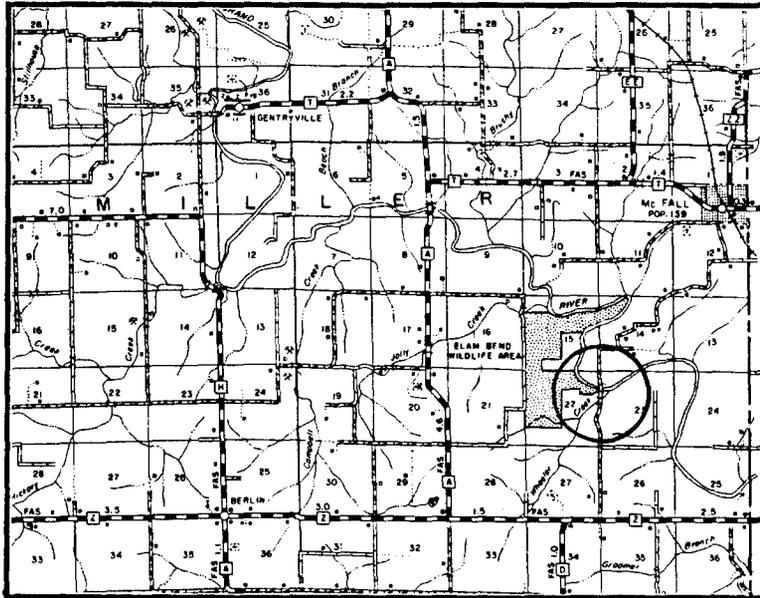
erection date: 1895	
erection cost: \$2800.00	
designer: St. Joseph Bridge and Iron Company, St. Joseph MO	
fabricator : Jones and Laughlin Steel Company, Pittsburgh PA	
contractor : St. Joseph Bridge and Iron Company, St. Joseph MO	
references:	Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number 469001.0; Gentry County Court Record, Book H: page 465 (7 August 1893); Book I: page 60 (7 January 1895) - located at the Gentry County Courthouse, Albany MO; Fraser-design, "Elam Bend Bridge: Preliminary Determination of NRHP Eligibility for the Missouri Historic Bridge Inventory," 8 February 1994; field inspection by Mitzi Rossillon, 15 September 1990.
sign. rating: 68	
evaluation:	NRHP determined eligible (well-preserved, early example of uncommon structural type)

inventoried by: Clayton B. Fraser 10 March 1994

NAME(S) OF STRUCTURE

Elam Bend Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES

Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number 469001.0; Gentry County Court Record, Book H: page 465 (7 August 1893); Book I: page 60 (7 January 1895) - located at the Gentry County Courthouse, Albany MO; Fraserdesign, "Elam Bend Bridge: Preliminary Determination of NRHP Eligibility for the Missouri Historic Bridge Inventory," 8 February 1994; field inspection by Mitzi Rossillon, 15 September 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

10 March 1994

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Elam Bend Bridge
MHTD: 469001.0

GENT15

DATE(S) OF CONSTRUCTION

1895

LOCATION

County Road 469 over Grand River; S22/23, T61N, R30W
3.1 miles southwest of McFall; Gentry County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP determined eligible (score: 68)

CONDITION

fair

OWNER

Gentry County

span number: 1
span length: 160.0'
total length: 231.0'
roadway wdt.: 14.9'

superstructure: steel, 11-panel, pin-connected Camelback through truss, with steel stringer approach spans at either end
substructure: concrete-filled steel cylinder piers, timber pile bent abutments with timber back- and wingwalls
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing; hip vertical: 2 looped square eyebars; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beam, U-bolted to verticals; guardrail: 2 angles

Located in rural Gentry County, some three miles southwest of McFall, this long-span truss carries a secondary county road over the Grand River. The bridge consists of a single, pin-connected Camelback through truss, supported by concrete-filled, steel cylinder piers and approached by steel stringer approach spans on both ends. Known locally as the Elam Bend Bridge, the structure dates to 1895. In January of that year the Gentry County Court contracted with the St. Joseph Bridge and Iron Company to rebuild the existing Elam Bend span over the Grand River. The St. Joseph, Missouri, firm was to be paid \$2,800.00 to supply and erect a new truss at the crossing. (It is not known whether this included replacement of the substructure as well.) Gentry County records are inconclusive, but the bridge was probably completed later that year. The Elam Bend Bridge has functioned in place since that time. Its approach spans, stringers and deck have been replaced over time, and a steel pile fender has been added to the upstream side of one pier, but the truss itself appears unaltered.

Through the 19th century, the pin-connected Pratt truss was the bridge of choice for medium- and long-span roadway crossings in Missouri. Late in the century, polygonal-chorded truss types began to replace the straight-chorded Pratts, however. The inclined upper chords of these structures afforded a degree of efficiency in long span trusses, where bending moment stresses at mid-span greatly exceed the shear stresses at the ends. Their drawback was that, unlike the straight-chorded Pratt truss, the polygonal chords necessitated different-length verticals and diagonals at each panel, increasing their fabrication costs somewhat. Because trusses were generally priced on the basis of their superstructural steel weight, the lighter overall weight of a polygonal-chord truss more than offset the slight increase in fabricating costs in spans greater than 160 feet. In the highly competitive bridge industry, this economy equated directly with profit.

These bridges generally employed Pratt-type web configurations, with upper chords and verticals in compression and lower chords and diagonals in tension. The most common of these Pratt variants was the Parker truss. Another was the Camelback truss, a Parker with five upper-chord facets. With its distinctive profile, the Camelback configuration was disdained by many engineers (including the redoubtable J.A.L. Waddell, who called it "uncompromisingly ugly") for its tendency under certain conditions to reverse compressive and tensile forces acting on the individual members. As a result, Camelback trusses never received widespread acceptance. Relatively few were ever built on Missouri's roads, and fewer than ten remain in place today. The Elam Bend Bridge in Gentry County is distinguished among these as the oldest Camelback truss: a relatively well-preserved, early example of an uncommon early structural type.

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Lost Creek Bridge
MHTD: 412004.2

GENT13

DATE(S) OF CONSTRUCTION

1895

LOCATION

County Road 412 over Lost Creek; S36, T61N, R32W
4.2 miles southeast of King City; Gentry County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 48)

CONDITION

fair

OWNER

Gentry County / DeKalb County

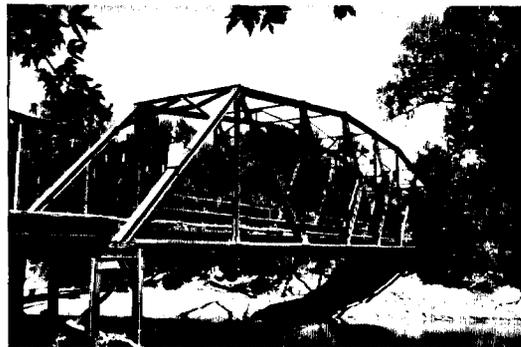
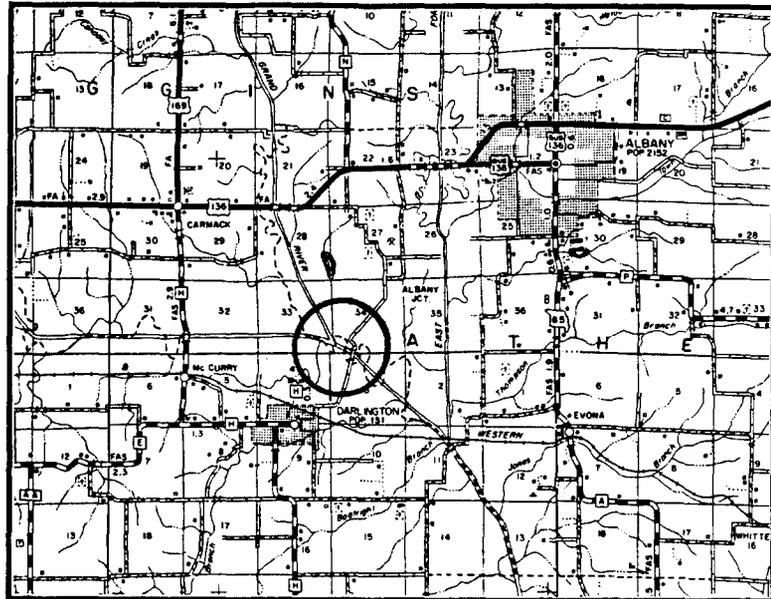
span number: 1
span length: 40.0'
total length: 43.0'
roadway wdt.: 15.0'

superstructure: steel, 3-panel, pin-connected Pratt pony truss
substructure: steel pile bent abutments, with timber back- and wingwalls
floor/decking: timber deck over steel stringers
other features: steel angle guardrails

This short-span truss spans Lost Creek on an unsurfaced county road southeast of King City. The structure is comprised of a single pin-connected Pratt pony truss that rests on steel pile bent abutments with timber backwalls. The Lost Creek Bridge was erected in 1895, probably by the Dildine Bridge Company of Cameron, Missouri, for \$300.00. The structure is today distinguished by its relatively early construction date and its well-preserved condition. It typifies pinned truss construction in Missouri from the late 19th century.

NAME(S) OF STRUCTURE
Grand River Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number 182001.5; field inspection by Mitzi Rossillon, 16 September 1990.

INVENTORIED BY
Clayton B. Fraser

AFFILIATION
Fraserdesign, Loveland CO

DATE
10 March 1994

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Grand River Bridge
MHTD: 182R01.5

GENT08

DATE(S) OF CONSTRUCTION

c1950

LOCATION

County Road 182 over Grand River; S34/33, T63N, R31W
3.2 miles southwest of Albany; Gentry County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 44)

CONDITION

fair

OWNER

Gentry County

span number: 1

span length: 160.0'

total length: 200.0'

roadway wdt.: 16.0'

superstructure: steel, 9-panel, rigid-connected Camelback through truss, with steel stringer approach span

substructure: steel pile bent piers and abutments, with timber back- and wingwalls

floor/decking: timber deck over steel stringers

other features: upper chord and inclined end post: 2 channels with cover plates and lacing; lower chord: 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 angles with batten plates; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

Carrying County Road 182 southwest of Albany, this large-scale truss spans the Grand River in central Gentry County. The structure is comprised of a riveted Camelback through truss, supported by steel pile bent piers and approached on the south by a steel stringer. County records are unclear regarding the bridge's origins, but physical attributes of the truss itself suggest that it was fabricated around 1930. The pile bent substructure may indicate that the bridge has been moved subsequently to this site, perhaps in 1979, when the south approach span was added.

Straight-chorded Pratt through trusses were used extensively throughout Missouri for medium-span crossings in the late 19th and 20th centuries. For longer crossings after the turn of the century, however, bridge companies could develop greater efficiency with polygonal-chorded Pratt variants - primarily Parker, Pennsylvania, and Camelback trusses. With its distinctive five-faceted upper chords, the Camelback configuration was disdained by some engineers (including the redoubtable J.A.L. Waddell, who called it "uncompromisingly ugly") for its tendency under certain conditions to reverse compressive and tensile forces acting on their individual members. As a result, Camelback trusses never received widespread acceptance. Relatively few were ever built on Missouri's roads, and only three rigid-connected examples have been identified as extant by the bridge inventory. The Grand River Bridge is thus distinguished as an intact example of this uncommon structural type. Its lack of documentation and uncertain structural history diminish its interpretive value substantially, however.

HARRISON COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
HARR01	F 595	Big Creek Bridge	(replaced)
HARR02	J 655	Thompson River Bridge	1-140' riveted Pratt through truss 1933 Fred M. Clark and Son
HARR03	005000.3	Lotts Creek Bridge	1- 60' pinned Pratt pony truss c1910
HARR04	023000.4	Big Creek Bridge	(replaced)
HARR05	059000.7	Indian Creek Bridge	(replaced)
HARR06	147001.9	Big Muddy Creek Bridge	1- 70' pinned Pratt pony truss c1910
HARR07	163000.3	Big Creek Bridge	1- 72' pinned Pratt pony truss c1910
HARR08	170001.1	Big Creek Bridge	1- 70' pinned Pratt bedstead 1910 county work force
HARR09	188000.2	Brush Creek Bridge	(replaced)
HARR10	229R01.5	Big Creek Bridge	1- 70' pinned Pratt bedstead c1910
HARR11	264R00.5	Big Muddy Creek Bridge	1- 40' pinned Pratt bedstead 1903 Dildine Bridge Co., Cameron MO
*HARR12	268000.0	Big Muddy Creek Bridge	1- 42' rivet 2-angle Warren pony truss c1920
HARR13	290R00.9	Big Creek Bridge	(replaced)
HARR14	296000.6	Big Creek Bridge	1- 80' pinned Pratt pony truss 1901 Dildine Bridge Co., Cameron MO
HARR15	313000.4	Panther Creek Bridge	(replaced)
HARR16	319R02.8	Panther Creek Bridge	1- 45' pinned Pratt pony truss 1913 county work force
HARR17	339000.9	Trail Creek Bridge	1- 54' pinned Pratt pony truss 1915 Kansas City Bridge Company
HARR18	359000.2	Big Creek Bridge	1- 80' pinned Pratt pony truss c1910
HARR19	368000.3	Big Creek Bridge	1- 80' pinned Pratt bedstead 1915 Dildine Bridge Co., Hannibal MO
*HARR20	436002.6	Big Creek Bridge	1- 90' pinned Pratt through truss 1905 Dildine Bridge Co., Cameron MO
HARR21	566000.7	Fox Creek Bridge	1- 40' pinned Pratt bedstead 1914 county work force
HARR22	574001.0	Sugar Creek Bridge	1- 60' pinned Pratt pony truss 1918 Dildine Bridge Co. / county crew
HARR23	597002.3	Sampson Creek Bridge	1- 60' pin Pratt half-hip pony truss c1910
HARR24	640000.3	Polecat Creek Bridge	1- 50' pinned Pratt pony truss 1917 Dildine Bridge Co. / county crew

HARRISON COUNTY

INCLUDED (cont.):

*HARR25	641001.5	Big Creek Bridge	1-100'	pinned Pratt through truss
			c1905	
*HARR26	687001.0	Big Creek Bridge	1-110'	pinned Pratt through truss
			c1910	Dildine Bridge Co., Cameron MO

EXCLUDED:

Pratt pony truss

017002.2	062500.2	102002.9	156000.1	165000.7	221000.6	242002.1
305500.1	405000.5	426000.5	463000.7	476000.7	559000.1	600000.16040005

Warren pony truss

H 753	J 653	017002.2
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Warren bedstead

019001.8	146000.7	158001.2	163002.1	175000.7	285000.4	287000.5
318000.5	373000.7	374000.8	376001.5	398000.4	508000.6	513001.8

Steel stringer

J 515	J 517	J 654	J 656	J 995	J 996	K 164
K 165	K 166	K 167	K 168	L 265	L 266	L 267
P 74	P 76	P 78	S 277	S 294	S 295	S 297
S 298	S 299	S 311	X 132	X 133	X 134	001000.8
005000.6	034500.3	034500.7	034500.8	045000.3	045001.1	055R02.4
057001.8	059000.6	061001.4	062500.1	086000.8	102001.8	113R00.5
115001.8	116R00.3	122001.7	123000.5	124000.6	135000.1	142002.5
146000.2	148000.1	162000.8	188001.5	192000.1	215000.3	222002.2
227R01.1	236000.4	248000.0	251000.7	251001.1	256000.5	279R01.0
258R00.9	263001.7	275001.1	283000.3	288R01.3	291000.1	299001.0
331R02.1	362000.3	363000.8	375000.7	379000.8	380000.9	381000.9
392000.2	393000.8	415001.1	417R01.4	434001.3	441000.3	442R00.3
448001.2	453003.1	461000.8	463R01.9	477000.1	483000.1	484001.8
494R01.3	495000.6	507000.4	507000.7	509R00.9	517000.2	527R00.1
535002.8	543000.3	550R00.7	573R01.9	575R01.7	577000.5	589000.2
593000.7	594000.5	594000.8	594001.7	599R00.6	603000.5	616000.3
625R00.9	629000.1	642000.3	666000.6	666000.7	692001.9	

Steel girder

048000.5	154001.0	331001.0	472000.3	475000.2
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Concrete girder

K 756	X 901	034500.4
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Concrete slab

H 789	239000.4
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HARRISON COUNTY

EXCLUDED (cont.):

Concrete box culvert

F 158R	H 754	J 518	J 519	K 566	L 263	P 73
S 276	T 495	X 131	X 902			

Timber stringer
034500.1

SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	1	19	0	0	20
Excluded	43	131	0	0	174
<hr/>					
	44	150	0	0	194 structures

Thompson River Bridge

HARR02

GENERAL DATA

structure no.: J 655	city/town: 9.5 miles southeast of Ridgeway
county: Harrison	feature inters.: Thompson River
	cadastral grid: S13/24, T64N, R26W
	highway route: State Highway 136
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 7-panel, rigid-connected Pratt through truss, with Warren pony truss approach spans	
substructure: concrete abutments, wingwalls and piers	
span number: 1; 2	condition: good
span length: 140.0'; 70.0'	alterations: none
total length: 288.0'	floor/decking : concrete deck over steel stringers
roadway width: 22.0'	other features: steel guardrails

HISTORICAL DATA

erection date: 1933	
erection cost: \$34,211.09	
designer: Missouri State Highway Department	
fabricator : unknown	
contractor: Fred M. Clark and Son	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number J 655; Files on primary system bridges located at Missouri Highway and Transportation Department, Jefferson City MO.	
sign. rating: 41	
evaluation: NRHP non-eligible (typically configured example of MSHD highway truss design)	

inventoried by: Michelle Crow-Dolby 21 August 1992

Lotts Creek Bridge

HARR03

GENERAL DATA

structure no.: 005000.3	city/town: 4.2 miles northwest of Hatfield
county: Harrison	feature inters.: Lotts Creek
	cadastral grid: S7, T66N, R29W
	highway route: County Road 5
	highway distr.: 1
	current owner: Harrison County / Worth County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 60.0'	alterations: unknown
total length: 60.0'	floor/decking : timber deck
roadway width: 11.7'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 005000.3.

sign. rating: 28
evaluation: NRHP non-eligible (typically configured, poorly documented example of common structural type)

inventoried by: Michelle Crow-Dolby 21 August 1992

Big Muddy Creek Bridge

HARR06

GENERAL DATA

structure no.: 147001.9	city/town: 9.3 miles southwest of Eagleville
county: Harrison	feature inters.: Big Muddy Creek
	cadastral grid: S9/10, T65N, R29W
	highway route: County Road 147
	highway distr.: 1
	current owner: Harrison County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 70.0'	alterations: unknown
total length: 99.0'	floor/decking : timber deck
roadway width: 13.8'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 147001.9.

sign. rating: 28
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 21 August 1992

Big Creek Bridge

HARR07

GENERAL DATA

structure no.: 163000.3	city/town: 2.6 miles southwest of Eagleville
county: Harrison	feature inters.: West Fork of Big Creek
	cadastral grid: S10, T65N, R28W
	highway route: County Road 163
	highway distr.: 1
	current owner: Harrison County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: unknown	
span number: 1	condition: fair
span length: 72.0'	alterations: unknown
total length: 132.0'	floor/decking : timber deck
roadway width: 13.8'	other features: unknown

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 163000.3.	
sign. rating: 28	
evaluation: NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 21 August 1992

Big Creek Bridge

HARR08

GENERAL DATA

structure no.:	170001.1	city/town:	3.0 miles southeast of Eagleville
county:	Harrison	feature inters.:	East Fork of Big Creek
		cadastral grid:	S17, T65N, R27W
		highway route:	County Road 170
		highway distr.:	1
		current owner:	Harrison County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt bedstead, with steel stringer approach span		
substructure:	steel pile bent piers and timber wingwalls		
span number:	1	condition:	fair
span length:	70.0'	alterations:	unknown
total length:	86.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1910
erection cost:	\$1740.00
designer:	unknown
fabricator :	unknown
contractor :	county work force
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 170001.1; Harrison County Court Record, Book R: page 132 (5 August 1910) - located at Harrison County Courthouse, Bethany MO.
sign. rating:	40
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 21 August 1992

Big Creek Bridge

HARR10

GENERAL DATA

structure no.: 229R01.5	city/town: 1.9 miles northwest of Ridgeway
county: Harrison	feature inters.: East Fork of Big Creek
	cadastral grid: S29, T65N, R27W
	highway route: County Road 229
	highway distr.: 1
	current owner: Harrison County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt bedstead, with steel stringer approach spans	
substructure: steel pile bent piers and timber wingwalls	
span number: 1	condition: fair
span length: 70.0'	alterations: west approach span rebuilt
total length: 92.0'	floor/decking : timber deck
roadway width: 11.7'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 229R01.5.

sign. rating: 30
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 21 August 1992

Big Muddy Creek Bridge

HARR11

GENERAL DATA

structure no.: 264R00.5	city/town: 5.4 miles northwest of Martinsville
county: Harrison	feature inters.: Big Muddy Creek
	cadastral grid: S20/29, T15N, R29W
	highway route: County Road 264
	highway distr.: 1
	current owner: Harrison County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt bedstead	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 40.0'	floor/decking : timber deck
roadway width: 14.0'	other features: unknown

HISTORICAL DATA

erection date: 1903
erection cost: \$445.00
designer: Dildine Bridge Company, Cameron MO
fabricator : unknown
contractor: Dildine Bridge Company, Cameron MO
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 264R00.5; Harrison County Court Record, Book O: page 487 (12 January 1903); Book P: page 55 (4 January 1904) - located at the Harrison County Courthouse, Bethany MO.
sign. rating: 36
evaluation: NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 21 August 1992

Big Muddy Creek Bridge

HARR12

GENERAL DATA

structure no.: 268000.0	city/town: 4.2 miles northwest of Martinsville
county: Harrison	feature inters.: Big Muddy Creek
	cadastral grid: S6, T64N, R29W
	highway route: County Road 268
	highway distr.: 1
	current owner: Harrison County

STRUCTURAL DATA

superstructure: steel, 3-panel, rigid-connected 2-angle Warren pony truss	
substructure: steel pile bent abutments with timber back- and wingwalls	
span number: 1	condition: fair
span length: 42.0'	alterations: abutments replaced, 1979
total length: 43.0'	floor/decking : timber deck over steel stringers
roadway width: 12.0'	other features: upper chord and inclined end post: 2 angles with cover plate; lower chord, vertical and diagonal: 2 angles; lateral bracing: round rods with threaded ends; floor beam: I-beams; guardrail: steel pipe

HISTORICAL DATA

erection date: c1920	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor : unknown	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 268000.0; field inspection by Mitzi Rossillon, 17 September 1990.
sign. rating: 22	
evaluation:	NRHP non-eligible (undistinguished example of uncommon structural type)

inventoried by: Michelle Crow-Dolby 21 August 1992

Big Creek Bridge

HARR14

GENERAL DATA

structure no.: 296000.6	city/town: 7.2 miles southwest of Bethany
county: Harrison	feature inters.: East Fork of Big Creek
	cadastral grid: S8, T64N, R27W
	highway route: County Road 296
	highway distr.: 1
	current owner: Harrison County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss, with timber stringer approach spans	
substructure: unknown	
span number: 1	condition: fair
span length: 80.0'	alterations: unknown
total length: 109.0'	floor/decking : timber deck
roadway width: 12.0'	other features: unknown

HISTORICAL DATA

erection date: 1901	
erection cost: \$1150.00	
designer: Dildine Bridge Company, Cameron MO	
fabricator : unknown	
contractor: Dildine Bridge Company, Cameron MO	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 296000.6; Harrison County Court Record, Book O: page 90 (8 March 1901) - located at the Harrison County Courthouse, Bethany MO.	
sign. rating: 46	
evaluation: NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 21 August 1992

Panther Creek Bridge

HARR16

GENERAL DATA

structure no.:	319R02.8	city/town:	6.8 miles southeast of Ridgeway
county:	Harrison	feature inters.:	Panther Creek
		cadastral grid:	S10/15, T64N, R26W
		highway route:	County Road 319
		highway distr.:	1
		current owner:	Harrison County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and timber wingwalls		
span number:	1	condition:	fair
span length:	45.0'	alterations:	truss widened by welding extensions onto floor beams
total length:	45.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.0'	other features:	steel angle guardrail

HISTORICAL DATA

erection date:	1913
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	county work force

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 319R02.8; Harrison County Court Record Book R: page 186 (6 May 1913), located at the Harrison County Courthouse, Bethany MO.

sign. rating:	26
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 21 August 1992

Trail Creek Bridge

HARR17

GENERAL DATA

structure no.:	339000.9	city/town:	7.4 miles southeast of Ridgeway
county:	Harrison	feature inters.:	Trail Creek
		cadastral grid:	S33, T64N, R26W
		highway route:	County Road 339
		highway distr.:	1
		current owner:	Harrison County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and timber wingwalls		
span number:	1	condition:	fair
span length:	54.0'	alterations:	unknown
total length:	54.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.8'	other features:	steel angle guardrail

HISTORICAL DATA

erection date:	1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	Kansas City Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 339000.9; Harrison County Court Record Book T: page 213 (4 November 1915), located at the Harrison County Courthouse, Bethany MO.
sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 21 August 1992

Big Creek Bridge

HARR18

GENERAL DATA

structure no.:	359000.2	city/town:	3.5 miles northeast of Bethany
county:	Harrison	feature inters.:	East Fork Big Creek
		cadastral grid:	S25, T64N, R27W
		highway route:	County Road 359
		highway distr.:	1
		current owner:	Harrison County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	80.0'	alterations:	unknown
total length:	116.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.5'	other features:	unknown

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 359000.2;

sign. rating:	30
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 21 August 1992

Big Creek Bridge

HARR19

GENERAL DATA

structure no.: 368000.3	city/town: 4.2 miles north of Bethany
county: Harrison	feature inters.: West Fork Big Creek
	cadastral grid: S22, T64N, R28W
	highway route: County Road 368
	highway distr.: 1
	current owner: Harrison County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt bedstead, with steel stringer approach span	
substructure: steel pile bent piers and abutments timber wingwalls	
span number: 1	condition: fair
span length: 80.0'	alterations: unknown
total length: 94.0'	floor/decking : timber deck over steel stringers
roadway width: 11.8'	other features: steel angle guardrail

HISTORICAL DATA

erection date: 1915	
erection cost: \$1697.00	
designer: Dildine Bridge Company, Cameron MO	
fabricator : unknown	
contractor : Dildine Bridge Company, Hannibal MO	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 368000.3; Harrison County Court Record Book T: page 131 (7 July 1915), located at the Harrison County Courthouse, Bethany MO.	
sign. rating: 45	
evaluation: NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 21 August 1992

Big Creek Bridge

HARR20

GENERAL DATA

structure no.:	436002.6	city/town:	0.7 mile west of Bethany
county:	Harrison	feature inters.:	West Fork Big Creek
		cadastral grid:	S9, T63N, R28W
		highway route:	County Road 436
		highway distr.:	1
		current owner:	Harrison County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt through truss; steel stringer approaches		
substructure:	concrete-filled steel cylinder piers and timber back- and wingwalls		
span number:	1	condition:	fair
span length:	90.0'	alterations:	none
total length:	138.0'	floor/decking :	timber deck over steel stringers
roadway width:	15.8'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 rectangular eyebars; counter: 2 square rods with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beams, field-bolted to verticals; guardrail: 2 steel channels;

HISTORICAL DATA

erection date:	1905
erection cost:	\$2395.00
designer:	Dildine Bridge Company, Cameron MO
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 436002.6; Harrison County Court Record Book P: page 403 (26 September 1905), located at the Harrison County Courthouse, Bethany MO; Field Inspected by Mitzi Rossillon, 17 September 1990.
sign. rating:	43
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 21 August 1992

Fox Creek Bridge

HARR21

GENERAL DATA

structure no.:	566000.7	city/town:	4.5 miles northeast of Gilman City
county:	Harrison	feature inters.:	Fox Creek
		cadastral grid:	S5, T62N, R26W
		highway route:	County Road 566
		highway distr.:	1
		current owner:	Harrison County

STRUCTURAL DATA

superstructure:	steel, 8-panel, pin-connected Pratt bedstead		
substructure:	steel pile bent piers and timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	40.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.6'	other features:	steel angle guardrail

HISTORICAL DATA

erection date:	1914
erection cost:	\$638.49
designer:	unknown
fabricator :	unknown
contractor :	county work force

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 566000.7; Harrison County Court Record Book R: page 406 (20 June 1914), located at the Harrison County Courthouse, Bethany MO.

sign. rating:	30
evaluation:	NRHP (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 21 August 1992

Sugar Creek Bridge

HARR22

GENERAL DATA

structure no.: 574001.0	city/town: 3.7 miles northeast of Gilman City
county: Harrison	feature inters.: Sugar Creek
	cadastral grid: S9, T62N, R26W
	highway route: County Road 574
	highway distr.: 1
	current owner: Harrison County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss	
substructure: steel pile bent piers and timber wingwalls	
span number: 1	condition: fair
span length: 60.0'	alterations: unknown
total length: 60.0'	floor/decking : timber deck over steel stringers
roadway width: 13.5'	other features: steel angle guardrail

HISTORICAL DATA

erection date: 1918	
erection cost: \$1185.26	
designer: unknown	
fabricator : unknown	
contractor: Dildine Bridge Company, Cameron MO; county work force	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 574001.0; Harrison County Record Book No. 1: page 23 (1917), located at the Harrison County Courthouse, Bethany MO.	
sign. rating: 38	
evaluation: NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 21 August 1992

Sampson Creek Bridge

HARR23

GENERAL DATA

structure no.:	597002.3	city/town:	8.0 miles north of New Hampton
county:	Harrison	feature inters.:	Sampson Creek
		cadastral grid:	S29, T62N, R29W
		highway route:	County Road 597
		highway distr.:	1
		current owner:	Harrison County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss, with steel stringer approach		
substructure:	steel pile bent piers and timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	81.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.1'	other features:	unknown

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 597002.3;
sign. rating:	28
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 21 August 1992

Polecat Creek Bridge

HARR24

GENERAL DATA

structure no.:	640000.3	city/town:	6.0 miles southwest of Bethany
county:	Harrison	feature inters.:	Polecat Creek
		cadastral grid:	S9, T62N, R28W
		highway route:	County Road 640
		highway distr.:	1
		current owner:	Harrison County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	50.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.7'	other features:	steel angle guardrail

HISTORICAL DATA

erection date:	1917
erection cost:	\$1349.78
designer:	Dildine Bridge Company, Cameron MO
fabricator :	unknown
contractor :	Dildine Bridge Company, Cameron MO; county work force
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 640000.3; Harrison County Record Book No. 1: page 8 (1917), located at the Harrison County Courthouse, Bethany MO.
sign. rating:	35
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 21 August 1992

Big Creek Bridge

HARR25

GENERAL DATA

structure no.: 641001.5	city/town: 6.2 miles southwest of Bethany
county: Harrison	feature inters.: Big Creek
	cadastral grid: S7, T62N, R28W
	highway route: County Road 641
	highway distr.: 1
	current owner: Harrison County

STRUCTURAL DATA

superstructure: steel, 6-panel, pin-connected Pratt through truss	
substructure: steel pile bent piers and timber back- and wingwalls	
span number: 1	condition: fair
span length: 100.0'	alterations: substructure and approach spans replaced
total length: 133.0'	floor/decking : timber deck over steel stringers
roadway width: 13.9'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles; floor beam: I-beams, u-bolted to verticals; guardrail: 2 steel channels;

HISTORICAL DATA

erection date: c1905	
erection cost: unknown	
designer: unknown	
fabricator : Cambria Steel Company, Pittsburgh PA	
contractor: unknown	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 641001.5; Field Inspected by Mitzi Rossillon, 17 September 1990.
sign. rating: 24	
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 21 August 1992

Big Creek Bridge

HARR26

GENERAL DATA

structure no.:	687001.0	city/town:	1.1 mile west of Bethany
county:	Harrison	feature inters.:	East Fork Big Creek
		cadastral grid:	S9, T63N, R28W
		highway route:	County Road 687
		highway distr.:	1
		current owner:	Harrison County

STRUCTURAL DATA

superstructure: steel, 7-panel, pin-connected Pratt through truss; steel stringer approaches

substructure: timber abutments and backwalls; concrete piers

span number:	1	condition:	fair
span length:	110.0'	alterations:	truss moved, floor system replaced
total length:	158.0'	floor/decking :	asphalt over timber deck
roadway width:	13.0'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 rectangular, punched eyebars; vertical: 2 channels with lacing; diagonal: 2 rectangular, punched eyebars; counter: 2 round rods with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beams, u-bolted to verticals; guardrail: 2 angles; builder's plate: DILDINE / B. CO. / CAMERON / MO.

HISTORICAL DATA

erection date: c1905
erection cost: unknown
designer: Dildine Bridge Company, Cameron MO
fabricator : Carnegie Steel Company, Pittsburgh PA
contractor : Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 687001.0; Field Inspected by Mitzi Rossillon, 17 September 1990.

sign. rating: 27
evaluation: NRHP non-eligible (typical example of common structural type, moved to this location)

inventoried by: Michelle Crow-Dolby 21 August 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Big Creek Bridge
MHTD: 436002.6

HARR20

DATE(S) OF CONSTRUCTION

1905

LOCATION

County Road 436 over West Fork Big Creek; S9, T63N, R28W
0.7 mile west of Bethany; Harrison County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 43)

CONDITION

fair

OWNER

Harrison County

span number: 1
span length: 90.0'
total length: 138.0'
roadway wdt.: 15.8'

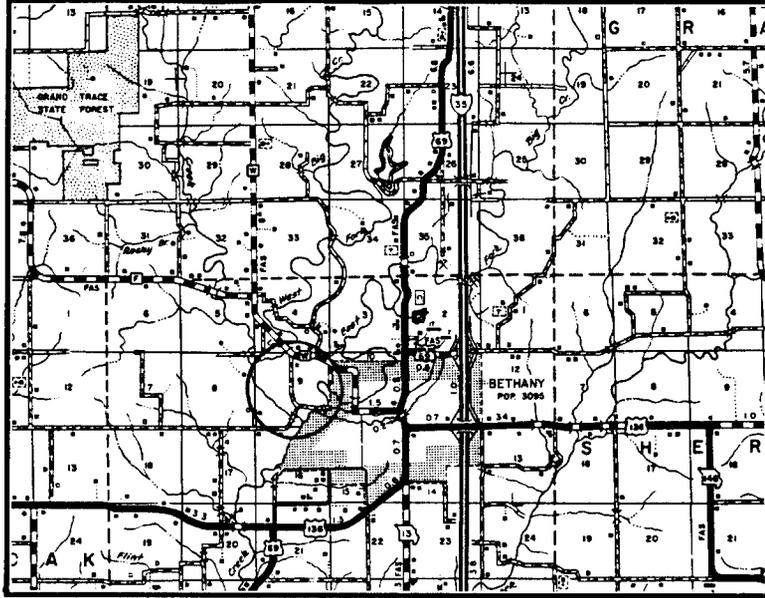
superstructure: steel, 5-panel, pin-connected Pratt through truss; steel stringer approaches
substructure: concrete-filled steel cylinder piers and timber back- and wingwalls
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 rectangular eyebars; counter: 2 square rods with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beams, field-bolted to verticals; guardrail: 2 steel channels;

During the first two decades of the 20th century, the Dildine Bridge Company of Cameron, Missouri, was responsible for building most of Harrison County's steel bridges. Bridge letting policies varied from county to county. Some counties awarded contracts for individual or multiple-bridge projects, while others, like Harrison County, issued annual contracts to bridge builders and fabricators. One of the structures erected by Dildine in 1905-for the sum of \$2395.00-was this short-span Pratt through truss built over the West Fork of Big Creek west of Bethany. Since its completion, the Big Creek Bridge has continued to carry intermittent vehicular traffic at this rural crossing for the past eighty-eight years. It is representative example of a mainstay structural type for Missouri: the pinned Pratt through truss.

NAME(S) OF STRUCTURE

Big Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 436002.6; Harrison County Court Record Book P: page 403 (26 September 1905), located at the Harrison County Courthouse, Bethany MO; Field Inspected by Mitzi Rossillon, 17 September 1990.

INVENTORIED BY
Mitzi Rossillon

AFFILIATION
Fraserdesign, Loveland CO

DATE
21 August 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Big Creek Bridge
MHTD: 641001.5

HARR25

DATE(S) OF CONSTRUCTION

c1905

LOCATION

County Road 641 over Big Creek; S7, T62N, R28W
6.2 miles southwest of Bethany; Harrison County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 24)

CONDITION

fair

OWNER

Harrison County

span number: 1
span length: 100.0'
total length: 133.0'
roadway wdt.: 13.9'

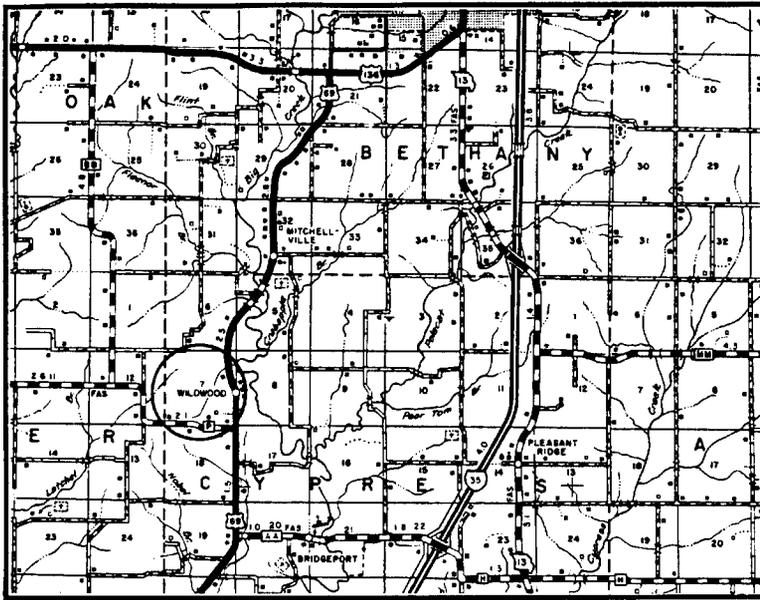
superstructure: steel, 6-panel, pin-connected Pratt through truss
substructure: steel pile bent piers and timber back- and wingwalls
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles; floor beam: I-beams, u-bolted to verticals; guardrail: 2 steel channels;

This single-span carries County Road 436 over the West Fork Big Creek in south-central Harrison County. Comprised of a pinned Pratt through truss supported by a steel pile bent substructure, the Big Creek Bridge was erected around 1905. County records do not contain any definitive documentation on this bridge, therefore its construction history remains vague. The Bridge's contractor used steel components rolled by Cambria Steel Company in Pittsburgh, Pennsylvania. Since its construction, this truss has functioned in place with the replacement of its substructure and approach spans the most serious alteration to date. It is typically configured example of a mainstay structural type in Missouri-the pinned Pratt through truss.

NAME(S) OF STRUCTURE

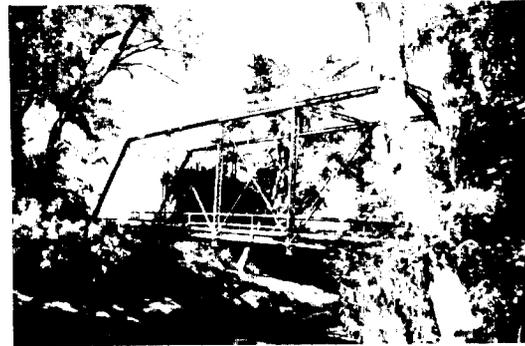
Big Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 641001.5; Field Inspected by Mitzi Rossillon, 17 September 1990.

INVENTORIED BY

Mitzi Rossillon

AFFILIATION

Fraserdesign, Loveland CO

DATE

21 August 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Big Creek Bridge
MHTD: 687001.0

HARR26

DATE(S) OF CONSTRUCTION

c1905

LOCATION

County Road 687 over East Fork Big Creek; S9, T63N, R28W
1.1 mile west of Bethany; Harrison County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 27)

CONDITION

fair

OWNER

Harrison County

span number: 1
span length: 110.0'
total length: 158.0'
roadway wdt.: 13.0'

superstructure: steel, 7-panel, pin-connected Pratt through truss; steel stringer approaches
substructure: timber abutments and backwalls; concrete piers
floor/decking: asphalt over timber deck
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 rectangular, punched eyebars; vertical: 2 channels with lacing; diagonal: 2 rectangular, punched eyebars; counter: 2 round rods with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beams, u-bolted to verticals; guardrail: 2 angle; builder's plate: DILDINE / B. CO. / CAMERON / MO.

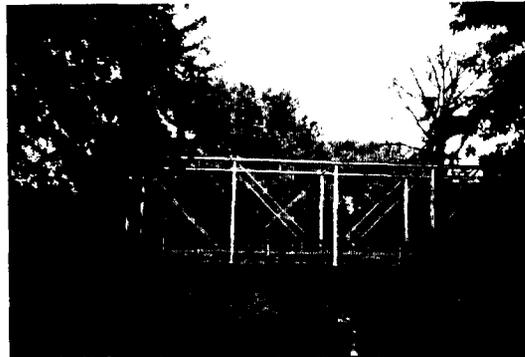
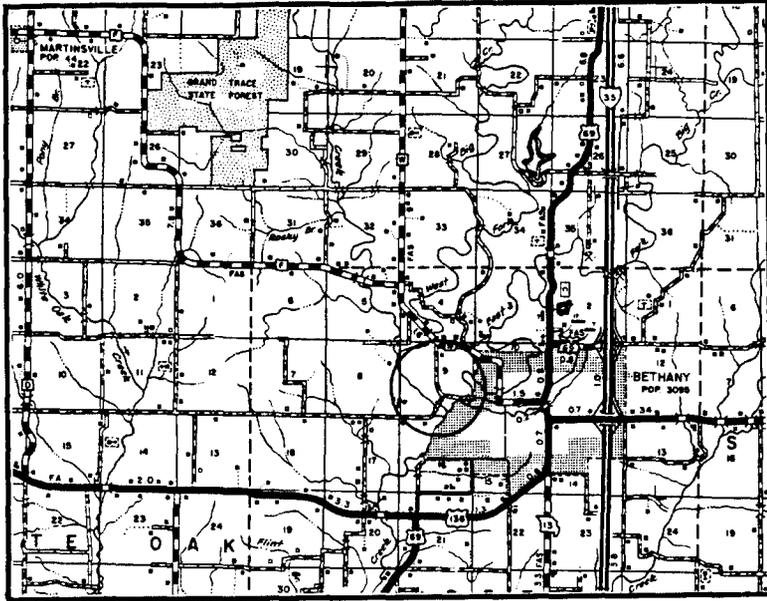
This Pratt through truss carries an unpaved county road over the East Fork Big Creek west of Bethany. This timber-decked bridge features a steel 110-foot truss, supported by a timber and concrete substructure. A builder's plate indicates that it was erected by the Cameron, Missouri-based Dildine Bridge Company, which used steel components fabricated by the Carnegie Steel Company. Additional construction history, however, remains vague, owing to the lack of conclusive documentation in county records. Since its erection, the Big Creek Bridge has apparently been moved and placed on new concrete piers.

Marketed exclusively by virtually all of the in-state and regional bridge contractors and promoted in the form of standardized designs, the pinned Pratt through truss was widely used by Missouri's counties to carry roads over the state's myriad watercourses. Thousands of such trusses were erected across the state in the late 19th and early 20th centuries, and many remain in place today. With an erection date circa 1905 and a span length of 110 feet, the Big Creek Bridge, typifies this widespread bridge building trend.

NAME(S) OF STRUCTURE

Big Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 687001.0; Field Inspected by Mitzi Rossillon, 17 September 1990.

INVENTORIED BY

Mitzi Rossillon

AFFILIATION

Fraserdesign, Loveland CO

DATE

21 August 1992

HOLT COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
HOLT01	H 457R	Little Tarkio Ditch Bridge	1-120' riveted Pratt through truss 1926 George W. Condon
*HOLT02	L 97	Rulo Bridge	2-375' riveted Pennsylv. through truss 1939 Kansas City Bridge Company / Missouri Valley B&I Company
HOLT03	T 104	Big Tarkio Creek Bridge	1-100' riveted Warren pony truss 1934 T.J. Morris
*HOLT04	006001.5	Corning Bridge	(replaced)
*HOLT05	012000.2	Tarkio River Bridge	1-100' pinned Pratt through truss c1910
HOLT06	030002.8	Little Tarkio Creek Bridge	1- 70' pinned Pratt pony truss c1910
HOLT07	034000.6	Little Tarkio Creek Bridge	1- 60' pinned Pratt pony truss c1910
HOLT08	040000.9	Little Tarkio Creek Bridge	1- 50' riveted Camelback pony truss c1925
HOLT09	049001.0	Minnesota Valley Ck. Bridge	1- 80' riveted Camelback pony truss c1925
HOLT10	049001.3	Minnesota Valley Ck. Bridge	1- 40' pinned Pratt pony truss c1910
HOLT11	057000.2	Middle Branch Bridge	1- 40' pinned Pratt pony truss c1910
HOLT12	062001.7	Squaw Creek Bridge	(replaced)
HOLT13	072000.7	Bridge	1- 40' pinned Pratt pony truss c1910
HOLT14	086000.6	Highly Creek Bridge	1- 62' pinned Pratt half-hip pony truss c1910
HOLT15	088000.8	Whig Valley Creek Bridge	1- 55' pinned Pratt pony truss c1910
HOLT16	089001.6	Whig Valley Creek Bridge	1- 60' pinned Pratt pony truss c1910
HOLT17	106000.5	Squaw Creek Bridge	1- 60' pinned Pratt pony truss c1910
HOLT18	107000.5	Squaw Creek Bridge	1- 80' pinned Pratt pony truss c1910
HOLT19	108000.8	Middle Branch Bridge	1- 60' pinned Pratt pony truss c1910
HOLT20	111000.7	Squaw Creek Bridge	1- 75' pinned Pratt pony truss c1915
HOLT21	115000.3	Davis Creek Bridge	1- 48' pinned Pratt half-hip pony truss c1910
HOLT22	116002.4	Davis Creek Bridge	1- 50' pinned Pratt pony truss c1910
HOLT23	119001.0	Davis Creek Bridge	1- 60' pinned Pratt pony truss c1910

HOLT COUNTY

INCLUDED (cont.):

*HOLT24	126000.5	Squaw Creek Bridge	1-120' c1910	pinned Pratt through truss
HOLT25	127001.7	Squaw Creek Bridge	1- 60' c1910	pinned Pratt pony truss
*HOLT26	128000.3	Minnesota Valley Ck. Bridge	1- 80' c1925	2-angle Camelback pony truss
HOLT27	131002.7	Wildcat Creek Bridge	1- 60' c1910	pinned Pratt pony truss
HOLT28	134R01.0	Wildcat Creek Bridge	1- 50' c1910	pinned Pratt pony truss
HOLT29	135000.8	Wildcat Creek Bridge	1- 50' c1910	pinned Pratt pony truss
HOLT30	145001.7	Little Tarkio Creek Bridge	1- 45' c1910	pinned Pratt pony truss
HOLT31	148500.1	Grand Ave Creek Bridge	1- 40' c1910	pinned Pratt pony truss
HOLT32	148500.2	Grand Ave Creek Bridge	1- 60' c1910	pinned Pratt pony truss
*HOLT33	165000.1	Tarkio River Bridge	1-100' c1915	pinned Pratt through truss
HOLT34	233000.7	Kunkel Mill Bridge	1- 65' 1919	pinned Pratt pony truss Kansas City Bridge Company (replaced)
HOLT35	234001.5	Kimsey Creek Bridge		
HOLT36	243000.8	Hog Creek Bridge	1- 50' c1910	pinned Pratt pony truss
HOLT37	247000.8	Hickory Creek Bridge	1- 60' c1910	pinned Pratt pony truss
*HOLT38	257001.6	Leach Mill Bridge	1- 72' 1913	pinned Pratt through truss A.S. Seybert
HOLT39	280000.5	Rock Creek Bridge	1- 50' c1910	pinned Pratt pony truss
HOLT40	291001.4	Grand Ave Creek Bridge	1- 60' c1910	pinned Pratt pony truss
HOLT41	292000.6	Kimsey Creek Bridge	1- 60' c1910	pinned Pratt pony truss
HOLT42	300001.8	Kimsey Creek Bridge		(replaced)
HOLT43	301R01.1	Kimsey Creek Bridge	1- 75' c1910	pinned Pratt pony truss
HOLT44	321000.9	Mill Creek Bridge	1- 75' c1910	pinned Pratt pony truss
HOLT45	324000.0	Mill Creek Bridge		(replaced)
HOLT46	328000.0	Schlotz Branch Bridge	1- 40' c1910	pinned Pratt pony truss
HOLT47	336000.9	Easter Creek Bridge		(replaced)
HOLT48	336002.4	Forbes Creek Bridge	1- 45' c1910	pinned Pratt half-hip pony truss
HOLT49	348000.8	Smith Creek Bridge		(replaced)

HOLT COUNTY

EXCLUDED:

Pratt pony truss

349002.7 363R01.9

Warren pony truss

H 704	074000.2	086000.8	088000.3	110R01.6	110000.3	111001.6
119002.2	123001.1	133000.4	133002.9	145000.6	225000.2	282001.3
301001.5	320000.1	346001.3	369000.1			

Steel stringer

H 705R	H 795	J 252	K 841	S 183R	S 430	S 431
S 433	T 587	T 588	X 165	X 265	X 266	012002.1
053000.3	064000.5	065000.1	072000.3	099000.5	100002.2	102001.8
107000.1	110003.5	119001.8	127000.1	127000.8	148500.4	220001.5
267000.7	282001.6	333000.6	350000.0	353000.2	360000.1	369001.0

Lattice pony truss

084001.9 298000.5

Steel girder

050000.8	054000.8	055001.2	061000.5	075000.8	085000.8	090000.1
114001.4	116000.6	116001.6	125000.5	148500.3	220002.5	231001.3
241000.2	243000.1	250000.9	278000.8	284000.5	330R00.5	343001.7

Concrete slab / girder

H 670	H 703	J 169	J 170	T 230	294500.1
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Concrete box culvert

H 828	S 429	S 432	T 59	X 602R	113000.2
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SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	3	39	0	0	42
Excluded	24	64	0	0	88
	27	103	0	0	130 structures

Little Tarkio Ditch Bridge

HOLT01

GENERAL DATA

structure no.:	H 457R	city/town:	1.8 miles east of Craig
county:	Holt	feature inters.:	Little Tarkio Ditch
		cadastral grid:	S7, T62N, R39W
		highway route:	U.S. Highway 59
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Pratt through truss, skewed, with steel stringer approach spans		
substructure:	concrete abutments and wingwalls; hammerhead spill through piers		
span number:	1	condition:	good
span length:	120.0'	alterations:	bridge renovated, 1943
total length:	245.0'	floor/decking :	concrete deck over steel stringers
roadway width:	20.0'	other features:	steel pipe guardrails

HISTORICAL DATA

erection date:	1926
erection cost:	\$20,066.30
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	George W. Condon Company (original construction); Otto K. Knutson (1943 renovation)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number H 457R; Primary System Bridge files, located at Missouri Highway and Transportation Department, Jefferson City MO.
sign. rating:	47
evaluation:	NRHP non-eligible (atypically configured example of MSHD highway truss design, altered)

inventoried by: Mark Hufstetler 21 August 1992

Rulo Bridge

HOLT02

GENERAL DATA

structure no.: L97	city/town: 5.3 miles west Fortescue
county: Holt / Richardson	feature inters.: Missouri River
	cadastral grid: S33, T61N, R40W
	highway route: U.S. Highway 159
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department / Nebraska Department of Roads

STRUCTURAL DATA

superstructure: steel, 16-panel rigid-connected Pennsylvania through truss, with rigid-connected Warren deck trusses

substructure: concrete abutments and wingwalls; concrete two-legged piers and solid channel piers

span number: 3	condition: good
span length: 375.0'	alterations: none
total length: 859.0'	floor/decking : concrete deck over I-beam transverse joists, which rest on I-beam stringers
roadway width: 20.0'	other features: upper chord and inclined end post: 2 channels with cover plate and double lacing; lower chord: 2 channels with lacing; vertical: built up I-beam girder with lacing or 4 angles with lacing; diagonal: 2 channels with lacing; lateral bracing: 4 angles with lacing; strut: 4 angles with lacing; floor beam: I-beams; guardrail: 3 angles; builder's plate: FEDERAL WORKS AGENCY / PUBLIC WORKS ADMINISTRATION / JOHN M. CARMODY / FEDERAL WORKS ADMINISTRATOR / FRANKLIN D. ROOSEVELT / PRESIDENT OF THE UNITED STATES / MISSOURI RIVER BRIDGE / 1939, and RICHARDSON COUNTY, NEBRASKA / KANSAS CITY BRIDGE COMPANY / MISSOURI VALLEY BRIDGE AND IRON COMPANY / CONTRACTORS / HARRINGTON AND CORTELYOU / CONSULTING ENGINEERS

HISTORICAL DATA

erection date: 1938-39
erection cost: \$651,296.01
designer: Harrington and Cortelyou, Kansas City MO
fabricator : unknown
contractor: Kansas City Bridge Company, Kansas City MO; Missouri Valley Bridge and Iron Company, Leavenworth KS

Rulo Bridge

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number L97; Nebraska Department of Roads and Irrigation, **Twenty-Third Biennial Report**, 1939-40, page 23; Henry G. Schlitt, "Missouri River Bridges, Report #2," 7 November 1945, located in Bridge Division, Nebraska Department of Roads, Lincoln NE; Proceedings of the Richardson County Commissioners, Book 13: pages 673-74 (14 February 1934); Book 14: page 276 (10 September 1935), pages 284-85 (24 September 1935), page 419 (23 June 1936), pages 52-53 (29 June 1938); Book 15: pages 67-70 (28 July 1938), pages 111-13 (13 September 1938), page 161 (29 November 1938), page 183 (17 January 1939), page 239 (25 July 1939), page 276 (7 November 1939), page 324 (2 April 1940); Book 16: page 73 (11 October 1966); Book 20: page 106 (17 January 1967), page 363 (10 December 1968), page 371 (7 January 1969), page 389 (25 February 1969), page 426 (24 June 1969)- located in Richardson County Courthouse, Falls City NE; field inspection by Demian Hess, 13 March 1990; field inspection by Mark Hufstetler, 7 September 1990.

sign. rating: 65

evaluation: NRHP listed (well-preserved example of long-span truss design, on regionally important crossing of the Missouri River)

inventoried by: Mark Hufstetler and Demian Hess 21 August 1992

Big Tarkio Creek Bridge

HOLT03

GENERAL DATA

structure no.: T104	city/town: 2.5 miles west of Craig
county: Holt	feature inters.: Big Tarkio Creek
	cadastral grid: S9/16, T62N, R40W
	highway route: State Highway 111
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 6-panel, rigid-connected, Warren pony truss with polygonal upper chords; steel stringer approach spans	
substructure: concrete abutments, wingwalls and piers	
span number: 1	condition: good
span length: 100.0'	alterations: none
total length: 180.0'	floor/decking : concrete deck over steel stringers
roadway width: 20.0'	other features: steel guardrails

HISTORICAL DATA

erection date: 1934	
erection cost: \$14,200.30	
designer: Missouri Highway and Transportation Department	
fabricator : unknown	
contractor : T.J. Morris	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number T104; Primary System Bridge files, located at Missouri Highway and Transportation Department, Jefferson City MO.	
sign. rating: 43	
evaluation: NRHP non-eligible (typically configured, long-span example of MSHD bridge design)	

inventoried by: Mark Hufstetler 21 August 1992

Tarkio River Bridge

HOLT05

GENERAL DATA

structure no.:	012000.2	city/town:	3.7 miles northwest of Craig
county:	Holt	feature inters.:	Tarkio River
		cadastral grid:	S33, T63N, R40W
		highway route:	County Road 12
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt through truss, with steel stringer approach spans		
substructure:	concrete-filled steel cylinder piers; steel pile bent abutments at east end		
span number:	1	condition:	fair
span length:	100.0'	alterations:	none
total length:	158.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.8'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; strut: 2 angles; floor beam: I-beam, field-bolted to verticals; guardrail: 2 steel channels

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	Cambria Steel Company, Pittsburgh PA
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 012000.2; field inspection by Mark Hufstetler, 7 September 1990.
sign. rating:	27
evaluation:	NRHP non-eligible (typically configured, inadequately documented example of common structural type)

Inventoried by: Mark Hufstetler 21 August 1992

Little Tarkio Creek Bridge

HOLT06

GENERAL DATA

structure no.: 030002.8 city/town: 5.3 miles northeast of Craig
county: Holt / Atchison feature inters.: Little Tarkio Creek
cadastral grid: S18/19, T63N, R39W
highway route: County Road 30
highway distr.: 1
current owner: Holt County / Atchison County

STRUCTURAL DATA

superstructure: steel, pin-connected Pratt pony truss
substructure: steel pile bent piers and timber wingwalls

span number: 1 condition: fair
span length: 70.0' alterations: unknown
total length: 74.0' floor/decking : timber deck
roadway width: 14.0' other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory
and Appraisal: Structure Number 030002.8.

sign. rating: 28
evaluation: NRHP non-eligible (undistinguished example of common structural type,
lacking in documentation)

inventoried by: Mark Hufstetler 21 August 1992

Little Tarkio Creek Bridge

HOLT07

GENERAL DATA

structure no.:	034000.6	city/town:	4.7 miles northeast of Craig
county:	Holt	feature inters.:	East Fork of Little Tarkio Creek
		cadastral grid:	S19/20, T63N, R39W
		highway route:	County Road 34
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	91.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 034000.6.
sign. rating:	28
evaluation:	NRHP non-eligible (undistinguished example of common structural type, lacking in documentation)

Inventoried by: Mark Hufstetler 21 August 1992

Little Tarkio Creek Bridge

HOLT08

GENERAL DATA

structure no.: 040000.9	city/town: 5.7 miles northeast of Craig
county: Holt / Atchison	feature inters.: East Fork of Little Tarkio Creek
	cadastral grid: S17/20, T63N, R39W
	highway route: County Road 40
	highway distr.: 1
	current owner: Holt County / Atchison County

STRUCTURAL DATA

superstructure: steel, 6-panel, rigid-connected Camelback pony truss	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: unknown
total length: 89.0'	floor/decking : timber deck
roadway width: 14.1'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1925
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 040000.9.

sign. rating: 33
evaluation: NRHP non-eligible (largely undocumented example of uncommon Pratt truss subtype)

inventoried by: Mark Hufstetler 21 August 1992

Minnesota Valley Creek Bridge

HOLT09

GENERAL DATA

structure no.:	049001.0	city/town:	8.0 miles north of Mound City
county:	Holt	feature inters.:	Minnesota Valley Creek
		cadastral grid:	S19/30, T63N, R38W
		highway route:	County Road 49
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Camelback pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	80.0'	alterations:	unknown
total length:	82.0'	floor/decking :	timber deck
roadway width:	15.9'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 049001.0.

sign. rating:	38
evaluation:	NRHP non-eligible (largely undocumented example of uncommon Pratt truss subtype)

inventoried by: Mark Hufstetler 21 August 1992

Minnesota Valley Creek Bridge

HOLT10

GENERAL DATA

structure no.: 049001.3	city/town: 8.0 miles north of Mound City
county: Holt	feature inters.: Minnesota Valley Creek
	cadastral grid: S19, T63N, R38W
	highway route: County Road 49
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: unknown	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 41.0'	floor/decking : timber deck
roadway width: 13.8'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 049001.3.

sign. rating: 20
evaluation: NRHP non-eligible (typically configured, modestly scaled example of common structural type)

inventoried by: Mark Hufstetler 21 August 1992

Middle Branch Bridge

HOLT11

GENERAL DATA

structure no.:	057000.2	city/town:	8.0 miles northeast of Mound City
county:	Holt	feature inters.:	Middle Branch
		cadastral grid:	S28/29, T63N, R38W
		highway route:	County Road 57
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	41.0'	floor/decking :	timber deck
roadway width:	16.0'	other features:	unknown

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 057000.2.

sign. rating:	20
evaluation:	NRHP non-eligible (typically configured, modestly scaled example of common structural type)

Inventoried by: Mark Hufstetler 21 August 1992

Bridge

HOLT13

GENERAL DATA

structure no.:	072000.7	city/town:	4.0 miles northwest of Maitland
county:	Holt	feature inters.:	branch of Nodaway River
		cadastral grid:	S19/20, T63N, R37W
		highway route:	County Road 72
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with timber stringer approach spans		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	75.0'	floor/decking :	timber deck
roadway width:	13.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 072000.7.

sign. rating:	25
evaluation:	NRHP non-eligible (typically configured, modestly scaled example of common structural type)

inventoried by: Mark Hufstetler 21 August 1992

Highly Creek Bridge

HOLT14

GENERAL DATA

structure no.:	086000.6	city/town:	7.5 miles northeast of Mound City
county:	Holt	feature inters.:	Highly Creek
		cadastral grid:	S17/18, T62N, R37W
		highway route:	County Road 86
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt half-hip pony truss
substructure: unknown

span number:	1	condition:	fair
span length:	62.0'	alterations:	unknown
total length:	63.0'	floor/decking :	timber deck
roadway width:	13.5'	other features:	steel lattice guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 086000.6.

sign. rating: 28
evaluation: NRHP non-eligible (typically configured example of common structural type, poorly documented)

inventoried by: Mark Hufstetler 21 August 1992

Whig Valley Creek Bridge

HOLT15

GENERAL DATA

structure no.: 088000.8	city/town: 7.0 miles northeast of Mound City
county: Holt	feature inters.: Whig Valley Creek
	cadastral grid: S19/20, T62N, R37W
	highway route: County Road 88
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: steel pile bent piers and timber wingwalls	
span number: 1	condition: fair
span length: 55.0'	alterations: unknown
total length: 55.0'	floor/decking : timber deck
roadway width: 13.7'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 088000.8.

sign. rating: 25
evaluation: NRHP non-eligible (typically configured, modestly scaled example of common structural type, poorly documented)

inventoried by: Mark Hufstetler 21 August 1992

Whig Valley Creek Bridge

HOLT16

GENERAL DATA

structure no.:	089001.6	city/town:	6.5 miles northeast of Mound City
county:	Holt	feature inters.:	Whig Valley Creek
		cadastral grid:	S19/30, T62N, R37W
		highway route:	County Road 89
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	62.0'	floor/decking :	timber deck
roadway width:	13.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 089001.6.

sign. rating:	28
evaluation:	NRHP non-eligible (typically configured example of common structural type, poorly documented)

inventoried by: Mark Hufstetler 21 August 1992

Squaw Creek Bridge

HOLT17

GENERAL DATA

structure no.:	106000.5	city/town:	5.7 miles northeast of Mound City
county:	Holt	feature inters.:	Squaw Creek
		cadastral grid:	S4/5, T62N, R38W
		highway route:	County Road 106
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	timber pile bent abutments and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	60.0'	floor/decking :	timber deck
roadway width:	15.5'	other features:	steel lattice guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 106000.5.
sign. rating:	28
evaluation:	NRHP non-eligible (typically configured example of common structural type, poorly documented)

inventoried by: Mark Hufstetler 21 August 1992

Squaw Creek Bridge

HOLT18

GENERAL DATA

structure no.:	107000.5	city/town:	6.0 miles northeast of Mound City
county:	Holt	feature inters.:	Squaw Creek Branch
		cadastral grid:	S5/6, T62N, R38W
		highway route:	County Road 107
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	80.0'	alterations:	unknown
total length:	99.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel lattice guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 107000.5.

sign. rating:	30
evaluation:	NRHP non-eligible (typically configured example of common structural type, poorly documented)

inventoried by: Mark Hufstetler 21 August 1992

Middle Branch Bridge

HOLT19

GENERAL DATA

structure no.: 108000.8	city/town: 5.5 miles northeast of Mound City
county: Holt	feature inters.: Middle Branch
	cadastral grid: S6, T62N, R38W
	highway route: County Road 108
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 60.0'	alterations: unknown
total length: 81.0'	floor/decking : timber deck
roadway width: 15.6'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 108000.8.	
sign. rating: 28	
evaluation: NRHP non-eligible (typically configured example of common structural type, poorly documented)	

inventoried by: Mark Hufstetler 21 August 1992

Squaw Creek Bridge

HOLT20

GENERAL DATA

structure no.:	111000.7	city/town:	4.4 miles northeast of Mound City
county:	Holt	feature inters.:	Squaw Creek
		cadastral grid:	S7/8, T62N, R38W
		highway route:	County Road 111
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	75.0'	alterations:	unknown
total length:	77.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	timber guardrails

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 111000.7.

sign. rating:	28
evaluation:	NRHP non-eligible (typically configured example of common structural type, poorly documented)

inventoried by: Mark Hufstetler 21 August 1992

Davis Creek Bridge

HOLT21

GENERAL DATA

structure no.:	115000.3	city/town:	5.0 miles northeast of Mound City
county:	Holt	feature inters.:	branch of Davis Creek
		cadastral grid:	S10/15, T62N, R38W
		highway route:	County Road 115
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	48.0'	alterations:	unknown
total length:	49.0'	floor/decking :	timber deck
roadway width:	15.4'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 115000.3.
sign. rating:	25
evaluation:	NRHP non-eligible (typically configured, modestly scaled example of common structural type, poorly documented)

inventoried by: Mark Hufstetler 21 August 1992

Davis Creek Bridge

HOLT22

GENERAL DATA

structure no.:	116002.4	city/town:	3.1 miles northeast of Mound City
county:	Holt	feature inters.:	branch of Davis Creek
		cadastral grid:	S21/22, T62N, R38W
		highway route:	County Road 116
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	69.0'	floor/decking :	timber deck
roadway width:	13.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 116002.4.

sign. rating:	25
evaluation:	NRHP non-eligible (typically configured, modestly scaled example of common structural type, poorly documented)

inventoried by: Mark Hufstetler 21 August 1992

Davis Creek Bridge

HOLT23

GENERAL DATA

structure no.: 119001.0	city/town: 2.3 miles northeast of Mound City
county: Holt	feature inters.: Davis Creek
	cadastral grid: S28/29, T62N, R38W
	highway route: County Road 119
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 60.0'	alterations: unknown
total length: 61.0'	floor/decking : timber deck
roadway width: 15.8'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 119001.0.

sign. rating: 28
evaluation: NRHP non-eligible (typically configured example of common structural type, poorly documented)

inventoried by: Mark Hufstetler 21 August 1992

Squaw Creek Bridge

HOLT24

GENERAL DATA

structure no.: 126000.5	city/town: 3.3 miles northwest of Mound City
county: Holt	feature inters.: Squaw Creek
	cadastral grid: S13/24, T62N, R39W
	highway route: County Road 126
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 7-panel, pin-connected Pratt through truss	
substructure: steel pile bent abutments with timber back- and wingwalls	
span number: 1	condition: fair
span length: 120.0'	alterations: unknown
total length: 120.0'	floor/decking : timber deck over steel stringers
roadway width: 15.7'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: 1 angle; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : Lackawanna Steel Company, Pittsburgh PA	
contractor: unknown	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 126000.5; field inspection by Mark Hufstetler, 7 September 1990.
sign. rating: 30	
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Mark Hufstetler 21 August 1992

Squaw Creek Bridge

HOLT25

GENERAL DATA

structure no.:	127001.7	city/town:	3.5 miles northwest of Mound City
county:	Holt	feature inters.:	branch of Squaw Creek
		cadastral grid:	S13/14, T62N, R39W
		highway route:	County Road 127
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	61.0'	floor/decking :	timber deck
roadway width:	15.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 127001.7.

sign. rating:	28
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Mark Hufstetler 21 August 1992

Minnesota Valley Creek Bridge

HOLT26

GENERAL DATA

structure no.: 128000.3	city/town: 5.2 miles northwest of Mound City
county: Holt	feature inters.: Minnesota Valley Creek
	cadastral grid: S1/12, T62N, R39W
	highway route: County Road 128
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 5-panel, rigid-connected, 2-angle Camelback pony truss, with steel stringer approach spans	
substructure: steel pile bent piers, timber pile abutments and wingwalls	
span number: 1	condition: fair
span length: 80.0'	alterations: none
total length: 98.0'	floor/decking : timber deck over steel stringers
roadway width: 14.0'	other features: upper chord and inclined end post, lower chord, vertical and diagonal: 2 angles; floor beam: I-beams, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date: c1925	
erection cost: unknown	
designer: unknown	
fabricator : Inland Steel Company, East Chicago IN	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 128000.3; field inspection by Mark Hufstetler, 7 September 1990.	
sign. rating: 40	
evaluation: NRHP non-eligible (uncommon structural type, largely undocumented)	

inventoried by: Mark Hufstetler 21 August 1992

Wildcat Creek Bridge

HOLT27

GENERAL DATA

structure no.: 131002.7	city/town: 3.0 miles northwest of Mound City
county: Holt	feature inters.: Wildcat Creek
	cadastral grid: S23, T62N, R39W
	highway route: County Road 131
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 60.0'	alterations: unknown
total length: 98.0'	floor/decking : timber deck
roadway width: 15.9'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 131002.7.

sign. rating: 28
evaluation: NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Mark Hufstetler 21 August 1992

Wildcat Creek Bridge

HOLT28

GENERAL DATA

structure no.: 134R01.0	city/town: 5.5 miles northwest of Mound City
county: Holt	feature inters.: Wildcat Creek
	cadastral grid: S3/10, T62N, R39W
	highway route: County Road 134
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: unknown
total length: 51.0'	floor/decking : timber deck
roadway width: 15.8'	other features: unknown

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 134R01.0.

sign. rating: 25
evaluation: NRHP non-eligible (typically configured, modestly scaled example of common structural type, largely undocumented)

inventoried by: Mark Hufstetler 21 August 1992

Wildcat Creek Bridge

HOLT29

GENERAL DATA

structure no.: 135000.8	city/town: 4.7 miles northwest of Mound City
county: Holt	feature inters.: Wildcat Creek
	cadastral grid: S10/15, T62N, R39W
	highway route: County Road 135
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach span	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: unknown
total length: 69.0'	floor/decking : timber deck
roadway width: 15.5'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 135000.8.

sign. rating: 25
evaluation: NRHP non-eligible (typically configured, modestly scaled example of common structural type, largely undocumented)

inventoried by: Mark Hufstetler 21 August 1992

Little Tarkio Creek Bridge

HOLT30

GENERAL DATA

structure no.: 145001.7	city/town: 7.7 miles northwest of Mound City
county: Holt	feature inters.: branch of Little Tarkio Creek
	cadastral grid: S5/6, T62N, R39W
	highway route: County Road 145
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 45.0'	alterations: unknown
total length: 45.0'	floor/decking : timber deck
roadway width: 15.7'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 145001.7.

sign. rating: 20
evaluation: NRHP non-eligible (typically configured, modestly scaled example of common structural type, largely undocumented)

inventoried by: Mark Hufstetler 21 August 1992

Grand Avenue Creek Bridge

HOLT31

GENERAL DATA

structure no.: 148500.1	city/town: Forest City
county: Holt	feature inters.: Grand Avenue Creek
	cadastral grid: S32, T60N, R38W
	highway route: County Road 148
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: none
total length: 41.0'	floor/decking : timber deck over steel stringers
roadway width: 13.8'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 148500.1.

sign. rating: 20
evaluation: NRHP non-eligible (typically configured, modestly scaled example of common structural type, largely undocumented)

inventoried by: Mark Hufstetler 21 August 1992

Grand Avenue Creek Bridge

HOLT32

GENERAL DATA

structure no.: 148500.2	city/town: Forest City
county: Holt	feature inters.: Grand Avenue Creek
	cadastral grid: S32, T60N, R38W
	highway route: County Road 148
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 60.0'	alterations: none
total length: 61.0'	floor/decking : timber deck
roadway width: 15.8'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 148500.2.	
sign. rating: 28	
evaluation: NRHP non-eligible (typically configured example of common structural type, largely undocumented)	

inventoried by: Mark Hufstetler 21 August 1992

Tarkio River Bridge

HOLT33

GENERAL DATA

structure no.: 165000.1	city/town: 3.0 miles northwest of Craig
county: Holt	feature inters.: Tarkio River
	cadastral grid: S4/9, T62N, R40W
	highway route: County Road 165
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt through truss, with steel stringer approach spans	
substructure: concrete pier with steel pile bent abutments	
span number: 1	condition: fair
span length: 100.0'	alterations: none
total length: 158.0'	floor/decking : timber deck over steel stringers
roadway width: 13.6'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing and strut: 2 angles; floor beam: I-beam, field-bolted to verticals; guardrail: 2 steel channels

HISTORICAL DATA

erection date: c1915	
erection cost: unknown	
designer: unknown	
fabricator : Jones and Laughlin Steel Company, Pittsburgh PA	
contractor : unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 165000.1; field inspection by Mark Hufstetler, 7 September 1990.	
sign. rating: 27	
evaluation: NRHP non-eligible (typical example of common structural type, inadequately documented)	

inventoried by: Mark Hufstetler 21 August 1992

Kunkel Mill Bridge

HOLT34

GENERAL DATA

structure no.:	233000.7	city/town:	4.6 miles northwest of Oregon
county:	Holt	feature inters.:	Kimsey Creek
		cadastral grid:	S34, T61N, R38W
		highway route:	County Road 233
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach span		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	65.0'	alterations:	unknown
total length:	103.0'	floor/decking :	timber deck
roadway width:	11.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1919
erection cost:	\$1678.00
designer:	Kansas City Bridge Company, Kansas City MO
fabricator :	Kansas City Bridge Company, Kansas City MO
contractor :	Kansas City Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 233000.7; Holt County Court Record, Book R: page 335 (18 July 1919), page 349 (22 September 1919) - located at the Holt County Courthouse, Oregon MO.
sign. rating:	42
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Mark Hufstetler 21 August 1992

Hog Creek Bridge

HOLT36

GENERAL DATA

structure no.: 243000.8	city/town: 8.0 miles east of Mound City
county: Holt	feature inters.: Hog Creek
	cadastral grid: S4, T61N, R37W
	highway route: County Road 243
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: unknown
total length: 100.0'	floor/decking : timber deck
roadway width: 13.6'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 243000.8.	
sign. rating: 25	
evaluation: NRHP non-eligible (typically configured, modestly scaled example of common structural type, largely undocumented)	

inventoried by: Mark Hufstetler 21 August 1992

Hickory Creek Bridge

HOLT37

GENERAL DATA

structure no.:	247000.8	city/town:	7.3 miles northeast of Oregon
county:	Holt	feature inters.:	Hickory Creek
		cadastral grid:	S17/20, T61N, R37W
		highway route:	County Road 247
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	61.0'	floor/decking :	timber deck
roadway width:	15.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 247000.8.

sign. rating:	28
evaluation:	NRHP non-eligible (typically configured example of common structural type, largely undocumented)

Inventoried by: Mark Hufstetler 21 August 1992

Leach Mill Bridge

HOLT38

GENERAL DATA

structure no.:	257001.6	city/town:	8.0 miles northeast of Oregon
county:	Holt	feature inters.:	Nodaway River
		cadastral grid:	S12, T60N, R37W
		highway route:	County Road 257
		highway distr.:	1
		current owner:	Holt County / Andrew County

STRUCTURAL DATA

superstructure: steel, 8-panel, pin-connected Pratt through truss, with pin-connected Pratt pony truss approach span

substructure: concrete-filled steel cylinder piers; steel pile bent abutments with timber back- and wingwalls

span number:	2	condition:	fair
span length:	72.0'	alterations:	none
total length:	130.0'	floor/decking :	timber deck over steel stringers
roadway width:	15.7'	other features:	through truss: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; floor beam: I-beam, field-bolted to verticals; guardrail: 1 angle; pony truss: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; floor beam: I-beam, field-bolted to verticals; guardrail: steel channel

HISTORICAL DATA

erection date:	1913
erection cost:	\$4225.00 (contract amount)
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN; Lackawanna Steel Company, Pittsburgh PA (verticals)
contractor:	A.S. Seybert

Leach Mill Bridge

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 257001.6; original plans "for Bridge across Nodaway River at Leach Mills" (№ 30, 1913), and contract with A.S. Seybert (1913) - located in old bridge specifications box **Bridge Contracts and Bonds 1911**, at the Andrew County Courthouse, Savannah MO; field inspection by Mark Hufstetler, 7 September 1990.

sign. rating: 37

evaluation: NRHP non-eligible (typically configured, short-span example of mainstay structural type)

inventoried by: Mark Hufstetler and Michelle Crow-Dolby 21 August 1992

Rock Creek Bridge

HOLT39

GENERAL DATA

structure no.: 280000.5	city/town: 2.3 miles east of Oregon
county: Holt	feature inters.: Rock Creek
	cadastral grid: S31/36, T60N, R37/38W
	highway route: County Road 280
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach span	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: unknown
total length: 73.0'	floor/decking : timber deck
roadway width: 15.7'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 280000.5.	
sign. rating: 25	
evaluation: NRHP non-eligible (typically configured, modestly scaled example of common structural type, largely undocumented)	

Inventoried by: Mark Hufstetler 21 August 1992

Grand Avenue Creek Bridge

HOLT40

GENERAL DATA

structure no.: 291001.4	city/town: 1.7 miles northwest of Oregon
county: Holt	feature inters.: Grand Avenue Creek
	cadastral grid: S21, T60N, R38W
	highway route: County Road 291
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach span	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 60.0'	alterations: unknown
total length: 76.0'	floor/decking : timber deck over steel stringers
roadway width: 13.6'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 291001.4.	
sign. rating: 28	
evaluation: NRHP non-eligible (typically configured example of common structural type, largely undocumented)	

inventoried by: Mark Hufstetler 21 August 1992

Kimsey Creek Bridge

HOLT41

GENERAL DATA

structure no.: 292000.6	city/town: 3.4 miles northwest of Oregon
county: Holt	feature inters.: branch of Kimsey Creek
	cadastral grid: S8/9, T60N, R38W
	highway route: County Road 292
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 60.0'	alterations: unknown
total length: 91.0'	floor/decking : timber deck
roadway width: 13.8'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor : unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 292000.6.	
sign. rating: 28	
evaluation: NRHP non-eligible (typically configured example of common structural type, largely undocumented)	

inventoried by: Mark Hufstetler 21 August 1992

Kimsey Creek Bridge

HOLT43

GENERAL DATA

structure no.: 301R01.1	city/town: 3.6 miles northwest of Oregon
county: Holt	feature inters.: Kimsey Creek
	cadastral grid: S8/17, T60N, R38W
	highway route: County Road 301
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 75.0'	alterations: unknown
total length: 106.0'	floor/decking : timber deck
roadway width: 16.1'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 301R01.1.

sign. rating: 28
evaluation: NRHP non-eligible (typically configured example of common structural type, largely undocumented)

Inventoried by: Mark Hufstetler 21 August 1992

Mill Creek Bridge

HOLT44

GENERAL DATA

structure no.:	321000.9	city/town:	4.3 miles south of Oregon
county:	Holt	feature inters.:	Mill Creek
		cadastral grid:	S15/22, T59N, R38W
		highway route:	County Road 321
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and timber wingwalls		
span number:	1	condition:	fair
span length:	75.0'	alterations:	unknown
total length:	118.0'	floor/decking :	timber deck
roadway width:	16.0'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 321000.9.

sign. rating: 28

evaluation: NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Mark Hufstetler 21 August 1992

Schlotz Branch Bridge

HOLT46

GENERAL DATA

structure no.: 328000.0	city/town: 2.4 miles south of Oregon
county: Holt	feature inters.: Schlotz Branch
	cadastral grid: S10/11, T59N, R38W
	highway route: County Road 328
	highway distr.: 1
	current owner: Holt County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 41.0'	floor/decking : timber deck
roadway width: 13.4'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 328000.0.

sign. rating: 20
evaluation: NRHP non-eligible (typically configured, small-scale example of common structural type, largely undocumented)

inventoried by: Mark Hufstetler 21 August 1992

Forbes Creek Bridge

HOLT48

GENERAL DATA

structure no.:	336002.4	city/town:	Forbes
county:	Holt	feature inters.:	Forbes Creek
		cadastral grid:	S29, T59N, R37W
		highway route:	County Road 336
		highway distr.:	1
		current owner:	Holt County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss
substructure: unknown

span number:	1	condition:	fair
span length:	45.0'	alterations:	unknown
total length:	46.0'	floor/decking :	timber deck
roadway width:	13.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 336002.4.

sign. rating: 20
evaluation: NRHP non-eligible (typically configured, small-scale example of common structural type, largely undocumented)

inventoried by: Mark Hufstetler 21 August 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Rulo Bridge
MHTD: L97

HOLT02

DATE(S) OF CONSTRUCTION

1938-39

LOCATION

U.S. Highway 159 over Missouri River; S33, T61N, R40W
5.3 miles west Fortescue; Holt / Richardson County, Missouri

USE (ORIGINAL / CURRENT)

highway bridge / highway bridge

RATING NRHP listed (score: 65)

CONDITION

good

OWNER

Missouri Highway and Transportation Department / Nebraska Department of Roads

span number: 3
span length: 375.0'
total length: 859.0'
roadway wdt.: 20.0'

superstructure: steel, 16-panel rigid-connected Pennsylvania through truss, with rigid-connected Warren deck trusses
substructure: concrete abutments and wingwalls; concrete two-legged piers and solid channel piers
floor/decking: concrete deck over I-beam transverse joists, which rest on I-beam stringers
other features: upper chord and inclined end post: 2 channels with cover plate and double lacing; lower chord: 2 channels with lacing; vertical: built up I-beam girder with lacing or 4 angles with lacing; diagonal: 2 channels with lacing; lateral bracing: 4 angles with lacing; strut: 4 angles with lacing; floor beam: I-beams; guardrail: 3 angles; builder's plate: **FEDERAL WORKS AGENCY / PUBLIC WORKS ADMINISTRATION / JOHN M. CARMODY / FEDERAL WORKS ADMINISTRATOR / FRANKLIN D. ROOSEVELT / PRESIDENT OF THE UNITED STATES / MISSOURI RIVER BRIDGE / 1939, and RICHARDSON COUNTY, NEBRASKA / KANSAS CITY BRIDGE COMPANY / MISSOURI VALLEY BRIDGE AND IRON COMPANY / CONTRACTORS / HARRINGTON AND CORTELYOU / CONSULTING ENGINEERS**

The Rulo Bridge carries U.S. Highway 159 over the Missouri River, some five miles west of Fortescue, Missouri. The bridge is located on the Nebraska/Missouri line, at the east edge of Section 17, T1N, T18E, Rulo Precinct. With a total length of 859 feet, the structure is comprised of ten riveted, Warren deck truss approaches (five on each end of the bridge), and three riveted Pennsylvania through trusses over the river's main channel. Each through truss extends 375 feet in length, with a roadway width of 20 feet and an overhead clearance of approximately 16 feet.

A group led by John C. Mullen of Falls City, Nebraska, laid the groundwork for the Rulo Bridge in 1933, when it secured permission from Congress to construct and operate a toll bridge over the Missouri River at Rulo. Either in that year or earlier, the group also engaged the services of the Kansas City engineering firm, Harrington and Cortelyou, to design the multiple-span structure. The plans were approved by the War Department in May 1933. Despite these efforts, it is not known whether Mullen and his associates ever intended to build the bridge. On February 14, 1934, Mullen approached Nebraska's Richardson County Board, offering to assign the county "all his rights, interests, contracts, and franchises which he possesses for the construction of a vehicular traffic bridge at Rulo."

The county accepted Mullen's offer, but only on the condition that it not have to pay for construction. Considering that the bridge was estimated to cost between \$700,000 and \$800,000, this was no small caveat. Fortunately, Mullen had a solution, proposing that the county apply for a federal grant and loan. The board was careful to stipulate that the loan would only be repaid through bridge tolls, at no expense to the county. The county clerk noted in his minutes:

It was explained that [Richardson County could]... obtain a grant from the Reconstruction Finance Corporation of the Federal Government to build such bridge, in the amount of about \$800,000, of which thirty percent of the portion thereof that is expended for labor and material would... be an outright gift, the balance to be repaid with interest of such bonds as debenture bonds only, the same to be retired from revenue from the use of such bridge so that the County of Richardson would not be subject to any deficiency judgement in the case that such bridge does not pay out...

Negotiations appear to have dragged on for several years, but in September 1938, the Public Works Administration agreed to fund 45 percent of the bridge's construction, the total grant not to exceed \$326,250.00. To cover the balance, the county released a bond issue which was to be repaid through bridge revenue. With funding secured, the board officially entered a contract with Harrington and Cortelyou, who had been operating under a verbal agreement. The firm was to prepare all plans and supervise the project for six percent of the cost of construction. On October 7, the board called for construction bids. The announcement stipulated that no bid was to exceed \$625,000.00 and stated that "the structure comprises, from east to west, 815 ft. of graded approach, five steel deck truss spans, each 100 ft. long, three steel through truss spans, each 376.5 ft. long, 5 steel deck truss spans, each 100 ft. long and 352 ft. of graded approach. A concrete floor slab is provided on all the truss spans and concrete paving on the graded approaches."

The county opened bids on November 7, 1938. Of the eleven bids received, the board selected the joint proposal of the Kansas City Bridge Company and the Missouri Valley Bridge and Iron Company, low bidders at \$599,425.35. In an effort to provide local employment during the Depression, the county required that the contractors hire Richardson County residents whenever possible. Construction apparently began immediately and continued through 1939. The Empire Construction Company of Omaha was subcontracted to grade the approaches, and the Interstate Construction Company of Lincoln installed the bridge lighting. The Rulo Bridge apparently opened to traffic in November 1939, and the Richardson County Board officially accepted the toll bridge on April 2, 1940. The total cost of construction amounted to \$651,296.01.

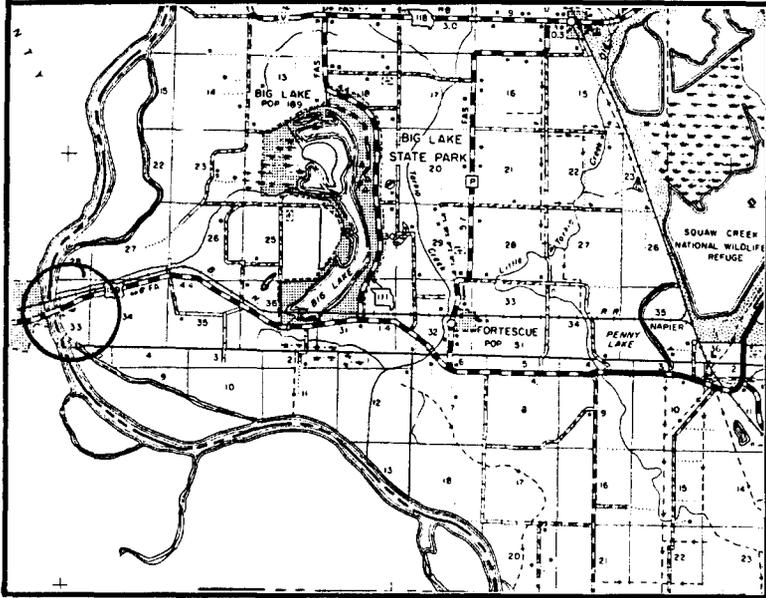
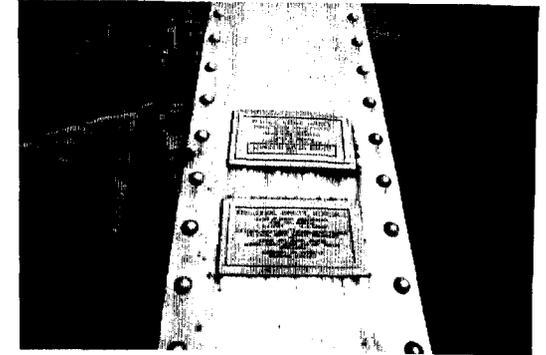
In 1945 an engineer from the Nebraska Highway Department observed that "the steel portions of the structure appear in excellent condition, although due to war conditions, the painting of the steel has been deferred and the paint coat is not in as good a condition as desired. This, no doubt, will be remedied when materials and labor again become available." More seriously, the writer also reported that the west approach fill had settled and "this has occasioned some movement in the abutment and in the truss span at the west end of the bridge. This movement is sufficient to entirely close the expansion joint between the trusses at the first pier." The engineer presumably was referring to the first approach span, and not the westernmost through truss. The state estimated the cost of the repairs at \$10,000.00, and the work was probably done in the late 1940s. Aside from these minor repairs, no major work appears to have been performed on the bridge after its opening.

In October 1966, Richardson County informed the State of Nebraska and Missouri that "there appears at this time [to be]... sufficient funds available for a premature retirement of the [bridge construction] bonds on or before October 1, 1967." Although tolls would no longer be required to repay the bridge bonds, they would be still collected to cover the cost of maintenance. Noting that Interstate 29 was currently being built in Missouri and that an access road was planned from the bridge, the board urged the states to assume ownership and make the Rulo Bridge a toll-free crossing to facilitate travel. The board stated: "It would be beneficial to the residents of Richardson County, Nebraska, and to the residents of Northwest Missouri that the bridge be made a free bridge...This is particularly true in light of the construction of Highway I 29 through Holt County, Missouri, and the proposed plans to build an access road from I 29 to the Rulo Bridge."

The State of Nebraska and Missouri agreed to assume joint ownership of the Rulo Bridge in 1969. Prior to the transfer, Richardson County contracted with the Capital Bridge Company to repair and waterproof the bridge's concrete surfaces, according to plans and specifications prepared by the State of Nebraska. The total cost of the project amounted to \$14,775.00. To celebrate the new ownership, the county board declared May 15 and June 19 as "free bridge" days, temporarily allowing traffic to cross the bridge free of charge. On June 24, 1969, the Richardson County Board signed the papers transferring ownership of the Rulo Bridge to the States of Missouri and Nebraska. A regionally important crossing of the Missouri River, the Rulo Bridge is significant for its long-span river bridge design.

NAME(S) OF STRUCTURE

Rulo Bridge

PHOTOS AND SKETCH MAP OF LOCATION**LOCATION MAP**TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number L97; Nebraska Department of Roads and Irrigation, **Twenty-Third Biennial Report**, 1939-40, page 23; Henry G. Schlitt, "Missouri River Bridges, Report #2," 7 November 1945, located in Bridge Division, Nebraska Department of Roads, Lincoln NE; Proceedings of the Richardson County Commissioners, Book 13: pages 673-74 (14 February 1934); Book 14: page 276 (10 September 1935), pages 284-85 (24 September 1935), page 419 (23 June 1936), pages 52-53 (29 June 1938); Book 15: pages 67-70 (28 July 1938), pages 111-13 (13 September 1938), page 161 (29 November 1938), page 183 (17 January 1939), page 239 (25 July 1939), page 276 (7 November 1939), page 324 (2 April 1940); Book 16: page 73 (11 October 1966); Book 20: page 106 (17 January 1967), page 363 (10 December 1968), page 371 (7 January 1969), page 389 (25 February 1969), page 426 (24 June 1969)- located in Richardson County Courthouse, Falls City NE; field inspection by Demian Hess, 13 March 1990; field inspection by Mark Hufstetler, 7 September 1990.

INVENTORIED BY

Mark Hufstetler

AFFILIATION

Fraserdesign, Loveland CO

DATE

21 August 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Leach Mill Bridge
MHTD: 257001.6

HOLT38

DATE(S) OF CONSTRUCTION

1913

LOCATION

County Road 257 over Nodaway River; S12, T60N, R37W
8.0 miles northeast of Oregon; Holt County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 37)

CONDITION

fair

OWNER

Holt County / Andrew County

span number: 2
span length: 72.0'
total length: 130.0'
roadway wdt.: 15.7'

superstructure: steel, 8-panel, pin-connected Pratt through truss, with pin-connected Pratt pony truss approach span
substructure: concrete-filled steel cylinder piers; steel pile bent abutments with timber back- and wingwalls
floor/decking: timber deck over steel stringers
other features: **through truss:** upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; floor beam: I-beam, field-bolted to verticals; guardrail: 1 angle; **pony truss:** upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; floor beam: I-beam, field-bolted to verticals; guardrail: steel channel

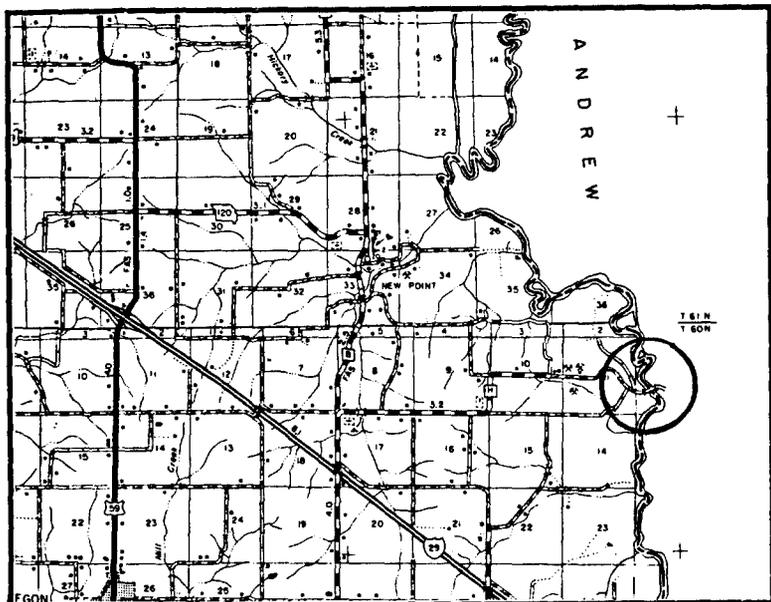
Located on the line between Holt and Andrew Counties northeast of Oregon, this pin-connected truss bridge carries County Road 257 in Holt County and County Road 48 in Andrew County over the Nodaway River. Consisting of both a Pratt through truss on the east end and a Pratt pony truss on the west end, the structure is supported by concrete-filled cylinder piers and steel pile bent abutments. The Leach Mill Bridge—as it is known locally—dates to 1913. In June of that year a contract was let to A.S. Seybert to erect the bridge, which, according to the agreement, was to be completed by the end of the year. The contract also stipulated that Seybert would furnish all necessary material and keep the bridge in good repair for two years after construction. Presumably the two counties split the contract cost of \$4225.00, although county minutes do not specifically refer to this. Presumably completed later in 1913, the Leach Mill Bridge continues to carry vehicular traffic in its rural setting while maintaining a high degree of structural integrity.

From the early 1880s until the establishment of state bridge standards in the late 1920s, the pin-connected Pratt through truss was almost the exclusive structural type for medium-span roadway crossings in Missouri. Its standardized fabrication, economy of materials and ease of erection made it a mainstay among the various state and regional bridge companies. Thousands of such trusses were built throughout the state during this period, and numerous examples remain in place today. With its 70-foot span and 1913 erection date, the Leach Mill Bridge typifies Pratt truss construction in Missouri.

NAME(S) OF STRUCTURE

Leach Mill Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 257001.6; original plans "for Bridge across Nodaway River at Leach Mills" (N^o 30, 1913), and contract with A.S. Seybert (1913) - located in old bridge specifications box Bridge Contracts and Bonds 1911, at the Andrew County Courthouse, Savannah MO; field inspection by Mark Hufstetler, 7 September 1990.

INVENTORIED BY

Mark Hufstetler

AFFILIATION

Fraserdesign, Loveland CO

DATE

21 August 1992

NODAWAY COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
NODA01	A37000.6	Turkey Creek Bridge	1- 40' pinned Pratt pony truss c1910
NODA02	H 698R	Nodaway River Bridge	(replaced)
NODA03	H 840R	102 River Bridge	1-120' riveted Pratt through truss 1928 George W. Condon
NODA04	S 511	Nodaway River Bridge	1-120' riveted Pratt through truss 1932 Pittsburgh-Des Moines Stl. Co.
NODA05	S 918	Nodaway River Bridge	1-120' riveted Pratt through truss 1933 Maxwell Construction Co.
*NODA06	003000.2	Middle Mill Creek Bridge	1- 78' pinned Pratt through truss c1910
*NODA07	010001.4	Mill Creek Bridge	1- 80' pinned Pratt through truss c1885
NODA08	030000.6	Mass Branch Bridge	1- 50' pinned Pratt pony truss 1906 county work force
NODA09	064000.8	Clear Creek Bridge	(replaced)
NODA10	069001.6	Clear Creek Bridge	1- 40' pinned Pratt pony truss c1910
NODA11	073000.3	Wolf Creek Bridge	1- 65' pinned Pratt pony truss 1907 J.H. Sparks
NODA12	090000.3	Clear Creek Bridge	1- 60' pinned Pratt pony truss c1915
NODA13	095000.4	Clear Creek Bridge	(replaced)
NODA14	098000.2	Clear Creek Bridge	(replaced)
NODA15	098000.8	Clear Creek Bridge	1- 40' pinned Pratt pony truss c1905
NODA16	105001.4	Bridge	(replaced)
*NODA17	106000.3	South Fork Bridge	1- 80' pinned Pratt through truss 1907 Sparks and Kinney, St. Joseph
*NODA18	116000.8	South Fork Bridge	(replaced)
NODA19	120000.4	Clear Creek Bridge	1- 40' pinned Pratt pony truss c1915
NODA20	167000.7	Norvey Creek Bridge	1- 84' pinned Pratt pony truss c1905
NODA21	183000.5	Honey Creek Bridge	1- 40' pinned Pratt pony truss c1915
NODA22	188000.5	Honey Creek Bridge	1- 40' pinned Pratt pony truss 1897 Youngstown Bridge Company
NODA23	196R00.5	Honey Creek Bridge	1- 40' pinned Pratt half-hip pony truss c1910
NODA24	209001.3	Norvey Creek Bridge	1- 60' pinned Pratt pony truss 1902 Sparks and Kinney, St. Joseph
NODA25	223001.5	Honey Creek Bridge	(replaced)

NODAWAY COUNTY

INCLUDED (cont.):

*NODA26	231R00.4	Platte River Bridge	1-120' c1915	pinned Pratt through truss
*NODA27	251001.0	102 River Bridge	1-140' 1909	pinned Pratt through truss
*NODA28	254000.8	102 River Bridge	1-150' c1915	pinned Pratt through truss
*NODA29	254001.7	Norvey Creek Bridge	1- 80' c1915	pinned Pratt pony truss
*NODA30	262000.3	Norvey Creek Bridge	1- 80' c1915	pinned Pratt through truss
NODA31	269000.6	Bridge	1- 40' c1915	pinned Pratt pony truss
NODA32	302000.0	Muddy Creek Bridge		(replaced)
NODA33	315001.1	Muddy Creek Bridge	1- 40' 1902	pinned Pratt pony truss
*NODA34	322R00.7	Mill Creek Bridge		(replaced)
NODA35	323000.8	Mill Creek Bridge	1- 85' c1915	pinned Pratt pony truss
NODA36	329001.0	Bridge	1- 60' 1907	pinned Pratt pony truss J.H. Sparks (replaced)
NODA37	346000.1	Bridge		(replaced)
NODA38	349000.6	Sand Creek Bridge	1- 50' c1915	pinned Pratt pony truss
NODA39	356000.1	Muddy Creek Bridge		(replaced)
NODA40	373000.9	White Cloud Creek Bridge		(replaced)
NODA41	382000.7	Bridge	1- 50' c1915	pinned Pratt pony truss
NODA42	383001.8	102 River Bridge	1-100' 1938	riveted Pratt through truss
*NODA43	387R00.4	102 River Bridge		(replaced)
NODA44	398001.8	Long Branch Bridge	1- 40' c1915	pinned Pratt pony truss
NODA45	420000.3	Honey Creek Bridge	1- 50' c1900	pinned Pratt pony truss
*NODA46	426000.8	Platte River Bridge	1- 80' 1894	pinned Pratt through truss Massillon Bridge Company (replaced)
NODA47	439000.3	Honey Creek Bridge		(replaced)
*NODA48	456R01.1	Platte River Bridge	1-150' c1910	pinned Parker through truss
*NODA49	474001.2	102 River Bridge	1-108' c1915	pinned Pratt through truss
*NODA50	496000.5	Big Slough Bridge	1- 40' c1910	pinned Pratt pony truss
NODA51	511000.5	Florida Creek Bridge		(replaced)
*NODA52	526000.8	Nodaway River Bridge	1-225' c1925	riveted Parker through truss
NODA53	534000.7	Sand Creek Bridge	1- 40' c1910	pinned Pratt half-hip pony truss

NODAWAY COUNTY

INCLUDED (cont.):

NODA54	560000.2	Florida Creek Bridge	1- 40'	pinned Pratt pony truss
			c1910	
NODA55	560000.6	Florida Creek Bridge	1- 70'	pinned Pratt pony truss
			c1910	
NODA56	577000.2	White Cloud Creek Bridge	1- 75'	pinned Pratt pony truss
			c1905	
*NODA57	628000.9	Mozingo Creek Bridge	1- 40'	pinned Pratt pony truss
			1904	
NODA58	643000.6	Long Branch Bridge	1- 40'	pinned Pratt pony truss
			c1910	
NODA59	646000.3	Long Branch Bridge	1- 40'	pinned Pratt pony truss
			1907	county work force (prob.)
*NODA60	652001.1	Long Branch Bridge	1- 40'	pinned Pratt half-hip pony
			1904	
NODA61	654000.2	Bridge	1- 50'	pinned Pratt pony truss
			c1900	
*NODA62	655001.4	Platte River Bridge	1- 80'	pinned Pratt through truss
			1904	
NODA63	655R01.5	Platte River Bridge	2- 80'	pinned Pratt pony truss
			c1910	
NODA64	657002.2	Platte River Bridge		(replaced)
NODA65	692000.7	Wildcat Creek Bridge	1- 65'	pinned Pratt pony truss
			c1915	
NODA66	694000.2	Wildcat Creek Bridge	1- 50'	pinned Pratt pony truss
			c1910	
*NODA67	697000.9	Wildcat Creek Bridge	1- 75'	pinned Pratt through truss
			c1905	
NODA68	701000.3	Wildcat Creek Bridge	1- 65'	pinned Pratt pony truss
			c1910	
NODA69	741000.0	102 River Bridge	1-100'	rivet polyg. Warren pony truss
			1946	
NODA70	769000.2	White Cloud Creek Bridge	1- 85'	pinned Pratt pony truss
			c1915	
NODA71	776000.1	Jenkins Creek Bridge	1- 40'	pinned Pratt pony truss
			c1910	
*NODA72	777R00.2	Jenkins Creek Bridge	1- 84'	pinned Pratt through truss
			c1907	
NODA73	780000.7	Coal Creek Bridge	1- 40'	pinned Pratt pony truss
			c1910	
NODA74	783000.2	Elkhorn Creek Bridge	1- 40'	pinned Pratt pony truss
			c1915	
NODA75	795000.1	Elkhorn Creek Bridge	1- 40'	pinned Pratt pony truss
			c1910	
NODA76	798000.7	Elkhorn Creek Bridge	1- 70'	pinned Pratt pony truss
			c1910	
NODA77	802000.8	Elkhorn Creek Bridge	1- 80'	riveted Pratt through truss
			c1925	

NODAWAY COUNTY

INCLUDED (cont.):

NODA78	821000.2	Florida Creek Bridge	1- 65'	pinned Pratt pony truss
			1908	Standard Bridge Co. (rebuilt)
*NODA79	845000.3	Bridge	1- 65'	pinned Pratt half-hip pony
			c1890	
NODA80	853000.3	Elkhorn Creek Bridge	1- 50'	pinned Pratt pony truss
			c1905	
*NODA81	870000.9	Jenkins Creek Bridge		(replaced)
NODA82	872000.1	Elkhorn Creek Bridge		(replaced)
NODA83	877000.6	Elkhorn Creek Bridge		(replaced)
NODA84	890000.7	Jenkins Creek Bridge	1- 40'	pinned Pratt pony truss
			c1900	
NODA85	890001.1	Jenkins Creek Bridge		(replaced)
NODA86	893000.4	Jenkins Creek Bridge	1- 60'	pinned Pratt pony truss
			c1910	
NODA87	897001.7	Owl Creek Bridge	1- 80'	pinned Pratt pony truss
			c1915	
*NODA88	916000.2	White Cloud Creek Bridge	1- 80'	pinned Pratt through truss
			1904	John Gilligan, Falls City NE
NODA89	941000.7	Bridge	1- 50'	pinned Pratt pony truss
			c1910	
NODA90	969000.4	Long Branch Bridge	1- 40'	pinned Pratt pony truss
			1902	Sparks & Kinney, St. Joseph
NODA91	972000.8	Platte River Bridge	1-140'	riveted Pratt through truss
			c1925	
NODA92	985000.1	Clear Creek Bridge	1- 50'	pinned Pratt pony truss
			c1900	

EXCLUDED:

Warren pony truss

H 697 J 588 039R00.2 089001.1 399000.9

Pratt pony truss

J 376 134000.1

Bailey truss

424000.0 458000.3

Lattice bedstead

296001.0

Steel stringer

A13R006	A200007	A35000.8	A370014	H 710	J 408R	J 586
J 590	J 591	J 592	J 593	J 794	K 888	L 168
L 186	L 309	S 109	S 186	S 360	S 512	S 513
S 919	S 920	S 921	S 922	T 44	T 48	T 49

NODAWAY COUNTY

EXCLUDED (cont.):

Steel stringer

T 50	T 384	T 579	X 671	X 673	002000.1	002000.3
016000.3	017000.1	018000.1	024000.4	024500.1	024500.2	024500.3
025R01.5	025000.5	028R00.5	032000.2	055000.3	071001.9	073001.1
077000.4	078000.8	081000.2	089000.5	119000.3	120000.2	173001.4
188000.8	188002.2	189000.1	198000.0	198000.8	205000.4	259000.0
263R01.1	271500.2	275000.2	276R00.4	285002.7	286000.4	286000.8
290001.0	298000.3	312000.1	313000.3	321001.0	327000.5	336000.0
341000.1	360000.1	373000.7	397001.3	406000.9	429000.4	431000.3
432000.9	445002.3	446000.6	460000.0	489000.5	510000.1	510000.5
513000.6	515000.7	523001.8	524000.1	524000.3	547000.0	547000.5
580000.5	592R00.2	617000.9	620000.5	626000.9	645000.9	664R00.6
666R02.4	672002.5	676R01.1	676000.4	682000.0	687R00.8	688000.2
748000.5	754000.5	788000.9	858000.5	874000.2	890R01.7	899R00.4
906000.7	918R00.0	930002.4	951000.2	952000.1	953000.2	965000.3
966002.3	972000.9	990R00.3	991001.3			

Steel girder

A41000.2	235001.0	316000.5	386000.5	458001.2	514000.1	521000.4
565000.6	640000.0	887000.2	894000.3	908R00.4	910R00.2	

Concrete girder

H 512R	H 513R	J 915
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Concrete slab

H 862R	K 258R
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Concrete box culvert

H 190R	J 793	K 173	S 185	T 871	X 584
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Timber stringer

U271500.1

SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	3	70	0	0	73
Excluded	43	125	1	0	169
	46	195	1	0	242 structures

Turkey Creek Bridge

NODA01

GENERAL DATA

structure no.: A37000.6	city/town: 6.5 miles southeast of Guilford
county: Nodaway	feature inters.: Turkey Creek
	cadastral grid: S33, T62N, R33W
	highway route: county road
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: concrete-filled steel cylinder piers	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 41.0'	floor/decking : timber deck over steel stringers
roadway width: 11.8'	other features: no guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number A37000.6.

sign. rating: 20
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

102 River Bridge

NODA03

GENERAL DATA

structure no.: H 840R	city/town: 2.0 miles east of Maryville
county: Nodaway	feature inters.: 102 River
	cadastral grid: S15/22, T64N, R35W
	highway route: State Highway 46
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 6-panel, rigid-connected Pratt through truss, with concrete deck girder approach spans

substructure: concrete abutments, wingwalls and piers

span number: 1	condition: good
span length: 120.0'	alterations: guardrails replaced, 1940
total length: 517.0'	floor/decking : concrete deck over steel stringers
roadway width: 20.0'	other features: steel guardrails

HISTORICAL DATA

erection date: 1928

erection cost: \$44,005.09

designer: Missouri State Highway Department

fabricator : unknown

contractor: George W. Condon

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number H 840R; Missouri Primary System Bridge Record - located at the Missouri Highway and Transportation Department, Jefferson City MO.

sign. rating: 41

evaluation: NRHP non-eligible (typically configured highway truss designed by MSHD)

inventoried by: Michelle Crow-Dolby 11 August 1993

Nodaway River Bridge

NODA04

GENERAL DATA

structure no.: S 511	city/town: 0.7 mile south of Skidmore
county: Nodaway	feature inters.: Nodaway River
	cadastral grid: S9, T63N, R37W
	highway route: State Highway 113
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 7-panel, rigid-connected Pratt through truss; 5-panel, rigid-connected Warren pony truss approach spans	
substructure: concrete abutments, wingwalls and piers	
span number: 1; 2	condition: good
span length: 120.0'; 80.0'	alterations: none
total length: 289.0'	floor/decking : concrete deck over steel stringers
roadway width: 20.0'	other features: steel guardrails

HISTORICAL DATA

erection date: 1932
erection cost: \$23,397.97
designer: Missouri State Highway Department
fabricator : Pittsburgh-Des Moines Steel Company
contractor: Pittsburgh-Des Moines Steel Company
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number S 511; Missouri Primary System Bridge Record - located at the Missouri Highway and Transportation Department, Jefferson City MO.
sign. rating: 46
evaluation: NRHP non-eligible (typically configured highway truss designed by MSHD)

inventoried by: Michelle Crow-Dolby 11 August 1993

Nodaway River Bridge

NODA05

GENERAL DATA

structure no.: S 918	city/town: 3.1 miles north of Skidmore
county: Nodaway	feature inters.: Nodaway River
	cadastral grid: S21/28, T64N, R37W
	highway route: State Highway K
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 6-panel, rigid-connected Pratt through truss, with steel stringer approach spans	
substructure: concrete abutments, wingwalls and piers	
span number: 1	condition: good
span length: 120.0'	alterations: none
total length: 222.0'	floor/decking : concrete deck over steel stringers
roadway width: 20.0'	other features: steel guardrails

HISTORICAL DATA

erection date: 1933	
erection cost: \$16,711.32	
designer: Missouri State Highway Department	
fabricator : unknown	
contractor: Maxwell Construction Company	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number S 918; Missouri Primary System Bridge Record - located at the Missouri Highway and Transportation Department, Jefferson City MO.	
sign. rating: 41	
evaluation: NRHP non-eligible (typically configured highway truss designed by MSHD)	

inventoried by: Michelle Crow-Dolby 11 August 1993

Middle Mill Creek Bridge

NODA06

GENERAL DATA

structure no.: 003000.2	city/town: 9.6 miles northwest of Burlington Junction
county: Nodaway	feature inters.: Middle Mill Creek
	cadastral grid: S31, T67N, R37W
	highway route: County Road 3
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt through truss, with timber stringer approach span	
substructure: timber pile bent pier and abutments	
span number: 1	condition: poor
span length: 78.0'	alterations: bridge abandoned; approach span removed
total length: 92.0'	floor/decking : timber deck over timber stringers
roadway width: 14.1'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: looped square and round eyerods; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, field-bolted to vertical

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 003000.2; field inspection by Mark Hufstetler, 8 September 1990.

sign. rating: 25
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Mill Creek Bridge

NODA07

GENERAL DATA

structure no.: 010001.4	city/town: 2.0 miles northwest of Elmo
county: Nodaway	feature inters.: Mill Creek
	cadastral grid: S7, T66N, R37W
	highway route: County Road 10
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel or wrought iron, 5-panel, pin-connected Pratt through truss, with steel stringer approach spans

substructure: steel pile bent piers and abutments

span number: 1	condition: fair
span length: 80.0'	alterations: substructure replaced
total length: 131.0'	floor/decking : timber deck over steel stringers
roadway width: 14.6'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: looped square and round eyerods; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beam, U-bolted to vertical; guardrail: timber; portal: lattice

HISTORICAL DATA

erection date: c1890
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 010001.4; Bridge Commissioner's Reports on Sale of Bridges, letter dated 17 October 1881 - located at Nodaway County Courthouse, Maryville MO; field inspection by Mark Hufstetler, 8 September 1990.

sign. rating: 25
evaluation: NRHP non-eligible (typically configured, relatively early example of mainstay structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 11 August 1993

Mass Branch Bridge

NODA08

GENERAL DATA

structure no.: 030000.6	city/town: 5.7 miles northwest of Burlington Junction
county: Nodaway	feature inters.: Mass Branch
	cadastral grid: S16, T66N, R37W
	highway route: County Road 30
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: steel pile bent piers with timber wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: approach span added, 1981
total length: 69.0'	floor/decking : timber deck
roadway width: 16.0'	other features: no guardrails

HISTORICAL DATA

erection date: 1906	
erection cost: \$325.00	
designer: unknown	
fabricator : unknown	
contractor: county work force	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 030000.6; Nodaway County Court Record, Book 16: page 33 (9 August 1906) - located at the Nodaway County Courthouse, Maryville MO.
sign. rating: 37	
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Clear Creek Bridge

NODA10

GENERAL DATA

structure no.:	069001.6	city/town:	7.0 miles southwest of Hopkins
county:	Nodaway	feature inters.:	Clear Creek
		cadastral grid:	S2, T66N, R36W
		highway route:	County Road 69
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with timber stringer approach span		
substructure:	unknown		
span number:	1	condition:	fair
span length:	40.0'	alterations:	approach span added, 1981
total length:	54.0'	floor/decking :	timber deck
roadway width:	14.1'	other features:	no guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 069001.6.
sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Wolf Creek Bridge

NODA11

GENERAL DATA

structure no.: 073000.3	city/town: 7.5 miles northeast of Burlington Junction
county: Nodaway	feature inters.: Wolf Creek
	cadastral grid: S7, T66N, R36W
	highway route: County Road 73
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss	
substructure: concrete abutments and wingwalls	
span number: 1	condition: fair
span length: 65.0'	alterations: unknown
total length: 80.0'	floor/decking : timber deck
roadway width: 13.6'	other features: steel angle guardrails

HISTORICAL DATA

erection date: 1907
erection cost: \$519.84 (contract amount)
designer: J.H. Sparks, St. Joseph MO
fabricator : unknown
contractor: J.H. Sparks, St. Joseph MO
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 073000.3; Nodaway County Court Records, page 155 (5 February 1907) -located at the Nodaway County Courthouse, Maryville MO.
sign. rating: 40
evaluation: NRHP non-eligible (typical example of common truss type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Clear Creek Bridge

NODA12

GENERAL DATA

structure no.: 090000.3	city/town: 7.3 miles southwest of Hopkins
county: Nodaway	feature inters.: branch of Clear Creek
	cadastral grid: S14, T66N, R36W
	highway route: County Road 90
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach span	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 60.0'	alterations: unknown
total length: 79.0'	floor/decking : timber deck
roadway width: 16.2'	other features: no guardrails

HISTORICAL DATA

erection date: c1915
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 090000.3.

sign. rating: 28
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Clear Creek Bridge

NODA15

GENERAL DATA

structure no.:	098000.8	city/town:	5.2 miles southwest of Hopkins
county:	Nodaway	feature inters.:	branch of Clear Creek
		cadastral grid:	S12, T66N, R36W
		highway route:	County Road 98
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	40.0'	floor/decking :	timber deck
roadway width:	14.0'	other features:	no guardrails

HISTORICAL DATA

erection date: c1905
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 098000.8.

sign. rating: 21
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

South Fork Bridge

NODA17

GENERAL DATA

structure no.: 106000.3	city/town: 5.3 miles northeast of Burlington Junction
county: Nodaway	feature inters.: South Fork
	cadastral grid: S28, T66N, R36W
	highway route: County Road 106
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt through truss, with steel stringer approach spans	
substructure: timber pile abutments; steel pile bent piers	
span number: 1	condition: fair
span length: 80.0'	alterations: approach span replaced
total length: 149.0'	floor/decking : timber deck over steel stringers
roadway width: 14.5'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: looped square eyebar; counter: round eyerod with turnbuckle; strut: 4 angles with lacing; floor beam: I-beam, field-bolted to vertical; no guardrails

HISTORICAL DATA

erection date: 1907	
erection cost: unknown	
designer: unknown	
fabricator : Carnegie Steel Company, Pittsburgh PA	
contractor : Sparks and Kinney, St. Joseph MO	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 106000.3; Nodaway County Court Record, Book 16: page 369 (22 July 1907) - located at the Nodaway County Courthouse, Maryville MO; field inspection by Mark Hufstetler, 8 September 1990.
sign. rating: 39	
evaluation:	NRHP non-eligible (typically configured, modestly scaled example of mainstay structural type)

Inventoried by: Michelle Crow-Dolby 11 August 1993

Clear Creek Bridge

NODA19

GENERAL DATA

structure no.: 120000.4	city/town: 9.0 miles northwest of Maryville
county: Nodaway	feature inters.: Clear Creek
	cadastral grid: S31, T66N, R35W
	highway route: County Road 120
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: none
total length: 41.0'	floor/decking : timber deck
roadway width: 13.9'	other features: no guardrails

HISTORICAL DATA

erection date: c1915
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 120000.4.

sign. rating: 21
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Norvey Creek Bridge

NODA20

GENERAL DATA

structure no.: 167000.7	city/town: 4.0 miles northwest of Hopkins
county: Nodaway	feature inters.: Norvey Creek
	cadastral grid: S20, T66N, R34W
	highway route: County Road 167
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 6-panel, pin-connected Pratt pony truss	
substructure: unknown	
span number: 1	condition: fair
span length: 84.0'	alterations: modified, 1981
total length: 100.0'	floor/decking : timber deck over steel stringers
roadway width: 13.6'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1905
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 167000.7.

sign. rating: 31
evaluation: NRHP non-eligible (typical example of common truss types)

inventoried by: Michelle Crow-Dolby 11 August 1993

Honey Creek Bridge

NODA21

GENERAL DATA

structure no.: 183000.5	city/town: 7.5 miles northeast of Hopkins
county: Nodaway	feature inters.: Honey Creek
	cadastral grid: S31, T67N, R33W
	highway route: County Road 183
	highway distr.: 1
	current owner: Nodaway County MO / Taylor County IA

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: none
total length: 41.0'	floor/decking : timber deck
roadway width: 13.5'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1915
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 183000.5.
sign. rating: 20
evaluation: NRHP non-eligible (typical example of common truss type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Honey Creek Bridge

NODA22

GENERAL DATA

structure no.:	188000.5	city/town:	7.5 miles east of Hopkins
county:	Nodaway	feature inters.:	Honey Creek
		cadastral grid:	S6, T66N, R33W
		highway route:	County Road 188
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	41.0'	floor/decking :	timber deck
roadway width:	16.0'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1897
erection cost:	\$635.00
designer:	unknown
fabricator :	unknown
contractor:	Youngstown Bridge Company, Youngstown OH

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 188000.5; Nodaway County Record, Book 13: pages 118-119 (14 June 1905) - located at the Nodaway County Courthouse, Maryville MO.

sign. rating:	41
evaluation:	NRHP non-eligible (typical example of common truss type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Honey Creek Bridge

NODA23

GENERAL DATA

structure no.: 196R00.5	city/town: 7.5 miles southeast of Hopkins
county: Nodaway	feature inters.: Honey Creek
	cadastral grid: S7, T66N, R33W
	highway route: County Road 196
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: approach span added, 1983
total length: 69.0'	floor/decking : timber deck
roadway width: 16.0'	other features: no guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 196000.5.

sign. rating: 25
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Norvey Creek Bridge

NODA24

GENERAL DATA

structure no.: 209001.3	city/town: 4.5 miles southeast of Hopkins
county: Nodaway	feature inters.: Norvey Creek
	cadastral grid: S30, T66N, R34W
	highway route: County Road 209
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 60.0'	alterations: unknown
total length: 61.0'	floor/decking : timber deck
roadway width: 15.4'	other features: steel angle guardrails

HISTORICAL DATA

erection date: 1902
erection cost: \$689.00
designer: unknown
fabricator : unknown
contractor: Sparks and Kinney, St. Joseph MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 209001.3; Nodaway County Record, Book 14: page 271 (15 May 1902) - located at the Nodaway County Courthouse, Maryville MO.

sign. rating: 44
evaluation: NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 11 August 1993

Platte River Bridge

NODA26

GENERAL DATA

structure no.: 231R00.4	city/town: 2.5 miles northeast of Parnell
county: Nodaway	feature inters.: Platte River
	cadastral grid: S4, T65N, R33W
	highway route: County Road 231
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 7-panel, pin-connected Pratt through truss, with steel stringer approaches	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 120.0'	alterations: approach span added, 1980
total length: 216.0'	floor/decking : timber deck over steel stringers
roadway width: 16.5'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing or 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerods with turnbuckles; lateral bracing: square bar; strut: 2 angles; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date: c1915
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 231000.4; field inspection by Mark Hufstetler, 8 September 1990.

sign. rating: 30
evaluation: NRHP non-eligible (typically configured, poorly documented example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

102 River Bridge

NODA27

GENERAL DATA

structure no.: 251R01.0	city/town: 1.0 mile northeast of Pickering
county: Nodaway	feature inters.: 102 River
	cadastral grid: S3, T65N, R35W
	highway route: County Road 251
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 6-panel, pin-connected Pratt through truss, with steel stringer approach spans	
substructure: concrete-filled steel cylinder piers, timber back- and wingwalls	
span number: 1	condition: good
span length: 140.0'	alterations: none
total length: 235.0'	floor/decking : timber deck over steel stringers
roadway width: 14.5'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with lacing or 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: square eyebars with turnbuckles; lateral bracing: 1 angle; strut: 4 angles with double lacing; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date: 1909	
erection cost: unknown	
designer: unknown	
fabricator : Cambria Steel Company, Pittsburgh PA	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 251R01.0; field inspection by Mark Hufstetler, 9 September 1990.	
sign. rating: 36	
evaluation: NRHP non-eligible (typical example of common truss type, partially documented)	

inventoried by: Michelle Crow-Dolby 11 August 1993

102 River Bridge

NODA28

GENERAL DATA

structure no.: 254000.8	city/town: 7.2 miles northeast of Maryville
county: Nodaway	feature inters.: 102 River
	cadastral grid: S10, T65N, R35W
	highway route: County Road 254
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 8-panel, pin-connected Pratt through truss, with steel stringer approach span	
substructure: steel pile piers and abutments with timber back- and wingwalls	
span number: 1	condition: fair
span length: 150.0'	alterations: none
total length: 150.0'	floor/decking : timber deck over steel stringers
roadway width: 15.7'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerods with turnbuckles; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, field-bolted to verticals; guard-rail: 2 angles

HISTORICAL DATA

erection date: c1915	
erection cost: unknown	
designer: unknown	
fabricator : Bethlehem Steel Company, Pittsburgh PA	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 254000.8; field inspection by Mark Hufstetler, 9 September 1990.	
sign. rating: 32	
evaluation: NRHP non-eligible (typically configured, long-span example of mainstay structural type, poorly documented)	

inventoried by: Michelle Crow-Dolby 11 August 1993

Norvey Creek Bridge

NODA29

GENERAL DATA

structure no.: 254001.7	city/town: 7.6 miles northeast of Maryville
county: Nodaway	feature inters.: Norvey Creek
	cadastral grid: S11, T65N, R35W
	highway route: County Road 254
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach span	
substructure: timber pile abutments and steel pile bent piers	
span number: 1	condition: fair
span length: 80.0'	alterations: none
total length: 128.0'	floor/decking : timber deck over steel stringers
roadway width: 13.7'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; counter: round eyerods with turnbuckles; floor beam: I-beam, U-bolted to verticals; guardrail: 2 angles

HISTORICAL DATA

erection date: c1915
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 254001.7; field inspection by Mark Hufstetler, 8 September 1990.

sign. rating: 33
evaluation: NRHP non-eligible (typical example of common truss type)

Inventoried by: Michelle Crow-Dolby 11 August 1993

Norvey Creek Bridge

NODA30

GENERAL DATA

structure no.: 262000.3	city/town: 7.0 miles northeast of Maryville
county: Nodaway	feature inters.: Norvey Creek
	cadastral grid: S15, T65N, R35W
	highway route: County Road 262
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt through truss, with steel stringer approach	
substructure: timber pile abutments reinforced with steel; steel pile bent piers	
span number: 1	condition: fair
span length: 80.0'	alterations: none
total length: 106.0'	floor/decking : timber deck over steel stringers
roadway width: 14.0'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerods with turnbuckles; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beam, U-bolted to verticals; guardrail: timber

HISTORICAL DATA

erection date: c1915	
erection cost: unknown	
designer: unknown	
fabricator : Carnegie Steel Company, Pittsburgh PA	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 262000.3; field inspection by Mark Hufstetler, 9 September 1990.	
sign. rating: 27	
evaluation: NRHP non-eligible (typical example of common truss type)	

inventoried by: Michelle Crow-Dolby 11 August 1993

Bridge

NODA31

GENERAL DATA

structure no.: 269000.6	city/town: 7.5 miles north of Maryville
county: Nodaway	feature inters.: tributary of 102 River
	cadastral grid: S8, T65N, R35W
	highway route: County Road 269
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 40.0'	floor/decking : timber deck
roadway width: 13.5'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1915
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 269000.6.

sign. rating: 20
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Muddy Creek Bridge

NODA33

GENERAL DATA

structure no.: 315001.1	city/town: 2.0 miles northeast of Burlington Junction
county: Nodaway	feature inters.: Muddy Creek
	cadastral grid: S11, T65N, R37W
	highway route: County Road 315
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent pier and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 53.0'	floor/decking : timber deck
roadway width: 15.7'	other features: no guardrails

HISTORICAL DATA

erection date: 1902
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 315001.1; Nodaway County Court Record, Book 16: page 369 (22 July 1907) - located at the Nodaway County Courthouse, Maryville MO.

sign. rating: 31
evaluation: NRHP non-eligible (typical example of common truss type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Mill Creek Bridge

NODA35

GENERAL DATA

structure no.: 323R00.8	city/town: 2.0 miles northwest of Burlington Junction
county: Nodaway	feature inters.: Mill Creek
	cadastral grid: S7, T65N, R37W
	highway route: County Road 323
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 85.0'	alterations: approach spans added, 1984
total length: 123.0'	floor/decking : timber deck
roadway width: 14.7'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1915
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 323R00.8.

sign. rating: 25
evaluation: NRHP non-eligible (typical example of common truss type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Bridge

NODA36

GENERAL DATA

structure no.:	329001.0	city/town:	3.9 miles west of Burlington Junction
county:	Nodaway	feature inters.:	tributary of Mill Creek
		cadastral grid:	S11, T65N, R38W
		highway route:	County Road 329
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach span		
substructure:	timber pile abutments, wingwalls and piers		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	76.0'	floor/decking :	timber deck
roadway width:	13.3'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1907
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	J.H. Sparks, St. Joseph MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 329001.0.
sign. rating:	40
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Sand Creek Bridge

NODA38

GENERAL DATA

structure no.:	349000.6	city/town:	8.7 miles northwest of Maryville
county:	Nodaway	feature inters.:	Sand Creek
		cadastral grid:	S35, T65N, R37W
		highway route:	County Road 349
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent pier and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	67.0'	floor/decking :	timber deck
roadway width:	14.0'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 349000.9.
sign. rating:	26
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Bridge

NODA41

GENERAL DATA

structure no.:	382000.7	city/town:	4.6 miles northeast of Maryville
county:	Nodaway	feature inters.:	tributary of 102 River
		cadastral grid:	S28, T65N, R35W
		highway route:	County Road 382
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	50.0'	floor/decking :	timber deck
roadway width:	13.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 382000.7.

sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 11 August 1993

102 River Bridge

NODA42

GENERAL DATA

structure no.: 383001.8	city/town: 4.5 miles northeast of Maryville
county: Nodaway	feature inters.: 102 River
	cadastral grid: S27, T65N, R35W
	highway route: County Road 383
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, rigid-connected Pratt through truss, with steel stringers	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 100.0'	alterations: unknown
total length: 172.0'	floor/decking : timber deck
roadway width: 16.0'	other features: steel lattice guardrails

HISTORICAL DATA

erection date: 1938
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 383001.8.

sign. rating: 25
evaluation: NRHP non-eligible (typically configured truss type from the 1930s)

inventoried by: Michelle Crow-Dolby 11 August 1993

Long Branch Bridge

NODA44

GENERAL DATA

structure no.: 398001.8	city/town: 7.7 miles northeast of Maryville
county: Nodaway	feature inters.: Long Branch
	cadastral grid: S29, T65N, R34W
	highway route: County Road 398
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: timber pile abutments and wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 41.0'	floor/decking : timber deck
roadway width: 14.6'	other features: no guardrails

HISTORICAL DATA

erection date: c1915
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 398001.8.
sign. rating: 21
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Honey Creek Bridge

NODA45

GENERAL DATA

structure no.: 420000.3	city/town: 2.5 miles north of Parnell
county: Nodaway	feature inters.: Honey Creek
	cadastral grid: S12/13, T65N, R33W
	highway route: County Road 420
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: concrete-filled steel cylinder piers	
span number: 1	condition: fair
span length: 50.0'	alterations: unknown
total length: 84.0'	floor/decking: timber deck
roadway width: 14.4'	other features: no guardrails

HISTORICAL DATA

erection date: c1900
erection cost: unknown
designer: unknown
fabricator: unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 420000.3.

sign. rating: 26
evaluation: NRHP non-eligible (typical example of common truss type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Platte River Bridge

NODA46

GENERAL DATA

structure no.:	426000.8	city/town:	1.0 mile southwest of Parnell
county:	Nodaway	feature inters.:	tributary of Platte River
		cadastral grid:	S19, T65N, R33W
		highway route:	County Road 426
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure: steel or wrought iron, 5-panel, pin-connected Pratt through truss; steel stringer approach spans

substructure: concrete-filled steel cylinder piers, timber pile abutments with timber back- and wingwalls

span number:	1	condition:	good
span length:	80.0'	alterations:	none
total length:	156.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.5'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: square eyebars with turnbuckles; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beam, U-bolted to verticals; guardrail: steel, 2 angles; portal builder's plate: THE MASSILLON / BRIDGE COMPANY / MASSILLON / 1894 OHIO

HISTORICAL DATA

erection date: 1894

erection cost: unknown

designer: Massillon Bridge Company, Massillon OH

fabricator : Massillon Bridge Company, Massillon OH

contractor: Massillon Bridge Company, Massillon OH

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 426000.8; Nodaway County Record, Book 17: page 128 (7 May 1908) - located at the Nodaway County Courthouse, Maryville MO; field inspection by Mark Hufstetler, 8 September 1990.

sign. rating: 50

evaluation: NRHP possibly eligible (well-preserved example of mainstay structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Platte River Bridge

NODA48

GENERAL DATA

structure no.: 456R01.1	city/town: 0.6 mile northwest of Ravenwood
county: Nodaway	feature inters.: Platte River
	cadastral grid: S12, T64N, R34W
	highway route: County Road 456
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 8-panel, pin-connected Parker through truss; steel stringer approaches	
substructure: steel pile bent piers, steel back- and wingwalls and steel abutments	
span number: 1	condition: fair
span length: 150.0'	alterations: modified, 1984
total length: 226.0'	floor/decking : timber deck over steel stringers
roadway width: 15.7'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, U-bolted to verticals; guard-rail: steel, 2 angles

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : Inland Steel Company, East Chicago IN	
contractor : unknown	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 456R01.1; Bridge Commissioner's Reports on Sale of Bridges, 14 November 1889, 5 December 1889 - located at the Nodaway County Courthouse, Maryville MO; field inspection by Mark Hufstetler, 8 September 1990.
sign. rating: 38	
evaluation:	NRHP non-eligible (inadequately documented example of uncommon structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

102 River Bridge

NODA49

GENERAL DATA

structure no.:	474001.2	city/town:	2.3 miles east of Maryville
county:	Nodaway	feature inters.:	102 River
		cadastral grid:	S10, T64N, R35W
		highway route:	County Road 474
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 6-panel, pin-connected Pratt through truss; steel stringer approach spans		
substructure:	steel pile bent piers and abutments; timber pile abutment on east end		
span number:	1	condition:	fair
span length:	108.0'	alterations:	new approach added, 1981
total length:	171.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: square eyobar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles with continuous plate; floor beam: I-beam, field-bolted to verticals; guardrail: steel, 2 angles

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	Lackawanna Steel Company, Pittsburgh PA
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 474001.2; Nodaway County Court Record: n.p. (7 May 1883) - located at the Nodaway County Court-house, Maryville MO; field inspection by Mark Hufstetler, 15 September 1990.
sign. rating:	27
evaluation:	NRHP non-eligible (typically configured, poorly documented example of common structural type)

Inventoried by: Michelle Crow-Dolby 11 August 1993

Big Slough Bridge

NODA50

GENERAL DATA

structure no.: 496000.5	city/town: 3.8 miles northwest of Maryville
county: Nodaway	feature inters.: Big Slough
	cadastral grid: S10, T64N, R36W
	highway route: County Road 496
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: timber pile abutments and timber back- and wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: none
total length: 40.0'	floor/decking : timber deck over steel stringers
roadway width: 14.0'	other features: upper chord and inclined end post: 2 channels with lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; counter: square eyebars with turnbuckles; floor beam: I-beam, U-bolted to verticals

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor : unknown	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 496000.5; field inspection by Mark Hufstetler, 11 September 1990.
sign. rating: 21	
evaluation:	NRHP non-eligible (typical example of a common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Nodaway River Bridge

NODA52

GENERAL DATA

structure no.: 526000.8	city/town: 4.8 miles south of Burlington Junction
county: Nodaway	feature inters.: Nodaway River
	cadastral grid: S5, T64N, R37W
	highway route: County Road 526
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 11-panel, rigid-connected Parker through truss; steel stringer approach spans	
substructure: steel pile abutments and double steel pile bent piers; concrete back-and wingwalls on east end	
span number: 1	condition: fair
span length: 225.0'	alterations: none
total length: 288.0'	floor/decking : timber deck over steel stringers
roadway width: 13.4'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 angles with batten plate; vertical: 2 channels with lacing; diagonal: 2 channels with batten plates; lateral bracing: round rods with threaded ends; strut: 2 angle; floor beam: I-beams, field-bolted to verticals; guardrail: angle

HISTORICAL DATA

erection date: c1925	
erection cost: unknown	
designer: unknown	
fabricator : Illinois Steel Company, Chicago IL	
contractor: unknown	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 526000.8; Bridge Commissioner's Reports on Sale of Bridges n.p. (14 November 1889) - located at the Nodaway County Courthouse, Maryville MO; field inspection by Mark Hufstetler, 11 September 1990.
sign. rating: 27	
evaluation:	NRHP non-eligible (long-span example of uncommon structural type, inadequately documented)

inventoried by: Michelle Crow-Dolby 11 August 1993

Sand Creek Bridge

NODA53

GENERAL DATA

structure no.: 534000.7	city/town: 9.4 miles west of Maryville
county: Nodaway	feature inters.: Sand Creek
	cadastral grid: S15, T64N, R37W
	highway route: County Road 534
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 74.0'	floor/decking : timber deck
roadway width: 14.2'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 534000.7.

sign. rating: 25
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Florida Creek Bridge

NODA54

GENERAL DATA

structure no.: 560000.2	city/town: 5.0 miles southeast of Maryville
county: Nodaway	feature inters.: tributary of Florida Creek
	cadastral grid: S26, T64N, R37W
	highway route: County Road 560
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent pier and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: modified, 1981
total length: 54.0'	floor/decking : timber deck
roadway width: 13.8'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor : unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 560000.2.	
sign. rating: 20	
evaluation: NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 11 August 1993

Florida Creek Bridge

NODA55

GENERAL DATA

structure no.: 560000.6	city/town: 8.4 miles southwest of Maryville
county: Nodaway	feature inters.: tributary of Florida Creek
	cadastral grid: S26, T64N, R37W
	highway route: County Road 560
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss	
substructure: steel pile bent abutments with timber wingwalls	
span number: 1	condition: fair
span length: 70.0'	alterations: modified, 1981
total length: 70.0'	floor/decking : timber deck
roadway width: 16.3'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 560000.6.

sign. rating: 28
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

White Cloud Creek Bridge

NODA56

GENERAL DATA

structure no.: 577000.2	city/town: 3.0 miles southwest of Maryville
county: Nodaway	feature inters.: White Cloud Creek
	cadastral grid: S23, T64N, R36W
	highway route: County Road 577
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 75.0'	alterations: unknown
total length: 108.0'	floor/decking : timber deck
roadway width: 14.5'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1905
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 577000.2.

sign. rating: 29
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Mozingo Creek Bridge

NODA57

GENERAL DATA

structure no.: 628000.9	city/town: 5.0 miles southeast of Maryville
county: Nodaway	feature inters.: Mozingo Creek
	cadastral grid: S24, T64N, R35W
	highway route: County Road 628
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss; timber stringer approaches	
substructure: concrete-filled steel cylinder piers and timber abutments	
span number: 1	condition: fair
span length: 40.0'	alterations: none
total length: 58.0'	floor/decking : timber deck over steel stringers
roadway width: 14.0'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; counter: round eyerod with turnbuckle; floor beam: I-beam, U-bolted to verticals

HISTORICAL DATA

erection date: 1904	
erection cost: unknown	
designer: unknown	
fabricator : Carnegie Steel Company, Pittsburgh PA	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 628000.9; field inspection by Mark Hufstetler, 9 September 1990.	
sign. rating: 33	
evaluation: NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 11 August 1993

Long Branch Bridge

NODA58

GENERAL DATA

structure no.:	643000.6	city/town:	7.7 miles southeast of Maryville
county:	Nodaway	feature inters.:	Long Branch
		cadastral grid:	S5, T63N, R34W
		highway route:	County Road 643
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	timber pile pier, abutments and wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	52.0'	floor/decking :	timber deck
roadway width:	12.1'	other features:	no guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 643000.6.

sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Long Branch Bridge

NODA59

GENERAL DATA

structure no.: 646000.3	city/town: 9.2 miles southeast of Maryville
county: Nodaway	feature inters.: Long Branch
	cadastral grid: S32, T64N, R34W
	highway route: County Road 646
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: timber pile abutments, wingwalls and piers	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 73.0'	floor/decking : timber deck
roadway width: 13.9'	other features: no guardrails

HISTORICAL DATA

erection date: 1907	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: county work force (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 646000.3; Nodaway County Court Record, Book 16: page 357 (3 July 1907) -located at the Nodaway County Courthouse, Maryville MO.	
sign. rating: 37	
evaluation: NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 11 August 1993

Long Branch Bridge

NODA60

GENERAL DATA

structure no.:	652001.1	city/town:	7.0 miles southeast of Maryville
county:	Nodaway	feature inters.:	Long Branch
		cadastral grid:	S20, T64N, R34W
		highway route:	County Road 652
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss
substructure: timber abutments, back- and wingwalls

span number:	1	condition:	fair
span length:	40.0'	alterations:	none
total length:	41.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.7'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched eyebars; vertical: 2 channels with lacing; diagonal: 2 punched eyebars; counter: round rods with turnbuckles; floor beam: I-beams, U-bolted to verticals

HISTORICAL DATA

erection date: 1904
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 652001.1; field inspection by Mark Hufstetler, 9 September 1990.

sign. rating: 28
evaluation: NRHP non-eligible (typical example of a common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Bridge

NODA61

GENERAL DATA

structure no.: 654000.2	city/town: 9.2 miles southeast of Maryville
county: Nodaway	feature inters.: tributary of Platte River
	cadastral grid: S27, T64N, R34W
	highway route: County Road 654
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: timber pile pier, abutments and wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: unknown
total length: 64.0'	floor/decking : timber deck
roadway width: 14.7'	other features: no guardrails

HISTORICAL DATA

erection date: c1900
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 654000.2.

sign. rating: 26
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Platte River Bridge

NODA62

GENERAL DATA

structure no.: 655001.4	city/town: 2.9 miles north of Conception Junction
county: Nodaway	feature inters.: old channel of Platte River
	cadastral grid: S35, T64N, R34W
	highway route: County Road 655
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, pin-connected Pratt through truss	
substructure: concrete-filled steel cylinder piers, concrete back-and wingwalls	
span number: 1	condition: fair
span length: 80.0'	alterations: none
total length: 80.0'	floor/decking : timber deck over steel stringers
roadway width: 14.1'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 square eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, U-bolted to verticals

HISTORICAL DATA

erection date: 1904	
erection cost: unknown	
designer: unknown	
fabricator : Cambria Steel Company, Pittsburgh PA	
contractor : unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 655001.4; field inspection by Mark Hufstetler, 14 September 1990.	
sign. rating: 33	
evaluation: NRHP non-eligible (typically configured, poorly documented example of mainstay structural type)	

inventoried by: Michelle Crow-Dolby 11 August 1993

Platte River Bridge

NODA63

GENERAL DATA

structure no.: 655R01.4	city/town: 2.8 miles north of Conception Junction
county: Nodaway	feature inters.: Platte River
	cadastral grid: S35, T64N, R34W
	highway route: County Road 655
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 2	condition: fair
span length: 80.0'	alterations: approach span added, 1984
total length: 192.0'	floor/decking : timber deck
roadway width: 14.2'	other features: no guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 655R01.4; Bridge Commissioner's Reports on Sale of Bridges: 1 June 1884.

sign. rating: 32
evaluation: NRHP non-eligible (typically configured, inadequately documented example of common structural type, distinguished somewhat by its two-span configuration)

inventoried by: Michelle Crow-Dolby 11 August 1993

Wildcat Creek Bridge

NODA65

GENERAL DATA

structure no.: 692000.7	city/town: 3.1 miles southeast of Conception Junction
county: Nodaway	feature inters.: branch of Wildcat Creek
	cadastral grid: S20, T63N, R33W
	highway route: County Road 692
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: unknown	
span number: 1	condition: fair
span length: 65.0'	alterations: unknown
total length: 65.0'	floor/decking : timber deck
roadway width: 14.0'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1915	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 692000.7.	
sign. rating: 28	
evaluation: NRHP non-eligible (typical example of common structural type)	

Inventoried by: Michelle Crow-Dolby 11 August 1993

Wildcat Creek Bridge

NODA66

GENERAL DATA

structure no.: 694000.2	city/town: 3.5 miles southeast of Conception Junction
county: Nodaway	feature inters.: Wildcat Creek
	cadastral grid: S16, T63N, R33W
	highway route: County Road 694
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: unknown
total length: 114.0'	floor/decking : timber deck
roadway width: 13.6'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 694000.2.	
sign. rating: 25	
evaluation: NRHP non-eligible (typical example of common structural type)	

Inventoried by: Michelle Crow-Dolby 11 August 1993

Wildcat Creek Bridge

NODA67

GENERAL DATA

structure no.: 697000.9	city/town: 5.8 miles southeast of Conception Junction
county: Nodaway	feature inters.: Wildcat Creek
	cadastral grid: S28, T63N, R33W
	highway route: County Road 697
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt through truss; steel stringer approaches	
substructure: steel pile bent piers and abutments with timber pile abutments	
span number: 1	condition: fair
span length: 75.0'	alterations: substructure replaced
total length: 108.0'	floor/decking : timber deck over steel stringers
roadway width: 13.8'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beam, U-bolted to verticals; guardrail: timber; portal: lattice

HISTORICAL DATA

erection date: c1905	
erection cost: unknown	
designer: unknown	
fabricator : Jones and Laughlin Steel Company, Pittsburgh PA	
contractor : unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 697000.9; field inspection by Mark Hufstetler, 9 September 1990.	
sign. rating: 25	
evaluation: NRHP non-eligible (typical example of common structural type, poorly documented)	

inventoried by: Michelle Crow-Dolby 11 August 1993

Wildcat Creek Bridge

NODA68

GENERAL DATA

structure no.: 701000.3	city/town: 5.2 miles southeast of Conception Junction
county: Nodaway	feature inters.: Wildcat Creek
	cadastral grid: S27, T63N, R33W
	highway route: County Road 701
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 65.0'	alterations: unknown
total length: 115.0'	floor/decking : timber deck
roadway width: 14.0'	other features: no guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 701000.3.	
sign. rating: 28	
evaluation: NRHP non-eligible (typical example of common structural type)	

Inventoried by: Michelle Crow-Dolby 11 August 1993

102 River Bridge

NODA69

GENERAL DATA

structure no.: 741000.0	city/town: 9.6 miles southeast of Maryville
county: Nodaway	feature inters.: 102 River
	cadastral grid: S4, T62N, R35W
	highway route: County Road 741
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 10-panel, rigid-connected Warren pony truss with polygonal upper chords; concrete deck girder approach spans	
substructure: steel pile piers with concrete caps	
span number: 1	condition: good
span length: 100.0'	alterations: unknown
total length: 222.0'	floor/decking : concrete deck over steel stringers
roadway width: 19.0'	other features: steel angle guardrails

HISTORICAL DATA

erection date: 1946	
erection cost: unknown	
designer: Missouri State Highway Department	
fabricator : unknown	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 741000.0.	
sign. rating: 33	
evaluation: NRHP non-eligible (typical example of MSHD pony truss design)	

inventoried by: Michelle Crow-Dolby 11 August 1993

White Cloud Creek Bridge

NODA70

GENERAL DATA

structure no.:	769000.2	city/town:	9.1 miles southwest of Maryville
county:	Nodaway	feature inters.:	White Cloud Creek
		cadastral grid:	S36, T63N, R36W
		highway route:	County Road 769
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	85.0'	alterations:	unknown
total length:	85.0'	floor/decking :	timber deck
roadway width:	15.6'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 769000.2.
sign. rating:	30
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 11 August 1993

Jenkins Creek Bridge

NODA71

GENERAL DATA

structure no.: 776000.1	city/town: 9.1 miles southwest of Maryville
county: Nodaway	feature inters.: Jenkins Creek
	cadastral grid: S27, T63N, R36W
	highway route: County Road 776
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach span	
substructure: concrete-filled steel cylinder piers	
span number: 1	condition: fair
span length: 40.0'	alterations: modified, 1940
total length: 54.0'	floor/decking : timber deck
roadway width: 13.8'	other features: timber guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 776000.1.	
sign. rating: 25	
evaluation: NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 11 August 1993

Jenkins Creek Bridge

NODA72

GENERAL DATA

structure no.:	777000.2	city/town:	9.2 miles southwest of Maryville
county:	Nodaway	feature inters.:	Jenkins Creek
		cadastral grid:	S33, T63N, R36W
		highway route:	County Road 777
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt through truss; steel stringer approach on south end		
substructure:	steel abutment on north end; steel pile bent piers		
span number:	1	condition:	fair
span length:	84.0'	alterations:	modified, 1984
total length:	100.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.6'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beam, U-bolted to verticals; guardrail: timber; portal: lattice

HISTORICAL DATA

erection date:	c1907
erection cost:	unknown
designer:	unknown
fabricator :	Jones and Laughlin Steel Company, Pittsburgh PA
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 777000.2; Nodaway County Court Record, Book 16: page 155 (5 February 1907) - located at the Nodaway County Courthouse, Maryville MO; field inspection by Mark Hufstetler, 11 September 1990.
sign. rating:	29
evaluation:	NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Coal Creek Bridge

NODA73

GENERAL DATA

structure no.: 780000.7	city/town: 8.2 miles southwest of Maryville
county: Nodaway	feature inters.: Coal Creek
	cadastral grid: S22, T63N, R36W
	highway route: County Road 780
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: timber pile abutments and wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 41.0'	floor/decking : timber deck
roadway width: 13.4'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 780000.7.	
sign. rating: 20	
evaluation: NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 11 August 1993

Elkhorn Creek Bridge

NODA74

GENERAL DATA

structure no.:	7830000.2	city/town:	7.9 miles southwest of Maryville
county:	Nodaway	feature inters.:	tributary of Elkhorn Creek
		cadastral grid:	S16, T63N, R36W
		highway route:	County Road 783
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	timber abutments and wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	40.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.3'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 783000.2.
sign. rating:	20
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Elkhorn Creek Bridge

NODA75

GENERAL DATA

structure no.: 795000.1	city/town: 5.4 miles southeast of Skidmore
county: Nodaway	feature inters.: tributary of Elkhorn Creek Tributary
	cadastral grid: S30, T63N, R36W
	highway route: County Road 795
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: concrete-filled steel cylinder piers	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 83.0'	floor/decking : timber deck
roadway width: 12.1'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 795000.1.

sign. rating: 25
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Elkhorn Creek Bridge

NODA76

GENERAL DATA

structure no.: 798000.7	city/town: 9.2 miles southwest of Maryville
county: Nodaway	feature inters.: Elkhorn Creek Tributary
	cadastral grid: S19, T63N, R36W
	highway route: County Road 798
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 70.0'	alterations: unknown
total length: 86.0'	floor/decking : timber deck
roadway width: 13.6'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 798000.7.

sign. rating: 28
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Elkhorn Creek Bridge

NODA77

GENERAL DATA

structure no.: 802000.8	city/town: 9.2 miles southwest of Maryville
county: Nodaway	feature inters.: Elkhorn Creek
	cadastral grid: S13, T63N, R36W
	highway route: County Road 802
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, rigid-connected Pratt through truss	
substructure: steel pile bent pier and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 80.0'	alterations: unknown
total length: 99.0'	floor/decking : timber deck
roadway width: 16.1'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1925
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 802000.8.

sign. rating: 23
evaluation: NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Florida Creek Bridge

NODA78

GENERAL DATA

structure no.: 821000.2	city/town: 9.5 miles southwest of Maryville
county: Nodaway	feature inters.: Florida Creek
	cadastral grid: S34, T64N, R37W
	highway route: County Road 821
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 65.0'	alterations: modified, 1950; possibly moved
total length: 124.0'	floor/decking : timber deck
roadway width: 13.9'	other features: no guardrails

HISTORICAL DATA

erection date: 1908 (reconstruction)	
erection cost: \$170.00 (reconstruction)	
designer: unknown	
fabricator : unknown	
contractor: Standard Bridge Company, Omaha NE (reconstruction)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 821000.2; Nodaway County Court Record, Book 17: page 128 (7 May 1908) -located at the Nodaway County Courthouse, Maryville MO.	
sign. rating: 42	
evaluation: NRHP non-eligible (typical example of common structural type)	

Inventoried by: Michelle Crow-Dolby 11 August 1993

Bridge

NODA79

GENERAL DATA

structure no.:	845000.3	city/town:	2.2 miles southwest of Skidmore
county:	Nodaway	feature inters.:	tributary of Nodaway River
		cadastral grid:	S7, T63N, R37W
		highway route:	County Road 845
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt half-hip pony truss; timber stringer approach on west end		
substructure:	steel pile abutment on east end; steel pile bent piers and timber pile abutment with timber back- and wingwalls on west end		
span number:	1	condition:	fair
span length:	64.0'	alterations:	modified, 1960
total length:	81.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.0'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with double lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; floor beam: I-beam, U-bolted to verticals

HISTORICAL DATA

erection date:	c1890
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 845000.3; field inspection by Mark Hufstetler, 11 September 1990.
sign. rating:	35
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Elkhorn Creek Bridge

NODA80

GENERAL DATA

structure no.: 853000.3	city/town: 4.5 miles southeast of Skidmore
county: Nodaway	feature inters.: Elkhorn Creek
	cadastral grid: S25, T63N, R37W
	highway route: County Road 853
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: unknown
total length: 87.0'	floor/decking : timber deck
roadway width: 18.0'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1905
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 853000.3; Nodaway County Court Record, Book 16: page 287 (5 May 1907), pages 338-9 (2 August 1909) - located at the Nodaway County Courthouse, Maryville MO.

sign. rating: 25
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Jenkins Creek Bridge

NODA84

GENERAL DATA

structure no.: 890000.7	city/town: 9.1 miles southeast of Skidmore
county: Nodaway	feature inters.: branch of Jenkins Creek
	cadastral grid: S17, T62N, R36W
	highway route: County Road 890
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: timber pile abutments and wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 41.0'	floor/decking : timber deck
roadway width: 14.5'	other features: no guardrails

HISTORICAL DATA

erection date: c1900
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 890000.7.

sign. rating: 21
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Jenkins Creek Bridge

NODA86

GENERAL DATA

structure no.:	893000.4	city/town:	8.4 miles southeast of Skidmore
county:	Nodaway	feature inters.:	Jenkins Creek
		cadastral grid:	S8, T62N, R36W
		highway route:	County Road 893
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	79.0'	floor/decking :	timber deck
roadway width:	14.5'	other features:	no guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 893000.4.
sign. rating:	28
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Owl Creek Bridge

NODA87

GENERAL DATA

structure no.:	897001.7	city/town:	8.4 miles southwest of Barnard
county:	Nodaway	feature inters.:	Owl Creek
		cadastral grid:	S32, T62N, R36W
		highway route:	County Road 897
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	80.0'	alterations:	unknown
total length:	81.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 897001.7.

sign. rating:	31
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 11 August 1993

White Cloud Creek Bridge

NODA88

GENERAL DATA

structure no.:	916000.2	city/town:	3.8 miles northwest of Barnard
county:	Nodaway	feature inters.:	White Cloud Creek
		cadastral grid:	S7, T62N, R35W
		highway route:	County Road 916
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt through truss; 3-span steel stringer approach on west end

substructure: timber pile abutment and steel pile bent piers

span number:	1	condition:	poor
span length:	80.0'	alterations:	bridge abandoned; road closed
total length:	145.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.5'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 square eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, U-bolted to verticals

HISTORICAL DATA

erection date: 1904

erection cost: unknown

designer: John Gilligan, Falls City NE

fabricator : John Gilligan, Falls City NE;
Carnegie Steel Company, Pittsburgh PA

contractor: John Gilligan, Falls City NE

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 916000.2; field inspection by Mark Hufstetler, 15 September 1990.

sign. rating: 39

evaluation: NRHP non-eligible (poorly preserved example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Bridge

NODA89

GENERAL DATA

structure no.:	941000.7	city/town:	1.0 mile north of Barnard
county:	Nodaway	feature inters.:	tributary of 102 River
		cadastral grid:	S10, T62N, R35W
		highway route:	County Road 941
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	timber pile abutments and wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	50.0'	floor/decking :	timber deck
roadway width:	16.0'	other features:	unknown

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 941000.7.

sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Long Branch Bridge

NODA90

GENERAL DATA

structure no.:	969000.4	city/town:	7.3 miles southwest of Conception Junction
county:	Nodaway	feature inters.:	Long Branch
		cadastral grid:	S7, T62N, R34W
		highway route:	County Road 969
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with timber stringer approach spans		
substructure:	concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	72.0'	floor/decking :	timber deck
roadway width:	13.0'	other features:	no guardrails

HISTORICAL DATA

erection date:	1902
erection cost:	\$599.00
designer:	unknown
fabricator :	unknown
contractor:	Sparks and Kinney, St. Joseph MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 969000.4; Nodaway County Court Record, Book 14: page 271 (15 May 1902) - located at the Nodaway County Courthouse, Maryville MO.
sign. rating:	39
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

Platte River Bridge

NODA91

GENERAL DATA

structure no.:	972R00.8	city/town:	5.4 miles northeast of Barnard
county:	Nodaway	feature inters.:	Platte River
		cadastral grid:	S33, T63N, R34W
		highway route:	County Road 972
		highway distr.:	1
		current owner:	Nodaway County

STRUCTURAL DATA

superstructure:	steel, 7-panel, rigid-connected Pratt through truss, with steel stringer approach spans		
substructure:	concrete abutments, wingwalls and piers; concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	140.0'	alterations:	unknown
total length:	196.0'	floor/decking :	timber deck
roadway width:	14.0'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 972R00.8; Nodaway County Record, Book 16: page 489 (April 1909) - located at the Nodaway County Courthouse, Maryville MO.

sign. rating:	26
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 11 August 1993

Clear Creek Bridge

NODA92

GENERAL DATA

structure no.: 985000.1	city/town: 6.3 miles southeast of Barnard
county: Nodaway	feature inters.: Clear Creek
	cadastral grid: S27, T62N, R34W
	highway route: County Road 985
	highway distr.: 1
	current owner: Nodaway County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: steel pile bent pier and abutments with timber wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: unknown
total length: 63.0'	floor/decking : timber deck
roadway width: 14.4'	other features: no guardrails

HISTORICAL DATA

erection date: c1900
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 985000.1.

sign. rating: 26
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 11 August 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Mill Creek Bridge
MHTD: 010001.4

NODA07

DATE(S) OF CONSTRUCTION

c1890

LOCATION

County Road 10 over Mill Creek; S7, T66N, R37W
2.0 miles northwest of Elmo; Nodaway County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 25)

CONDITION

fair

OWNER

Nodaway County

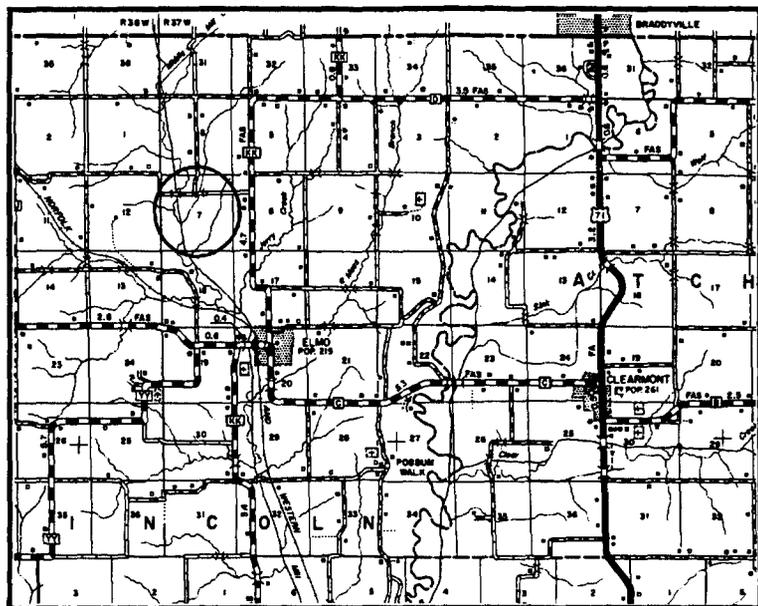
span number: 1
span length: 80.0'
total length: 131.0'
roadway wdt.: 14.6'

superstructure: steel or wrought iron, 5-panel, pin-connected Pratt through truss, with steel stringer approach spans
substructure: steel pile bent piers and abutments
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: looped square and round eyerods; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beam, U-bolted to vertical; guardrail: timber; portal: lattice

Located in northwestern Nodaway County, some two miles northwest of Elmo, this bridge carries County Road 10 across Mill Creek. The single-span structure consists of a five-panel, pinned Pratt through truss, supported by a steel pile bent substructure. Featuring a lattice portal and timber guardrails, the Mill Creek Bridge dates to 1881. In October of that year the county court received a letter from a representative of the Lincoln Township Board. Contained in the letter was a recommendation to the court "that an order be made for a bridge on Mill Creek on public road... near the residence of James L. Davis." In addition, the board concluded "that said bridge is considered by the Board to be of no ordinary importance and that [we hope] you will give the matter your earliest attention." The "township believes there should be a bridge at this location" to be built at a "probable cost of \$300," a second letter from the board in September 1881 announced. When the bridge was actually erected is unknown, but physical attributes of the truss itself suggest a fabrication date of around 1890. The Mill Creek Bridge is a typically configured example of a mainstay structural type in Missouri—the pinned Pratt through truss.

NAME(S) OF STRUCTURE

Mill Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION**LOCATION MAP**TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 010001.4; Bridge Commissioner's Reports on Sale of Bridges, letter dated 17 October 1881 - located at Nodaway County Courthouse, Maryville MO; field inspection by Mark Hufstetler, 8 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE11 August 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

102 River Bridge
MHTD: 251R01.0

NODA27

DATE(S) OF CONSTRUCTION

1909

LOCATION

County Road 251 over 102 River; S3, T65N, R35W
1.0 mile northeast of Pickering; Nodaway County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 36)

CONDITION

good

OWNER

Nodaway County

span number: 1
span length: 140.0'
total length: 235.0'
roadway wdt.: 14.5'

superstructure: steel, 6-panel, pin-connected Pratt through truss, with steel stringer approach spans
substructure: concrete-filled steel cylinder piers, timber back- and wingwalls
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with lacing or 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: square eyebars with turnbuckles; lateral bracing: 1 angle; strut: 4 angles with double lacing; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

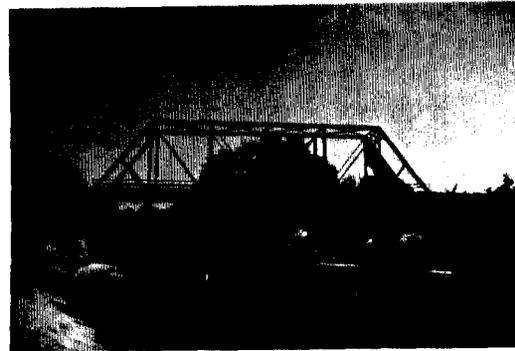
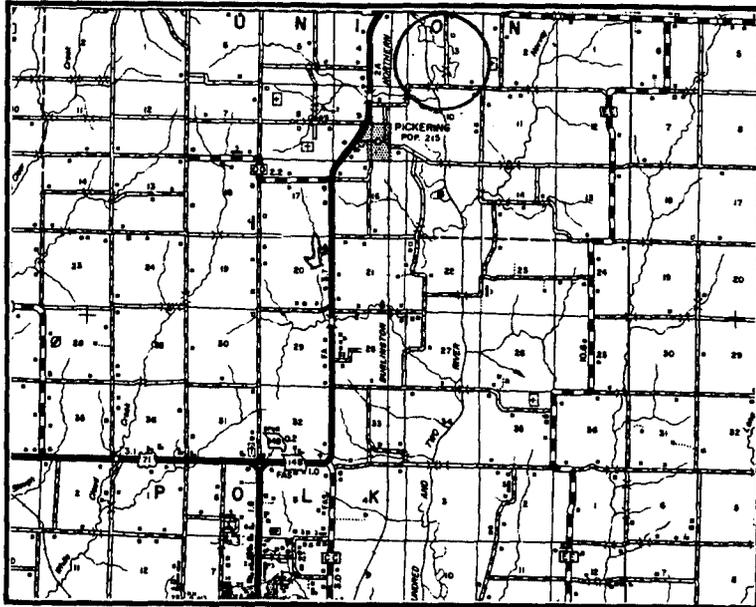
This bridge carries County Road 251 across the 102 River in Section 1 of Union Township, about a mile northeast of Pickering. Records in the Nodaway County Engineer's Office reveal that the structure was erected in 1909 as a six-panel, pin-connected Pratt through truss. County courthouse records, however, fail to provide germane documentation pertaining to the bridge's construction. Retaining an excellent degree of structural integrity, the 102 River Bridge features an overall length of 235 feet and a timber deck over steel stringers. During the crossing's service in central Nodaway County, this tall steel truss has carried regional traffic in essentially unaltered condition.

The 102 River Bridge is one of thousands of medium-span, pinned Pratt trusses erected for Missouri's county roads in the late 1800s and early 1900s. With standard detailing, it lacks technological significance.

NAME(S) OF STRUCTURE

102 River Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES
Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 251R01.0; field inspection by Mark Hufstetler, 9 September 1990.

INVENTORIED BY
Michelle Crow-Dolby

AFFILIATION
Fraserdesign, Loveland CO

DATE
11 August 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

102 River Bridge
MHTD: 254000.8

NODA28

DATE(S) OF CONSTRUCTION

c1915

LOCATION

County Road 254 over 102 River; S10, T65N, R35W
7.2 miles northeast of Maryville; Nodaway County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 32)

CONDITION

fair

OWNER

Nodaway County

span number: 1

span length: 150.0'

total length: 150.0'

roadway wdt.: 15.7'

superstructure: steel, 8-panel, pin-connected Pratt through truss, with steel stringer approach span

substructure: steel pile piers and abutments with timber back- and wingwalls

floor/decking: timber deck over steel stringers

other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerods with turnbuckles; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, field-bolted to verticals; guardrail: 2 angles

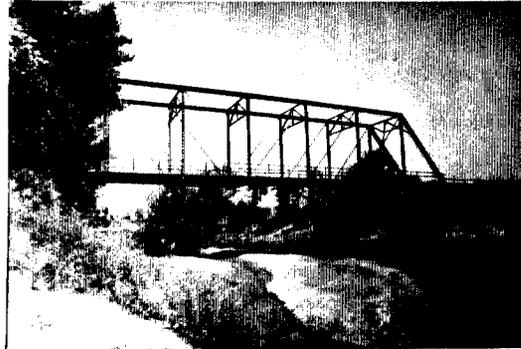
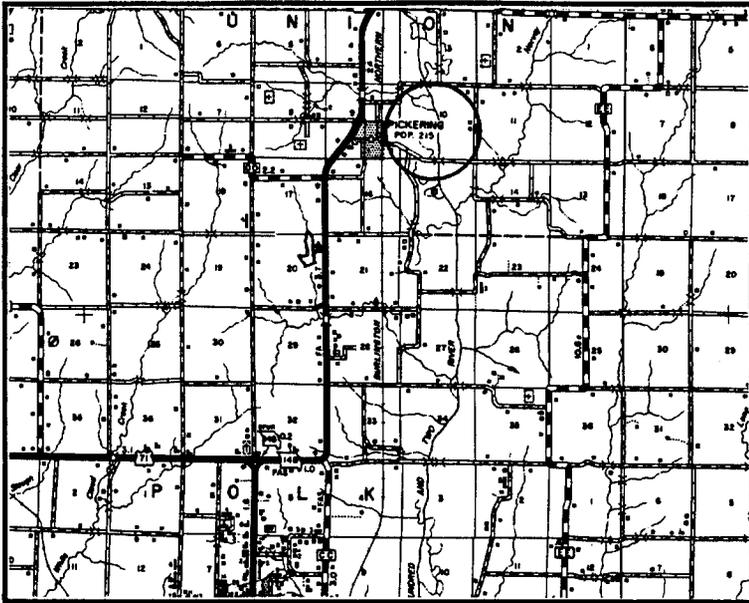
This bridge carries County Road 254 across the 102 River in Section 10 of Union Township, less than a mile east of Pickering. Records in the Nodaway County Engineer's Office reveal that the structure was erected circa 1915 as an eight-panel, pin-connected Pratt through truss. County courthouse records, however, fail to provide germane documentation pertaining to the bridge's construction. Retaining an excellent degree of structural integrity, the 102 River Bridge, approached by a steel stringer on the east end, features an overall length of 150 feet. During the 102 River Bridge's nearly eighty years of service in central Nodaway County, this steel truss has carried regional traffic in essentially unaltered condition.

In Missouri, the pinned Pratt truss was the bridge of choice for short- and medium-span applications in the early 1900s. As a result, thousands of Pratts were built across the state, and today Pratts constitute the most populous group of through trusses. Though it retains a relatively high degree of physical integrity, the 102 River Bridge is unremarkable in its design, dimensions and detailing.

NAME(S) OF STRUCTURE

102 River Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 254000.8; field inspection by Mark Hufstetler, 9 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

11 August 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Platte River Bridge
MHTD: 426000.8

NODA46

DATE(S) OF CONSTRUCTION

1894

LOCATION

County Road 426 over tributary of Platte River; S19, T65N, R33W
1.0 mile southwest of Parnell; Nodaway County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 50)

CONDITION

good

OWNER

Nodaway County

span number: 1
span length: 80.0'
total length: 156.0'
roadway wdt.: 13.5'

superstructure: steel or wrought iron, 5-panel, pin-connected Pratt through truss; steel stringer approaches
substructure: concrete-filled steel cylinder piers, timber pile abutments with timber back- and wingwalls
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: square eyebars with turnbuckles; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beam, U-bolted to verticals; guardrail: steel, 2 angles; portal builder's plate: **THE MASSILLON / BRIDGE COMPANY / MASSILLON / 1894 OHIO**

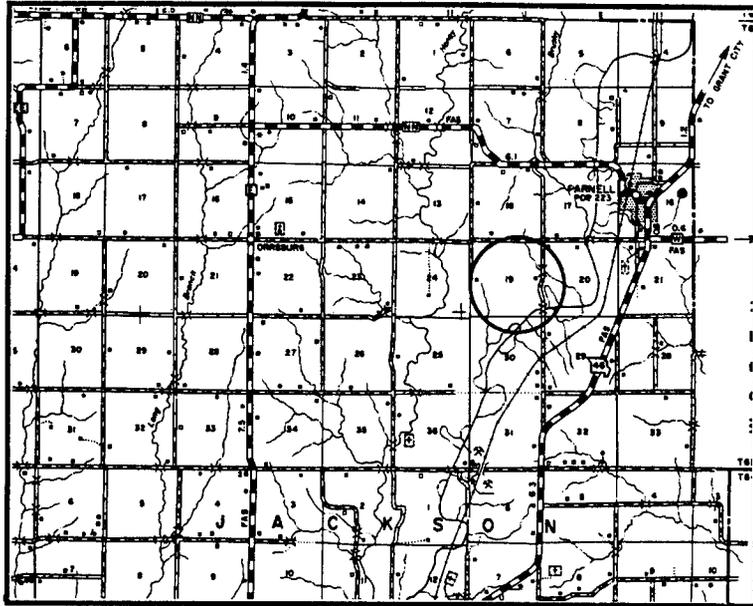
Although Nodaway County Courthouse records do not refer to the construction of this bridge during the 1890s, the structure's bridge plate reveals that Massillon Bridge Company erected the 80-foot truss in 1894. Either the Ohio-based contractor built the five-panel Pratt through truss outside the county and it was moved to this location at a later date, or county records simply omitted the 1894 transaction. Fourteen years later in 1908, county records report that the approach span at this location was rebuilt by Standard Bridge Company for \$90.00. Perhaps Standard was repairing the original Massillon structure. Furthermore, a 1990 field inspection indicates that the eastern Nodaway County bridge, spanning the Platte River southwest of Parnell, exhibits a high degree of locational and structural integrity. Nevertheless, the Platte River Bridge, since its construction, continues to carry local traffic in its rural setting.

From the early 1880s through the 1920s, the pinned Pratt truss was virtually the exclusive structural type for medium-span roadway crossings in Missouri. Its standardized fabrication, economy of materials and ease of erection made it a mainstay among the various state and regional bridge companies, as well as the state highway department itself in later years. Thousands of such trusses were built throughout the state during this period, and numerous examples remain in place today. The Platte River Tributary Bridge is distinguished among these by its early erection date and well-preserved condition. An example of this mainstay structural type, the Platte River Tributary Bridge, one of the earliest bridges in Nodaway County, is a significant transportation-related resource.

NAME(S) OF STRUCTURE

Platte River Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 426000.8; Nodaway County Record, Book 17: page 128 (7 May 1908) - located at the Nodaway County Courthouse, Maryville MO; field inspection by Mark Hufstetler, 8 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

11 August 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Platte River Bridge
MHTD: 456R01.1

NODA48

DATE(S) OF CONSTRUCTION

c1910

LOCATION

County Road 456 over Platte River; S12, T64N, R34W
0.6 mile northwest of Ravenwood; Nodaway County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 38)

CONDITION

fair

OWNER

Nodaway County

span number: 1
span length: 150.0'
total length: 226.0'
roadway wdt.: 15.7'

superstructure: steel, 8-panel, pin-connected Parker through truss; steel stringer approaches
substructure: steel pile bent piers, steel back- and wingwalls and steel abutments
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord:
2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched
rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod
with threaded ends; strut: 2 angles; floor beam: I-beam, U-bolted to verticals; guardrail:
steel, 2 angles

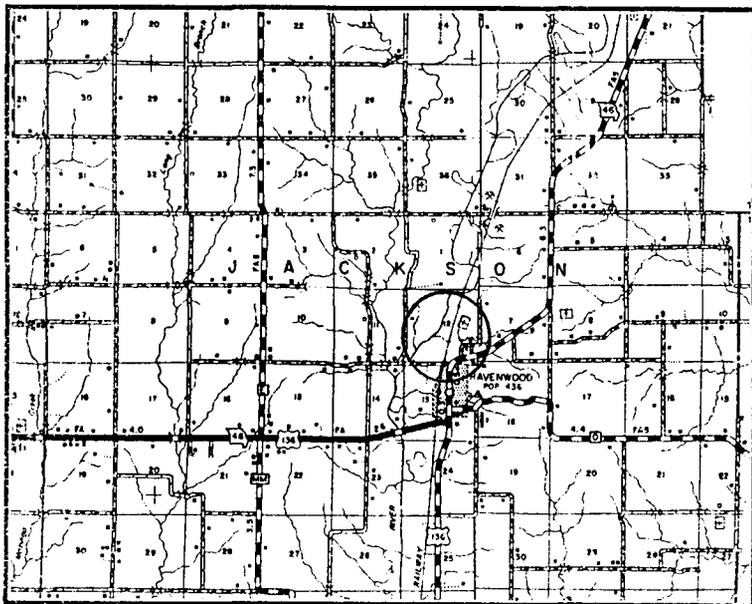
This Parker through truss spans the Platte River in Jackson Township, northwest of Ravenwood. An earlier truss at this location predates this structure by approximately twenty years. J.H. Sparks, acting as an agent for the Pittsburgh Bridge Company, was paid \$970.00 by Nodaway County in November of 1889 to erect a permanent crossing over the Platte River. One month later county officials examined and approved Sparks' bridge, issuing a warrant for the remainder due to the local contractor. Apparently experiencing structural failure sometime in the 1910s, the present Parker through truss, approached on either end by steel stringers, was built to take its place. Although modified in 1984, the Platte River Bridge continues to carry local traffic in east-central Nodaway County with no additional alterations.

The Platte River Bridge is technologically significant as a well-preserved example of a relatively uncommon structural type. Pin-connected Pratt trusses were built by the thousands throughout Missouri in the late 19th and early 20th centuries. Pinned Parker trusses, a polygonal-chorded Pratt variant, were typically used for crossings requiring long spans, where a savings in materials could be effected by angling the upper chords. Pinned Parker trusses such as the Platte River Bridge were built far less often than Pratts; fewer than three dozen remain in place today on Missouri's road system. Among these, the Platte River Bridge is an excellent example: an early, long-span Parker that has retained a high degree of physical integrity. A graceful structure that spans high above the Platte River, it is a well-known local landmark.

NAME(S) OF STRUCTURE

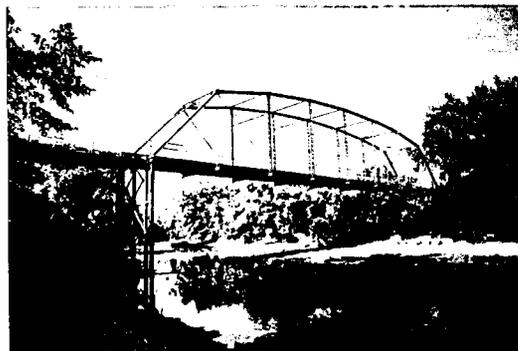
Platte River Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 456R01.1; Bridge Commissioner's Reports on Sale of Bridges, 14 November 1889, 5 December 1889 - located at the Nodaway County Courthouse, Maryville MO; field inspection by Mark Hufstetler, 8 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

11 August 1993

WORTH COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
WORT01	060R01.5	X Branch Bridge	1- 50' pinned Pratt pony truss c1910 Midland Bridge Co. (prob.)
*WORT02	103001.0	Marlowe Creek Bridge	1- 90' pinned Pratt through truss c1885 CB&Q Railroad bridge crew
WORT03	116000.3	Middle Fork Bridge	2- 80' pinned Pratt pony truss c1915
WORT04	140R00.3	East Fork Bridge	1- 64' pinned Pratt pony truss 1908 Midland Bridge Company (replaced)
WORT05	171000.8	Little Rock Creek Bridge	
WORT06	174002.1	Savacool Bridge	1- 44' pinned Pratt pony truss 1905 M.D. Haas
*WORT07	251001.0	Lotts Creek Bridge	1- 80' riveted polyg. Warren pony truss c1920
WORT08	254000.3	Sand Creek Bridge	1- 40' pinned Pratt pony truss c1910 Midland Bridge Co. (prob.) (replaced)
WORT09	258R00.1	Lotts Creek Bridge	
WORT10	266R00.3	Grand River Bridge	1- 60' pinned Pratt pony truss c1910 Midland Bridge Co. (prob.)
WORT11	281001.9	Middle Fork Bridge	1-100' riveted Warren pony truss 1932 Graham Brothers

EXCLUDED:

Pratt pony truss
037000.0

Warren pony truss

G 795 H 617 J 840 X 111 112000.2 119000.1 261001.2

Steel stringer

S 50 T 898 X 112 X 141 X 142 X 970 001000.2
 002000.8 011000.3 022000.4 023000.1 035000.2 036000.8 053000.3
 059000.4 059001.0 059001.6 062R00.7 062000.1 071000.8 076R00.4
 076R01.3 081000.6 083000.0 091000.4 098R00.2 106000.7 111001.7
 125001.1 129000.3 133001.7 140000.2 144001.1 150001.3 151001.1
 155000.1 164000.9 170000.8 180000.9 200000.2 203000.2 207000.9
 209000.1 209000.7 216000.9 228000.1 246000.8 257R01.0 260000.2
 263000.8

Concrete girder

J 24

Concrete box culvert

H 649 H 650 H 883

WORTH COUNTY

SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	0	9	0	0	9
Excluded	14	48	0	0	62
<hr/>					
	14	57	0	0	71 structures

X Branch Bridge

WORT01

GENERAL DATA

structure no.: 060R01.5	city/town: 8.9 miles southwest of Grant City
county: Worth	feature inters.: X Branch
	cadastral grid: S11/14, T65N, R13W
	highway route: County Road 60
	highway distr.: 1
	current owner: Worth County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss	
substructure: timber abutments and wingwalls	
span number: 1	condition: fair
span length: 50.0'	alterations: unknown
total length: 63.0'	floor/decking : timber deck
roadway width: 13.9'	other features: unknown

HISTORICAL DATA

erection date: c1910
erection cost: Midland Bridge Company, Kansas City MO (probable)
designer: Midland Bridge Company, Kansas City MO (probable)
fabricator : Midland Bridge Company, Kansas City MO (probable)
contractor: unknown
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 060R01.5.
sign. rating: 25
evaluation: NRHP non-eligible (typically configured, poorly documented example of common structural type)

inventoried by: Mark Hufstetler 31 July 1992

Marlowe Creek Bridge

WORT02

GENERAL DATA

structure no.:	103001.0	city/town:	4.0 miles southwest of Grant City
county:	Worth	feature inters.:	Marlowe Creek
		cadastral grid:	S13, T65N, R32W
		highway route:	County Road 103
		highway distr.:	1
		current owner:	Worth County

STRUCTURAL DATA

superstructure:	wrought iron or steel, 4-panel, pin-connected Pratt through truss, with timber stringer approach spans		
substructure:	timber pile bent abutments, wingwalls and piers		
span number:	1	condition:	good
span length:	90.0'	alterations:	converted from railroad to roadway use
total length:	228.0'	floor/decking:	timber deck over steel stringers
roadway width:	10.0'	other features:	upper chord / inclined end post: 2 face-to-face channels with cover plate and double lacing; lower chord: 2 face-to-face channels with lacing; vertical: 2 face-to-face channels with lacing (2 looped square eyebars at the hip); diagonal: 2 punched rectangular eyebars; counter: square eyobar with turnbuckle; lateral bracing: square rod with threaded ends; floor beam: riveted plate girder, riveted to vertical

HISTORICAL DATA

erection date:	c1885
erection cost:	unknown
designer:	unknown
fabricator:	unknown
contractor:	Chicago, Burlington and Quincy Railroad bridge crew (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 103001.0; Missouri Highway and Transportation Department, "Missouri Rail Plan, 1984 Update," page 4-1; field inspection by Mark Hufstetler, 10 September 1990.
sign. rating:	43
evaluation:	NRHP possibly eligible (well-preserved, early railroad truss, converted in place with the adjacent right-of-way to roadway use)

Inventoried by: Mark Hufstetler 31 July 1992

Middle Fork Bridge

WORT03

GENERAL DATA

structure no.:	116000.3	city/town:	5.9 miles southwest of Grant City
county:	Worth	feature inters.:	Middle Fork of Grand River
		cadastral grid:	S30/31, T65N, R31W
		highway route:	County Road 116
		highway distr.:	1
		current owner:	Worth County

STRUCTURAL DATA

superstructure:	steel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	steel pile bent abutments and piers		
span number:	2	condition:	fair
span length:	80.0'	alterations:	two dissimilar trusses, moved to this site
total length:	162.0'	floor/decking :	timber deck
roadway width:	14.0'	other features:	unknown

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 116000.3.

sign. rating:	21
evaluation:	NRHP non-eligible (two dissimilar trusses, moved to this site)

inventoried by: Mark Hufstetler 31 July 1992

East Fork Bridge

WORT04

GENERAL DATA

structure no.: 140R00.3	city/town: 4.8 miles southeast of Grant City
county: Worth	feature inters.: East Fork of the Grand River
	cadastral grid: S12/13, T65N, R31W
	highway route: County Road 140
	highway distr.: 1
	current owner: Worth County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss	
substructure: timber back- and wingwalls, steel pile bent piers	
span number: 1	condition: fair
span length: 64.0'	alterations: unknown
total length: 114.0'	floor/decking : timber deck
roadway width: 12.1'	other features: unknown

HISTORICAL DATA

erection date: 1908	
erection cost: \$935.00	
designer: Midland Bridge Company, Kansas City MO	
fabricator : Midland Bridge Company, Kansas City MO	
contractor: Midland Bridge Company, Kansas City MO	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 140R00.3; Worth County Bridge Commissioner's Report, 4 February 1909, located at Worth County Courthouse, Grant City MO.	
sign. rating: 44	
evaluation: NRHP non-eligible (typically configured example of common structural type)	

Inventoried by: Mark Hufstetler 31 July 1992

Savacool Bridge

WORT06

GENERAL DATA

structure no.:	174002.1	city/town:	9.2 miles southeast of Grant City
county:	Worth	feature inters.:	Little Rock Creek
		cadastral grid:	S14/15, T65N, R30W
		highway route:	County Road 174
		highway distr.:	1
		current owner:	Worth County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	timber abutments and wingwalls		
span number:	1	condition:	fair
span length:	44.0'	alterations:	unknown
total length:	45.0'	floor/decking :	timber deck
roadway width:	13.0'	other features:	unknown

HISTORICAL DATA

erection date:	1905
erection cost:	\$300.00
designer:	unknown
fabricator :	unknown
contractor:	M.D. Haas

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 174002.1; Worth County Court Record Book I: 20 March 1905, located at the Worth County Courthouse, Grant City MO.

sign. rating:	37
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Mark Hufstetler 31 July 1992

Lotts Creek Bridge

WORT07

GENERAL DATA

structure no.: 251001.0	city/town: 6.2 miles northeast of Grant City
county: Worth	feature inters.: Lotts Creek
	cadastral grid: S28, T66N, R30W
	highway route: County Road 251
	highway distr.: 1
	current owner: Worth County

STRUCTURAL DATA

superstructure: steel, 5-panel, rigid-connected Warren pony truss, with polygonal upper chords; timber and steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber backwalls	
span number: 1	condition: good
span length: 80.0'	alterations: none
total length: 124.0'	floor/decking : timber deck over timber stringers
roadway width: 14.0'	other features: upper chord / inclined end post: 2 channels with cover and batten plates; lower chord: 2 angles; vertical: 4 angles with batten plates; diagonal: 2 angles with batten plates; floor beam: I-beam, field bolted to vertical

HISTORICAL DATA

erection date: c1920	
erection cost: unknown	
designer: unknown	
fabricator : Cambria Steel Company, Pittsburgh PA	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 251001.0; Field Inspected by Mark Hufstetler, 10 September 1990.	
sign. rating: 45	
evaluation: NRHP non-eligible (salvaged railroad truss with unusual Warren configuration)	

inventoried by: Mark Hufstetler 31 July 1992

Sand Creek Bridge

WORT08

GENERAL DATA

structure no.:	254000.3	city/town:	10.0 miles northeast of Grant City
county:	Worth	feature inters.:	Sand Creek
		cadastral grid:	S25, T66N, R29/30W
		highway route:	County Road 254
		highway distr.:	1
		current owner:	Worth County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	52.0'	floor/decking :	timber deck
roadway width:	12.4'	other features:	unknown

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Midland Bridge Company, Kansas City MO (probable)
fabricator :	Midland Bridge Company, Kansas City MO (probable)
contractor:	Midland Bridge Company, Kansas City MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 254000.3.
sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Mark Hufstetler 31 July 1992

Grand River Bridge

WORT10

GENERAL DATA

structure no.: 266R00.3	city/town: Worth	8.4 miles northeast of Grant City
county:		feature inters.: Grand River
		cadastral grid: S3, T66N, R30W
		highway route: County Road 266
		highway distr.: 1
		current owner: Worth County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss	
substructure: unknown	
span number: 1	condition: fair
span length: 60.0'	alterations: unknown
total length: 79.0'	floor/decking : timber deck
roadway width: 12.2'	other features: unknown

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: Midland Bridge Company, Kansas City MO (prob.)
fabricator : Midland Bridge Company, Kansas City MO (prob.)
contractor: Midland Bridge Company, Kansas City MO (prob.)
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 266R00.3.
sign. rating: 28
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Mark Hufstetler 31 July 1992

Middle Fork Bridge

WORT11

GENERAL DATA

structure no.:	281001.9	city/town:	2.2 miles south of Grant City
county:	Worth	feature inters.:	Middle Fork of the Grand River
		cadastral grid:	S8, T65N, R31W
		highway route:	County Road 281
		highway distr.:	1
		current owner:	Worth County

STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Warren pony truss		
substructure:	concrete abutments and piers		
span number:	1	condition:	good
span length:	100.0'	alterations:	none
total length:	292.0'	floor/decking :	concrete deck over steel stringers
roadway width:	21.9'	other features:	unknown

HISTORICAL DATA

erection date:	1932
erection cost:	\$22,776.87
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	Graham Brothers
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 281001.9; Missouri Highway and Transportation Department, Primary Bridge Record, located at the Bridge Division, MHTD, Jefferson City MO.
sign. rating:	43
evaluation:	NRHP non-eligible (typical MSHD highway pony truss)

inventoried by: Mark Hufstetler 31 July 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Marlowe Creek Bridge
MHTD: 103001.0

WORT02

DATE(S) OF CONSTRUCTION

c1885

LOCATION

County Road 103 over Marlowe Creek; S13, T65N, R32W
4.0 miles southwest of Grant City; Worth County, Missouri

USE (ORIGINAL / CURRENT)

railroad bridge / roadway bridge

RATING NRHP potentially eligible (score: 43)

CONDITION

good

OWNER

Worth County

span number: 1
span length: 90.0'
total length: 228.0'
roadway wdt.: 10.0'

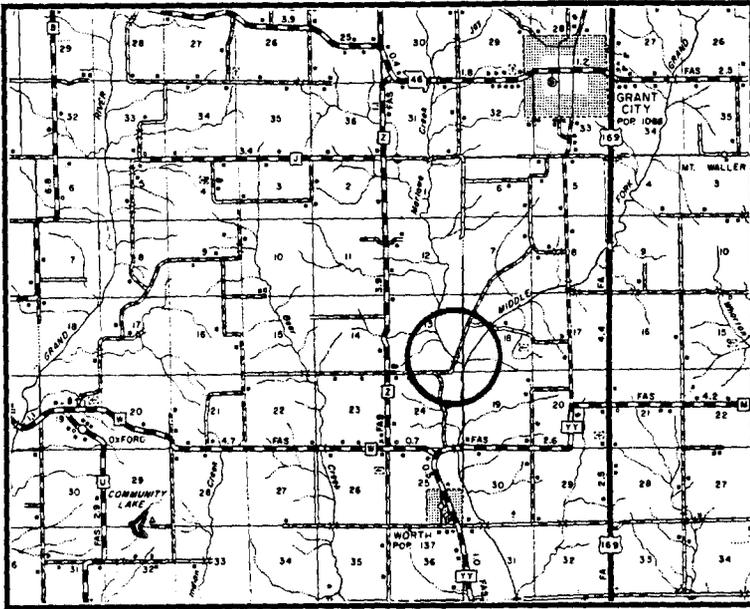
superstructure: wrought iron or steel, 4-panel, pin-connected Pratt through truss, with timber stringer approach spans
substructure: timber pile bent abutments, wingwalls and piers
floor/decking: timber deck over steel stringers
other features: upper chord / inclined end post: 2 face-to-face channels with cover plate and double lacing; lower chord: 2 face-to-face channels with lacing; vertical: 2 face-to-face channels with lacing (2 looped square eyebars at the hip); diagonal: 2 punched rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: square rod with threaded ends; floor beam: riveted plate girder, riveted to vertical

Located on the grade of the former Chicago, Burlington and Quincy Railroad - Grant City Branch, this short-span, pinned through truss was built by the railroad to span Marlowe Creek. The truss' configuration and detailing indicate an approximate fabrication date of 1885. The Albany Junction / Grant City line, some 20 miles long, was abandoned by the Burlington Railroad in October 1981. This bridge was subsequently converted from railroad to roadway use.

All of the railroads that once served Nodaway and Worth counties have since been abandoned. The Marlowe Creek Bridge appears to be the only bridge that was not dismantled during the abandonment process. It is thus historically significant as one of the few remaining resources associated with early railroad transportation in the region. With its heavily latticed portals, the truss is a distinctive representative of the mainstay railroad - and vehicular - truss type of the late 19th century - the pinned Pratt through truss.

NAME(S) OF STRUCTURE
Marlowe Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES
Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 103001.0; Missouri Highway and Transportation Department, "Missouri Rail Plan, 1984 Update," page 4-1; field inspection by Mark Hufstetler, 10 September 1990.

INVENTORIED BY
Mark Hufstetler

AFFILIATION
Renewable Technologies, Inc., Butte MT

DATE
31 July 1992
