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# ADAIR COUNTY

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**INCLUDED:** [Significant feature(s) of bridge given in boldface]  
 [Field inventoried bridge indicated by asterisk]

Inv.No.	MHTD	Bridge Name	Description
ADAI01	K 22	Chariton River Bridge	13-30' steel stringer 1933 M.E. Gillioz
ADAI02	K 23	Chariton River Bridge	1-100' <b>riveted Pratt through truss</b> 1933 M.E. Gillioz
ADAI03	K 304	Mussel Fork Bridge	1-100' riveted Warren pony truss 1933 M.E. Gillioz
ADAI04	K 305	Mussel Fork Bridge	1-100' riveted Warren pony truss 1933 M.E. Gillioz
ADAI05	004000.8	Danforth Bridge	1- 68' pinned Pratt pony truss 1905 Fair Williams Br. & Mfg. Co.
ADAI06	019002.6	Blackbird Creek Bridge	(replaced)
ADAI07	019003.0	Chariton River Bridge	1-100' riveted Pratt pony truss c1930
ADAI08	036000.8	Wayman Bridge	1- 30' concrete deck girder 1915 Carter and Hughes
*ADAI09	042000.6	Salt River Bridge	1- 35' pinned Pratt bedstead c1910
ADAI10	091004.1	Fabius River Bridge	(replaced)
*ADAI11	159000.1	Capps Bridge	1- 70' pinned Pratt pony truss 1900 Fair Williams B&M Company
*ADAI12	176000.8	Billy Creek Bridge	1- 68' pinned Pratt pony truss c1910
*ADAI13	223000.1	Sloan Point Bridge	1- 70' <b>pinned Pratt through truss</b> 1899 Theodore Moyer
ADAI14	269003.6	Salt River Bridge	1-100' riveted Pratt pony truss c1925
ADAI15	280000.3	Bridge	1- 24' concrete deck girder c1915 Carter and Hughes (prob.)
ADAI16	360001.6	Eitel Bridge	1- 30' pin Pratt half-hip pony truss 1903 Fair Williams B&M Company
ADAI17	410001.9	Bridge	1- 30' pin Pratt half-hip pony truss c1910
ADAI18	423000.6	Taylor Bridge	(replaced)
ADAI19	424000.8	Darr Bridge	(replaced)
ADAI20	430001.1	Long Bridge	(destroyed)
ADAI21	440000.9	Indian Creek Bridge	(replaced)

**EXCLUDED:**

Pratt pony truss  
 110000.7 162000.9 167002.6 174002.5 211000.5 283000.4 301R01.7  
 315OR0.1 334001.1

# ADAIR COUNTY

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## EXCLUDED (cont.):

Lattice pony truss  
366001.6

Warren pony truss  
J 547 284001.0

### Steel stringer

J 548	K 298	K 299	K 300	K 301	K 302	K 303
P 92	S 53	S 393	S 568	S 569	S 620	S 807
T 883	T885	X 120	X 160	X 357	035002.0	068001.7
092001.0	121R03.0	138003.3	254000.4	264003.6	277001.1	281000.8
291000.5	3150R0.1	320000.2	365R02.6			

Steel girder  
316000.4 322001.1

Concrete girder  
H 688 006002.7

Concrete box culvert

H 221R1	H 222R	H 395	H 508	J 377	K 494	T 884
T 889	X 896	X 897	X 944			

### Timber stringer

009000.4	032000.1	066001.2	071R01.5	073000.1	075000.8	078003.4
092000.5	094000.2	127000.3	131000.3	181R03.3	182000.2	203R00.5
237001.4	238000.9	239001.8	293000.5	339000.7	340001.1	345R00.8
352001.0	358000.1	462000.5				

## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	4	10	0	0	14
Excluded	32	61	0	0	93
	<hr/>				
	36	71	0	0	107 structures

# Chariton River Bridge

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ADAI01

## GENERAL DATA

<b>structure no.:</b> K 22	<b>city/town:</b> 2.2 miles north of Yarrow
<b>county:</b> Adair	<b>feature inters.:</b> Chariton River
	<b>cadastral grid:</b> S33/34, T62N, R16W
	<b>highway route:</b> State Highway 11
	<b>highway distr.:</b> 2
	<b>current owner:</b> Missouri Highway and Transportation Department

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> concrete abutments and wingwalls; timber pile bent piers with concrete caps	
<b>span number:</b> 13	<b>condition:</b> good
<b>span length:</b> 30.0'	<b>alterations:</b> none
<b>total length:</b> 423.0'	<b>floor/decking :</b> concrete deck
<b>roadway width:</b> 22.0'	<b>other features:</b> steel guardrails

## HISTORICAL DATA

<b>erection date:</b> 1933	
<b>erection cost:</b> \$14,505.20	
<b>designer:</b> Missouri State Highway Department	
<b>fabricator :</b> unknown	
<b>contractor:</b> M.E. Gillioz, Monett MO	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number K 22; Missouri Highway and Transportation Department, Primary System Bridge Record, Book 1 (see entries for Adair County), on file at MHTD, Jefferson City MO.	
<b>sign. rating:</b> 42	
<b>evaluation:</b> NRHP non-eligible (distinguished somewhat by its multiple spans, but otherwise an undistinguished, late example of an exceedingly common structural type)	

**inventoried by:** Clayton Fraser and Ron Ashbach-Sladek    22 January 1992

# Chariton River Bridge

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ADAI02

## GENERAL DATA

<b>structure no.:</b> K 23	<b>city/town:</b> 2.2 miles north of Yarrow
<b>county:</b> Adair	<b>feature inters.:</b> Chariton River
	<b>cadastral grid:</b> S4, T61N, R16W
	<b>highway route:</b> State Highway 11
	<b>highway distr.:</b> 2
	<b>current owner:</b> Missouri Highway and Transportation Department

## STRUCTURAL DATA

<b>superstructure:</b> steel, 6-panel, rigid-connected Pratt through truss with steel stringer approach spans	
<b>substructure:</b> concrete abutments, wingwalls and piers	
<b>span number:</b> 1	<b>condition:</b> good
<b>span length:</b> 100.0'	<b>alterations:</b> none
<b>total length:</b> 324.0'	<b>floor/decking :</b> concrete deck over steel stringers
<b>roadway width:</b> 22.0'	<b>other features:</b> steel guardrails

## HISTORICAL DATA

<b>erection date:</b> 1933	
<b>erection cost:</b> \$22,095.36	
<b>designer:</b> Missouri State Highway Department	
<b>fabricator :</b> unknown	
<b>contractor :</b> M.E. Gillioz, Monett MO	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number K 23; Missouri Highway and Transportation Department, Primary System Bridge Record, Book 1 (see entries for Adair County), on file at MHTD, Jefferson City MO.	
<b>sign. rating:</b> 38	
<b>evaluation:</b> NRHP non-eligible (typically configured example of MSHD truss design in the 1930s)	

**inventoried by:** Clayton Fraser and Ron Ashbach-Sladek    22 January 1992

# Mussel Fork River Bridge

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ADAI03

## GENERAL DATA

<b>structure no.:</b> K 304	<b>city/town:</b> 4.0 miles west of Nind
<b>county:</b> Adair	<b>feature inters.:</b> Mussel Fork River
	<b>cadastral grid:</b> S29, T61N, R17W
	<b>highway route:</b> State Highway 11
	<b>highway distr.:</b> 2
	<b>current owner:</b> Missouri Highway and Transportation Department

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, rigid-connected Warren pony truss, with steel stringer approach spans	
<b>substructure:</b> concrete abutments, wingwalls and piers	
<b>span number:</b> 1	<b>condition:</b> good
<b>span length:</b> 100.0'	<b>alterations:</b> none
<b>total length:</b> 197.0'	<b>floor/decking :</b> concrete deck over steel stringers
<b>roadway width:</b> 22.0'	<b>other features:</b> steel guardrails

## HISTORICAL DATA

<b>erection date:</b> 1933	
<b>erection cost:</b> \$10,965.08	
<b>designer:</b> Missouri State Highway Department	
<b>fabricator :</b> unknown	
<b>contractor:</b> M.E. Gillioz, Monett MO	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number K 304; Missouri Highway and Transportation Department, Primary System Bridge Record, Book 1 (see entries for Adair County), on file at MHTD, Jefferson City MO.	
<b>sign. rating:</b> 43	
<b>evaluation:</b> NRHP non-eligible (typically configured, long-span example of MSHD truss design in the 1930s)	

**inventoried by:** Clayton Fraser and Ron Ashbach-Sladek    22 January 1992

# Mussel Fork River Bridge

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ADAI04

## GENERAL DATA

<b>structure no.:</b> K 305	<b>city/town:</b> 4.7 miles west of Nind
<b>county:</b> Adair	<b>feature inters.:</b> Mussel Fork River
	<b>cadastral grid:</b> S31, T61N, R17W
	<b>highway route:</b> State Highway 11
	<b>highway distr.:</b> 2
	<b>current owner:</b> Missouri Highway and Transportation Department

## STRUCTURAL DATA

**superstructure:** steel, 5-panel, rigid-connected Warren pony truss, with steel stringer approach spans

**substructure:** concrete abutments, wingwalls and piers

<b>span number:</b> 1	<b>condition:</b> good
<b>span length:</b> 100.0'	<b>alterations:</b> none
<b>total length:</b> 297.0'	<b>floor/decking :</b> concrete deck over steel stringers
<b>roadway width:</b> 22.0'	<b>other features:</b> steel guardrails

## HISTORICAL DATA

**erection date:** 1933

**erection cost:** \$14,348.18

**designer:** Missouri State Highway Department

**fabricator :** unknown

**contractor:** M.E. Gillioz, Monett MO

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number K 305; Missouri Highway and Transportation Department, Primary System Bridge Record, Book 1 (see entries for Adair County), on file at MHTD, Jefferson City MO.

**sign. rating:** 43

**evaluation:** NRHP non-eligible (typically configured, long-span example of MSHD truss design in the 1930s)

**inventoried by:** Clayton Fraser and Ron Ashbach-Sladek    22 January 1992

# Danforth Bridge

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ADAI05

## GENERAL DATA

structure no.:	004000.8	city/town:	8.9 miles northwest of Novinger
county:	Adair	feature inters.:	Spring Creek
		cadastral grid:	S31, T64N, R17W
		highway route:	County Road 4
		highway distr.:	2
		current owner:	Adair County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled steel cylinder piers; timber pile bent piers and abutments		
span number:	1	condition:	fair
span length:	68.0'	alterations:	unknown
total length:	108.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1905
erection cost:	\$1760.00 (multiple-bridge contract)
designer:	Fair Williams Bridge and Manufacturing Company, Ottumwa IA
fabricator :	Fair Williams Bridge and Manufacturing Company, Ottumwa IA
contractor:	Fair Williams Bridge and Manufacturing Company, Ottumwa IA
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 004000.8; Adair County Court Record, Book S: page 540 (9 February 1905), page 562 (8 March 1905), located at Adair County Courthouse, Kirksville MO.
sign. rating:	42
evaluation:	NRHP non-eligible (typical example of common truss type)

inventoried by: Clayton Fraser and Ron Ashbach-Sladek    22 January 1992

# Chariton River Bridge

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ADAI07

## GENERAL DATA

<b>structure no.:</b> 019003.0	<b>city/town:</b> 5.8 miles north of Novinger
<b>county:</b> Adair	<b>feature inters.:</b> Chariton River
	<b>cadastral grid:</b> S27/34, T64N, R16W
	<b>highway route:</b> County Road 19
	<b>highway distr.:</b> 2
	<b>current owner:</b> Adair County

## STRUCTURAL DATA

**superstructure:** steel, 5-panel, rigid-connected Pratt pony truss  
**substructure:** steel pile bent piers with timber back- and wingwalls

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 100.0'	<b>alterations:</b> unknown
<b>total length:</b> 129.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 12.1'	<b>other features:</b> steel angle guardrails

## HISTORICAL DATA

**erection date:** c1930  
**erection cost:** unknown  
**designer:** unknown  
**fabricator :** unknown  
**contractor:** unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 019003.0.

**sign. rating:** 28  
**evaluation:** NRHP non-eligible (typically configured, inadequately documented example of common structural type)

**inventoried by:** Clayton Fraser and Ron Ashbach-Sladek 22 January 1992

# Wayman Bridge

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ADAI08

## GENERAL DATA

<b>structure no.:</b> 036000.8	<b>city/town:</b> 9.4 miles north of Kirksville
<b>county:</b> Adair	<b>feature inters.:</b> unnamed stream
	<b>cadastral grid:</b> S20, T64N, R15W
	<b>highway route:</b> County Road 36
	<b>highway distr.:</b> 2
	<b>current owner:</b> Adair County

## STRUCTURAL DATA

<b>superstructure:</b> concrete deck girder	
<b>substructure:</b> concrete abutments and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 30.0'	<b>alterations:</b> none
<b>total length:</b> 30.0'	<b>floor/decking :</b> concrete deck
<b>roadway width:</b> 15.7'	<b>other features:</b> concrete guardrails with circular cutouts

## HISTORICAL DATA

<b>erection date:</b> 1915	
<b>erection cost:</b> \$740.00	
<b>designer:</b> unknown	
<b>fabricator :</b> none	
<b>contractor:</b> Carter and Hughes, Kirksville MO	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 036000.8; Adair County Court Record, Book U: page 29 (9 June 1915), located at Adair County Courthouse, Kirksville MO.
<b>sign. rating:</b> 45	
<b>evaluation:</b>	NRHP possibly eligible (technologically modest example of early concrete bridge type)

**Inventoried by:** Clayton Fraser and Ron Ashbach-Sladek    22 January 1992

# Salt River Bridge

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ADAI09

## GENERAL DATA

<b>structure no.:</b> 042000.6	<b>city/town:</b> 9.8 miles north of Kirksville
<b>county:</b> Adair	<b>feature inters.:</b> Salt River
	<b>cadastral grid:</b> S23, T64N, R15W
	<b>highway route:</b> County Road 42
	<b>highway distr.:</b> 2
	<b>current owner:</b> Adair County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt truss-leg bedstead with timber stringer approach span	
<b>substructure:</b> steel bedstead legs with timber back- and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 35.0'	<b>alterations:</b> none
<b>total length:</b> 46.0'	<b>floor/decking :</b> timber deck over timber stringers
<b>roadway width:</b> 11.8'	<b>other features:</b> upper chord: 2 channels with lacing; upright end post: 2 channels with lacing; lower chord: 2 looped rectangular eyebars, or 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: looped square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical

## HISTORICAL DATA

<b>erection date:</b> c1910	
<b>erection cost:</b> unknown	
<b>designer:</b> unknown	
<b>fabricator :</b> Jones and Laughlin Steel Company, Pittsburgh PA	
<b>contractor:</b> unknown	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 042000.6; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 5 October 1991.	
<b>sign. rating:</b> 27	
<b>evaluation:</b> NRHP non-eligible (undocumented example of common structural type)	

**inventoried by:** Clayton Fraser and Ron Ashbach-Sladek    22 January 1992

# Capps Bridge

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ADAI1

## GENERAL DATA

structure no.: 159000.1      city/town: 1.4 miles west of Novinger  
county: Adair      feature inters.: Spring Creek  
cadastral grid: S30, T63N, R16W  
highway route: County Road 159  
highway distr.: 2  
current owner: Adair County

## STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss, with timber stringer approach spans  
substructure: concrete-filled steel cylinder piers; timber pile bent abutments

span number: 1      condition: fair  
span length: 70.0'      alterations: none  
total length: 99.0'      floor/decking : timber deck over timber stringers  
roadway width: 13.3'      other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: looped square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to verticals; guardrail: steel lattice

## HISTORICAL DATA

erection date: 1900  
erection cost: \$1100.00 (contract amount)  
designer: Fair Williams Bridge and Manufacturing Company, Ottumwa IA  
fabricator : Fair Williams Bridge and Manufacturing Company, Ottumwa IA  
contractor: Fair Williams Bridge and Manufacturing Company, Ottumwa IA

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 159000.1; Adair County Court Record, Book O: page 549 (3 July 1900); Book Q: page 64 (4 December 1900), page 75 (5 December 1900); field inspection by Clayton Fraser and Ron Ashbach-Sladek, 5 October 1991.

sign. rating: 44  
evaluation: NRHP non-eligible (early example of mainstay structural type)

inventoried by: Clayton Fraser and Ron Ashbach-Sladek      22 January 1992

# Billy Creek Bridge

ADAI12

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## GENERAL DATA

structure no.:	176000.8	city/town:	3.1 miles west of Novinger
county:	Adair	feature inters.:	Billy Creek
		cadastral grid:	S35, T63N, R17W
		highway route:	County Road 176
		highway distr.:	2
		current owner:	Adair County

## STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss  
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	68.0'	alterations:	none
total length:	69.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.8'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: looped round eyebar with turnbuckle; floor beam: I-beam, field-bolted to verticals; guardrail: steel lattice

## HISTORICAL DATA

erection date: c1910  
erection cost: unknown  
designer: unknown  
fabricator : unknown  
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 176000.8; field inspection by Clayton Fraser and Ron Ashback-Sladek, 5 October 1991.

sign. rating: 30  
evaluation: NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Clayton Fraser and Ron Ashbach-Sladek 22 January 1992

# Sloan Point Bridge

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ADAI13

## GENERAL DATA

<b>structure no.:</b> 223000.1	<b>city/town:</b> 4.5 miles southwest of Kirksville
<b>county:</b> Adair	<b>feature inters.:</b> Big Creek
	<b>cadastral grid:</b> S14, T62N, R16W
	<b>highway route:</b> County Road 223
	<b>highway distr.:</b> 2
	<b>current owner:</b> Adair County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt through truss	
<b>substructure:</b> concrete-filled steel cylinder piers with timber back- and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 122.0'	<b>alterations:</b> one portal strut replaced
<b>total length:</b> 122.0'	<b>floor/decking :</b> timber deck over timber stringers
<b>roadway width:</b> 14.0'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: 2 looped square eye-bars with turnbuckles; lateral bracing: round rods with threaded ends; strut: 2 angles; portal strut: 4 latticed angles at the east, 2 back-to-back channels at the west; floor beam: I-beam, U-bolted to vertical

## HISTORICAL DATA

<b>erection date:</b> 1899	
<b>erection cost:</b> \$1225.00; \$399.00 for approaches	
<b>designer:</b> unknown	
<b>fabricator :</b> unknown	
<b>contractor:</b> Theodore Moyer	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 223000.1; Adair County Court Record, Book N: page 203 (10 April 1896); Book O: page 177 (5 June 1899), page 221 (6 July 1899), page 290 (2 October 1899), page 293 (2 October 1899), page 529 (6 June 1900), page 547 (2 July 1900), located at Adair County Courthouse, Kirksville MO; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 5 October 1991.
<b>sign. rating:</b> 42	
<b>evaluation:</b>	NRHP non-eligible (early example of mainstay structural type, somewhat altered)

inventoried by: Clayton Fraser and Ron Ashbach-Sladek 22 January 1992

# Salt River Bridge

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ADAI14

## GENERAL DATA

structure no.:	269003.6	city/town:	9.1 miles east of Kirksville
county:	Adair	feature inters.:	Salt River
		cadastral grid:	S12/13, T62N, R14W
		highway route:	County Road 269
		highway distr.:	2
		current owner:	Adair County

## STRUCTURAL DATA

superstructure:	steel, 5-panel, rigid-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	100.0'	alterations:	unknown
total length:	116.0'	floor/decking :	timber deck
roadway width:	12.0'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	c1925
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 269003.6.

sign. rating:	28
evaluation:	NRHP non-eligible (typically configured, long-span example of common structural type)

inventoried by: Clayton Fraser and Ron Ashbach-Sladek    22 January 1992

# Bridge

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ADAI15

## GENERAL DATA

structure no.:	280000.3	city/town:	9.7 miles southeast of Kirksville
county:	Adair	feature inters.:	unnamed stream
		cadastral grid:	S25/36, T62N, R14W
		highway route:	County Road 280
		highway distr.:	2
		current owner:	Adair County

## STRUCTURAL DATA

superstructure:	concrete deck girder	condition:	fair
substructure:	concrete abutments and wingwalls	alterations:	none
span number:	1	floor/decking :	concrete deck
span length:	24.0'	other features:	concrete guardrails
total length:	26.0'		
roadway width:	19.0'		

## HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	none
contractor :	Carter and Hughes, Kirksville MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 280000.3.
sign. rating:	20
evaluation:	NRHP non-eligible (inadequately documented example of early concrete bridge type)

inventoried by: Clayton Fraser and Ron Ashbach-Sladek    22 January 1992

# Eitel Bridge

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ADAI16

## GENERAL DATA

<b>structure no.:</b> 360001.6	<b>city/town:</b> 6.9 miles south of Kirksville
<b>county:</b> Adair	<b>feature inters.:</b> East Fork Chariton River
	<b>cadastral grid:</b> S15, T61N, R15W
	<b>highway route:</b> County Road 360
	<b>highway distr.:</b> 2
	<b>current owner:</b> Adair County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 2-panel, pin-connected Pratt half-hip pony truss	
<b>substructure:</b> timber pile bent abutments and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 30.0'	<b>alterations:</b> unknown
<b>total length:</b> 31.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.5'	<b>other features:</b> steel lattice guardrails

## HISTORICAL DATA

<b>erection date:</b> 1903
<b>erection cost:</b> \$270.00
<b>designer:</b> Fair Williams Bridge and Manufacturing Company, Ottumwa IA (probable)
<b>fabricator :</b> Fair Williams Bridge and Manufacturing Company, Ottumwa IA (probable)
<b>contractor :</b> J.H. Mooney
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 360001.6; Adair County Court Record, Book R: page 340 (5 February 1903), page 364 (3 March 1903), page 587 (9 September 1903), located at Adair County Courthouse, Kirksville MO.
<b>sign. rating:</b> 37
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)

**inventoried by:** Clayton Fraser and Ron Ashbach-Sladek    22 January 1992

# Bridge

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ADAI17

## GENERAL DATA

<b>structure no.:</b> 410001.9	<b>city/town:</b> 0.7 mile west of Nind
<b>county:</b> Adair	<b>feature inters.:</b> Walnut Creek tributary
	<b>cadastral grid:</b> S23, T61N, R17W
	<b>highway route:</b> County Road 410
	<b>highway distr.:</b> 2
	<b>current owner:</b> Adair County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 2-panel, pin-connected Pratt half-hip pony truss, with timber stringer approach span	
<b>substructure:</b> steel pile bent piers with timber back- and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 30.0'	<b>alterations:</b> unknown
<b>total length:</b> 43.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.9'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> c1910
<b>erection cost:</b> unknown
<b>designer:</b> unknown
<b>fabricator :</b> unknown
<b>contractor:</b> unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 410001.9.

<b>sign. rating:</b> 25
<b>evaluation:</b> NRHP non-eligible (typical, undocumented example of common structural type)

**inventoried by:** Clayton Fraser and Ron Ashbach-Sladek    22 January 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Wayman Bridge  
MHTD: 036000.8

ADAI08

**DATE(S) OF CONSTRUCTION**

1915

**LOCATION**

County Road 36 over unnamed stream; S20, T64N, R15W  
9.4 miles north of Kirksville; Adair County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 45)

**CONDITION**

fair

**OWNER**

Adair County

span number:	1	superstructure:	concrete deck girder
span length:	30.0'	substructure:	concrete abutments and wingwalls
total length:	30.0'	floor/decking:	concrete deck
roadway wdt.:	15.7'	other features:	concrete guardrails with circular cutouts

This single-span concrete bridge carries a county road over an unnamed stream north of Kirksville. Known locally as the Wayman Bridge, the structure features a concrete deck supported between the concrete mass abutments by concrete girders. It is simply detailed, with circular cutouts in the guardrails as the only architectural expression. The Wayman Bridge dates to 1915. Early that year the Adair County Court contracted with local builders Carter and Hughes for this and several other small-scale concrete and steel structures. Carter and Hughes completed the Wayman Bridge by June and were paid \$740.00 for its construction. Since that time, the Wayman Bridge has functioned in place, without substantial alteration.

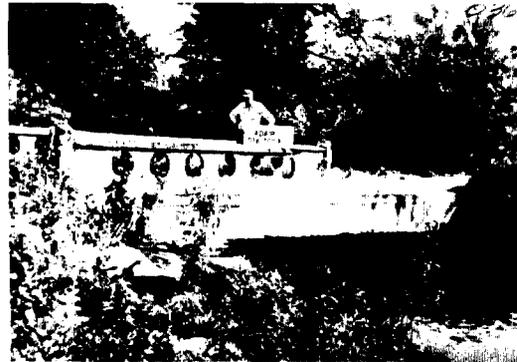
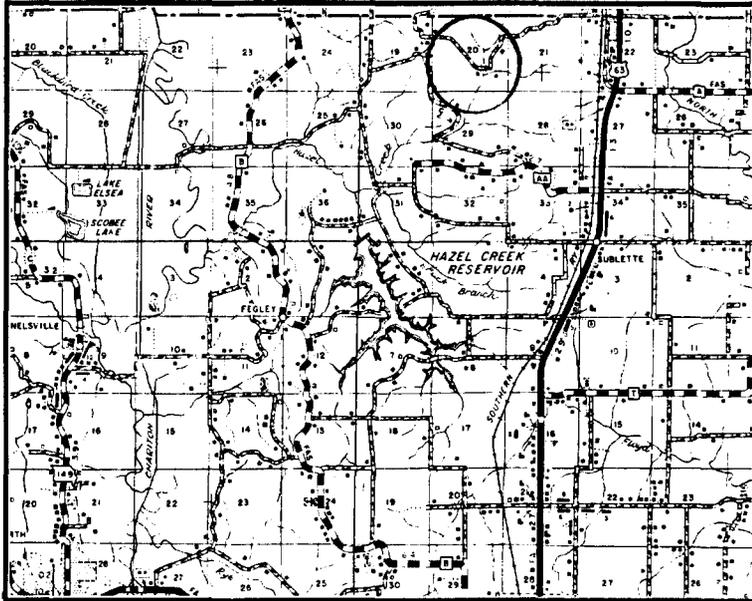
Unlike many Midwestern states, Missouri did not employ reinforced concrete extensively for construction of vehicular bridge superstructures in the 1910s. The various counties and, to a lesser extent, the state highway department continued to prefer steel for bridge superstructures well after concrete had received widespread acceptance elsewhere. This, combined with subsequent attrition, has resulted in a relatively small number of concrete bridges that exist today from this formative period. The Wayman Bridge is distinguished among these as one of the earliest remaining examples in the state of what would later become a mainstay structural type—the concrete deck girder bridge. Modestly scaled and simply detailed, it is noteworthy as an early, well-preserved example of concrete bridge construction in Missouri.

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**NAME(S) OF STRUCTURE**

Wayman Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 036000.8; Adair County Court Record, Book U: page 29 (9 June 1915), located at Adair County Courthouse, Kirksville MO.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

22 January 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Sloan Point Bridge (Big Creek Bridge)  
MHTD: 223000.1

ADAI13

**DATE(S) OF CONSTRUCTION**

1899

**LOCATION**

county road over Big Creek; S14, T62N, R16W  
4.5 miles southwest of Kirksville; Adair County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 42)

**CONDITION**

fair

**OWNER**

Adair County

span number: 1

span length: 122.0'

total length: 122.0'

roadway wdt.: 14.0'

superstructure: steel, 5-panel, pin-connected Pratt through truss

substructure: concrete-filled steel cylinder piers with timber back- and wingwalls

floor/decking: timber deck over timber stringers

other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: 2 looped square eyebars with turnbuckles; lateral bracing: round rods with threaded ends; strut: 2 angles; portal strut: 4 latticed angles at the east, 2 back-to-back channels at the west; floor beam: I-beam, U-bolted to vertical

This medium-span through truss carries an unpaved county road across Big Creek, a tributary of the Chariton River, southwest of Kirksville in Benton Township. Located near Sloan Point, the structure's origins date to 1899. That June the Adair County Court awarded a contract for the construction of the Sloan Point Bridge to Theodore Moyer for \$1225.00. The structure was completed by early October of the same year. The bridge is now located within the boundaries of Thousand Hills State Park and continues to carry rural traffic. One of its portal struts has been replaced, perhaps as the result of collision damage, but the truss remains otherwise intact.

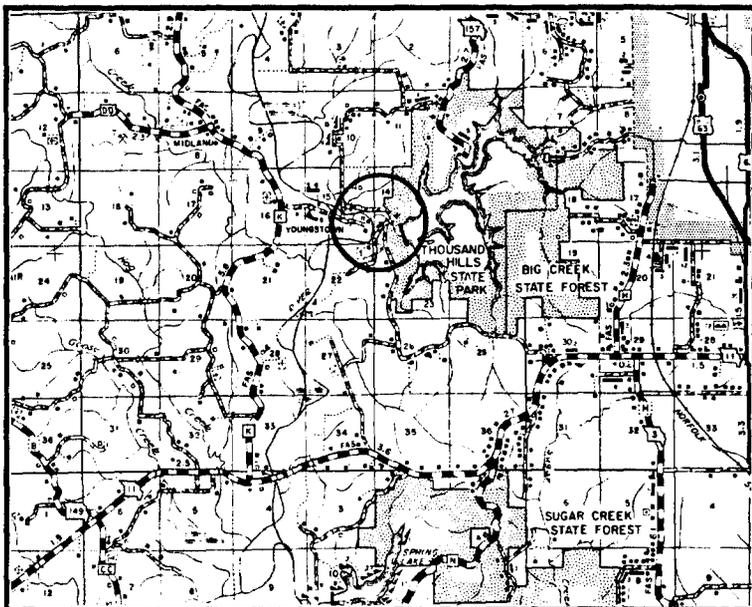
As the oldest documented wagon bridge in Adair County, the Sloan Point Bridge is historically noteworthy as an intact remnant of early transportation. The structure is technologically representative as a pinned Pratt through truss - Missouri's mainstay structural type for medium-span crossings built in the late 19th century.

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**NAME(S) OF STRUCTURE**

Sloan Point Bridge (Big Creek Bridge)

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 223000.1; Adair County Court Record, Book N: page 203 (10 April 1896); Book O: page 177 (5 June 1899), page 221 (6 July 1899), page 290 (2 October 1899), page 293 (2 October 1899), page 529 (6 June 1900), page 547 (2 July 1900), located at Adair County Courthouse, Kirksville MO; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 5 October 1991.

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**INVENTORIED BY**

Clayton Fraser and Ron Ashbach-Sladek

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

22 January 1992

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# CARROLL COUNTY

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INCLUDED: [Significant feature(s) of bridge given in boldface]  
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
*CARR01	F 404	Carrollton Bridge	1-100' riveted Warren pony truss 1919 Kansas City Bridge Company
*CARR02	G 54R	Waverly Bridge	6-416' riveted Pennsylv. through truss 1926 Wisconsin Bridge and Iron Co.
*CARR03	K 999R	McDaniel Memorial Bridge	3-475' riveted cantilever through truss 1940 Massman Constr., Kansas City
CARR04	007001.8	Cottonwood Creek Bridge	1- 28' riveted Pratt bedstead 1904 Interstate Bridge Company
CARR05	015000.9	Bridge	1- 30' steel stringer 1909 R.E. Bates (replaced)
CARR06	039000.7	Bunch Hollow Bridge	1- 30' steel stringer 1910 R.G. Culbertson, Hale MO
CARR07	050000.4	Bridge	1- 40' riveted Warren bedstead 1906 Frank Yehle
CARR08	060000.9	Bridge	1- 25' steel stringer 1911 R.E. Bates
CARR09	093000.8	Rock Branch Bridge	1- 30' steel stringer 1914 Frank Yehle (replaced)
CARR10	100000.2	Bridge	1- 30' steel stringer 1907 R.G. Culbertson
CARR11	100001.0	Bridge	1- 26' steel stringer 1909 R.E. Bates
CARR12	103000.1	Bridge	1- 30' steel stringer 1914 R.E. Bates
CARR13	107000.4	Hurricane Creek Bridge	1- 65' riveted Pratt pony truss 1909 R.E. Bates
CARR14	117000.2	Hurricane Creek Bridge	1- 34' steel stringer 1908 Robert Bates
CARR15	122000.9	Bridge Creek Bridge	1- 60' pinned Pratt pony truss 1904 Frank Yehle
CARR16	122001.4	Bridge Creek Bridge	1- 31' steel stringer 1909 Midland Bridge Company
CARR17	133000.7	Hurricane Creek Bridge	1- 30' steel stringer 1908 R.E. Bates
CARR18	142000.2	Burr Oak Creek Bridge	1- 40' riveted Warren bedstead 1910 R.E. Bates
CARR19	144000.3	Bridge	1- 42' pinned Pratt bedstead 1905 Frank Yehle
CARR20	160000.3	Turkey Creek Bridge	1- 24' steel stringer 1909 R.E. Bates
CARR21	169000.5	Turkey Creek Bridge	1- 40' riveted Warren bedstead 1909 Kansas City Bridge Company
CARR22	177001.6	Bridge	
CARR23	196R00.4	Bridge	

# CARROLL COUNTY

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## INCLUDED (cont.):

CARR24	202000.1	Bridge	1- 26'	steel stringer
			1910	R.E. Bates (replaced)
CARR25	211000.8	Bridge		
CARR26	214002.1	Big Creek Bridge	1- 60'	riveted Pratt pony truss
			1919	Kansas City Bridge Company
CARR27	224001.0	Bridge	1- 48'	riveted Warren pony truss
			1908	Midland Bridge Company
CARR28	230000.1	Bridge	1- 50'	pinned Pratt pony truss
			1909	Leavenworth Bridge Company
CARR29	238000.8	Shootman Creek Bridge	1- 29'	steel stringer
			1914	M.E. Grant
*CARR30	247000.9	Shootman Creek Bridge	1- 60'	pinned Pratt bedstead
			1908	Midland Bridge Company
CARR31	256000.5	Hurricane Creek Bridge	1- 24'	steel stringer
			1910	R.E. Bates
CARR32	261000.7	Wolf Creek Bridge	1- 35'	steel stringer
			1910	R.E. Bates (replaced)
CARR33	266002.3	Mud Creek Bridge		
CARR34	274000.1	AT&SF Railroad Overpass	1- 80'	pinned Pratt pony truss
			1906	AT&SF Railroad
CARR35	286000.9	Bridge	1- 32'	steel stringer
			1909	R.E. Bates
CARR36	289000.6	Bridge	1- 30'	steel stringer
			1911	Kansas City Bridge Company
CARR37	292R00.1	Sand Branch Bridge	1- 26'	steel stringer
			1909	R.E. Bates
CARR38	293001.4	Sand Branch Bridge	1- 32'	steel stringer
			1910	R.E. Bates
CARR39	300000.8	Turkey Creek Bridge	1- 50'	pinned Pratt bedstead
			1908	Frank Yehle
CARR40	310001.5	Wakenda Creek Bridge	1- 72'	pinned Pratt pony truss
			1909	Illinois Steel Bridge Company
*CARR41	313001.3	Wakenda Creek Bridge	1-125'	riveted Camelback pony truss
			1946	
CARR42	322000.7	Turkey Creek Bridge	1- 56'	pinned Pratt half-hip pony truss
			1900	Kansas City Bridge Company
CARR43	330000.5	McCroskie Creek Bridge	1- 36'	steel stringer
			1912	R.E. Bates
CARR44	336000.1	McGill Creek Bridge	1- 21'	steel stringer
			1909	Kansas City Bridge Company
CARR45	337R00.2	McGill Creek Bridge	1- 33'	riveted Pratt bedstead
			1904	Interstate Bridge Company
CARR46	344000.5	McCroskie Creek Bridge	1- 50'	pinned Pratt pony truss
			1910	R.G. Culbertson, Hale MO
CARR47	349000.3	Fish Pond Creek Bridge	1- 36'	steel stringer
			1911	Kansas City Bridge Company
CARR48	349001.2	McCroskie Creek Bridge	1- 30'	steel stringer
			1918	

# CARROLL COUNTY

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## INCLUDED (cont.):

CARR49	358000.5	Bridge	1- 22'	1909	steel stringer R.G. Culbertson, Hale MO
*CARR50	365R02.3	Shootman Creek Bridge	1- 50'	1911	riveted Pratt pony truss R.G. Culbertson, Hale MO
CARR51	367000.7	Bridge	1- 28'	1908	steel stringer Kansas City Bridge Company (replaced)
CARR52	373001.2	Little Wakenda Creek Bridge			(replaced)
CARR53	375003.2	Bridge			(replaced)
CARR54	388000.4	Bridge	1- 32'	1909	steel stringer R.E Bates
CARR55	389000.6	Bridge	1- 28'	1908	steel stringer Kansas City Bridge Company
CARR56	393000.3	Little Wakenda Creek Bridge	1- 45'	1911	riveted Pratt pony truss R.G. Culbertson, Hale MO
*CARR57	407000.1	Santa Fe Railroad Overpass	1-160'	c1895	<b>pinned Pratt through truss</b> Edge Moor Iron Works
CARR58	407000.6	Bridge	1- 28'	1911	steel stringer R.E. Bates
CARR59	424000.4	Bridge	1- 30'	1910	steel stringer R.E. Bates
CARR60	438000.1	Big Creek Bridge	1- 30'	1908	steel stringer R.G. Culbertson, Hale MO
CARR61	453002.7	Bridge	1- 28'	1909	steel stringer Kansas City Bridge Company
CARR62	496000.6	Weatherholt Bridge	1- 70'	1913	riveted Pratt pony truss R.G. Culbertson, Hale MO (replaced)
CARR63	503000.8	Booker Slough Bridge			
CARR64	514001.1	Moss Creek Bridge	1- 60'	1905	pinned Pratt pony truss Frank Yehle
CARR65	523001.9	Bridge	1- 32'	1908	steel stringer Kansas City Bridge Company
CARR66	530001.8	Bridge	1- 30'	1908	steel stringer Kansas City Bridge Company
CARR67	547001.2	Miller Branch Bridge	1- 27'	1910	steel stringer Kansas City Bridge Company (replaced)
*CARR68	548002.2	Becker Bridge			
CARR69	559000.6	Bridge	1- 24'	1909	steel stringer Midland Bridge Company
CARR70	594001.3	Bridge	1- 26'	1910	steel stringer Kansas City Bridge Company
CARR71	619000.4	Bridge	1- 34'	1909	steel stringer Kansas City Bridge Company
CARR72	621000.5	Bridge	1- 28'	1909	steel stringer Kansas City Bridge Company
CARR73	623000.4	Bridge	1- 28'	1909	steel stringer Kansas City Bridge Company
CARR74	640000.2	County Line Bridge	1- 80'	1906	pinned Pratt pony truss Kansas City Bridge Company

# CARROLL COUNTY

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## EXCLUDED:

### Pratt pony truss

058R00.4	061000.5	122000.9	176001.4	216001.8	305001.0	321000.5
353001.7	368000.2	486001.0	486003.0	490001.0	510000.5	511000.5

### Warren pony truss

002000.9	146002.0	159000.5	161001.5	216002.3	217000.8	224001.4
226001.8	302002.2	319001.5	322001.0	328000.4	357000.7	476000.4

### Steel stringer / girder

G 36R	H 587R	H 614R1	J 4R	L 1R	L 2R	L 3R
L 181	L 182	L 183	L 184	S 10	S 485	S 549
S 605	S 868	T 267	T 271	T 368	Y 492	007001.1
014000.6	014001.5	017001.0	019000.1	019000.5	034000.3	052003.0
052003.8	059001.1	071000.2	071000.5	071000.7	071001.1	071001.2
080000.6	085001.1	085001.5	088002.3	091000.9	092R02.8	092000.7
092001.1	092002.7	093R00.1	095000.5	098000.8	103001.0	110001.2
113R00.5	118R04.5	118001.3	119000.5	129000.8	129001.4	138000.5
139002.7	145001.6	149R00.3	150000.7	150001.3	152R00.1	154000.5
156R00.7	157000.3	161R02.9	165000.3	166000.4	170000.5	170000.8
171000.3	175001.9	176000.2	193000.5	204000.1	204001.4	204001.8
207000.1	209001.4	209001.6	210000.2	214000.6	214001.4	226001.3
228000.9	234000.3	250001.1	254002.5	256000.9	264000.9	269001.3
284000.9	287000.4	301001.1	304000.3	305000.8	309000.2	313001.0
317001.4	317001.8	323001.3	328001.8	329000.2	339R01.0	342000.6
346000.6	347000.1	357003.0	359000.3	365001.0	375000.5	381R00.9
383001.9	389000.5	390R00.8	391000.3	399000.9	400001.1	403000.9
408000.3	415R00.8	415003.3	416R01.8	422000.7	438R03.5	443R01.7
448000.7	453R02.2	455R01.2	458000.6	460001.1	460001.4	462000.4
465R01.1	467000.6	472000.3	478001.1	484000.2	484000.8	488000.5
492000.3	495000.3	497R01.2	499000.8	508001.3	513R00.6	516001.4
537000.2	542R01.6	559R00.7	560000.1	561R01.0	561000.2	562000.4
563000.6	582R00.7	586R01.4	586001.7	599000.6	606000.8	622000.4
635000.9	640001.8	641001.2				

### Concrete girder

H 423R	H 603	H 606R1	H 607R1	H 941R1	J 28	J 110R
J 154R	J 280R	K 98	K 57R1	633000.1		

### Concrete box culvert

H 424	H 605	J 268	L 387	T 102	T 110	T 269
T 270	X 629	X 870	X 991	071000.4	071001.0	

## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	4	62	0	0	66
Excluded	42	175	0	0	217
	46	237	0	0	283 structures

# Carrollton Bridge

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CARR01

## GENERAL DATA

structure no.:	F 404	city/town:	Carrollton
county:	Carroll	feature inters.:	Wakenda Creek
		cadastral grid:	S5, T52N, R23W
		highway route:	Business Route 65
		highway distr.:	2
		current owner:	Missouri Highway and Transportation Department

## STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Warren pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	good
span length:	100.0'	alterations:	none
total length:	105.0'	floor/decking :	concrete deck over steel stringers
roadway width:	18.6'	other features:	steel guardrails

## HISTORICAL DATA

erection date:	1919
erection cost:	\$16,000.00
designer:	Missouri State Highway Department (possible)
fabricator :	Kansas City Bridge Company, Kansas City MO; Inland Steel Company, East Chicago IN
contractor:	Kansas City Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number F 404; Carroll County Road Record 1909-1920, page 591 (2 September 1918); Carroll County Court Record, Book 2: page 432 (6 March 1919), page 51 (7 April 1919), page 98 (7 May 1919) - located at Carroll County Courthouse, Carrollton MO; Harold Calvert, <b>Twenty-Twenty Hindsight: Part II</b> , (collection of articles previously published in Missouri newspapers, no publisher, 1975), n.p.; field inspection by Clayton Fraser, 30 September 1991.
sign. rating:	56
evaluation:	NRHP possibly eligible (prototype for MSHD-designed mainstay structural type)

inventoried by: Clayton B. Fraser 14 October 1992

# Waverly Bridge

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CARR02

## GENERAL DATA

structure no.:	G 54R	city/town:	Waverly
county:	Carroll	feature inters.:	Missouri River
		cadastral grid:	S19, T51N, R23W
		highway route:	U.S. Highway 24/65
		highway distr.:	2
		current owner:	Missouri Highway and Transportation Department

## STRUCTURAL DATA

**superstructure:** two steel, 18-panel, rigid-connected Pennsylvania through truss spans; three 11-panel, rigid-connected Parker through truss spans; one deck truss approach span; two plate girder approach spans; one steel stringer approach span

**substructure:** concrete abutments, wingwalls and piers

**span number:** 2; 3; 1

**span length:** 416.0'; 255.0'

**total length:** 1945.0'

**roadway width:** 20.0'

**condition:** good

**alterations:** original concrete deck replaced

**floor/decking :** asphalt over corrugated steel with steel stringers

**other features:** upper chord and inclined end post: 2 built-up channels with cover plate and double lacing; lower chord: 2 channels with lacing; vertical: 2 channels with lacing (4 angles with lacing at the hips); diagonal: 2 channels with batten plates, 2 and 4 angles with batten plates; lateral bracing: 2 angles with lacing; strut: 4 angles with lacing; floor beam: I-beam, field bolted; guardrail: Armco; builder's plate: Waverly Bridge; designed by Harrington, Howard & Ash, consulting engineers, Kansas City, MO; (unreadable) Missouri State Highway Department, G. H. Piepmeier, chief engineer, L. J. Sverdrup, bridge engineer; Contractors: substructure: Missouri Valley Bridge & Iron Co., Leavenworth, Kansas, superstructure: Wisconsin Bridge & Iron Co., North Milwaukee, Wisconsin; completed 1925

## HISTORICAL DATA

**erection date:** 1924-25

**erection cost:** \$583,000.00 (approximate cost)

**designer:** Harrington, Howard and Ash, Kansas City MO

**fabricator :** Wisconsin Bridge and Iron Company, North Milwaukee WI; Illinois Steel Company, Chicago IL

## Waverly Bridge

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**contractor:** Wisconsin Bridge and Iron Company, North Milwaukee WI (superstructure);  
Missouri Valley Bridge and Iron Company, Leavenworth KS (substructure)

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number G 54R; Harold Calvert, **Twenty-Twenty Hindsight: Part II** (collection of articles published in Missouri newspapers, no publisher, 1975), n.p.; **History of Carroll County, Missouri** (Carrollton MO: Carroll County Historical Society, 1968), pages 28-29; **Bridges, Missouri River: Data, History and Laws**, U.S. Engineer Office, Kansas City, Missouri, 1933; L.J. Sverdrup, "Cantilever Erection of Long Spans of Missouri River Bridge," **Engineering News-Record**, 13 October 1927; field inspection by Clayton Fraser, 30 September 1991.

**sign. rating:** 72

**evaluation:** NRHP eligible (excellent large-scale bridge positioned at important Missouri River crossing)

**Inventoried by:** Clayton B. Fraser    14 October 1992

# McDaniel Memorial Bridge

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CARR03

## GENERAL DATA

structure no.:	K 999R	city/town:	Miami
county:	Carroll	feature inters.:	Missouri River
		cadastral grid:	S11, T52N, R21W
		highway route:	State Highway 41
		highway distr.:	2
		current owner:	Missouri Highway and Transportation Department

## STRUCTURAL DATA

**superstructure:** three steel, rigid-connected cantilever through truss spans; five steel stringer approach spans; three deck truss approach spans

**substructure:** concrete abutments and piers

span number:	1; 2; 3	condition:	good
span length:	475.0'; 416.0'; 305.0'	alterations:	original concrete deck replaced
total length:	2072.0'	floor/decking :	concrete deck over steel stringers
roadway width:	23.0'	other features:	upper chord, lower chord, and inclined end posts: 2 built-up channels with cover plate and double lacing; vertical: 2 channels with lacing, 4 angles with batten plates; diagonal: 2 channels with double lacing, 4 angles with batten plates; lateral bracing: 2 angles with lacing; strut: 4 angles with lacing and braces; floor beam: steel I-beam; guardrail: 2 channels; upper builder's plate text: Federal Emergency Administration of Public Works; Franklin D. Roosevelt, President of the United States; Harold L. Ickes, Administrator of Public Works; McDaniel Memorial Bridge; 1939; lower builder's plate text: McDaniel Memorial Bridge; built by Saline County Missouri with the cooperation of Missouri State Highway Commission; county court of Saline County, Robert L. Hyatt - presiding judge, T. M. Smith - judge, O. Boyd Davis - judge; Sverdrup and Parcel, consulting engineers; Massman Construction Co., general contractors; 1939

## HISTORICAL DATA

erection date:	1939
erection cost:	\$771,517.00
designer:	Sverdrup and Parcel, Consulting Engineers, St. Louis MO
fabricator :	Inland Steel Company, East Chicago IN
contractor:	Massman Construction Company, Kansas City MO

## McDaniel Memorial Bridge

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**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal (Structure Number K-999R); Carroll County Court Record, Book 5: pages 527-539, 595 (5 July 1938 - 20 October 1938); Book 6: pages 1-16 (December 1938) - located at Carroll County Courthouse, Carrollton MO; **History of Carroll County** (Carrollton MO: Carroll County Historical Society, 1968), page 29; **Bridge Over Missouri River at Miami, Missouri**, P.W.A. Docket Missouri 1075-F, Contract No. 2, Saline County, Missouri (St. Louis MO: Sverdrup and Parcel, Consulting Engineers, no date); **Bridges, Missouri River: Data, History and Laws**, U.S. Engineer Office, Kansas City, Missouri, 1933; field inspection by Clayton Fraser, 9 October 1991.

**sign. rating:** 63

**evaluation:** NRHP possibly eligible (excellent large-scale bridge positioned at important Missouri River crossing)

**inventoried by:** Clayton B. Fraser 14 October 1992

# Cottonwood Creek Bridge

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CARR04

## GENERAL DATA

<b>structure no.:</b>	007001.8	<b>city/town:</b>	7.5 miles northwest of Mandeville
<b>county:</b>	Carroll	<b>feature inters.:</b>	Cottonwood Creek
		<b>cadastral grid:</b>	S2/11, T55N, R25W
		<b>highway route:</b>	County Road 7
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Carroll County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 3-panel, rigid-connected Pratt bedstead truss		
<b>substructure:</b>	concrete abutments and wingwalls		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	30.0'	<b>alterations:</b>	none
<b>total length:</b>	30.0'	<b>floor/decking :</b>	concrete deck over steel stringers
<b>roadway width:</b>	17.9'	<b>other features:</b>	no guardrails

## HISTORICAL DATA

<b>erection date:</b>	1904
<b>erection cost:</b>	\$450.00
<b>designer:</b>	Interstate Bridge Company, Kansas City MO
<b>fabricator :</b>	Interstate Bridge Company, Kansas City MO
<b>contractor :</b>	Interstate Bridge Company, Kansas City MO
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 007001.8; Carroll County Court Record, Book Q: page 33 (2 August 1904), page 177 (10 November 1904), pages 432-433 (3 May 1905) - located at Carroll County Courthouse, Carrollton MO.
<b>sign. rating:</b>	46
<b>evaluation:</b>	NRHP possibly eligible (oldest example in state of small-scale Pratt truss subtype)

inventoried by: Clayton B. Fraser    14 October 1992

# Bridge

CARR05

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## GENERAL DATA

structure no.: 015000.9      city/town: 6.0 miles northwest of Mandeville  
county: Carroll      feature inters.: branch of Cottonwood Creek  
cadastral grid: S12/13, T55N, R25W  
highway route: County Road 15  
highway distr.: 2  
current owner: Carroll County

## STRUCTURAL DATA

superstructure: steel stringer  
substructure: unknown

span number: 1      condition: fair  
span length: 30.0'      alterations: unknown  
total length: 30.0'      floor/decking : unknown  
roadway width: 16.0'      other features: unknown

## HISTORICAL DATA

erection date: 1909  
erection cost: \$445.00  
designer: unknown  
fabricator : unknown  
contractor: R.E. Bates

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 015000.9; Carroll County Court Record, Book X: page 37 (4 May 1910) - located at Carroll County Courthouse, Carrollton MO.

sign. rating: 32  
evaluation: NRHP non-eligible (typical, small-scale example of common structural type)

inventoried by: Clayton B. Fraser      14 October 1992

# Bridge

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CARR07

## GENERAL DATA

structure no.:	050000.4	city/town:	5.0 miles northwest of Mandeville
county:	Carroll	feature inters.:	branch of Cottonwood Creek
		cadastral grid:	S18/19, T55N, R24W
		highway route:	County Road 50
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer		
substructure:	unknown		
span number:	1	condition:	fair
span length:	30.0'	alterations:	unknown
total length:	32.0'	floor/decking :	unknown
roadway width:	16.0'	other features:	unknown

## HISTORICAL DATA

erection date:	1909-10
erection cost:	\$400.00
designer:	unknown
fabricator :	unknown
contractor:	R.G. Culbertson, Hale MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 050000.4; Carroll County Court Record, Book X: pages 40-41 (4 May 1910 (February/April 1910 recapitulation)) - located at Carroll County Courthouse, Carrollton MO; S.K. Turner and S.A. Clark, *Twentieth Century History of Carroll County, Missouri* (Indianapolis: B.F. Bowen & Company, 1911), page 194.

sign. rating:	30
evaluation:	NRHP non-eligible (undistinguished, small-scale example of common structural type)

inventoried by: Clayton B. Fraser    15 October 1992

# Bridge

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CARR08

## GENERAL DATA

structure no.:	060000.9	city/town:	7.0 miles northwest of Bogard
county:	Carroll	feature inters.:	tributary of Tater Hill Creek
		cadastral grid:	S23/26, T55N, R24W
		highway route:	County Road 60
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, rigid-connected Warren bedstead		
substructure:	unknown		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	42.0'	floor/decking :	concrete deck over steel stringers
roadway width:	15.6'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1906
erection cost:	\$335.00
designer:	unknown
fabricator :	unknown
contractor :	Frank Yehle

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 060000.9; Carroll County Court Record, Book V: pages 222-224 (7 May 1907) - located at Carroll County Courthouse, Carrollton MO.

sign. rating:	40
evaluation:	NRHP non-eligible (typical example of small-scale truss construction)

inventoried by: Clayton B. Fraser    15 October 1992

# Rock Branch Bridge

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CARR09

## GENERAL DATA

structure no.:	093000.8	city/town:	1.0 mile northwest of Tina
county:	Carroll	feature inters.:	Rock Branch
		cadastral grid:	S26/27, T55N, R23W
		highway route:	County Road 93
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer		
substructure:	unknown		
span number:	1	condition:	fair
span length:	25.0'	alterations:	unknown
total length:	25.0'	floor/decking :	unknown
roadway width:	17.0'	other features:	unknown

## HISTORICAL DATA

erection date:	1911
erection cost:	\$696.00
designer:	unknown
fabricator :	unknown
contractor:	R.E. Bates

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 093000.8; Carroll County Court Record, Book Y: page 96 (4 June 1912 (May 1911 recapitulation)) - located at Carroll County Courthouse, Carrollton MO.

sign. rating:	30
evaluation:	NRHP non-eligible (undistinguished, small-scale example of common structural type)

inventoried by: Clayton B. Fraser 15 October 1992

# Bridge

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CARR10

## GENERAL DATA

structure no.:	100000.2	city/town:	4.0 miles west of Tina
county:	Carroll	feature inters.:	branch of Tater Hill Creek
		cadastral grid:	S30/31, T55N, R23W
		highway route:	County Road 10
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	concrete abutments	alterations:	unknown
span number:	1	floor/decking :	concrete deck over steel stringers
span length:	30.0'	other features:	unknown
total length:	30.0'		
roadway width:	17.4'		

## HISTORICAL DATA

erection date:	1914
erection cost:	\$656.00
designer:	unknown
fabricator :	unknown
contractor:	Frank Yehle

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 100000.2; Carroll County Court Record, Book Z: page 157 (9 July 1914) - located at Carroll County Courthouse, Carrollton MO.

sign. rating:	30
evaluation:	NRHP non-eligible (undistinguished, small-scale example of common structural type)

Inventoried by: Clayton B. Fraser    16 October 1992

# Bridge

CARR12

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## GENERAL DATA

structure no.:	103000.1	city/town:	5.0 miles northwest of Tina
county:	Carroll	feature inters.:	branch of Tater Hill Creek
		cadastral grid:	S19/30, T55N, R23W
		highway route:	County Road 103
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure: steel stringer  
substructure: unknown

span number:	1	condition:	fair
span length:	30.0'	alterations:	unknown
total length:	30.0'	floor/decking :	unknown
roadway width:	16.0'	other features:	unknown

## HISTORICAL DATA

erection date: 1907  
erection cost: \$325.00  
designer: unknown  
fabricator : unknown  
contractor: R.G. Culbertson

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 103000.1; Carroll County Court Record, Book V: page 357 (3 September 1907), pages 588-597 (5 May 1908 (December 1907 recapitulation)) - located at Carroll County Courthouse, Carrollton MO.

sign. rating: 32  
evaluation: NRHP non-eligible (undistinguished, small-scale example of common structural type)

inventoried by: Clayton B. Fraser 16 October 1992

# Hurricane Creek Bridge

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CARR13

## GENERAL DATA

structure no.:	107000.4	city/town:	1.0 mile west of Hale
county:	Carroll	feature inters.:	Hurricane Creek
		cadastral grid:	S3/4, T55N, R22W
		highway route:	County Road 107
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	stone masonry abutments	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	26.0'	other features:	unknown
total length:	27.0'		
roadway width:	14.4'		

## HISTORICAL DATA

erection date: 1909  
erection cost: \$380.00  
designer: unknown  
fabricator : unknown  
contractor: R.E. Bates

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 107000.4; Carroll County Court Record, Book X: pages 38-41 (4 May 1910 (September 1909 and January/-April 1910 recapitulations)) - located at Carroll County Courthouse, Carrollton MO; S.K. Turner and S.A. Clark, **Twentieth Century History of Carroll County, Missouri** (Indianapolis: B.F. Bowen & Company, 1911), page 194.

sign. rating: 32  
evaluation: NRHP non-eligible (typical, small-scale example of common structural type)

inventoried by: Clayton B. Fraser 16 October 1992

# Hurricane Creek Bridge

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CARR14

## GENERAL DATA

<b>structure no.:</b> 117000.2	<b>city/town:</b> 1.0 mile south of Hale
<b>county:</b> Carroll	<b>feature inters.:</b> Hurricane Creek
	<b>cadastral grid:</b> S11/14, T55N, R22W
	<b>highway route:</b> County Road 117
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 30.0'	<b>alterations:</b> unknown
<b>total length:</b> 30.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 14.1'	<b>other features:</b> unknown

## HISTORICAL DATA

**erection date:** 1914  
**erection cost:** \$655.00  
**designer:** unknown  
**fabricator :** unknown  
**contractor:** R.E. Bates

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 117000.2; Carroll County Court Record, Book Z: page 171 (6 August 1914), page 231 (4 November 1914) - located at Carroll County Courthouse, Carrollton MO.

**sign. rating:** 30  
**evaluation:** NRHP non-eligible (typical, small-scale example of common structural type)

**inventoried by:** Clayton B. Fraser    17 October 1992

# Bridge Creek Bridge

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CARR15

## GENERAL DATA

<b>structure no.:</b> 122000.9	<b>city/town:</b> 3.0 miles northeast of Tina
<b>county:</b> Carroll	<b>feature inters.:</b> Bridge Creek
	<b>cadastral grid:</b> S18/19, T55N, R22W
	<b>highway route:</b> County Road 122
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, rigid-connected Pratt pony truss	
<b>substructure:</b> concrete abutments and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 65.0'	<b>alterations:</b> unknown
<b>total length:</b> 69.0'	<b>floor/decking :</b> concrete deck over steel stringers
<b>roadway width:</b> 19.8'	<b>other features:</b> steel angle guardrails

## HISTORICAL DATA

<b>erection date:</b> 1909
<b>erection cost:</b> \$700.00
<b>designer:</b> unknown
<b>fabricator :</b> unknown
<b>contractor :</b> R.E. Bates

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 122000.9; Carroll County Court Record, Book X: page 38 (4 May 1910 (September 1909 recapitulation)) - located at Carroll County Courthouse, Carrollton MO.

<b>sign. rating:</b> 47
<b>evaluation:</b> NRHP possibly eligible (typically configured example of common structural type)

**inventoried by:** Clayton B. Fraser    17 October 1992

# Bridge Creek Bridge

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CARR16

## GENERAL DATA

structure no.:	122001.4	city/town:	3.5 miles northeast of Tina
county:	Carroll	feature inters.:	Bridge Creek
		cadastral grid:	S17/18, T55N, R22W
		highway route:	County Road 122
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	35.0'	other features:	unknown
total length:	35.0'		
roadway width:	14.0'		

## HISTORICAL DATA

erection date:	1908
erection cost:	\$372.00
designer:	unknown
fabricator :	unknown
contractor:	Robert Bates
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 122001.4; Carroll County Court Record, Book W: page 324 (4 May 1909 (July 1908 recapitulation)) - located at Carroll County Courthouse, Carrollton MO.
sign. rating:	32
evaluation:	NRHP non-eligible (typical, small-scale example of common structural type)

inventoried by: Clayton B. Fraser    17 October 1992

# Hurricane Creek Bridge

CARR17

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## GENERAL DATA

structure no.:	133000.7	city/town:	4.5 miles southeast of Hale
county:	Carroll	feature inters.:	Hurricane Creek
		cadastral grid:	S29/30, T55N, R21W
		highway route:	County Road 133
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	75.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.7'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1904
erection cost:	\$991.00
designer:	unknown
fabricator :	unknown
contractor:	Frank Yehle

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 133000.7; Carroll County Court Record, Book S: page 617 (7 June 1904) - located at Carroll County Courthouse, Carrollton MO.

sign. rating:	40
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Clayton B. Fraser    17 October 1992

# Burr Oak Creek Bridge

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CARR18

## GENERAL DATA

<b>structure no.:</b> 142000.2	<b>city/town:</b> 7.5 miles west of Mandeville
<b>county:</b> Carroll	<b>feature inters.:</b> Burr Oak Creek
	<b>cadastral grid:</b> S6/7, T54N, R25W
	<b>highway route:</b> County Road 142
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 30.0'	<b>alterations:</b> unknown
<b>total length:</b> 32.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 15.9'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1909
<b>erection cost:</b> \$371.00
<b>designer:</b> Midland Bridge Company, Kansas City MO
<b>fabricator :</b> Midland Bridge Company, Kansas City MO
<b>contractor :</b> Midland Bridge Company, Kansas City MO

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 142000.2; Carroll County Court Record, Book X: page 39 (14 May 1910 (November 1909 recapitulation)) - located at Carroll County Courthouse, Carrollton MO.

<b>sign. rating:</b> 36
<b>evaluation:</b> NRHP non-eligible (typical, small-scale example of common structural type)

**inventoried by:** Clayton B. Fraser    21 October 1992

# Bridge

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CARR19

## GENERAL DATA

<b>structure no.:</b>	144000.3	<b>city/town:</b>	5.5 miles northwest of Mandeville
<b>county:</b>	Carroll	<b>feature inters.:</b>	branch of Turkey Creek
		<b>cadastral grid:</b>	S33, T55N, R25W / S4, T54N, R25W
		<b>highway route:</b>	County Road 144
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Carroll County

## STRUCTURAL DATA

<b>superstructure:</b>	steel stringer	<b>condition:</b>	fair
<b>substructure:</b>	unknown	<b>alterations:</b>	unknown
<b>span number:</b>	1	<b>floor/decking :</b>	unknown
<b>span length:</b>	30.0'	<b>other features:</b>	unknown
<b>total length:</b>	30.0'		
<b>roadway width:</b>	15.9'		

## HISTORICAL DATA

**erection date:** 1908  
**erection cost:** \$445.00  
**designer:** unknown  
**fabricator :** unknown  
**contractor :** R.E. Bates

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 144000.3; Carroll County Court Record, Book W: page 326 (4 May 1909 (January 1909 recapitulation)) - located at Carroll County Courthouse, Carrollton MO.

**sign. rating:** 32  
**evaluation:** NRHP non-eligible (typical, small-scale example of common structural type)

**inventoried by:** Clayton B. Fraser    21 October 1992

# Turkey Creek Bridge

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CARR20

## GENERAL DATA

structure no.:	160000.3	city/town:	3.5 miles west of Mandeville
county:	Carroll	feature inters.:	tributary of Turkey Creek
		cadastral grid:	S14/15, T54N, R25W
		highway route:	County Road 16
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel, rigid-connected Warren bedstead		
substructure:	unknown		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	41.0'	floor/decking :	unknown
roadway width:	15.5'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1910
erection cost:	\$722.00
designer:	unknown
fabricator :	unknown
contractor:	R.E. Bates

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 160000.3; Carroll County Court Record, Book Y: page 394 (May 1911 (November 1910 recapitulation)) - located at Carroll County Courthouse, Carrollton MO; S.K. Turner and S.A. Clark, **Twentieth Century History of Carroll County, Missouri** (Indianapolis: B.F. Bowen & Company, 1911), page 195.

sign. rating:	38
evaluation:	NRHP non-eligible (typically configured example of small-scale truss construction)

inventoried by: Clayton B. Fraser    21 October 1992

# Turkey Creek Bridge

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CARR21

## GENERAL DATA

<b>structure no.:</b> 169000.5	<b>city/town:</b> 5.0 miles southwest of Mandeville
<b>county:</b> Carroll	<b>feature inters.:</b> Turkey Creek
	<b>cadastral grid:</b> S27/34, T54N, R25W
	<b>highway route:</b> County Road 169
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt bedstead	
<b>substructure:</b> steel pile bent abutments with timber plank wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 40.0'	<b>alterations:</b> unknown
<b>total length:</b> 78.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.1'	<b>other features:</b> steel angle guardrails

## HISTORICAL DATA

<b>erection date:</b> 1905
<b>erection cost:</b> \$435.00
<b>designer:</b> unknown
<b>fabricator :</b> unknown
<b>contractor:</b> Frank Yehle

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 169000.5; Carroll County Court Record, Book T: pages 432-33 (3 May 1905) - located at Carroll County Courthouse, Carrollton MO.

<b>sign. rating:</b> 37
<b>evaluation:</b> NRHP non-eligible (typically configured example of small-scale truss construction)

**inventoried by:** Clayton B. Fraser    21 October 1992

# Bridge

CARR22

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## GENERAL DATA

structure no.:	177001.6	city/town:	1.5 miles northwest of Mandeville
county:	Carroll	feature inters.:	tributary of Turkey Creek
		cadastral grid:	S6/7, T54N, R24W
		highway route:	County Road 177
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	24.0'	other features:	unknown
total length:	24.0'		
roadway width:	16.2'		

## HISTORICAL DATA

erection date: 1909  
erection cost: \$295.00  
designer: unknown  
fabricator : unknown  
contractor : R.E. Bates

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 177001.6; Carroll County Court Record, Book X: page 41 (4 May 1910 (March 1910 recapitulation)) - located at Carroll County Courthouse, Carrollton MO; S.K. Turner and S.A. Clark, *Twentieth Century History of Carroll County, Missouri* (Indianapolis: B.F. Bowen & Company, 1911), page 194.

sign. rating: 32  
evaluation: NRHP non-eligible (typical, small-scale example of common structural type)

inventoried by: Clayton B. Fraser 22 October 1992

# Bridge

CARR23

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## GENERAL DATA

<b>structure no.:</b> 196R00.4	<b>city/town:</b> 2.5 miles south of Mandeville
<b>county:</b> Carroll	<b>feature inters.:</b> tributary of Turkey Creek
	<b>cadastral grid:</b> S29, T54N, R24W
	<b>highway route:</b> County Road 196
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

**superstructure:** steel, 3-panel, rigid-connected Warren bedstead  
**substructure:** steel pile bent abutments and piers with timber plank wingwalls

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 40.0'	<b>alterations:</b> unknown
<b>total length:</b> 62.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 15.9'	<b>other features:</b> steel angle guardrails

## HISTORICAL DATA

**erection date:** 1909  
**erection cost:** \$675.00 (approximate cost)  
**designer:** Kansas City Bridge Company, Kansas City MO  
**fabricator :** Kansas City Bridge Company, Kansas City MO  
**contractor:** Kansas City Bridge Company, Kansas City MO

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 196R00.4; Carroll County Court Record, Book X: page 38 (4 May 1910 (July 1909 recapitulation)) - located at Carroll County Courthouse, Carrollton MO.

**sign. rating:** 49  
**evaluation:** NRHP non-eligible (typically configured example of small-scale truss construction)

**inventoried by:** Clayton B. Fraser    22 October 1992

# Bridge

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CARR24

## GENERAL DATA

structure no.:	202000.1	city/town:	2.5 miles southeast of Mandeville
county:	Carroll	feature inters.:	branch of Tater Hill Creek
		cadastral grid:	S15, T54N, R24W
		highway route:	County Road 202
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	26.0'	other features:	unknown
total length :	26.0'		
roadway width:	16.3'		

## HISTORICAL DATA

erection date: 1910  
erection cost: unknown  
designer: unknown  
fabricator : unknown  
contractor: R.E. Bates

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 202000.1; Carroll County Court Record, Book Y: page 392 (May 1911 (May 1910 recapitulation)) - located at Carroll County Courthouse, Carrollton MO.

sign. rating: 30  
evaluation: NRHP non-eligible (undistinguished, small-scale example of common structural type)

inventoried by: Clayton B. Fraser 22 October 1992

# Big Creek Bridge

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CARR26

## GENERAL DATA

structure no.:	214002.1	city/town:	1.0 mile south of Tina
county:	Carroll	feature inters.:	Big Creek
		cadastral grid:	S1/2, T54N, R23W
		highway route:	County Road 214
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, rigid-connected Pratt pony truss, skewed		
substructure:	unknown		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	101.0'	floor/decking :	concrete deck over steel stringers
roadway width:	15.1'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1919
erection cost:	\$2998.00
designer:	Kansas City Bridge Company, Kansas City MO
fabricator :	Kansas City Bridge Company, Kansas City MO
contractor :	Kansas City Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 214002.1; Carroll County Court Record, Book Z: page 157 (1 September 1919), page 177 (3 November 1919) - located at Carroll County Courthouse, Carrollton MO.
sign. rating:	72
evaluation:	NRHP eligible (typical example of common structural type, noteworthy for its skewed configuration)

inventoried by: Clayton B. Fraser    22 October 1992

# Bridge

CARR27

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## GENERAL DATA

structure no.:	224001.0	city/town:	3.0 miles southwest of Tina
county:	Carroll	feature inters.:	tributary of Big Creek
		cadastral grid:	S4/5, T54N, R23W
		highway route:	County Road 224
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, rigid-connected Warren pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	48.0'	alterations:	unknown
total length:	49.0'	floor/decking :	concrete deck over steel stringers
roadway width:	15.1'	other features:	lattice guardrails

## HISTORICAL DATA

erection date:	1908
erection cost:	\$424.00
designer:	Midland Bridge Company, Kansas City MO
fabricator :	Midland Bridge Company, Kansas City MO
contractor :	Midland Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 224001.0; Carroll County Court Record, Book V: page 495 (3 March 1908) - located at Carroll County Courthouse, Carrollton MO.
sign. rating:	46
evaluation:	NRHP non-eligible (early example of common structural type)

inventoried by: Clayton B. Fraser    22 October 1992

# Bridge

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CARR28

## GENERAL DATA

<b>structure no.:</b> 230000.1	<b>city/town:</b> 2.0 miles north of Bogard
<b>county:</b> Carroll	<b>feature inters.:</b> tributary of Big Creek
	<b>cadastral grid:</b> S18, T54N, R23W
	<b>highway route:</b> County Road 23
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt pony truss	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 50.0'	<b>alterations:</b> unknown
<b>total length:</b> 63.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 16.0'	<b>other features:</b> lattice guardrails

## HISTORICAL DATA

<b>erection date:</b> 1909	
<b>erection cost:</b> \$770.00	
<b>designer:</b> Leavenworth Bridge Company, Leavenworth KS	
<b>fabricator :</b> Leavenworth Bridge Company, Leavenworth KS	
<b>contractor:</b> Leavenworth Bridge Company, Leavenworth KS	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 230000.1; Carroll County Court Record, page 37 (4 May 1910), May 1909 recapitulation - located at Carroll County Courthouse, Carrollton MO.	
<b>sign. rating:</b> 35	
<b>evaluation:</b> NRHP non-eligible (typically configured example of common structural type)	

**inventoried by:** Clayton B. Fraser    22 October 1992

# Shootman Creek Bridge

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CARR29

## GENERAL DATA

structure no.:	238000.8	city/town:	1.5 miles east of Bogard
county:	Carroll	feature inters.:	Shootman Creek
		cadastral grid:	S28/29, T54N, R23W
		highway route:	County Road 239
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	30.0'	other features:	unknown
total length:	30.0'		
roadway width:	16.8'		

## HISTORICAL DATA

erection date:	1914
erection cost:	\$524.00
designer:	unknown
fabricator :	unknown
contractor:	M. E. Grant

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 238000.8; Carroll County Court Record, Book Z: page 240 (7 December 1914) - located at Carroll County Courthouse, Carrollton Mo.

sign. rating:	30
evaluation:	NRHP non-eligible (undistinguished, small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser    23 October 1992

# Shootman Creek Bridge

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CARR30

## GENERAL DATA

<b>structure no.:</b>	247000.9	<b>city/town:</b>	3.5 miles east of Bogard
<b>county:</b>	Carroll	<b>feature inters.:</b>	Shootman Creek
		<b>cadastral grid:</b>	S34, T54N, R23W / S3, T53N, R23W
		<b>highway route:</b>	County Road 247
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Carroll County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 4-panel, pin-connected Pratt bedstead truss		
<b>substructure:</b>	steel pile bent abutments with timber back- and wingwalls		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	60.0'	<b>alterations:</b>	one non-original, rectangular eyebar counter has been welded to each of the four middle panels (eye cut off); legs also cut off
<b>total length:</b>	60.0'	<b>floor/decking :</b>	concrete deck over steel stringers
<b>roadway width:</b>	15.0'	<b>other features:</b>	upper chord and upright end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: looped square eyebar with slotted turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: lattice

## HISTORICAL DATA

<b>erection date:</b>	1908
<b>erection cost:</b>	\$1082.00
<b>designer:</b>	Midland Bridge Company, Kansas City MO
<b>fabricator :</b>	Midland Bridge Company, Kansas City MO; Jones and Laughlin Steel Company, Pittsburgh PA
<b>contractor :</b>	Midland Bridge Company, Kansas City MO
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 247000.9; Carroll County Court Record, Book V: page 495 (3 March 1908), page 325 (4 May 1909), September 1908 recapitulation - located at Carroll County Courthouse, Carrollton MO; field inspection, by Clayton Fraser, 2 October 1991.
<b>sign. rating:</b>	42
<b>evaluation:</b>	NRHP non-eligible (typically configured example of common structural type, altered)

inventoried by: Clayton B. Fraser    23 October 1992

# Hurricane Creek Bridge

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CARR31

## GENERAL DATA

structure no.:	256000.5	city/town:	3.0 miles north of Bosworth
county:	Carroll	feature inters.:	Hurricane Creek
		cadastral grid:	S1/12, T54N, R22W
		highway route:	County Road 256
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	24.0'	other features:	unknown
total length:	24.0'		
roadway width:	16.1'		

## HISTORICAL DATA

erection date:	1910
erection cost:	\$285.00
designer:	unknown
fabricator :	unknown
contractor:	R.E. Bates
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 256000.5; S.K. Turner and S.A. Clark, <i>Twentieth Century History of Carroll County, Missouri</i> (Indianapolis: B.F. Bowen & Company, 1911), page 196.
sign. rating:	30
evaluation:	NRHP non-eligible (small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser    23 October 1992

# Wolf Creek Bridge

CARR32

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## GENERAL DATA

<b>structure no.:</b>	261000.7	<b>city/town:</b>	4.0 miles northwest of Bosworth
<b>county:</b>	Carroll	<b>feature inters.:</b>	Wolf Creek
		<b>cadastral grid:</b>	S17/18, T54N, R22W
		<b>highway route:</b>	County Road 261
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Carroll County

## STRUCTURAL DATA

<b>superstructure:</b>	steel stringer	<b>condition:</b>	fair
<b>substructure:</b>	unknown	<b>alterations:</b>	unknown
<b>span number:</b>	1	<b>floor/decking :</b>	unknown
<b>span length:</b>	35.0'	<b>other features:</b>	unknown
<b>total length:</b>	35.0'		
<b>roadway width:</b>	16.1'		

## HISTORICAL DATA

<b>erection date:</b>	1910
<b>erection cost:</b>	\$440.00
<b>designer:</b>	unknown
<b>fabricator :</b>	unknown
<b>contractor:</b>	R.E. Bates
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 261000.7; Carroll County Court Record, page 393 (May 1911), September 1910 recapitulation - located at Carroll County Courthouse, Carrollton MO.
<b>sign. rating:</b>	30
<b>evaluation:</b>	NRHP non-eligible (small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser    23 October 1992

# AT&SF Railroad Overpass

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CARR34

## GENERAL DATA

<b>structure no.:</b> 274000.1	<b>city/town:</b> 0.5 mile southwest of Bosworth
<b>county:</b> Carroll	<b>feature inters.:</b> Atchison, Topeka & Santa Fe Railroad
	<b>cadastral grid:</b> S27/34, T54N, R22W
	<b>highway route:</b> County Road 274
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt pony truss, with timber stringer approach spans	
<b>substructure:</b> concrete abutments, wingwalls and piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 80.0'	<b>alterations:</b> unknown
<b>total length:</b> 209.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 14.6'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1906	
<b>erection cost:</b> unknown	
<b>designer:</b> unknown	
<b>fabricator :</b> unknown	
<b>contractor:</b> Atchison, Topeka & Santa Fe Railroad	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 274000.1; Carroll County Court Record, Book U: page 518 (1 October 1906) - located at Carroll County Courthouse, Carrollton MO.	
<b>sign. rating:</b> 43	
<b>evaluation:</b> NRHP non-eligible (undistinguished example of common structural type)	

**inventoried by:** Clayton B. Fraser    23 October 1992

# Bridge

CARR35

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## GENERAL DATA

structure no.:	286000.9	city/town:	2.0 miles northeast of Bosworth
county:	Carroll	feature inters.:	branch of Grand River
		cadastral grid:	S17/18, T54N, R21W
		highway route:	County Road 286
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	32.0'	other features:	unknown
total length:	32.0'		
roadway width:	16.0'		

## HISTORICAL DATA

erection date: 1909  
erection cost: unknown  
designer: unknown  
fabricator : unknown  
contractor: R.E. Bates

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 286000.9; Carroll County Court Record, page 327 (4 May 1909), April 1909 recapitulation - located at Carroll County Courthouse, Carrollton MO.

sign. rating: 32  
evaluation: NRHP non-eligible (undistinguished, small-scale example of common structural type)

inventoried by: Clayton B. Fraser    23 October 1992

# Bridge

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CARR36

## GENERAL DATA

structure no.:	289000.6	city/town:	2.5 miles east of Bosworth
county:	Carroll	feature inters.:	branch of Grand River
		cadastral grid:	S20, T54N, R21W
		highway route:	County Road 289
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	30.0'	other features:	unknown
total length:	30.0'		
roadway width:	16.0'		

## HISTORICAL DATA

erection date:	1911
erection cost:	\$222.00
designer:	Kansas City Bridge Company, Kansas City MO
fabricator :	Kansas City Bridge Company, Kansas City MO
contractor :	Kansas City Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 289000.6; Carroll County Court Record, page 396 (May 1911), March 1911 recapitulation - located at Carroll County Courthouse, Carrollton MO.
sign. rating:	30
evaluation:	NRHP non-eligible (undistinguished, small-scale example of common structural type)

inventoried by: Clayton B. Fraser    23 October 1992

# Sand Branch Bridge

CARR37

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## GENERAL DATA

<b>structure no.:</b> 292R00.1	<b>city/town:</b> 1.0 mile southeast of Bosworth
<b>county:</b> Carroll	<b>feature inters.:</b> Sand Branch
	<b>cadastral grid:</b> S30/31, T54N, R21W / S25/36, T54N, R22W
	<b>highway route:</b> County Road 292
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 26.0'	<b>alterations:</b> unknown
<b>total length:</b> 26.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 17.9'	<b>other features:</b> unknown

## HISTORICAL DATA

**erection date:** 1909  
**erection cost:** unknown  
**designer:** unknown  
**fabricator :** unknown  
**contractor:** R.E. Bates

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 292R00.1; Carroll County Court Record, page 327 (4 May 1909), April 1909 recapitulation - located at Carroll County Courthouse, Carrollton MO.

**sign. rating:** 32  
**evaluation:** NRHP non-eligible (undistinguished, small-scale example of common structural type)

**inventoried by:** Clayton B. Fraser    23 October 1992

# Sand Branch Bridge

CARR38

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## GENERAL DATA

structure no.:	293001.4	city/town:	2.0 miles southeast of Bosworth
county:	Carroll	feature inters.:	Sand Branch
		cadastral grid:	S31/32, T54N, R21W
		highway route:	County Road 293
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	32.0'	other features:	unknown
total length :	32.0'		
roadway width:	16.3'		

## HISTORICAL DATA

erection date:	1910
erection cost:	\$781.26
designer:	unknown
fabricator :	unknown
contractor :	R.E. Bates
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 293001.4; Carroll County Court Record, page 393 (May 1911), August 1910 recapitulation - located at Carroll County Courthouse, Carrollton MO.
sign. rating:	30
evaluation:	NRHP non-eligible (undistinguished, small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser    23 October 1992

# Turkey Creek Bridge

CARR39

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## GENERAL DATA

<b>structure no.:</b> 300000.8	<b>city/town:</b> 5.5 miles southwest of Mandeville
<b>county:</b> Carroll	<b>feature inters.:</b> Turkey Creek
	<b>cadastral grid:</b> S34, T54N, R25W / S3, T53N, R25W
	<b>highway route:</b> County Road 30
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

**superstructure:** steel, 3-panel, pin-connected Pratt bedstead truss  
**substructure:** steel pile bent abutments and piers, with timber wingwalls

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 50.0'	<b>alterations:</b> unknown
<b>total length:</b> 85.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.3'	<b>other features:</b> steel angle guardrails

## HISTORICAL DATA

**erection date:** 1908  
**erection cost:** \$750.00  
**designer:** unknown  
**fabricator :** unknown  
**contractor:** Frank Yehle

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 300000.8; Carroll County Court Record, page 324 (4 May 1909), May 1908 recapitulation - located at Carroll County Courthouse, Carrollton MO.

**sign. rating:** 39  
**evaluation:** NRHP non-eligible (typically configured example of common structural type)

**inventoried by:** Clayton B. Fraser    23 October 1992

# Wakenda Creek Bridge

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CARR40

## GENERAL DATA

<b>structure no.:</b> 310001.5	<b>city/town:</b> 7.0 miles northwest of Norborne
<b>county:</b> Carroll	<b>feature inters.:</b> East Fork of Wakenda Creek
	<b>cadastral grid:</b> S19/20, T53N, R25W
	<b>highway route:</b> County Road 31
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt pony truss	
<b>substructure:</b> concrete-filled steel cylinder piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 70.0'	<b>alterations:</b> unknown
<b>total length:</b> 117.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> steel angle guardrails

## HISTORICAL DATA

<b>erection date:</b> 1909	
<b>erection cost:</b> unknown	
<b>designer:</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>fabricator :</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>contractor :</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 310001.5; Carroll County Court Record, page 39 (14 May 1910), September 1909 recapitulation - located at Carroll County Courthouse, Carrollton MO.	
<b>sign. rating:</b> 42	
<b>evaluation:</b> NRHP non-eligible (typically configured example of common structural type)	

inventoried by: Clayton B. Fraser    23 October 1992

# Wakenda Creek Bridge

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CARR41

## GENERAL DATA

<b>structure no.:</b> 313001.3	<b>city/town:</b> 6.0 miles northwest of Norborne
<b>county:</b> Carroll	<b>feature inters.:</b> East Fork of Wakenda Creek
	<b>cadastral grid:</b> S20/29, T53N, R25W
	<b>highway route:</b> County Road 313
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

**superstructure:** steel, 7-panel, rigid-connected Camelback pony truss  
**substructure:** steel pile bent, concrete back and wingwalls

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 125.0'	<b>alterations:</b> none
<b>total length:</b> 127.0'	<b>floor/decking :</b> concrete deck over steel stringers
<b>roadway width:</b> 16.0'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord, vertical, diagonal: wide flange; middle panel diagonal: 2 channels with batten plates; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: 2 angles; welding on hip cover plate: <b>Carroll County Court</b> , and names of officials, county engineer, construction foreman; welding on inside of hip channel: <b>1946</b>

## HISTORICAL DATA

**erection date:** 1946  
**erection cost:** unknown  
**designer:** unknown  
**fabricator :** Inland Steel Company, East Chicago IN  
**contractor:** unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 313001.3; field inspection by Clayton Fraser, 2 October 1991.

**sign. rating:** 41  
**evaluation:** NRHP non-eligible (longest-span example of uncommon Pratt truss sub-type, built relatively late in Missouri's bridge building milieu)

**inventoried by:** Clayton B. Fraser    23 October 1992

# Turkey Creek Bridge

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CARR42

## GENERAL DATA

<b>structure no.:</b> 322000.7	<b>city/town:</b> 4.0 miles northeast of Norborne
<b>county:</b> Carroll	<b>feature inters.:</b> Turkey Creek Ditch
	<b>cadastral grid:</b> S31, T53N, R24W / S36, T53N, R25W
	<b>highway route:</b> County Road 322
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt half-hip pony truss	
<b>substructure:</b> concrete-filled steel cylinders with concrete beams on top	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 60.0'	<b>alterations:</b> unknown
<b>total length:</b> 88.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 10.8'	<b>other features:</b> lattice guardrails

## HISTORICAL DATA

<b>erection date:</b> 1899
<b>erection cost:</b> \$655.00
<b>designer:</b> Kansas City Bridge Company, Kansas City MO
<b>fabricator :</b> Kansas City Bridge Company, Kansas City MO
<b>contractor :</b> Kansas City Bridge Company, Kansas City MO
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 322000.7; Carroll County Court Record, Book Q: page 267 (1 May 1900), September 1899 recapitulation - located at Carroll County Courthouse, Carrollton Mo.
<b>sign. rating:</b> 46
<b>evaluation:</b> NRHP non-eligible (typically configured example of common structural type)

**inventoried by:** Clayton B. Fraser    23 October 1992

# McCroskie Creek Bridge

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CARR43

## GENERAL DATA

structure no.:	330000.5	city/town:	3.0 miles southwest of Bogard
county:	Carroll	feature inters.:	McCroskie Creek
		cadastral grid:	S2/3, T53N, R24W
		highway route:	County Road 33
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	36.0'	other features:	unknown
total length:	36.0'		
roadway width:	18.0'		

## HISTORICAL DATA

erection date:	1912
erection cost:	\$638.20
designer:	unknown
fabricator :	unknown
contractor:	R.E. Bates

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 330000.5; Carroll County Court Record, Book Y: pages 426-430 (8 May 1913), December 1912 recapitulation - located at Carroll County Courthouse, Carrollton MO.

sign. rating:	30
evaluation:	NRHP non-eligible (undistinguished example of small-scale structural type)

inventoried by: Clayton B. Fraser    23 October 1992

# McGill Creek Bridge

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CARR44

## GENERAL DATA

<b>structure no.:</b> 336000.1	<b>city/town:</b> 4.0 miles northwest of Carrollton
<b>county:</b> Carroll	<b>feature inters.:</b> McGill Creek
	<b>cadastral grid:</b> S23, T53N, R24W
	<b>highway route:</b> County Road 336
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 20.0'	<b>alterations:</b> unknown
<b>total length:</b> 21.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 16.0'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1909	
<b>erection cost:</b> unknown	
<b>designer:</b> Kansas City Bridge Company, Kansas City MO	
<b>fabricator :</b> Kansas City Bridge Company, Kansas City MO	
<b>contractor:</b> Kansas City Bridge Company, Kansas City MO	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 336000.1; Carroll County Court Record, page 327 (4 May 1909), April 1909 recapitulation - located at Carroll County Courthouse, Carrollton MO.
<b>sign. rating:</b> 36	
<b>evaluation:</b>	NRHP non-eligible (undistinguished example of small-scale structural type)

**inventoried by:** Clayton B. Fraser    23 October 1992

# McGill Creek Bridge

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CARR45

## GENERAL DATA

<b>structure no.:</b> 337R00.2	<b>city/town:</b> 4.0 miles northwest of Carrollton
<b>county:</b> Carroll	<b>feature inters.:</b> McGill Creek
	<b>cadastral grid:</b> S23, T53N, R24W
	<b>highway route:</b> County Road 337
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 6-panel, rigid-connected Pratt bedstead truss	
<b>substructure:</b> steel pile bent piers and abutments with timber back- and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 33.0'	<b>alterations:</b> substructure replaced
<b>total length:</b> 51.0'	<b>floor/decking :</b> concrete deck over steel stringers
<b>roadway width:</b> 14.9'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1904	
<b>erection cost:</b> \$490.00	
<b>designer:</b> Interstate Bridge Company, Kansas City MO	
<b>fabricator :</b> Interstate Bridge Company, Kansas City MO	
<b>contractor :</b> Interstate Bridge Company, Kansas City MO	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 337R00.2; Carroll County Court Record, Book T: pages 7-8 (5 July 1904), pages 432-2 (3 May 1905) - located at Carroll County Courthouse, Carrollton MO.	
<b>sign. rating:</b> 44	
<b>evaluation:</b> NRHP non-eligible (early example of small-scale Pratt truss subtype, altered)	

**inventoried by:** Clayton B. Fraser    23 October 1992

# McCroskie Creek Bridge

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CARR46

## GENERAL DATA

structure no.:	344000.5	city/town:	7.0 miles northwest of Carrollton
county:	Carroll	feature inters.:	McCroskie Creek
		cadastral grid:	S20, T53N, R24W
		highway route:	County Road 344
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	40.0'	floor/decking :	timber deck over steel stringers
roadway width:	16.0'	other features:	lattice guardrails

## HISTORICAL DATA

erection date:	1910
erection cost:	\$800.00
designer:	unknown
fabricator :	unknown
contractor:	R.G. Culbertson, Hale MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 344000.5; Carroll County Court Record, page 392 (May 1911), June 1910 recapitulation - located at Carroll County Courthouse, Carrollton MO.
sign. rating:	30
evaluation:	NRHP non-eligible (typically configured, relatively late example of common structural type)

inventoried by: Clayton B. Fraser    23 October 1992

# Fish Pond Creek Bridge

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CARR47

## GENERAL DATA

<b>structure no.:</b> 349000.3	<b>city/town:</b> 7.0 miles northwest of Carrollton
<b>county:</b> Carroll	<b>feature inters.:</b> Fish Pond Creek
	<b>cadastral grid:</b> S30, T53N, R24W
	<b>highway route:</b> County Road 349
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 36.0'	<b>alterations:</b> unknown
<b>total length:</b> 36.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 14.0'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1911	
<b>erection cost:</b> \$413.00	
<b>designer:</b> Kansas City Bridge Company, Kansas City MO	
<b>fabricator :</b> Kansas City Bridge Company, Kansas City MO	
<b>contractor:</b> Kansas City Bridge Company, Kansas City MO	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 349000.3; Carroll County Court Record, page 396 (May 1911), March 1911 recapitulation - located at Carroll County Courthouse, Carrollton MO.
<b>sign. rating:</b> 34	
<b>evaluation:</b>	NRHP non-eligible (undistinguished, small-scale example of exceedingly common structural type)

**inventoried by:** Clayton B. Fraser    23 October 1992

# McCroskie Creek Bridge

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CARR48

## GENERAL DATA

<b>structure no.:</b> 349001.2	<b>city/town:</b> 7.0 miles northwest of Carrollton
<b>county:</b> Carroll	<b>feature inters.:</b> McCroskie Creek
	<b>cadastral grid:</b> S30/31, T53N, R24W
	<b>highway route:</b> County Road 349
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 30.0'	<b>alterations:</b> unknown
<b>total length:</b> 31.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 16.0'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1918	
<b>erection cost:</b> unknown	
<b>designer:</b> unknown	
<b>fabricator :</b> unknown	
<b>contractor:</b> unknown	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 349001.2.
<b>sign. rating:</b> 24	
<b>evaluation:</b>	NRHP non-eligible (small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser 23 October 1992

# Bridge

CARR49

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## GENERAL DATA

structure no.:	358000.5	city/town:	3.0 miles northwest of Carrollton
county:	Carroll	feature inters.:	branch of Wakenda Creek
		cadastral grid:	S25/36, T53N, R24W
		highway route:	County Road 358
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	20.0'	other features:	unknown
total length:	22.0'		
roadway width:	16.0'		

## HISTORICAL DATA

erection date:	1909
erection cost:	\$260.00
designer:	unknown
fabricator :	unknown
contractor :	R.G. Culbertson, Hale MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 358000.5; Carroll County Court Record, page 40 (14 May 1910), December 1909 recapitulation - located at Carroll County Courthouse, Carrollton MO.
sign. rating:	32
evaluation:	NRHP non-eligible (small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser    23 October 1992

# Shootman Creek Bridge

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CARR50

## GENERAL DATA

<b>structure no.:</b> 365R02.3	<b>city/town:</b> 7.0 miles northeast of Carrollton
<b>county:</b> Carroll	<b>feature inters.:</b> Shootman Creek
	<b>cadastral grid:</b> S1, T53N, R23W / S6, T53N, R22W
	<b>highway route:</b> County Road 365
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, rigid-connected Pratt pony truss, with steel stringer approach span	
<b>substructure:</b> steel pile bent pier and abutments with timber wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 50.0'	<b>alterations:</b> substructure braced
<b>total length:</b> 75.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.3'	<b>other features:</b> upper chord and inclined end post: two channels with cover and batten plates; lower chord: two angles with batten plates; vertical: two channels with batten plates; diagonal: two angles with batten plates; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: lattice

## HISTORICAL DATA

<b>erection date:</b> 1911
<b>erection cost:</b> \$885.00
<b>designer:</b> unknown
<b>fabricator :</b> unknown
<b>contractor:</b> R.G. Culbertson, Hale MO

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 365R02.3; Carroll County Court Record, page 97 (4 June 1912), July/August 1911 recapitulation - located at Carroll County Courthouse, Carrollton MO; field inspection by Clayton Fraser, 2 October 1991.

<b>sign. rating:</b> 35
<b>evaluation:</b> NRHP non-eligible (relatively early example of mainstay structural type)

**inventoried by:** Clayton B. Fraser    24 October 1992

# Bridge

CARR51

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## GENERAL DATA

<b>structure no.:</b>	367000.7	<b>city/town:</b>	6.5 miles northeast of Carrollton
<b>county:</b>	Carroll	<b>feature inters.:</b>	branch of Shootman Creek
		<b>cadastral grid:</b>	S2, T53N, R23W
		<b>highway route:</b>	County Road 367
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Carroll County

## STRUCTURAL DATA

<b>superstructure:</b>	steel stringer	<b>condition:</b>	fair
<b>substructure:</b>	unknown	<b>alterations:</b>	unknown
<b>span number:</b>	1	<b>floor/decking :</b>	unknown
<b>span length:</b>	28.0'	<b>other features:</b>	unknown
<b>total length:</b>	28.0'		
<b>roadway width:</b>	16.0'		

## HISTORICAL DATA

**erection date:** 1908  
**erection cost:** \$298.00  
**designer:** Kansas City Bridge Company, Kansas City MO  
**fabricator :** Kansas City Bridge Company, Kansas City MO  
**contractor:** Kansas City Bridge Company, Kansas City MO

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 367000.7; Carroll County Court Record, Book V: page 394 (8 October 1907); Book W: pages 324-328 (4 May 1909), May 1908 recapitulation - located at Carroll County Courthouse, Carrollton MO.

**sign. rating:** 36  
**evaluation:** NRHP non-eligible (small-scale example of exceedingly common structural type)

**inventoried by:** Clayton B. Fraser    24 October 1992

# Bridge

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CARR54

## GENERAL DATA

structure no.:	388000.4	city/town:	4.0 miles northeast of Carrollton
county:	Carroll	feature inters.:	branch of Little Wakenda Creek
		cadastral grid:	S24/25, T53N, R23W
		highway route:	County Road 388
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	32.0'	other features:	unknown
total length:	32.0'		
roadway width:	17.8'		

## HISTORICAL DATA

erection date: 1909  
erection cost: \$845.00  
designer: unknown  
fabricator : unknown  
contractor: R.E. Bates

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 388000.4; Carroll County Court Record, page 41 (4 May 1910), April 1910 recapitulation; page 392 (May 1911), May 1910 recapitulation - located at Carroll County Courthouse, Carrollton MO; S.K. Turner and S.A. Clark, *Twentieth Century History of Carroll County, Missouri* (Indianapolis: B.F. Bowen & Company, 1911), page 194.

sign. rating: 32  
evaluation: NRHP non-eligible (small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser 24 October 1992

# Bridge

CARR55

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## GENERAL DATA

<b>structure no.:</b> 389000.6	<b>city/town:</b> 3.5 miles northeast of Carrollton
<b>county:</b> Carroll	<b>feature inters.:</b> branch of Little Wakenda Creek
	<b>cadastral grid:</b> S25/26, T53N, R23W
	<b>highway route:</b> County Road 389
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 28.0'	<b>alterations:</b> unknown
<b>total length:</b> 28.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 15.9'	<b>other features:</b> unknown

## HISTORICAL DATA

**erection date:** 1908  
**erection cost:** \$325.00  
**designer:** Kansas City Bridge Company, Kansas City MO  
**fabricator :** Kansas City Bridge Company, Kansas City MO  
**contractor:** Kansas City Bridge Company, Kansas City MO

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 389000.6; Carroll County Court Record, Book V: pages 222-224 (7 May 1907), pages 588-591 (5 May 1908), March 1908 recapitulation - located at Carroll County Courthouse, Carrollton MO.

**sign. rating:** 36  
**evaluation:** NRHP non-eligible (small-scale example of exceedingly common structural type)

**inventoried by:** Clayton B. Fraser    24 October 1992

# Little Wakenda Creek Bridge

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CARR56

## GENERAL DATA

structure no.:	393000.3	city/town:	2.0 miles north of Carrollton
county:	Carroll	feature inters.:	Little Wakenda Creek
		cadastral grid:	S27, T53N, R23W
		highway route:	County Road 393
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure: steel, 3-panel, rigid-connected Pratt pony truss  
substructure: unknown

span number:	1	condition:	fair
span length:	45.0'	alterations:	unknown
total length:	45.0'	floor/decking :	unknown
roadway width:	15.3'	other features:	lattice guardrails

## HISTORICAL DATA

erection date: 1911  
erection cost: \$790.00  
designer: unknown  
fabricator : unknown  
contractor: R.G. Culbertson, Hale MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 393000.3; Carroll County Court Record, page 96 (4 June 1912), July 1911 recapitulation - located at Carroll County Courthouse, Carrollton MO.

sign. rating: 30  
evaluation: NRHP non-eligible (relatively early example of mainstay structural type)

inventoried by: Clayton B. Fraser 24 October 1992

# Santa Fe Railroad Overpass

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CARR57

## GENERAL DATA

<b>structure no.:</b>	407000.1	<b>city/town:</b>	3.5 miles southwest of Bosworth
<b>county:</b>	Carroll	<b>feature inters.:</b>	Atchison, Topeka & Santa Fe Railroad
		<b>cadastral grid:</b>	S4, T53N, R22W
		<b>highway route:</b>	County Road 407
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Carroll County

## STRUCTURAL DATA

**superstructure:** steel, 9-panel, pin-connected Pratt through truss  
**substructure:** concrete abutments and wingwalls

<b>span number:</b>	1	<b>condition:</b>	good
<b>span length:</b>	160.0'	<b>alterations:</b>	truss moved to this location
<b>total length:</b>	162.0'	<b>floor/decking :</b>	timber deck with steel wearing plates over timber stringers
<b>roadway width:</b>	13.3'	<b>other features:</b>	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with lacing, (on outer panels), 4 punched rectangular eyebars (on center panels); vertical: 2 channels with continuous plate and lacing; hip vertical: 2 channels with batten plates; diagonal: 2 punched rectangular eyebars with unslotted sleeve bolts; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; portal strut: lattice; floor beam: riveted plate girder; guard-rail: timber

## HISTORICAL DATA

**erection date:** c1895  
**erection cost:** unknown  
**designer:** Edge Moor Iron Works, Wilmington DE  
**fabricator :** Edge Moor Iron Works, Wilmington DE  
**contractor:** Atchison, Topeka & Santa Fe Railroad

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 407000.1; field inspection by Clayton Fraser, 1 October 1991.

**sign. rating:** 31  
**evaluation:** NRHP non-eligible (salvaged railroad truss, moved to this location)

**inventoried by:** Clayton B. Fraser .24 October 1992

# Bridge

CARR58

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## GENERAL DATA

structure no.:	407000.6	city/town:	3.5 miles southwest of Bosworth
county:	Carroll	feature inters.:	branch of Turkey Creek
		cadastral grid:	S4/9, T53N, R22W
		highway route:	County Road 407
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	28.0'	other features:	unknown
total length:	28.0'		
roadway width:	16.2'		

## HISTORICAL DATA

erection date: 1911  
erection cost: \$365.00  
designer: unknown  
fabricator : unknown  
contractor: R.E. Bates

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 407000.6; Carroll County Court Record, page 96 (4 June 1912), June 1911 recapitulation - located at Carroll County Courthouse, Carrollton MO.

sign. rating: 30  
evaluation: NRHP non-eligible (small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser 24 October 1992

# Bridge

CARR59

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## GENERAL DATA

structure no.:	424000.4	city/town:	9.0 miles east of Carrollton
county:	Carroll	feature inters.:	branch of Big Creek
		cadastral grid:	S35/36, T53N, R22W
		highway route:	County Road 424
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	30.0'	other features:	unknown
total length:	30.0'		
roadway width:	16.0'		

## HISTORICAL DATA

erection date:	1910
erection cost:	\$845.63
designer:	unknown
fabricator :	unknown
contractor :	R.E. Bates

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 424000.4; Carroll County Court Record, page 327 (4 May 1909), April 1909 recapitulation; page 393 (May 1911), September 1910 recapitulation - located at Carroll County Courthouse, Carrollton MO; S.K. Turner and S.A. Clark, **Twentieth Century History of Carroll County, Missouri** (Indianapolis: B.F. Bowen & Company, 1911), page 195.

sign. rating:	30
evaluation:	NRHP non-eligible (small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser    24 October 1992

# Bridge

CARR60

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## GENERAL DATA

<b>structure no.:</b>	438000.1	<b>city/town:</b>	4.0 miles southeast of Bosworth
<b>county:</b>	Carroll	<b>feature inters.:</b>	branch of Big Creek
		<b>cadastral grid:</b>	S7/8, T53N, R21W
		<b>highway route:</b>	County Road 438
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Carroll County

## STRUCTURAL DATA

<b>superstructure:</b>	steel stringer	<b>condition:</b>	fair
<b>substructure:</b>	unknown	<b>alterations:</b>	unknown
<b>span number:</b>	1	<b>floor/decking :</b>	unknown
<b>span length:</b>	30.0'	<b>other features:</b>	unknown
<b>total length:</b>	30.0'		
<b>roadway width:</b>	16.0'		

## HISTORICAL DATA

<b>erection date:</b>	1908
<b>erection cost:</b>	\$331.00
<b>designer:</b>	unknown
<b>fabricator :</b>	unknown
<b>contractor:</b>	R.G. Culbertson, Hale MO
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 438000.1; Carroll County Court Record, page 324 (4 May 1909), July 1908 recapitulation - located at Carroll County Courthouse, Carrollton MO.
<b>sign. rating:</b>	32
<b>evaluation:</b>	NRHP non-eligible (small-scale example of exceedingly common structural type)

**inventoried by:** Clayton B. Fraser    24 October 1992

# Bridge

CARR61

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## GENERAL DATA

<b>structure no.:</b> 453002.7	<b>city/town:</b> 0.5 mile north of Dewitt
<b>county:</b> Carroll	<b>feature inters.:</b> tributary of Missouri River
	<b>cadastral grid:</b> S23, T53N, R21W
	<b>highway route:</b> County Road 453
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 28.0'	<b>alterations:</b> unknown
<b>total length:</b> 28.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 16.0'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1909	
<b>erection cost:</b> unknown	
<b>designer:</b> Kansas City Bridge Company, Kansas City MO	
<b>fabricator :</b> Kansas City Bridge Company, Kansas City MO	
<b>contractor:</b> Kansas City Bridge Company, Kansas City MO	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 453002.7; Carroll County Court Record, page 39 (14 May 1910), October 1909 recapitulation - located at Carroll County Courthouse, Carrollton MO.
<b>sign. rating:</b> 36	
<b>evaluation:</b>	NRHP non-eligible (small-scale example of exceedingly common structural type)

**inventoried by:** Clayton B. Fraser    24 October 1992

# Weatherholt Bridge

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CARR62

## GENERAL DATA

<b>structure no.:</b> 496000.6	<b>city/town:</b> 1.5 miles south of Norborne
<b>county:</b> Carroll	<b>feature inters.:</b> Moss Creek
	<b>cadastral grid:</b> S26/35, T52N, R25W
	<b>highway route:</b> County Road 496
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, rigid-connected Pratt pony truss	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 70.0'	<b>alterations:</b> unknown
<b>total length:</b> 72.0'	<b>floor/decking :</b> concrete deck over steel stringers
<b>roadway width:</b> 16.1'	<b>other features:</b> no guardrails

## HISTORICAL DATA

<b>erection date:</b> 1913	
<b>erection cost:</b> \$1724.00	
<b>designer:</b> unknown	
<b>fabricator :</b> unknown	
<b>contractor :</b> R.G. Culbertson, Hale MO	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 496000.6; Carroll County Court Record, Book Y: page 467 (3 June 1913); Book Z: page 139 (3 May 1914) - located at Carroll County Courthouse, Carrollton MO.
<b>sign. rating:</b> 38	
<b>evaluation:</b>	NRHP non-eligible (relatively early example of mainstay structural type)

**inventoried by:** Clayton B. Fraser    24 October 1992

# Moss Creek Bridge

CARR64

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## GENERAL DATA

structure no.:	514001.1	city/town:	5.0 miles southwest of Carrollton
county:	Carroll	feature inters.:	Moss Creek
		cadastral grid:	S15/22, T52N, R24W
		highway route:	County Road 514
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	unknown		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	92.0'	floor/decking :	unknown
roadway width:	14.9'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1905
erection cost:	\$675.00
designer:	unknown
fabricator :	unknown
contractor:	Frank Yehle

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 514001.1; Carroll County Court Record, Book T: page 180 (10 November 1904) - located at Carroll County Courthouse, Carrollton MO.

sign. rating:	40
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser    25 October 1992

# Bridge

CARR65

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## GENERAL DATA

<b>structure no.:</b> 523001.9	<b>city/town:</b> 5.0 miles southeast of Norborne
<b>county:</b> Carroll	<b>feature inters.:</b> tributary of Missouri River
	<b>cadastral grid:</b> S33, T52N, R24W
	<b>highway route:</b> County Road 523
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 32.0'	<b>alterations:</b> unknown
<b>total length:</b> 32.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 17.9'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1908	
<b>erection cost:</b> unknown	
<b>designer:</b> Kansas City Bridge Company, Kansas City MO	
<b>fabricator :</b> Kansas City Bridge Company, Kansas City MO	
<b>contractor :</b> Kansas City Bridge Company, Kansas City MO	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 523001.9; Carroll County Court Record, page 325 (4 May 1909) - located at Carroll County Courthouse, Carrollton MO.	
<b>sign. rating:</b> 36	
<b>evaluation:</b> NRHP non-eligible (small-scale example of exceedingly common structural type)	

Inventoried by: Clayton B. Fraser    25 October 1992

# Bridge

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CARR66

## GENERAL DATA

<b>structure no.:</b>	530001.8	<b>city/town:</b>	5.5 miles southeast of Norborne
<b>county:</b>	Carroll	<b>feature inters.:</b>	tributary of Missouri River
		<b>cadastral grid:</b>	S33/34, T52N, R24W
		<b>highway route:</b>	County Road 53
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Carroll County

## STRUCTURAL DATA

<b>superstructure:</b>	steel stringer	<b>condition:</b>	fair
<b>substructure:</b>	unknown	<b>alterations:</b>	unknown
<b>span number:</b>	1	<b>floor/decking :</b>	unknown
<b>span length:</b>	30.0'	<b>other features:</b>	unknown
<b>total length:</b>	30.0'		
<b>roadway width:</b>	17.9'		

## HISTORICAL DATA

<b>erection date:</b>	1908
<b>erection cost:</b>	unknown
<b>designer:</b>	Kansas City Bridge Company, Kansas City MO
<b>fabricator :</b>	Kansas City Bridge Company, Kansas City MO
<b>contractor:</b>	Kansas City Bridge Company, Kansas City MO
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 530001.8; Carroll County Court Record, pages 325-326 (4 May 1909) - located at Carroll County Courthouse, Carrollton MO.
<b>sign. rating:</b>	36
<b>evaluation:</b>	NRHP non-eligible (small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser 25 October 1992

# Miller Branch Bridge

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CARR67

## GENERAL DATA

structure no.:	547001.2	city/town:	3.5 miles east of Carrollton
county:	Carroll	feature inters.:	Miller Branch
		cadastral grid:	S1, T52N, R23W
		highway route:	County Road 547
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	27.0'	other features:	unknown
total length:	27.0'		
roadway width:	16.8'		

## HISTORICAL DATA

erection date:	1910
erection cost:	\$852.00
designer:	Kansas City Bridge Company, Kansas City MO
fabricator :	Kansas City Bridge Company, Kansas City MO
contractor :	Kansas City Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 547001.2; Carroll County Court Record, page 394 (May 1911), November 1910 recapitulation - located at Carroll County Courthouse, Carrollton MO.
sign. rating:	34
evaluation:	NRHP non-eligible (small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser    25 October 1992

# Bridge

CARR69

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## GENERAL DATA

<b>structure no.:</b> 559000.6	<b>city/town:</b> 4.5 miles southeast of Carrollton
<b>county:</b> Carroll	<b>feature inters.:</b> branch of Little Wakenda Creek
	<b>cadastral grid:</b> S7, T52N, R22W
	<b>highway route:</b> County Road 559
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 24.0'	<b>alterations:</b> unknown
<b>total length:</b> 24.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 16.0'	<b>other features:</b> unknown

## HISTORICAL DATA

**erection date:** 1909  
**erection cost:** unknown  
**designer:** Midland Bridge Company, Kansas City MO  
**fabricator :** Midland Bridge Company, Kansas City MO  
**contractor:** Midland Bridge Company, Kansas City MO

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 559000.6; Carroll County Court Record, page 38 (4 May 1910) - located at Carroll County Courthouse, Carrollton MO.

**sign. rating:** 36  
**evaluation:** NRHP non-eligible (small-scale example of exceedingly common structural type)

**inventoried by:** Clayton B. Fraser    25 October 1992

# Bridge

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CARR70

## GENERAL DATA

structure no.:	594001.3	city/town:	7.0 miles east of Carrollton
county:	Carroll	feature inters.:	branch of Camp Creek
		cadastral grid:	S3, T52N, R22W
		highway route:	County Road 594
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	26.0'	other features:	unknown
total length :	26.0'		
roadway width:	16.1'		

## HISTORICAL DATA

erection date: 1910  
erection cost: \$307.00  
designer: Kansas City Bridge Company, Kansas City MO  
fabricator : Kansas City Bridge Company, Kansas City MO  
contractor: Kansas City Bridge Company, Kansas City MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 594001.3; Carroll County Court Record, page 395 (May 1911), December 1910 recapitulation - located at Carroll County Courthouse, Carrollton MO; S.K. Turner and S.A. Clark, *Twentieth Century History of Carroll County, Missouri* (Indianapolis: B.F. Bowen & Company, 1911), page 195.

sign. rating: 34  
evaluation: NRHP non-eligible (small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser 25 October 1992

# Bridge

CARR71

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## GENERAL DATA

structure no.:	619000.4	city/town:	7.0 miles southeast of Norborne
county:	Carroll	feature inters.:	tributary of Missouri River
		cadastral grid:	S2/3, T51N, R24W
		highway route:	County Road 619
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	34.0'	other features:	unknown
total length:	34.0'		
roadway width:	16.3'		

## HISTORICAL DATA

erection date:	1909
erection cost:	\$465.00
designer:	Kansas City Bridge Company, Kansas City MO
fabricator :	Kansas City Bridge Company, Kansas City MO
contractor :	Kansas City Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 619000.4; Carroll County Court Record, page 39 (14 May 1910), September 1909 recapitulation - located at Carroll County Courthouse, Carrollton MO; S.K. Turner and S.A. Clark, <i>Twentieth Century History of Carroll County, Missouri</i> (Indianapolis: B.F. Bowen & Company, 1911), page 194.
sign. rating:	36
evaluation:	NRHP non-eligible (small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser    25 October 1992

# Bridge

CARR72

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## GENERAL DATA

structure no.:	621000.5	city/town:	8.0 miles southwest of Carrollton
county:	Carroll	feature inters.:	tributary of Missouri River
		cadastral grid:	S11/12, T51N, R24W
		highway route:	County Road 621
		highway distr.:	2
		current owner:	Carroll County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	28.0'	other features:	unknown
total length:	28.0'		
roadway width:	18.0'		

## HISTORICAL DATA

erection date: 1909  
erection cost: \$415.00  
designer: Kansas City Bridge Company, Kansas City MO  
fabricator : Kansas City Bridge Company, Kansas City MO  
contractor : Kansas City Bridge Company, Kansas City MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 621000.5; Carroll County Court Record, page 38 (4 May 1910), July 1909 recapitulation; page 39 (September 1909) - located at Carroll County Courthouse, Carrollton MO; S.K. Turner and S.A. Clark, **Twentieth Century History of Carroll County, Missouri** (Indianapolis: B.F. Bowen & Company, 1911), page 194.

sign. rating: 36  
evaluation: NRHP non-eligible (small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser 25 October 1992

# Bridge

CARR73

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## GENERAL DATA

<b>structure no.:</b> 623000.4	<b>city/town:</b> 7.0 miles southwest of Carrollton
<b>county:</b> Carroll	<b>feature inters.:</b> tributary of Missouri River
	<b>cadastral grid:</b> S7, T51N, R23W / S12, T51N, R24W
	<b>highway route:</b> County Road 623
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 28.0'	<b>alterations:</b> unknown
<b>total length:</b> 28.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 16.3'	<b>other features:</b> unknown

## HISTORICAL DATA

**erection date:** 1909  
**erection cost:** \$415.00  
**designer:** Kansas City Bridge Company, Kansas City MO  
**fabricator :** Kansas City Bridge Company, Kansas City MO  
**contractor:** Kansas City Bridge Company, Kansas City MO

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 623000.4; Carroll County Court Record, pages 38-39 (4 May 1910), July/September 1909 recapitulation - located at Carroll County Courthouse, Carrollton MO.

**sign. rating:** 36  
**evaluation:** NRHP non-eligible (small-scale example of exceedingly common structural type)

**inventoried by:** Clayton B. Fraser    25 October 1992

# County Line Bridge

CARR74

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## GENERAL DATA

<b>structure no.:</b> 640000.2	<b>city/town:</b> 5.5 miles northwest of Norborne
<b>county:</b> Carroll	<b>feature inters.:</b> West Fork of Wakenda Creek
	<b>cadastral grid:</b> S6, T52N, R25W / S1, T52N, R26W
	<b>highway route:</b> County Road 64
	<b>highway distr.:</b> 2
	<b>current owner:</b> Carroll County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt pony truss	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 80.0'	<b>alterations:</b> unknown
<b>total length:</b> 80.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 15.2'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1906
<b>erection cost:</b> \$1647.66
<b>designer:</b> Kansas City Bridge Company, Kansas City MO
<b>fabricator :</b> Kansas City Bridge Company, Kansas City MO
<b>contractor:</b> Kansas City Bridge Company, Kansas City MO
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 640000.2; Carroll County Court Record, Book V: pages 222-224 (7 May 1907), November 1906 recapitulation; pages 588-591 (5 May 1908), September 1907 recapitulation - located at Carroll County Courthouse, Carrollton Mo.
<b>sign. rating:</b> 46
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)

Inventoried by: Clayton B. Fraser 25 October 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Carrollton Bridge  
MHTD: F 404

CARR01

**DATE(S) OF CONSTRUCTION**

1919

**LOCATION**

Business Route 65 over Wakenda Creek; S5, T52N, R23W  
Carrollton; Carroll County, Missouri

**USE (ORIGINAL / CURRENT)**

highway bridge / highway bridge

**RATING** NRHP possibly eligible (score: 56)

**CONDITION**

good

**OWNER**

Missouri Highway and Transportation Department

span number: 1

superstructure: steel, 6-panel, rigid-connected Warren pony truss

span length: 100.0'

substructure: concrete abutments and wingwalls

total length: 105.0'

floor/decking: concrete deck over steel stringers

roadway wdt.: 18.6'

other features: steel guardrails

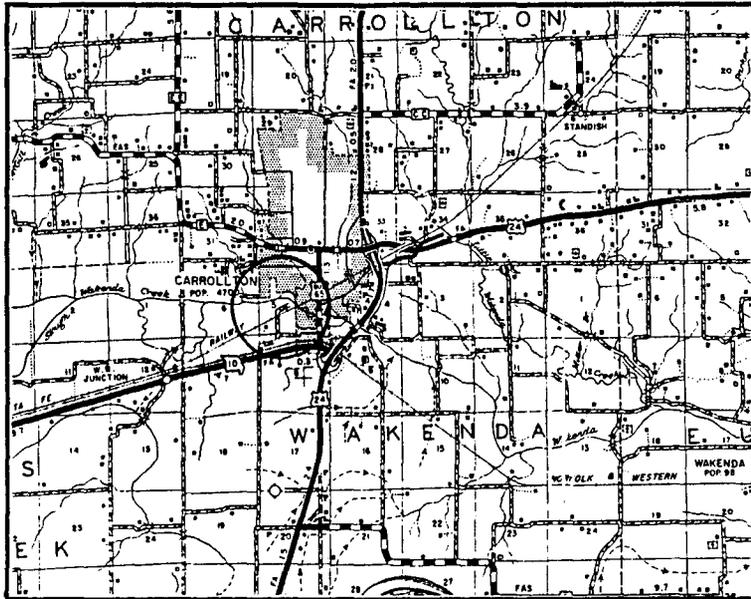
Located in Carrollton, the Carroll County seat, this long-span pony truss carries Business Route 65 over Wakenda Creek. The superstructure consists of a rigid-connected Warren pony truss carried by a concrete substructure. The Carrollton Bridge dates to 1919. In August of that year the Carroll County Court solicited competitive bids for a 100-foot truss to replace an existing covered bridge over Wakenda Creek. On September 2nd, the county contracted with the Kansas City Bridge Company to fabricate and erect the truss for \$16,000.00. Whether the truss was designed by KCBCo, the county engineer, or the Missouri State highway Department is unclear; the bridge is virtually identical with later MSHD-designed Warren trusses. Kansas City Bridge began work on the Carrollton Bridge that fall, completing the structure by the following April. The bridge carried county-road traffic until about 1935, when it and the adjacent roadway were acquired by the state highway department. Today, the Carrollton Bridge functions in place with no major alterations.

The Missouri State Highway Department used riveted Warren configurations for its pony trusses almost from the time the agency developed its first bridge standards in the late 1910s. Structurally straightforward and versatile, these ubiquitous trusses were erected by the hundreds throughout the state in span lengths ranging from 40 to 100 feet. The Carrollton Bridge is distinguished among Missouri's Warren trusses as among the oldest extant examples of this mainstay structural type.

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**NAME(S) OF STRUCTURE**

Carrollton Bridge

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number F 404; Carroll County Road Record 1909-1920, page 591 (2 September 1918); Carroll County Court Record, Book 2: page 432 (6 March 1919), page 51 (7 April 1919), page 98 (7 May 1919) - located at Carroll County Courthouse, Carrollton MO; Harold Calvert, *Twenty-Twenty Hindsight: Part II*, (collection of articles previously published in Missouri newspapers, no publisher, 1975), n.p.; field inspection by Clayton Fraser, 30 September 1991.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**23 October 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Waverly Bridge  
MHTD: G 54R

CARR02

**DATE(S) OF CONSTRUCTION**

1924-25

**LOCATION**

U.S. Highway 24/65 over Missouri River; S19, T51N, R23W  
Waverly; Carroll County, Missouri

**USE (ORIGINAL / CURRENT)**

highway bridge / highway bridge

**RATING** NRHP eligible (score: 72)

**CONDITION**

good

**OWNER**

Missouri Highway and Transportation Department

span number: 2; 3; 1  
span length: 416.0'; 255.0'  
145.0'  
total length: 1945.0'  
roadway wdt.: 20.0'

superstructure: two steel, 18-panel, rigid-connected Pennsylvania through truss spans; three 11-panel, rigid-connected Parker through truss spans; one deck truss approach span; two plate girder approach spans; one steel stringer approach span  
substructure: concrete abutments, wingwalls and piers  
floor/decking: asphalt over corrugated steel, with steel stringers  
other features: upper chord and inclined end post: 2 built-up channels with cover plate and double lacing; lower chord: 2 channels with lacing; vertical: 2 channels with lacing (4 angles with lacing at the hips); diagonal: 2 channels with batten plates, 2 and 4 angles with batten plates; lateral bracing: 2 angles with lacing; strut: 4 angles with lacing; floor beam: I-beam, field bolted; guardrail: Armco; builder's plate: **Waverly Bridge; designed by Harrington, Howard & Ash, consulting engineers, Kansas City, MO; (unreadable) Missouri State Highway Department, G. H. Piepmeier, chief engineer, L. J. Sverdrup, bridge engineer; Contractors: substructure: Missouri Valley Bridge & Iron Co., Leavenworth, Kansas, superstructure: Wisconsin Bridge & Iron Co., North Milwaukee, Wisconsin; completed 1925**

For decades, the citizens of Missouri's north-central counties saw a need for a reliable, all-weather crossing over the Missouri River. Ferries transported people and goods during much of the year, but only a few daring souls crossed the river ice during the winter months, when cross-river traffic all but came to a halt. The process of planning a bridge over the Missouri at Waverly began as early as 1922, when the citizens of Carroll and Lafayette Counties voted to authorize the sale of \$125,000.00 in revenue bonds to fund the bridge's construction. Additional financing was contributed by other counties along U.S. Highway 65 (except Saline County, which was promoting a bridge of its own at Miami), bringing the total funds raised to \$583,000.00, including a federal appropriation. (The total cost of the bridge is placed at \$603,000.00 by some sources.) Congressional approval for the bridge was granted to the Lafayette County Court on July 1, 1922. Delineated by Harrington, Howard and Ash of Kansas City, the design of the Waverly Bridge was approved by the War Department in December. As drawn by HH&A, the structure consisted of two long-span Pennsylvania through trusses over the river's navigation channel, with four shorter Parker trusses and a series of deck trusses, plate girders and stringer approach spans, all supported high above the river by tapered concrete piers and abutments.



Contracts for the bridge's construction were awarded by the state highway commission in January 1923, with the Wisconsin Bridge and Iron Company of North Milwaukee, Wisconsin, responsible for the superstructure and the Missouri Valley Bridge and Iron Company of Leavenworth, Kansas, the substructure. Work on the substructural excavation began in April 1923. Using steel components rolled by the Illinois Steel Company of Chicago, Wisconsin B&I fabricated the rigid-connected trusses and erected them on the completed piers in 1924-25. The trusses themselves were to be simply supported between the piers when completed, but they were erected using a cantilever method, rather than over traditional falseworks, to reduce the risks to equipment during floods. Two temporary bents were built under each 416-foot span, and the trusses extended out over these. "No particular difficulty was experienced during the erection," MSHD engineer L.J. Sverdrup stated, "as all of the connections matched nicely and things in general went as anticipated. Field riveting of truss connections followed closely the erection of the members, so that all main members were practically fully riveted under a condition of no stress." By any measure, the Waverly Bridge was immense, consuming some 4.26 million pounds of structural steel and 24 million pounds of concrete. On July 15, 1925, some 10,000 people listened as Governor Baker and other dignitaries formally dedicated the bridge and opened it to traffic. Since that time the Waverly Bridge has functioned in place, with the replacement of its original concrete deck with the present open steel grid in 1976 as the only serious alteration to its historic fabric.

Known originally as the "Lakes to the Gulf Highway," a transcontinental route from the Great Lakes to the Gulf of Mexico, the route over the bridge was later designated U.S. Highway 65. In addition to the Highway 65 traffic, the bridge also carries the traffic of U.S. Highway 24. The Waverly Bridge, significant as one of the first over the Missouri River in this area of the state, has continuously provided a reliable year-round route to local and interstate vehicular traffic over the Missouri River since its construction. The structure is also significant as a well-preserved example of large-scale bridge construction in Missouri. Like several bridges built over the Missouri River in the 1920s, it incorporated long, fixed-span, simply supported trusses on concrete piers. The Pennsylvania trusses of the Waverly Bridge fit well within the mainstream of great river bridge construction in the 1920s. Just as the pin-connected Whipple truss had been the industry standard for Missouri River railroad bridges in the 1880s and 1890s, the riveted Pennsylvania (or Petit) truss was the standard configuration for long-span highway bridge applications in the 1910s, 1920s and 1930s.

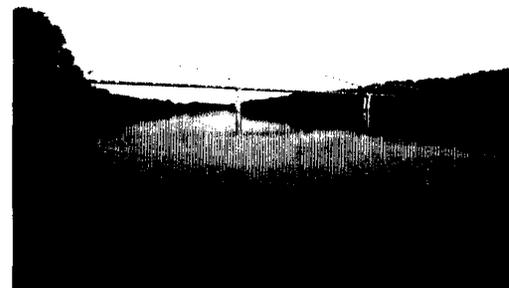
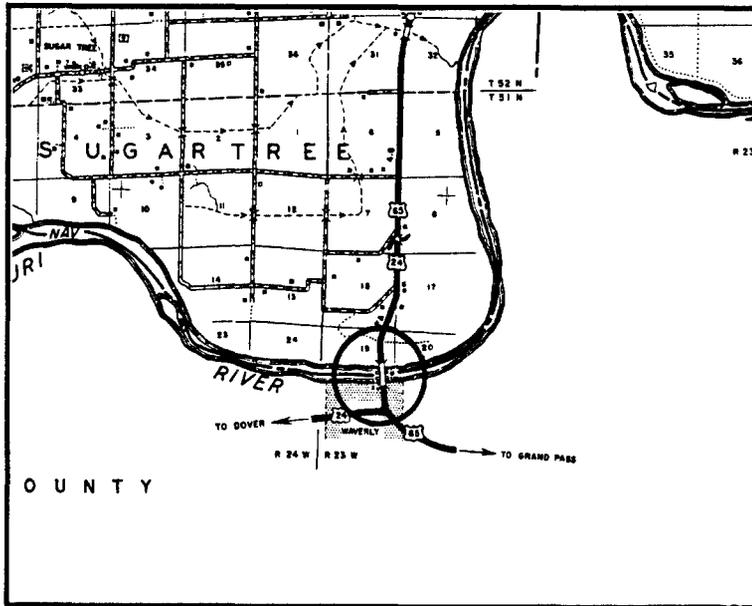
Its name derived from extensive use in the 19th century by the Pennsylvania Railroad, the Pennsylvania truss was patented in 1875 as a refinement of the standard Pratt truss, from which it was derived. Pennsylvania trusses featured vertical compression members and two-panel diagonals in tension, like the Whipple trusses they superseded. The primary differences lay in the substitution of a polygonal top chords for economy of materials and the addition of sub-struts or sub-ties. These relatively short diagonal members severed to provide lateral support for the diagonals and stiffen the truss under heavy moving loads. Like most truss types in America, Pennsylvanias were pioneered by the railroads and were later used to carry vehicular traffic.

The first polygonal-chorded bridge over the Mississippi River - the double-track Merchants' Bridge at St. Louis, was a Pennsylvania truss. During the bridge boom along the Missouri River in the 1920s, fifteen of the eighteen fixed-span bridges featured Pennsylvania trusses. Almost identical, they varied only in span lengths, which were determined largely by adjacent railroad bridges. With no railroad bridge nearby, the Waverly Bridge had more latitude than usual in its design, but the structure otherwise fits within the milieu of Missouri River bridge construction in the 1920s.

**NAME(S) OF STRUCTURE**

Waverly Bridge (Missouri River Bridge)

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number G 54R; Harold Calvert, *Twenty-Two Hindsight: Part II* (collection of articles published in Missouri newspapers, no publisher, 1975), n.p.; *History of Carroll County, Missouri* (Carrollton MO: Carroll County Historical Society, 1968), pages 28-29; *Bridges, Missouri River: Data, History and Laws*, U.S. Engineer Office, Kansas City, Missouri, 1933; field inspection by Clayton Fraser, 30 September 1991.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

23 October 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

McDaniel Memorial Bridge  
MHTD: K 999R

CARR03

**DATE(S) OF CONSTRUCTION**

1939

**LOCATION**

State Highway 41 over Missouri River; S11, T52N, R21W  
Miami; Carroll County, Missouri

**USE (ORIGINAL / CURRENT)**

highway toll bridge / highway bridge

**RATING** NRHP 23.0 (score: 2072.0)

**CONDITION**

good

**OWNER**

Missouri Highway and Transportation Department

span number: 1; 2; 3  
span length: 475.0'; 416.0';  
305.0'  
total length: 2072.0'  
roadway wdt.: 23.0'

superstructure: three steel, rigid-connected cantilever through truss spans; five steel stringer approach spans; three deck truss approach spans  
substructure: concrete abutments and piers  
floor/decking: concrete deck over steel stringers  
other features: upper chord, lower chord, and inclined end posts: 2 built-up channels with cover plate and double lacing; vertical: 2 channels with lacing, 4 angles with batten plates; diagonal: 2 channels with double lacing, 4 angles with batten plates; lateral bracing: 2 angles with lacing; strut: 4 angles with lacing and braces; floor beam: steel I-beam; guardrail: 2 channels; upper builder's plate text: **Federal Emergency Administration of Public Works; Franklin D. Roosevelt, President of the United States; Harold L. Ickes, Administrator of Public Works; McDaniel Memorial Bridge; 1939;** lower builder's plate text: **McDaniel Memorial Bridge; built by Saline County Missouri with the cooperation of Missouri State Highway Commission; county court of Saline County, Robert L. Hyatt - presiding judge, T. M. Smith - judge, O. Boyd Davis - judge; Sverdrup and Parcel, consulting engineers; Massman Construction Co., general contractors; 1939**

Although Saline County had been promoting construction of a highway bridge over the Missouri River at Miami for some time, the process that led to the bridge's actual building began early in 1938, when the Saline County Court applied for Congressional approval for the bridge. The authorization was given on June 16, and the Federal Emergency Administration of Public Works immediately offered to pay 45 percent of the project's cost, up to \$334,517.00. The county accepted the money and decided to sell toll revenue bridge bonds, totaling \$460,000.00, to raise its portion of the necessary funds (the bonds would be retired through toll collected from the bridge itself). On July 5, 1938, the county accepted an offer from Stifel, Nicolaus & Company, Inc., of St. Louis, to purchase all of the revenue bonds, thus effectively financing the county's financial obligation. The Missouri State Highway Commission also agreed to contribute a sum of \$125,000.00, to be used for payment of bonds and interest. The county hired St. Louis consulting engineers Sverdrup and Parcel to design the bridge. Sverdrup used a rigid-connected truss configuration similar to the one that the firm had employed on large-scale bridges at Nebraska City, Hannibal, Hermann and Gasconade, among others. As delineated by Sverdrup in 1938, the Miami Bridge consisted of three cantilevered through spans over the river's navigation channel, with a series of riveted deck truss and steel stringer



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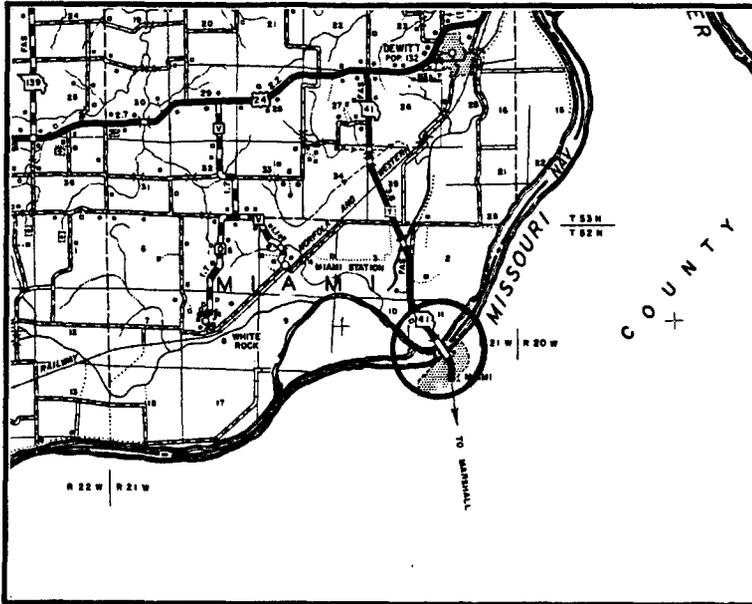
approach spans, all supported by concrete piers and abutments. In early July, the county court approved Sverdrup's plans for the bridge and sent an application for approval to the Secretary of War in Washington, D.C. In mid-December 1938, Saline County then accepted a bid from the Massman Construction Company of Kansas City, Missouri, to construct the bridge for \$493,788.00. Work began almost immediately on the substructural excavation; on June 27, 1940, the Miami Bridge was opened to traffic. Since that time the Waverly Bridge has functioned in place, with the replacement of its original concrete deck with the present open steel grid in 1983-84 as the only serious alteration to its historic fabric.

Situated between the Waverly Bridge and the Glasgow Bridge, the Miami Bridge has not significantly altered traffic patterns in western Missouri. It has, however, functioned as a regionally important crossing of a major river for which crossings are relatively scarce. The Miami Bridge is perhaps more noteworthy as a superlative example of long-span truss construction. Employing rigid-connected Warren trusses that cantilever from the channel piers, the bridge is one of several such spans based on what Sverdrup and Parcel had developed as a trademark design. With its 475-foot main spans, the Miami Bridge fits well within the mainstream of great river highway bridge construction in Missouri.

**NAME(S) OF STRUCTURE**

Miami Bridge (McDaniel Memorial Bridge)

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal (Structure Number K-999R); Carroll County Court Record, Book 5: pages 527-539, 595 (5 July 1938 - 20 October 1938); Book 6: pages 1-16 (December 1938) - located at Carroll County Courthouse, Carrollton MO; History of Carroll County (Carrollton MO: Carroll County Historical Society, 1968), page 29; Bridge Over Missouri River at Miami, Missouri, P.W.A. Docket Missouri 1075-F, Contract No. 2, Saline County, Missouri (St. Louis MO: Sverdrup and Parcel, Consulting Engineers, no date); Bridges, Missouri River: Data, History and Laws, U.S. Engineer Office, Kansas City, Missouri, 1933; field inspection by Clayton Fraser, 9 October 1991.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

23 October 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Cottonwood Creek Bridge  
MHTD: 007001.8

CARR04

**DATE(S) OF CONSTRUCTION**

1904

**LOCATION**

County Road 7 over Cottonwood Creek; S2/11, T55N, R25W  
7.5 miles northwest of Mandeville; Carroll County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 46)

**CONDITION**

fair

**OWNER**

Carroll County

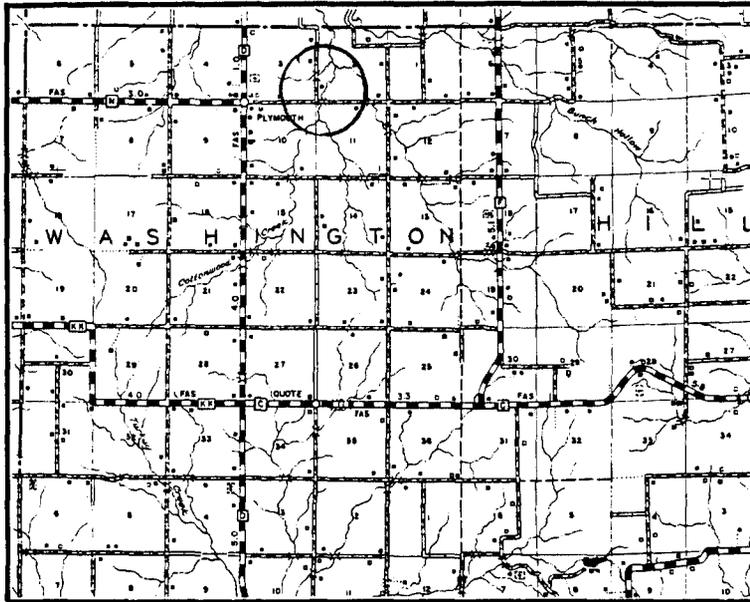
span number:	1	superstructure:	steel, 3-panel, rigid-connected Pratt bedstead truss
span length:	30.0'	substructure:	concrete abutments and wingwalls
total length:	30.0'	floor/decking:	concrete deck over steel stringers
roadway wdt.:	17.9'	other features:	no guardrails

Though not a true truss-leg bedstead, with its upright end posts extending beneath the truss floor to form a substructure, this small-scale steel bridge displays the other structural characteristics of the bedstead configuration. The Cottonwood Creek Bridge is comprised of a single bedstead truss with a rigid-connected Pratt web configuration, concrete deck and concrete substructure. The bridge is located on a gravel-surfaced county road about 7½ miles northwest of Mandeville. The Cottonwood Creek Bridge was fabricated and erected in 1904 by the Interstate Bridge Company of Kansas City, under contract with the Carroll County Court. Total cost: \$450.00. Since its completion, the truss has functioned in place with only maintenance-related repairs. It is today noteworthy as the earliest example remaining in Missouri of the riveted Pratt bedstead—a staple structural type for short-span crossings around the turn of the century.

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**NAME(S) OF STRUCTURE**

Cottonwood Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 007001.8; Carroll County Court Record, Book Q: page 33 (2 August 1904), page 177 (10 November 1904), pages 432-433 (3 May 1905) - located at Carroll County Courthouse, Carrollton MO.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

23 October 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

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**NAME(S) OF STRUCTURE**

Bridge Creek Bridge  
MHTD: 122000.9

CARR15

**DATE(S) OF CONSTRUCTION**

1909

**LOCATION**

County Road 122 over Bridge Creek; S18/19, T55N, R22W  
3.0 miles northeast of Tina; Carroll County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 45)

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**CONDITION**

fair

**OWNER**

Carroll County

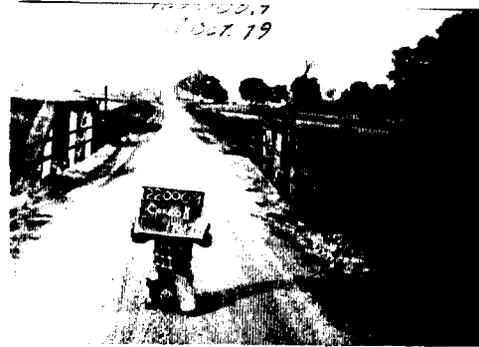
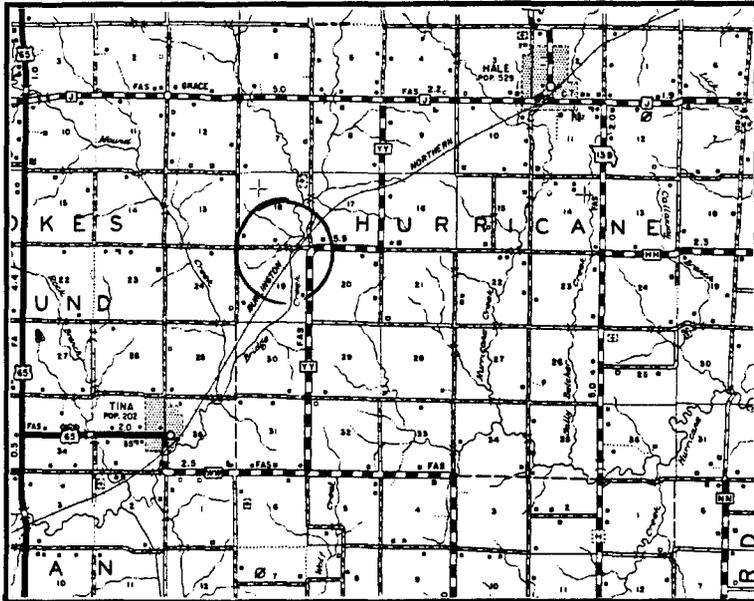
span number: 1  
span length: 65.0'  
total length: 69.0'  
roadway wdt.: 19.8'

superstructure: steel, 4-panel, rigid-connected Pratt pony truss  
substructure: concrete abutments and wingwalls  
floor/decking: concrete deck over steel stringers  
other features: steel angle guardrails

Located on a gravel-surfaced county road three miles northeast of Tina, this medium-span pony truss crosses Bunch Hollow. It is comprised of a single rigid-connected Pratt truss, supported by concrete abutments. The Bridge Creek Bridge was built in 1909 by R.E. Bates, under contract with the Carroll County Court. Total cost: \$700.00. Since its completion, it has carried vehicular traffic, with no serious alterations. It is today noteworthy as an early example in Missouri of a structural mainstay—the riveted Pratt pony truss.

**NAME(S) OF STRUCTURE**  
Bridge Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 122000.9; Carroll County Court Record, Book X: page 38 (4 May 1910 (September 1909 recapitulation)) - located at Carroll County Courthouse, Carrollton MO.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

23 October 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Big Creek Bridge  
MHTD: 214002.1

CARR26

**DATE(S) OF CONSTRUCTION**

1919

**LOCATION**

County Road 214 over Big Creek; S1/2, T54N, R23W  
1.0 mile south of Tina; Carroll County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP eligible (score: 72)

**CONDITION**

fair

**OWNER**

Carroll County

span number: 1

span length: 60.0'

total length: 101.0'

roadway wdt.: 15.1'

superstructure: steel, 4-panel, rigid-connected Pratt pony truss, skewed

substructure: unknown

floor/decking: concrete deck over steel stringers

other features: steel angle guardrails

Located a mile south of Tina, this medium-span pony truss carries a gravel-surfaced county road over Big Creek. The Big Creek Bridge is comprised of a single, four-panel Pratt truss, with rigid connections. The Big Creek Bridge was fabricated and erected in 1919 by the Kansas City Bridge Company for slightly less than \$3000. Since the structure's completion, it has functioned in place with only maintenance-related repairs.

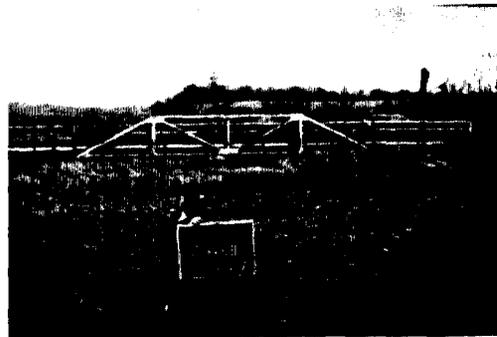
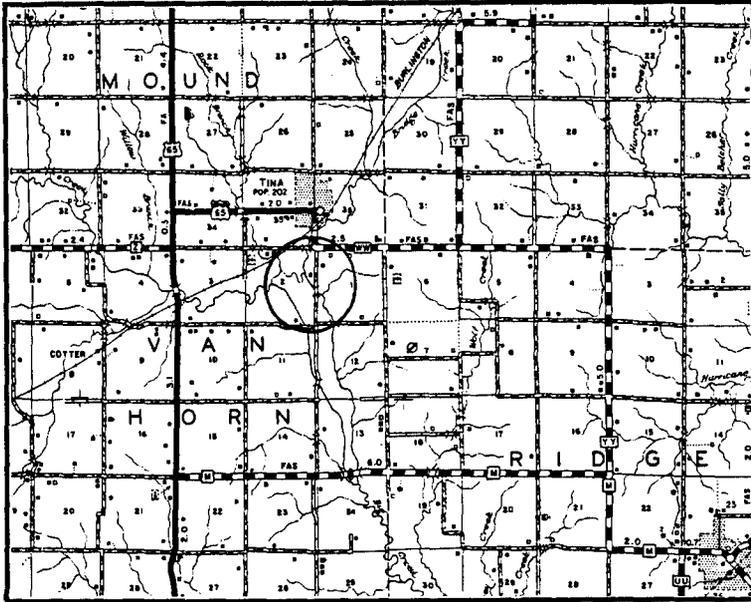
Thousands of Pratt trusses were erected throughout Missouri in the late 19th and early 20th centuries. Marketed by virtually all of the in-state and regional bridge companies, this versatile structural type was used overwhelmingly by the counties for short- and medium-span applications. These all featured what was termed the American method of construction, with chord members joined by pins. After the turn of the century, however, riveted connections began to supersede pinned, and eventually the riveted Pratt pony displaced its pinned predecessor. The Big Creek Bridge in Carroll County is noteworthy as one of the earliest riveted Pratt pony truss—a harbinger of what would soon become a mainstay structural type in Missouri. The truss is further distinguished by its skewed configuration—the only such skewed Pratt pony truss identified by the statewide inventory.

**NAME(S) OF STRUCTURE**

Big Creek Bridge

179

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 214002.1; Carroll County Court Record, Book Z: page 157 (1 September 1919), page 177 (3 November 1919) - located at Carroll County Courthouse, Carrollton MO.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

23 October 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

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**NAME(S) OF STRUCTURE**

Shootman Creek Bridge  
MHTD: 247000.9

CARR30

**DATE(S) OF CONSTRUCTION**

1908

**LOCATION**

County Road 247 over Shootman Creek; S34, T54N, R23W / S3, T53N, R23W  
3.5 miles east of Bogard; Carroll County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 42)

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**CONDITION**

fair

**OWNER**

Carroll County

span number: 1  
span length: 60.0'  
total length: 60.0'  
roadway wdt.: 15.0'

superstructure: steel, 4-panel, pin-connected Pratt bedstead truss  
substructure: steel pile bent abutments with timber back- and wingwalls  
floor/decking: concrete deck over steel stringers  
other features: upper chord and upright end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: looped square eyebar with slotted turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: lattice

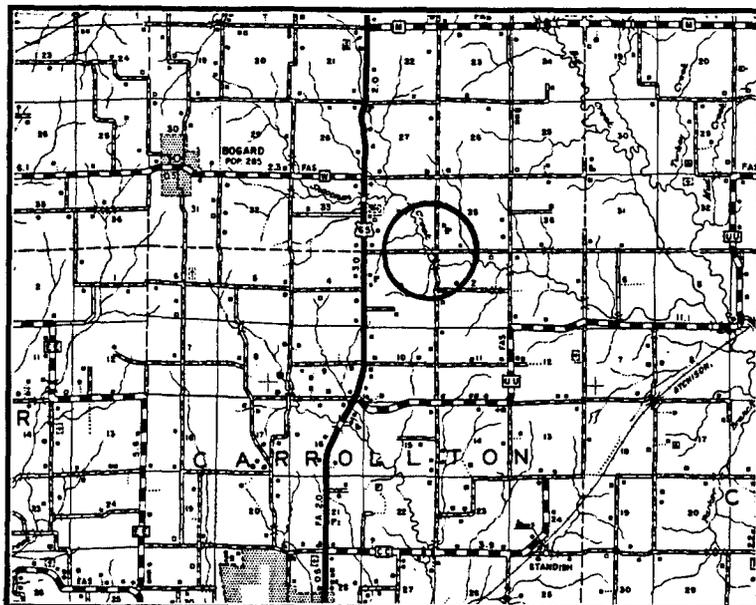
Located in central Missouri about four miles east of Bogard, this structure carries County Road 247 over Shootman Creek. The bridge is comprised of a pinned Pratt bedstead truss, supported by steel pile bent abutments. The Shootman Creek Bridge was built in 1908 by the Midland Bridge Company, under contract with Carroll County. Costing \$1082.00, it was fabricated in the firm's Kansas City shops using steel components rolled by Jones and Laughlin. Since its completion, the Shootman Creek Bridge has carried vehicular traffic. The structure has suffered several alterations over time, including the removal of its original truss leg abutments and the welded mending of its center-panel diagonals.

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**NAME(S) OF STRUCTURE**

Shootman Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 247000.9; Carroll County Court Record, Book V: page 495 (3 March 1908), page 325 (4 May 1909), September 1908 recapitulation - located at Carroll County Courthouse, Carrollton MO; field inspection, by Clayton Fraser, 2 October 1991.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

23 October 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Santa Fe Railroad Overpass  
MHTD: 407000.1

CARR57

**DATE(S) OF CONSTRUCTION**

c1895

**LOCATION**

County Road 407 over Atchison, Topeka & Santa Fe Railroad; S4, T53N, R22W  
3.5 miles southwest of Bosworth; Carroll County, Missouri

**USE (ORIGINAL / CURRENT)**

railroad bridge / roadway bridge

**RATING** NRHP non-eligible (score: 31)

**CONDITION**

good

**OWNER**

Carroll County

span number: 1

span length: 160.0'

total length: 162.0'

roadway wdt.: 13.3'

superstructure: wrought iron, 9-panel, pin-connected Pratt through truss

substructure: concrete abutments and wingwalls

floor/decking: timber deck with steel wearing plates over timber stringers

other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with lacing, (on outer panels), 4 punched rectangular eyebars (on center panels); vertical: 2 channels with continuous plate and lacing; hip vertical: 2 channels with batten plates; diagonal: 2 punched rectangular eyebars with unslotted sleeve bolts; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; portal strut: lattice; floor beam: riveted plate girder; guardrail: timber

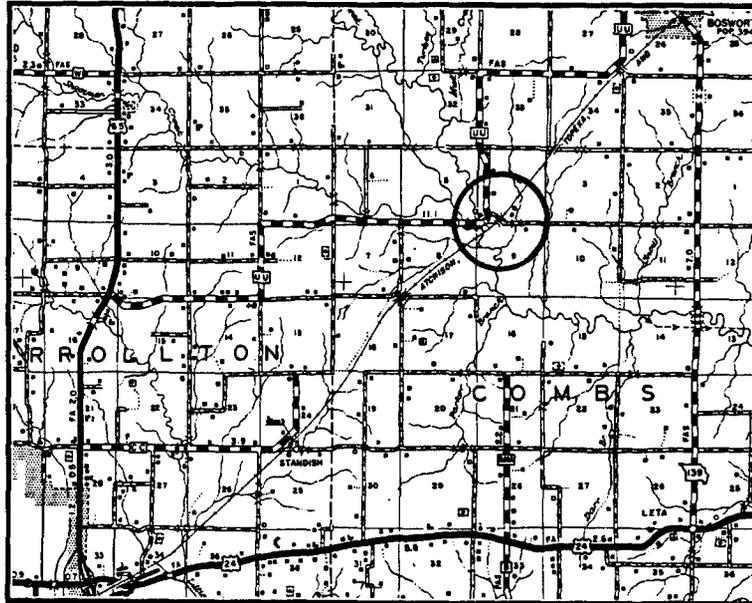
The Santa Fe Railroad constructed this long-span overpass, identified by the railroad as Structure Number 378, over its main line through Carroll County. The overpass carries an east-west county road some three miles southwest of Bosworth. The structure is comprised of a wrought iron, pinned Pratt through truss, the only through truss in Carroll County aside from the Missouri River bridges. The design of the structure, specifically the use of heavy-gauge steel members, points to the overpass' earlier use as a railroad bridge, which was then salvaged and subsequently moved to its present location. The use of sleeve bolts on the counters dates the structure to circa 1895. The truss was fabricated by the Edge Moor Iron Works of Wilmington, Delaware, and has carried intermittent rural vehicular traffic since its reconstruction at this rural Carroll County crossing. It is one of numerous pinned Pratt trusses erected on Missouri's county roads in the late 19th century.

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**NAME(S) OF STRUCTURE**

Santa Fe Railroad Overpass

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 407000.1; field inspection by Clayton Fraser, 1 October 1991.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

23 October 1992

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# CHARITON COUNTY

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**INCLUDED:** [Significant feature(s) of bridge given in boldface]  
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
CHAR01	H 520R	Mussel Creek Bridge	1-100' riveted Pratt through truss 1927 George W. Condon
CHAR02	H 667	Doxie Creek Bridge	5-100' riveted Warren pony truss, skewed 1929 McGlone and Walesby
CHAR03	J 728	Mussel Fork Bridge	(replaced)
CHAR04	J 729	Bridge	(replaced)
*CHAR05	009001.1	Big Brushy Creek Bridge	1- 48' pinned Pratt bedstead c1905
*CHAR06	024003.8	Yellow Creek Bridge	1-100' riveted polyg. Warren pony truss 1934 Pan-American Bridge Company
*CHAR07	031000.0	Locust Creek Bridge	1-100' riveted Pratt through truss 1925 Pan American Bridge Company
CHAR08	066000.7	Long Branch Bridge	1- 40' riveted Warren pony truss 1910 Pan American Bridge Company
CHAR09	074000.6	Locust Branch Bridge	1- 50' riveted Warren pony truss 1911 Pan American Bridge Company
CHAR10	085001.1	Van Dosen Branch Bridge	1- 40' riveted Warren bedstead 1912 Pan American Bridge Company
CHAR11	091000.7	Fork Creek Bridge	1- 60' pinned Pratt pony truss c1910
CHAR12	122001.5	Bridge	1- 45' riveted Warren bedstead 1912 Pan American Bridge Company
*CHAR13	126001.9	Puzzle Creek Bridge	1-106' pinned Pratt through truss 1904 A.M. Blodgett, Kansas City MO
*CHAR14	146000.9	Elm Creek Bridge	1- 60' pinned Pratt pony truss c1910
*CHAR15	158001.2	Jones Branch Bridge	1- 29' steel stringer 1916 Vincennes Bridge Company
*CHAR16	173001.0	Jones Branch Bridge	1- 40' riveted Warren pony truss 1916 Vincennes Bridge Company
CHAR17	201000.8	Clark Branch Bridge	1- 45' riveted Warren bedstead 1910 Pan American Bridge Company
*CHAR18	212001.2	Clark Branch Bridge	1- 70' pinned Pratt pony truss 1916
*CHAR19	278001.4	Salt Creek Bridge	1- 64' pinned Pratt pony truss 1912
CHAR20	280001.5	Salt Creek Bridge	(replaced)
CHAR21	365001.2	Palmer Creek Bridge	1- 36' riveted lattice bedstead 1911 Pan American Bridge Company
CHAR22	373000.8	Bridge	1- 40' riveted Warren pony truss 1912 Pan American Bridge Company
CHAR23	377000.3	Palmer Creek Bridge	1- 30' riveted lattice bedstead 1911 Pan American Bridge Company

# CHARITON COUNTY

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## INCLUDED (cont.):

CHAR24	385000.7	Long Branch Bridge	1- 60'	riveted Warren pony truss
			1911	Pan American Bridge Company (replaced)
*CHAR25	409000.9	Chariton River Bridge		(replaced)
CHAR26	423000.3	Bridge		(replaced)
*CHAR27	440000.2	Bridge	1- 35'	riveted Warren pony truss
			1916	Vincennes Bridge Company
CHAR28	463001.4	Middle Fork Bridge	1- 50'	riveted Warren popny truss
			1907	Kansas City Bridge Company
CHAR29	482000.3	Muncas Creek Bridge	1- 40'	riveted Warren pony truss
			1911	Pan American Bridge Company
*CHAR30	499000.1	Chariton River Bridge	1-100'	riveted Pratt through truss
			1906	Pan American Bridge Company (replaced)
CHAR31	534000.1	Mussel Fork Bridge		

## EXCLUDED:

### Warren pony truss

T 459A	017001.4	055001.6	065000.4	085001.6	088000.9	095000.9
103000.7	122001.4	124003.1	130001.6	142001.0	210000.3	260002.5
316000.7	316000.8	337000.4	337002.1	340000.1	361000.3	370000.7
443000.2	498003.2	528001.4	581000.1	589000.2		

### Pratt pony truss

137000.1	346000.5	490001.1	578001.7
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### Warren bedstead

072000.7	231001.2	259000.2	273000.4	314000.6	319001.9	436001.5
525AR0.5						

### Lattice bedstead

028003.2	078R00.6	112001.2	120000.7	161000.6	207000.2	357001.4
377000.3	425001.8	439000.6				

### Steel stringer

G 448R	J 685	L 344	T 463	T 765	T 768	X 121
X 808	Y 479	016000.6	016001.7	017001.2	029000.6	031002.6
036R01.7	036001.9	048R00.7	068000.6	076000.4	099R03.2	101R00.7
124R00.5	136001.4	153R00.3	170R01.1	170000.2	173000.8	175000.4
231001.3	236R01.1	241000.2	241000.7	258R00.3	275000.6	306001.3
325R00.7	341R00.8	343R02.5	359000.2	371A00.0	398000.7	405001.4
414000.7	419000.4	427000.1	429000.6	433000.7	434000.2	436000.1
436000.6	446000.5	467R03.9	467000.8	471000.5	547000.1	553000.1
580002.5	591000.8					

# CHARITON COUNTY

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## EXCLUDED (cont.):

Steel girder  
T 458

Concrete girder  
G 826R1    K 664

Concrete slab  
G 891R1    H 20R        H 616R        H 631R        H 647R

Concrete box culvert  
H 692        J 290        X 151

## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	2	23	0	0	25
Excluded	21	94	0	0	115
<hr/>					
	23	117	0	0	140 structures

# Mussel River Bridge

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CHAR01

## GENERAL DATA

structure no.:	H 520R	city/town:	Keytesville
county:	Chariton	feature inters.:	Mussel River
		cadastral grid:	S5, T53N, R18W
		highway route:	U.S. Highway 24
		highway distr.:	2
		current owner:	Missouri Highway and Transportation Department

## STRUCTURAL DATA

**superstructure:** steel, 5-panel, rigid-connected Pratt through truss, with concrete deck girder approach spans

**substructure:** concrete abutments, wingwalls and piers

span number:	1	condition:	good
span length:	100.0'	alterations:	girders and truss widened, 1940
total length:	443.0'	floor/decking :	concrete deck over steel stringers
roadway width:	26.0'	other features:	steel guardrails

## HISTORICAL DATA

**erection date:** 1927

**erection cost:** \$36,455.66

**designer:** Missouri State Highway Department

**fabricator :** unknown

**contractor:** George W. Condon, Omaha NE

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number H 520R; **Sixth Biennial Report of the Missouri State Highway Commission**, 1927-28, page 218; Missouri Primary System Bridge Record, located at Bridge Division, Missouri Highway and Transportation Department, Jefferson City MO.

**sign. rating:** 36

**evaluation:** NRHP non-eligible (typically configured example of MSHD standard highway truss design)

**inventoried by:** Michelle Crow-Dolby    6 March 1993

# Doxie Creek Bridge

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CHAR02

## GENERAL DATA

structure no.:	H 667	city/town:	2.5 miles north of Glasgow
county:	Chariton	feature inters.:	Doxie Creek
		cadastral grid:	S32, T52N, R17W
		highway route:	State Highway 5
		highway distr.:	2
		current owner:	Missouri Highway and Transportation Department

## STRUCTURAL DATA

superstructure:	steel, rigid-connected Warren pony truss, skewed		
substructure:	concrete abutments, wingwalls and piers		
span number:	1; 4	condition:	good
span length:	100.0'; 70.0'	alterations:	none
total length:	403.0'	floor/decking :	concrete deck over steel stringers
roadway width:	20.0'	other features:	steel pipe guardrails

## HISTORICAL DATA

erection date:	1928-29
erection cost:	\$44,044.05
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	McGlone and Walesby
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number H 667; <b>Sixth Biennial Report of the Missouri State Highway Commission (1927-28)</b> , page 218; Missouri Primary System Bridge Record, located at Bridge division, Missouri Highway and Transportation Department, Jefferson City MO.
sign. rating:	67→72
evaluation:	NRHP determined eligible (typically configured example of MSHD standard highway truss design, distinguished somewhat by its multiplicity of spans and skewed configuration)

Inventoried by: Michele Crow-Dolby 6 March 1993

# Big Brushy Creek Bridge

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CHAR05

## GENERAL DATA

structure no.:	009001.1	city/town:	2.0 miles north of Sumner
county:	Chariton	feature inters.:	Big Brushy Creek
		cadastral grid:	S3/10, T56N, R21W
		highway route:	County Road 9
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead  
substructure: truss-leg abutments with timber wingwalls

span number:	1	condition:	fair
span length:	48.0'	alterations:	none
total length:	49.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.7'	other features:	upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plates, 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: steel lattice

## HISTORICAL DATA

erection date: c1905  
erection cost: unknown  
designer: unknown  
fabricator : unknown  
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 009001.1; field inspection by Richard Collier, 9 June 1991.

sign. rating: 31  
evaluation: NRHP non-eligible (undocumented, short-span example of relatively common Pratt truss type with marginal integrity)

inventoried by: Michelle Crow-Dolby 6 March 1993

# Yellow Creek Bridge

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CHAR06

## GENERAL DATA

structure no.:	024003.8	city/town:	5.0 miles south of Sumner
county:	Chariton	feature inters.:	Yellow Creek
		cadastral grid:	S14/15, T55N, R21W
		highway route:	county road
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure: steel, 6-panel, rigid-connected Warren pony truss, with polygonal upper chords  
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	100.0'	alterations:	none
total length:	100.0'	floor/decking :	concrete deck over steel stringers
roadway width:	13.8'	other features:	upper chord and inclined end posts: 2 channels with cover plate and lacing; lower chord: two angles with batten plates; vertical: 2 angles with batten plates and 2-angle outriders; diagonal: 4 angles with batten plates; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: 2 channels; bridge plate: 1934 / C.W. GARHART PRES. JUDGE / LEE HOCKER ASSOC JUDGE / C.E. FLEETWOOD ASSOC JUDGE / J.B. ROBERTSON COUNTY CLERK / W.F. ARRINGTON ENGINEER / PAN AMERICAN BRIDGE CORP. / MOBERLY MISSOURI

## HISTORICAL DATA

erection date: 1934  
erection cost: unknown  
designer: unknown  
fabricator : Bethlehem Steel Company, Bethlehem PA  
contractor: Pan American Bridge Company, Moberly MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 024003.8; Chariton County Court Record, Book I: page 525 (6 February 1900); Book J: page 22 (8 August 1900); Book V: page 632 (7 May 1934); Book W: page 72 (28 December 1934) - located at Chariton County Courthouse, Keytesville MO; field inspection by Richard Collier, 9 June 1991.

sign. rating: 52  
evaluation: NRHP possibly eligible (typically configured example of uncommon structural type)

inventoried by: Michelle Crow-Dolby 6 March 1993

# Locust Creek Bridge

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CHAR07

## GENERAL DATA

<b>structure no.:</b> 031000.0	<b>city/town:</b> 3.0 miles northwest of Sumner
<b>county:</b> Chariton	<b>feature inters.:</b> Locust Creek
	<b>cadastral grid:</b> S5, T56N, R21W
	<b>highway route:</b> County Road 31
	<b>highway distr.:</b> 2
	<b>current owner:</b> Chariton County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 6-panel, rigid-connected Pratt through truss, with steel stringer approach span	
<b>substructure:</b> steel pile bent abutments; timber pile bent pier	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 100.0'	<b>alterations:</b> truss moved, 1963
<b>total length:</b> 131.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 15.6'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 angles with batten plates; vertical: 4 angles with batten plates; diagonal: 2 angles with batten plates; lateral bracing: round rod with threaded ends; strut: 2 angles with angle cross-bracing; floor beam: I-beam; guardrail: steel angle; bridge plate: CHARITON COUNTY / STATE OF MISSOURI / W.D. HERRING presiding judge / W.A. WASHBURN, LEE HOCKER associate judges / J.O. BROAD-DUSDEL, WARREN ROBERTS, O.C. DAMERON county engineers / CONTRACTED MARCH 18, 1925 / ERECTED BY PAN AMERICAN BRIDGE CORP OF MOBERLY MO

## HISTORICAL DATA

<b>erection date:</b> 1925; moved, 1963	
<b>erection cost:</b> \$26,500.00 (for original 5-span Grand River Bridge)	
<b>designer:</b> Missouri State Highway Department	
<b>fabricator :</b> Illinois Steel Company, Chicago IL	
<b>contractor :</b> Pan American Bridge Company, Moberly MO	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 031000.0; Chariton County Court Record, Book I: page 325 (18 February 1925), page 332 (18 March 1925), page 338 (30 March 1925), page 389 (16 September 1925), page 393 (7 October 1925), page 397 (26 October 1925), page 467 (3 May 1927) - located at Chariton County Courthouse, Keytesville MO; field inspection by Richard Collier, 9 June 1991.	

## Locust Creek Bridge

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**sign. rating:** 28

**evaluation:** NRHP non-eligible (typically configured example of MSHD truss design dating from the 1920s, moved within the past fifty years)

**inventoried by:** Michelle Crow-Dolby 6 March 1993

# Long Branch Bridge

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CHAR08

## GENERAL DATA

<b>structure no.:</b> 066000.7	<b>city/town:</b> 5.5 miles southeast of Rothville
<b>county:</b> Chariton	<b>feature inters.:</b> Long Branch
	<b>cadastral grid:</b> S19/30, T56N, R18W
	<b>highway route:</b> County Road 66
	<b>highway distr.:</b> 2
	<b>current owner:</b> Chariton County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, rigid-connected Warren pony truss	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 40.0'	<b>alterations:</b> unknown
<b>total length:</b> 42.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.5'	<b>other features:</b> steel lattice guardrails

## HISTORICAL DATA

<b>erection date:</b> 1910
<b>erection cost:</b> unknown
<b>designer:</b> unknown
<b>fabricator :</b> unknown
<b>contractor :</b> Pan American Bridge Company, Moberly MO
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 066000.7; Chariton County Court Record, Book P: page 279 (9 February 1911) - located at the Chariton County Courthouse, Keytesville MO.
<b>sign. rating:</b> 37
<b>evaluation:</b> NRHP non-eligible (typically configured example of a common structural type)

**inventoried by:** Michelle Crow-Dolby    9 March 1993

# Locust Branch Bridge

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CHAR09

## GENERAL DATA

structure no.:	074000.6	city/town:	6.5 miles east of Rothville
county:	Chariton	feature inters.:	Locust Branch
		cadastral grid:	S17/20, T56N, R18W
		highway route:	County Road 74
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, rigid-connected Warren pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	50.0'	alterations:	none
total length:	51.0'	floor/decking :	concrete deck over steel stringers
roadway width:	15.5'	other features:	steel lattice guardrails

## HISTORICAL DATA

erection date:	1911
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	Pan American Bridge Company, Moberly MO
references:	Missouri Highway and Transportation Department, Structure Inventory an Appraisal: Structure Number 074000.6; Chariton County Court Record, Book P: page 279 (9 February 1911) - located at the Chariton County Courthouse, Keytesville MO.
sign. rating:	35
evaluation:	NRHP non-eligible (typical example of common truss type)

inventoried by: Michelle Crow-Dolby 9 March 1993

# Van Dosen Branch Bridge

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CHAR10

## GENERAL DATA

structure no.:	085001.1	city/town:	9.4 miles southeast of Rothville
county:	Chariton	feature inters.:	Van Dosen Branch
		cadastral grid:	S14/23, T56N, R18W
		highway route:	County Road 85
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, rigid-connected Warren bedstead truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	40.0'	alterations:	none
total length:	56.0'	floor/decking :	concrete deck over steel stringers
roadway width:	15.6'	other features:	steel lattice guardrails

## HISTORICAL DATA

erection date:	1912
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	Pan American Bridge Company, Moberly MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 085001.1; Chariton County Court Record, Book P: page 616 (14 August 1912) - located at Chariton County Courthouse, Keytesville MO.
sign. rating:	43
evaluation:	NRHP non-eligible (typically configured and modestly scaled example of somewhat uncommon structural type)

inventoried by: Michelle Crow-Dolby 9 March 1993

# Fork Creek Bridge

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CHAR11

## GENERAL DATA

structure no.:	091000.7	city/town:	4.0 miles east of Marceline
county:	Chariton	feature inters.:	Fork Creek
		cadastral grid:	S1, T56N, R18W
		highway route:	County Road 91
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	101.0'	floor/decking :	timber deck
roadway width:	11.6'	other features:	steel lattice guardrails

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 091000.7

sign. rating:	28
evaluation:	NRHP non-eligible (undistinguished, undocumented, example of a common structural type)

inventoried by: Michelle Crow-Dolby 6 March 1993

# Bridge

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CHAR12

## GENERAL DATA

structure no.:	122001.5	city/town:	2.1 miles northeast of Wien
county:	Chariton	feature inters.:	unnamed stream
		cadastral grid:	S7/18, T56N, R16W
		highway route:	County Road 122
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, rigid-connected Warren bedstead truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	45.0'	alterations:	none
total length:	46.0'	floor/decking :	timber deck over steel stringers
roadway width:	15.1'	other features:	steel lattice guardrails

## HISTORICAL DATA

erection date:	1912
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	Pan American Bridge Company, Moberly MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 122001.5; Chariton County Court Record, Book P: page 616 (14 August 1912) - located at Chariton County Courthouse, Keytesville MO.
sign. rating:	38
evaluation:	NRHP non-eligible (typically configured and modestly scaled example of somewhat uncommon structural type)

inventoried by: Michelle Crow-Dolby 9 March 1993

# Puzzle Creek Bridge

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CHAR13

## GENERAL DATA

structure no.:	126001.9	city/town:	2.5 miles east of Bynumville
county:	Chariton	feature inters.:	Puzzle Creek
		cadastral grid:	S5/32, T55/56N, R16W
		highway route:	County Road 126
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

**superstructure:** steel, 7-panel, pin-connected Pratt through truss  
**substructure:** concrete-filled steel cylinder piers with timber backwalls

span number:	1	condition:	poor
span length:	105.0'	alterations:	bridge closed, deck removed
total length:	107.0'	floor/decking :	timber deck (removed) over steel stringers
roadway width:	11.8'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (2 square looped eyebars at hip); diagonal: 2 looped rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles with 2-angle knee braces; portal strut: A-frame; floor beam: I-beams, U-bolted to lower chord pins

## HISTORICAL DATA

**erection date:** 1904  
**erection cost:** \$2700.00  
**designer:** O.G. Dameron  
**fabricator :** Kansas City Bridge Company, Kansas City MO (probable)  
**contractor :** A.M. Blodgett, Kansas City MO

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 126001.9; Chariton County Court Record, Book K: page 280 (3 November 1903), Book L: pages 3-4 (17 November 1904) - located at Chariton County Courthouse, Keytesville MO; field inspection by Richard Collier, 9 June 1991.

**sign. rating:** 39  
**evaluation:** NRHP non-eligible (typical example of common truss configuration, with standard detailing, unremarkable dimensions and below-average degree of physical integrity)

**inventoried by:** Michelle Crow-Dolby 6 March 1993

# Elm Creek Bridge

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CHAR14

## GENERAL DATA

structure no.:	146000.9	city/town:	1.0 mile east of Bynumville
county:	Chariton	feature inters.:	Elm Branch
		cadastral grid:	S1/36, T55/56N, R17W
		highway route:	County Road 146
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled steel cylinder piers, with timber backwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	61.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.3'	other features:	upper chord and inclined end posts: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with batten plates; diagonal: 2 looped square eyebars; counter: square eyebar with turnbuckles; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pins; guardrail: 2 channels

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 146000.9; field inspection by Richard Collier, 9 June 1991.

sign. rating:	30
evaluation:	NRHP non-eligible (poorly documented example of common structural type)

inventoried by: Michelle Crow-Dolby 6 March 1993

# Jones Branch Bridge

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CHAR15

## GENERAL DATA

<b>structure no.:</b> 158001.2	<b>city/town:</b> 4.0 miles southwest of Bynumville
<b>county:</b> Chariton	<b>feature inters.:</b> Jones Branch
	<b>cadastral grid:</b> S8/17, T55N, R17W
	<b>highway route:</b> County Road 158
	<b>highway distr.:</b> 2
	<b>current owner:</b> Chariton County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> concrete abutments and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 29.0'	<b>alterations:</b> none
<b>total length:</b> 30.0'	<b>floor/decking :</b> concrete deck
<b>roadway width:</b> 17.8'	<b>other features:</b> steel lattice guardrails with continuous ends; bridge plate: A NUMBER 14 / BUILT 1916 BY VINCENNES BR. CO. / VINCENNES, IND. / W.N. HAMILTON presiding judge / W.D. HER- RING western judge / J.W. HUGHES eastern judge / E.F. HAYES engr.

## HISTORICAL DATA

<b>erection date:</b> 1916	
<b>erection cost:</b> unknown	
<b>designer:</b> unknown	
<b>fabricator :</b> Vincennes Bridge Company, Vincennes IN	
<b>contractor :</b> Vincennes Bridge Company, Vincennes IN	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 158001.2; field inspection by Richard Collier, 9 June 1991.	
<b>sign. rating:</b> 41	
<b>evaluation:</b> NRHP non-eligible (undistinguished, small-scale structure, lacking in technological significance)	

**inventoried by:** Michelle Crow-Dolby    6 March 1993

# Jones Branch Bridge

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CHAR16

## GENERAL DATA

structure no.:	173001.0	city/town:	7.0 northwest of Salisbury
county:	Chariton	feature inters.:	Jones Branch
		cadastral grid:	S5/32, T54/55N, R17W
		highway route:	County Road 173
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, rigid-connected Warren pony truss		
substructure:	timber pile bent abutments and wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	none
total length:	42.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.8'	other features:	upper chord and inclined end posts: 2 channels with cover and batten plates; lower chord: 2 angles with angle batten plates; vertical: 4 angles with batten plates; diagonal: 2 angles with batten plates; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: steel lattice

## HISTORICAL DATA

erection date:	1916
erection cost:	unknown
designer:	unknown
fabricator :	Vincennes Bridge Company, Vincennes IN; Cambria Steel Company, Pittsburgh PA
contractor :	Vincennes Bridge Company, Vincennes IN
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 173001.0; field inspection by Richard Collier, 9 June 1991.
sign. rating:	34
evaluation:	NRHP non-eligible (typically configured example of Warren pony truss design)

inventoried by: Michelle Crow-Dolby 6 March 1993

# Clark Branch Bridge

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CHAR17

## GENERAL DATA

<b>structure no.:</b>	201000.8	<b>city/town:</b>	8.8 miles southeast of Rothville
<b>county:</b>	Chariton	<b>feature inters.:</b>	Clark Branch
		<b>cadastral grid:</b>	S9/16, T55N, R18W
		<b>highway route:</b>	County Road 201
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Chariton County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 4-panel, rigid-connected Warren bedstead truss		
<b>substructure:</b>	unknown		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	45.0'	<b>alterations:</b>	none
<b>total length:</b>	46.0'	<b>floor/decking :</b>	timber deck
<b>roadway width:</b>	15.3'	<b>other features:</b>	steel lattice guardrails

## HISTORICAL DATA

<b>erection date:</b>	1910
<b>erection cost:</b>	unknown
<b>designer:</b>	unknown
<b>fabricator :</b>	unknown
<b>contractor :</b>	Pan American Bridge Company, Moberly MO
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory an Appraisal: Structure Number 201000.8; Chariton County Court Record, Book P: page 81 (10 March 1910) - located at Chariton County Courthouse, Keytesville MO.
<b>sign. rating:</b>	40
<b>evaluation:</b>	NRHP non-eligible (typically configured and modestly scaled example of somewhat uncommon structural type)

**inventoried by:** Michelle Crow-Dolby    9 March 1993

# Clark Branch Bridge

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CHAR18

## GENERAL DATA

structure no.:	212001.2	city/town:	7.0 miles south of Marceline
county:	Chariton	feature inters.:	Clark Branch
		cadastral grid:	S4/33, T55/56N, R18W
		highway route:	County Road 212
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

**superstructure:** steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach span  
**substructure:** steel pile bent abutments and pier

span number:	1	condition:	fair
span length:	70.0'	alterations:	none
total length:	83.0'	floor/decking :	concrete deck over steel stringers
roadway width:	13.1'	other features:	upper chord and inclined end posts: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice with continuous ends; bridge plate: A NO. 11 1916 E.F. HAYES eng.

## HISTORICAL DATA

**erection date:** 1916  
**erection cost:** unknown  
**designer:** unknown  
**fabricator :** unknown  
**contractor :** unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 212001.2; field inspection by Richard Collier, 9 June 1991.

**sign. rating:** 34  
**evaluation:** NRHP non-eligible (technologically undistinguished example of common structural type)

**inventoried by:** Michelle Crow-Dolby 6 March 1993

# Salt Creek Bridge

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CHAR19

## GENERAL DATA

structure no.:	278001.4	city/town:	3.5 miles south of Mendon
county:	Chariton	feature inters.:	Salt Creek
		cadastral grid:	S34, T55N, R20W
		highway route:	County Road 278
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber backwalls		
span number:	1	condition:	fair
span length:	64.0'	alterations:	none
total length:	66.0'	floor/decking :	timber deck over steel stringers
roadway width:	15.6'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars, or 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice

## HISTORICAL DATA

erection date:	1912
erection cost:	unknown
designer:	unknown
fabricator :	Illinois Steel Company, Chicago IL
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 278001.4; Chariton County Court Record, Book P: page 616 (14 August 1912) - located at Chariton County Courthouse, Keytesville MO; field inspection by Richard Collier, 9 June 1991.
sign. rating:	34
evaluation:	NRHP non-eligible (largely undocumented example of common structural type)

inventoried by: Michelle Crow-Dolby 6 March 1993

# Palmer Creek Bridge

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CHAR21

## GENERAL DATA

structure no.:	365001.2	city/town:	6.5 miles northwest Keytesville
county:	Chariton	feature inters.:	Palmer Creek
		cadastral grid:	S10/11, T54N, R19W
		highway route:	County Road 365
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure:	steel, rigid-connected lattice bedstead truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	36.0'	alterations:	none
total length:	53.0'	floor/decking :	concrete deck over steel stringers
roadway width:	11.7'	other features:	no guardrails

## HISTORICAL DATA

erection date:	1911
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	Pan American Bridge Company. Moberly MO
references:	Missouri Highway and Transportation Department, Structure and Inventory Appraisal: Structure Number 365001.2; Chariton County Court Record, Book P: page 279 (9 February 1911) - located at the Chariton County Courthouse, Keytesville MO.
sign. rating:	37
evaluation:	NRHP non-eligible (short-span example of relatively common structural type)

inventoried by: Michelle Crow-Dolby 9 March 1993

# Bridge

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CHAR22

## GENERAL DATA

structure no.: 373000.8      city/town: 6.2 miles northwest of Keytesville  
county: Chariton      feature inters.: unnamed stream  
cadastral grid: S2/11, T54N, R19W  
highway route: County Road 373  
highway distr.: 2  
current owner: Chariton County

## STRUCTURAL DATA

superstructure: steel, 3-panel, rigid-connected Warren pony truss  
substructure: concrete abutments and wingwalls

span number: 1      condition: fair  
span length: 40.0'      alterations: none  
total length: 41.0'      floor/decking : concrete deck over steel stringers  
roadway width: 15.6'      other features: no guardrails

## HISTORICAL DATA

erection date: 1912  
erection cost: unknown  
designer: unknown  
fabricator : unknown  
contractor: Pan American Bridge Company, Moberly MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 373000.8; Chariton County Court Record, Book P: page 575 (9 May 1912) - located at Chariton County Courthouse, Keytesville MO.

sign. rating: 37  
evaluation: NRHP non-eligible (typically configured and modestly scaled example of common structural type)

inventoried by: Michelle Crow-Dolby      9 March 1993

# Palmer Creek Bridge

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CHAR23

## GENERAL DATA

structure no.:	377000.3	city/town:	5.0 miles northwest of Keytesville
county:	Chariton	feature inters.:	Palmer Creek
		cadastral grid:	S14/23, T54N, R19W
		highway route:	County Road 377
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure:	steel, rigid-connected lattice bedstead truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	30.0'	alterations:	none
total length:	31.0'	floor/decking :	concrete deck over steel stringers
roadway width:	15.4'	other features:	no guardrails

## HISTORICAL DATA

erection date:	1911
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	Pan American Bridge Company, Moberly MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 377000.3; Chariton County Court Record, Book P: page 279 (9 February 1911) - located at Chariton County Courthouse, Keytesville MO.
sign. rating:	37
evaluation:	NRHP non-eligible (short-span example of common structural type)

Inventoried by: Michelle Crow-Dolby 9 March 1993

# Long Branch Bridge

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CHAR24

## GENERAL DATA

structure no.:	385000.7	city/town:	5.5 miles northwest of Keytesville
county:	Chariton	feature inters.:	Long Branch
		cadastral grid:	S5/6, T54N, R18W
		highway route:	County Road 385
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, rigid-connected Warren pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	62.0'	floor/decking :	concrete deck over steel stringers
roadway width:	13.1'	other features:	unknown

## HISTORICAL DATA

erection date:	1911
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	Pan American Bridge Company, Moberly MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 385000.7; Chariton County Court Record, Book P: page 340 (4 May 1911) - located at Chariton County Courthouse, Keytesville MO.
sign. rating:	35
evaluation:	NRHP non-eligible (typically configured and modestly scaled example of common structural type)

inventoried by: Michelle Crow-Dolby 9 March 1993

# Bridge

CHAR27

## GENERAL DATA

structure no.: 440000.2      city/town: 1.0 mile north of Prairie Hill  
county: Chariton      feature inters.: unnamed stream  
cadastral grid: S29/32, T55N, R16W  
highway route: County Road 440  
highway distr.: 2  
current owner: Chariton County

## STRUCTURAL DATA

superstructure: steel, 2-panel, rigid-connected Warren pony truss  
substructure: steel pile bent abutments with timber back- and wingwalls

span number: 1      condition: fair  
span length: 35.0'      alterations: none  
total length: 37.0'      floor/decking : timber deck over steel stringers  
roadway width: 15.7'      other features: upper chord and inclined end posts: 2 channels with cover and batten plates; lower chord: 2 angles with angle batten plates; vertical: 4 angles with batten plates; diagonal: 2 angles with batten plates; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: 1 channel; bridge plate: A NUMBER 20 BUILT 1916 BY VINCENNES BR. CO. / VINCENNES, IND. / W.N. HAMILTON presiding judge / W.D. HERRING western judge / J.W. HUGHES eastern judge / E.F. HAYES engr.

## HISTORICAL DATA

erection date: 1916  
erection cost: unknown  
designer: unknown  
fabricator : Vincennes Bridge Company, Vincennes IN;  
Cambria Steel Company, Pittsburgh PA  
contractor : Vincennes Bridge Company, Vincennes IN  
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 440000.2; field inspection by Richard Collier, 9 June 1991.  
sign. rating: 39  
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby      6 March 1993

# Middle Fork Bridge

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CHAR28

## GENERAL DATA

structure no.:	463001.4	city/town:	6.0 miles northeast of Salisbury
county:	Chariton	feature inters.:	Middle Fork
		cadastral grid:	S16/21, T54N, R16W
		highway route:	County Road 463
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure: steel, 3-panel, rigid-connected Warren pony truss  
substructure: unknown

span number:	1	condition:	fair
span length:	50.0'	alterations:	none
total length:	75.0'	floor/decking :	timber deck
roadway width:	11.5'	other features:	steel lattice guardrails

## HISTORICAL DATA

erection date: 1907  
erection cost: \$669.00  
designer: Kansas City Bridge Company, Kansas City MO  
fabricator : Kansas City Bridge Company, Kansas City MO  
contractor: Kansas City Bridge Company, Kansas City MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 463001.4; Chariton County Court Record, Book O: page 36 (8 November 1907) - located at the Chariton County Courthouse, Keytesville MO.

sign. rating: 48  
evaluation: NRHP possibly eligible (typically configured and modestly scaled example of a common structural type)

Inventoried by: Michelle Crow-Dolby 9 March 1992

# Muncas Creek Bridge

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CHAR29

## GENERAL DATA

structure no.:	482000.3	city/town:	3.8 miles east of Salisbury
county:	Chariton	feature inters.:	Muncas Creek
		cadastral grid:	S5, T53N, R16W
		highway route:	County Road 482
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, rigid-connected Warren pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	40.0'	alterations:	none
total length:	41.0'	floor/decking :	timber deck
roadway width:	16.0'	other features:	steel lattice guardrails

## HISTORICAL DATA

erection date:	1911
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	Pan American Bridge Company, Moberly MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 482000.3; Chariton County Court Record, Book P: page 340 (4 May 1911) - located at Chariton County Courthouse, Keytesville MO.
sign. rating:	35
evaluation:	NRHP non-eligible (typically configured and modestly scaled example of a common structural type)

inventoried by: Michelle Crow-Dolby 9 March 1992

# Chariton River Bridge

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CHAR30

## GENERAL DATA

structure no.:	499000.1	city/town:	5.5 miles south of Salisbury
county:	Chariton	feature inters.:	East Fork of Chariton River
		cadastral grid:	S35, T53N, R17W
		highway route:	County Road 499
		highway distr.:	2
		current owner:	Chariton County

## STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Pratt through truss, with steel stringer approach span		
substructure:	concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	100.0'	alterations:	none
total length:	126.0'	floor/decking :	concrete deck over steel stringers
roadway width:	17.5'	other features:	steel lattice guardrails

## HISTORICAL DATA

erection date:	1906
erection cost:	\$2600.00
designer:	unknown
fabricator :	unknown
contractor :	Pan American Bridge Company, Moberly MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 499000.1; Chariton County Court Record, Book M: page 570 (8 August 1906) - located at Chariton County Courthouse, Keytesville MO.
sign. rating:	51
evaluation:	NRHP possibly eligible (earliest example in state of riveted Pratt through truss)

inventoried by: Michelle Crow-Dolby 6 March 1991

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Doxie Creek Bridge  
MHTD: H 667

CHAR02

**DATE(S) OF CONSTRUCTION**

1928-29

**LOCATION**

State Highway 5 over Doxie Creek; S32, T52N, R17W  
2.5 miles north of Glasgow; Chariton County, Missouri

**USE (ORIGINAL / CURRENT)**

highway bridge / highway bridge

**RATING** NRHP determined eligible (score: 72)

**CONDITION**

good

**OWNER**

Missouri Highway and Transportation Department

span number: 1; 4  
span length: 100.0'; 70.0'  
total length: 403.0'  
roadway wdt.: 20.0'

superstructure: steel, rigid-connected Warren pony truss, skewed  
substructure: concrete abutments, wingwalls and piers  
floor/decking: concrete deck over steel stringers  
other features: steel pipe guardrails

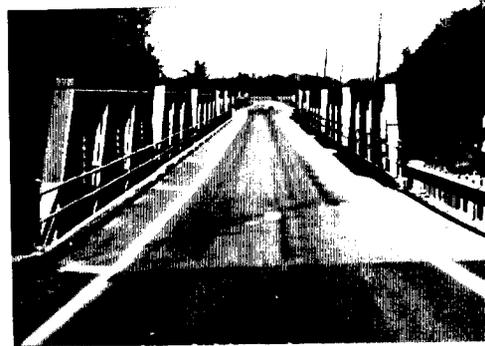
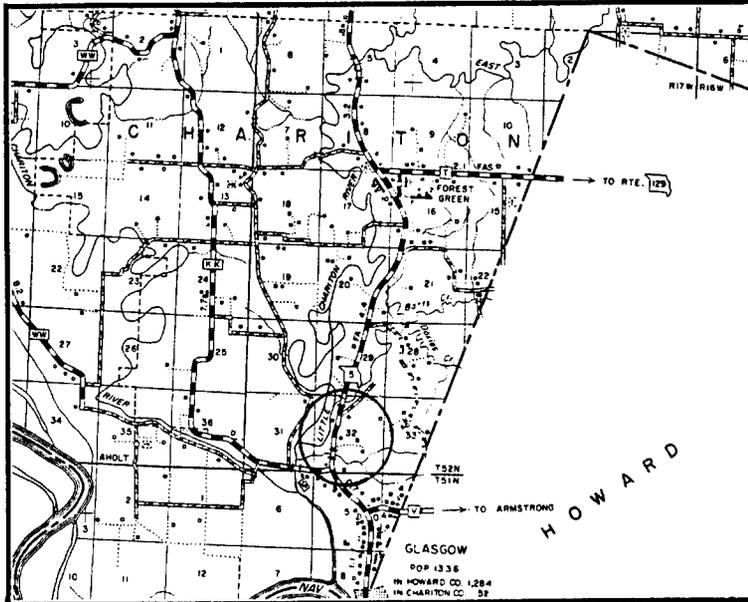
Located 2½ miles north of Glasgow, this five-span pony truss carries State Highway 5 over Doxie Creek. The unequal-length channel spans are rigid-connected Warren pony trusses, carried on a skew by a concrete substructure. The Doxie Creek Bridge was designed in the summer of 1928 by engineers for the Missouri State Highway Department. On August 10th a contract to build the bridge was awarded to McGlone and Walesby. Probably completed later that year, the Doxie Creek Bridge has functioned in place with no serious alterations.

The Missouri State Highway Department used riveted Warren configurations for its pony trusses almost from the time the agency developed its first bridge standards around 1920. Structurally straightforward and versatile, these ubiquitous trusses were erected by the hundreds throughout the state in span lengths ranging from 40 to 100 feet. The Doxie Creek Bridge is distinguished among Missouri's Warren trusses as among the few skewed examples of this mainstay structural type.

**NAME(S) OF STRUCTURE**

Doxie Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number H 667; **Sixth Biennial Report of the Missouri State Highway Commission (1927-28)**, page 218; Missouri Primary System Bridge Record, located at Bridge division, Missouri Highway and Transportation Department, Jefferson City MO.

**INVENTORIED BY**

Michelle Crow-Dolby

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

9 March 1993

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Yellow Creek Bridge  
MHTD: 024003.8

CHAR06

**DATE(S) OF CONSTRUCTION**

1934

**LOCATION**

county road over Yellow Creek; S14/15, T55N, R21W  
5.0 miles south of Sumner; Chariton County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 52)

**CONDITION**

fair

**OWNER**

Chariton County

span number: 1  
span length: 100.0'  
total length: 100.0'  
roadway wdt.: 13.8'

superstructure: steel, 6-panel, rigid-connected Warren pony truss, with polygonal upper chords  
substructure: concrete abutments and wingwalls  
floor/decking: concrete deck over steel stringers  
other features: upper chord and inclined end posts: 2 channels with cover plate and lacing; lower chord: two angles with batten plates; vertical: 2 angles with batten plates and 2-angle outriders; diagonal: 4 angles with batten plates; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: 2 channels; bridge plate: 1934 C.W. GARHART PRES. JUDGE / LEE HOCKER ASSO JUDGE / C.E. FLEETWOOD ASSOC JUDGE / J.B. ROBERTSON COUNTY CLERK / W.F. ARRINGTON ENGINEER / PAN AMERICAN BRIDGE CORP. / MOBERLY MISSOURI

Located five miles south of Sumner, this long-span, riveted pony truss carries a gravel road over Yellow Creek. The existing Warren truss replaced an earlier structure, erected in the summer of 1900 by Kansas City contractor A.M. Blodgett. In May 1934 the Chariton County Court ordered that the structure built at the turn of the century be replaced under an emergency contract. The new bridge, the court announced, would be built by the Pan American Bridge Company of Moberly, Missouri. Completed that year, the riveted polygonal Warren pony truss has since carried intermittent rural traffic in unaltered condition.

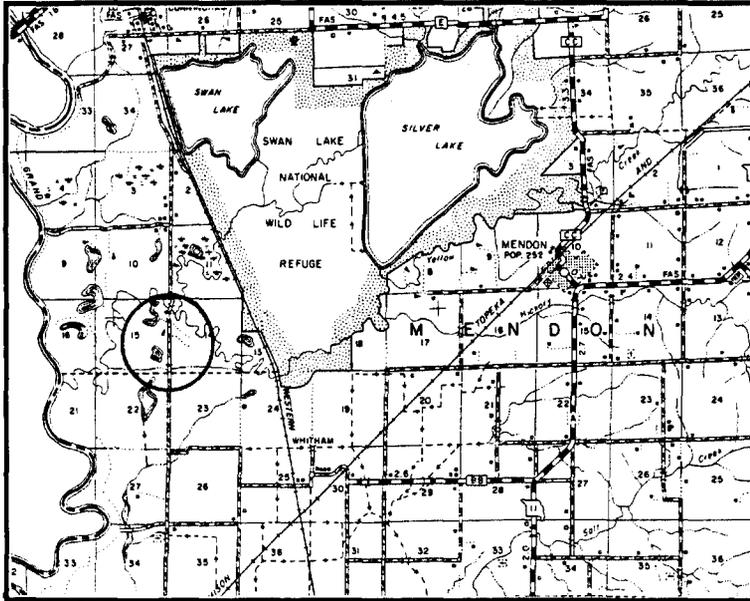
The Yellow Creek Bridge is one of nine rigid-connected, polygonal, Warren pony trusses included in Missouri's statewide bridge inventory. All of these crossings were built in the 1930s, most on primary system roadways. With its 100-foot span and 1934 construction date, this span fits well within the mainstream of this construction trend.

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**NAME(S) OF STRUCTURE**

Yellow Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 024003.8; Chariton County Court Record, Book I: page 525 (6 February 1900); Book J: page 22 (8 August 1900); Book V: page 632 (7 May 1934); Book W: page 72 (28 December 1934) - located at Chariton County Courthouse, Keytesville MO; field inspection by Richard Collier, 9 June 1991.

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**INVENTORIED BY**

Michelle Crow-Dolby

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

9 March 1993

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Puzzle Creek Bridge  
MHTD: 126001.9

CHAR13

**DATE(S) OF CONSTRUCTION**

1904

**LOCATION**

County Road 126 over Puzzle Creek; S5/32, T55/56N, R16W  
2.5 miles east of Bynumville; Chariton County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / abandoned

**RATING** NRHP non-eligible (score: 39)

**CONDITION**

poor

**OWNER**

Chariton County

span number: 1

span length: 105.0'

total length: 107.0'

roadway wdt.: 11.8'

superstructure: steel, 7-panel, pin-connected Pratt through truss

substructure: concrete-filled steel cylinder piers with timber backwalls

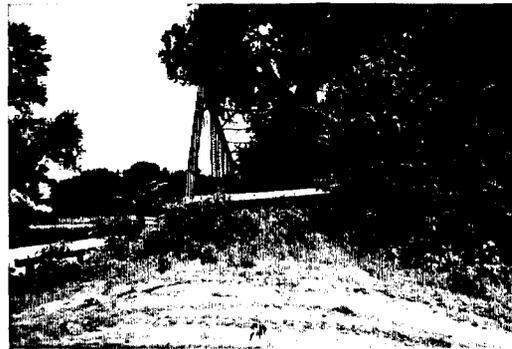
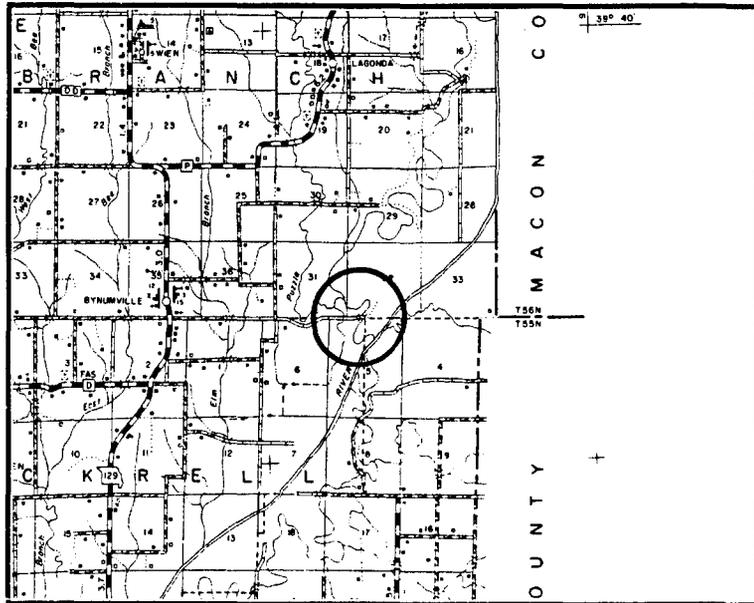
floor/decking: timber deck (removed) over steel stringers

other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (2 square looped eyebars at hip); diagonal: 2 looped rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles with 2-angle knee braces; portal strut: A-frame; floor beam: I-beams, U-bolted to lower chord pins

The Puzzle Creek Bridge was originally constructed to span the Chariton River, before the river was channelized and moved  $\frac{1}{4}$  mile to the east. Puzzle Creek formerly entered the Chariton approximately  $\frac{1}{2}$  mile upstream from the bridge, the confluence having since been moved  $\frac{1}{2}$  mile downstream. Today the bridge spans the old Chariton River bed, which has been renamed Puzzle Creek. In November 1903 Chariton County Surveyor and Ex Officio Road and Bridge Commissioner, O.G. Dameron presented plans and specifications for a new steel bridge across the Chariton River to the county court. The county contracted for the structure's construction with A. M. Blodgett of Kansas City for \$2700.00. A year later, on November 17, 1904, Dameron reported that the bridge was completed. The structure has been closed to traffic and its deck has been removed. A new steel stringer bridge with a concrete deck has been constructed adjacent to the old span. The Puzzle Creek Bridge is a typical example of a common truss configuration, with standard detailing, unremarkable dimensions and below-average degree of physical integrity.

**NAME(S) OF STRUCTURE**  
Puzzle Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**  
Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 126001.9; Chariton County Court Record, Book K: page 280 (3 November 1903), Book L: pages 3-4 (17 November 1904) - located at Chariton County Courthouse, Keytesville MO; field inspection by Richard Collier, 9 June 1991.

**INVENTORIED BY**  
Michelle Crow-Dolby

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
9 March 1993

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**  
Middle Fork Bridge  
MHTD: 463001.4

CHAR28

**DATE(S) OF CONSTRUCTION**  
1907

**LOCATION**

County Road 463 over Middle Fork; S16/21, T54N, R16W  
6.0 miles northeast of Salisbury; Chariton County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 48)

**CONDITION**

fair

**OWNER**

Chariton County

span number:	1	superstructure:	steel, 3-panel, rigid-connected Warren pony truss
span length:	50.0'	substructure:	unknown
total length:	75.0'	floor/decking:	timber deck
roadway wdt.:	11.5'	other features:	steel lattice guardrails

Located some six miles northeast of Salisbury, this small-scale steel truss carries an unpaved county road over the Middle Fork. The Middle Fork Bridge is comprised of a single rigid-connected Warren pony truss, with a timber deck and steel lattice guardrails. The bridge dates to 1907. That year the Chariton County Court contracted with the Kansas City Bridge Company to supply and erect the structure. The Middle Fork Bridge was finished by November 1907, at which time KCBCo was paid \$669.00. Since its completion, the structure has carried vehicular traffic, with only maintenance-related repairs.

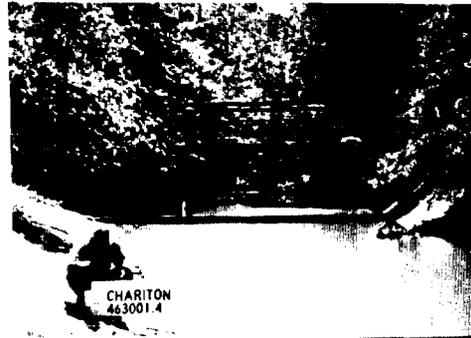
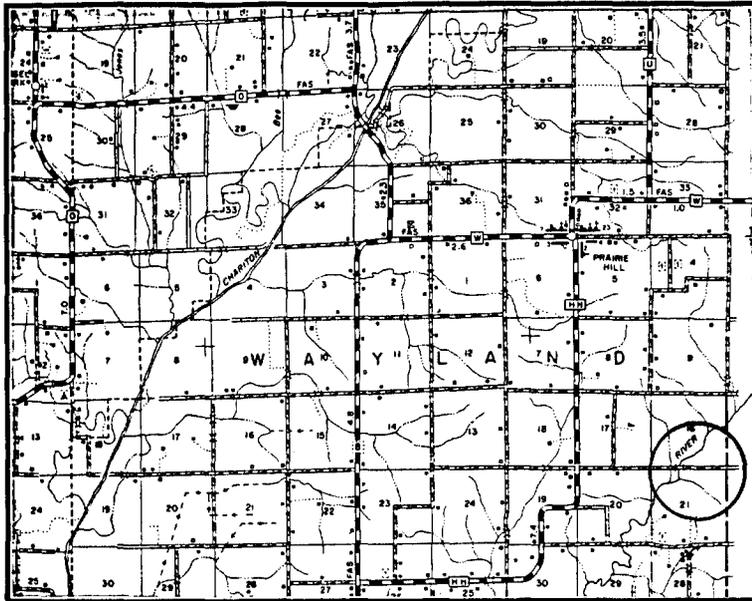
Patented in 1848 by Captain James Warren and Theobald Monzani, the Warren truss in its classic form features a web configuration that relies on simple triangulation for its rigidity. "The term Warren truss or Warren girder was originally applied only to the particular case of the Triangular truss in which the web triangles are all equilateral; but later writers generally use the name for any triangular truss," noted bridge engineer J.A.L. Waddell stated in his 1916 *Bridge Engineering*. "As there is no special advantage in making the web triangles equilateral, there does not appear to be any good *raison d'être* for the use of the true Warren type." Warrens were built sparingly in the 19th century, a period in which the pin-connected Pratt dominated the bridge industry. After the turn of the century, however, rigid-connected Warren trusses began to supersede earlier pinned Pratt configurations for use on short- to intermediate-span highway bridges. The Middle Fork Bridge in Chariton County is significant as one of the earliest documented examples in Missouri of the riveted Warren truss—a small-scale example of what would later become a mainstay structural type in the state.

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**NAME(S) OF STRUCTURE**

Middle Fork Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 463001.4; Chariton County Court Record, Book O: page 36 (8 November 1907) - located at the Chariton County Courthouse, Keytesville MO.

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**INVENTORIED BY**

Michelle Crow-Dolby

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

9 March 1993

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Chariton River Bridge  
MHTD: 499000.1

CHAR30

**DATE(S) OF CONSTRUCTION**

1906

**LOCATION**

County Road 499 over East Fork of Chariton River; S35, T53N, R17W  
5.5 miles south of Salisbury; Chariton County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 51)

**CONDITION**

fair

**OWNER**

Chariton County

span number: 1

span length: 100.0'

total length: 126.0'

roadway wdt.: 17.5'

superstructure: steel, 6-panel, rigid-connected Pratt through truss, with steel stringer approach span

substructure: concrete-filled steel cylinder piers

floor/decking: concrete deck over steel stringers

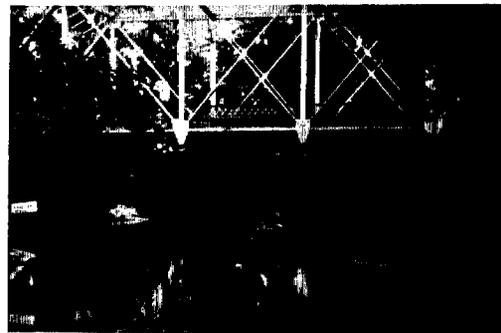
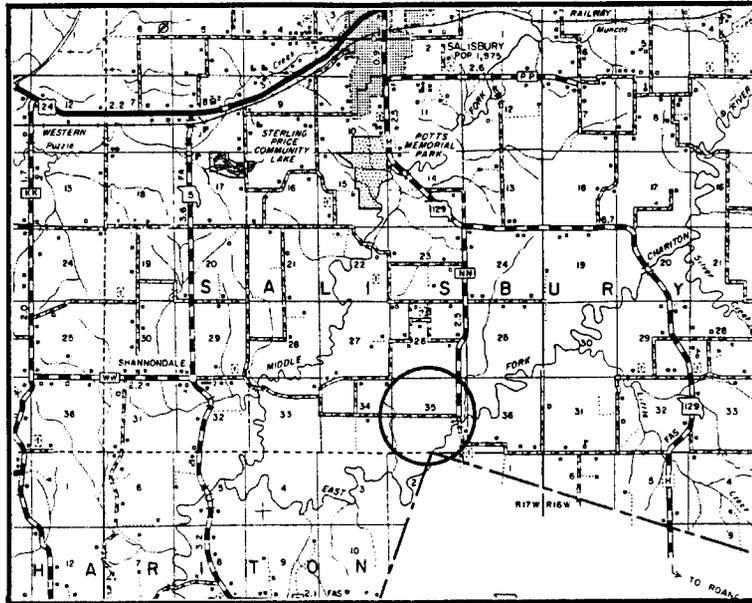
other features: steel lattice guardrails

This short-span through truss carries an unpaved county road across the East Fork of the Chariton River south of Salisbury. The bridge consists of a single rigid-connected Pratt truss, supported by steel cylinder piers, with a steel stringer approach span on one end. Chariton County records make only terse references to bridge construction, but they indicate that this bridge was constructed here in 1906. The truss was completed in August of that year by the Pan American Bridge Company of Moberly, Missouri, a local bridge firm that may have been an adjunct to the regionally active Pan American Bridge Company of Indiana. Total cost of the bridge: \$2600.00. Since its completion, the Chariton River Bridge has carried intermittent traffic at this rural crossing, in unaltered condition.

The Pratt truss was the steel bridge of choice for an overwhelming majority of Missouri county road crossings in the late 19th and early 20th centuries. Although virtually all of these early spans featured pinned connections, a few Pratts with bolted or field-riveted connections began to appear after 1905. In the 1920s the Missouri State Highway Department adopted the rigid-connected Pratt through truss as one of its standards, and numerous trusses remain in the state from this later period. Only a handful of pre-MSHD rigid-connected Pratts remain in place, however. The Chariton River Bridge in Chariton County is distinguished among these as the earliest dateable example in Missouri of this mainstay structural type.

**NAME(S) OF STRUCTURE**  
Chariton River Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 499000.1; Chariton County Court Record, Book M: page 570 (8 August 1906) - located at Chariton County Courthouse, Keytesville MO.

**INVENTORIED BY**  
Michelle Crow-Dolby

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
9 March 1993

# GRUNDY COUNTY

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**INCLUDED:** [Significant feature(s) of bridge given in boldface]  
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
GRUN01	F1093R1	Honey Creek Bridge	(replaced)
GRUN02	J 771	W. Medicine Creek Bridge	1-100' riveted Warren pony truss 1932 Ebbe Construction Company
GRUN03	K 943	Trenton Viaduct	1-160' riveted Pratt through truss 1931 CRI&P Railroad
GRUN04	T 899	Weldon River Bridge	3-105' steel plate through girder 1936 Bushman Construction Co.
GRUN05	022001.9	Woods Creek Bridge	1- 60' pinned Pratt half-hip pony truss c1910
*GRUN06	042000.9	Honey Creek Bridge	1-105' pinned Pratt through truss c1915 Illinois Steel Bridge Co. (prob.)
GRUN07	047000.2	West Honey Creek Bridge	1- 40' riveted Pratt half-hip pony truss c1915
GRUN08	051000.7	Honey Creek Bridge	1- 44' pinned Pratt half-hip pony truss c1910
GRUN09	056000.2	Honey Creek Bridge	1- 30' steel stringer 1911 county work force
GRUN10	058000.8	Big No Creek Bridge	(replaced)
GRUN11	066001.5	Big No Creek Bridge	1- 28' steel stringer 1913 county work force
GRUN12	070001.1	Big No Creek Bridge	1- 22' steel stringer 1912 county work force
GRUN13	076001.7	Medicine Creek Bridge	1-108' pinned Pratt through truss c1910
GRUN14	079000.2	Little No Creek Bridge	1- 28' steel stringer 1907 county work force
*GRUN15	082001.2	Medicine Creek Bridge	1-100' pinned Pratt through truss c1890
*GRUN16	083001.2	Medicine Creek Bridge	(replaced)
GRUN17	087002.5	Bridge	(replaced)
GRUN18	089002.0	Medicine Creek Bridge	(replaced)
*GRUN19	089003.0	Bridge	1- 22' steel stringer 1915 county work force
GRUN20	097000.3	Little No Creek Bridge	1- 35' riveted lattice pony truss c1910
GRUN21	110000.8	No Creek Bridge	1- 45' pinned Pratt half-hip pony truss c1910
*GRUN22	113000.5	Honey Creek Bridge	1-120' 2-angle Parker through truss c1925 county work force (prob.)
GRUN23	132001.0	Muddy Creek Bridge	1-120' riveted Pratt through truss 1912 county work force
GRUN24	140000.4	Old Weldon River Bridge	(replaced)

# GRUNDY COUNTY

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## INCLUDED (cont.):

GRUN25	161001.7	Grand River Bridge		(replaced)
GRUN26	161002.0	Thompson River Bridge		(replaced)
*GRUN27	167000.6	Sugar Creek Bridge		(replaced)
*GRUN28	169003.4	Furnace Creek Bridge		(replaced)
GRUN29	179000.8	Old Raccoon Creek Bridge	1- 48' 1905	pinned Pratt half-hip pony truss Midland Bridge Company
GRUN30	200000.4	Hickory Creek Bridge	1- 24' 1909	steel stringer county work force
GRUN31	201001.2	Hickory Creek Bridge	1- 48' 1917	riveted Warren pony truss Illinois Steel Bridge Company
*GRUN32	209000.3	West Wolf Creek Bridge	1- 48' 1911	pinned Pratt half-hip pony truss Canton Bridge Company
GRUN33	235000.3	Bachelor Branch Bridge	1- 24' 1907	steel stringer county work force
*GRUN34	239001.2	Honey Creek Bridge	1-130' c1925	<b>2-angle Parker through truss</b> county work force (prob.)
*GRUN35	251001.5	Honey Creek Bridge	1-130' 1926	<b>2-angle Parker through truss</b> county work force
GRUN36	260000.2	Crooked Creek Bridge	1- 26' 1907	steel stringer county work force
GRUN37	273000.1	Bridge	1- 36' 1910	steel stringer county work force
*GRUN38	280000.1	Medicine Creek Bridge	1-180' c1925	<b>2-angle Parker through truss</b> county work force (prob.)
GRUN39	282002.4	Black Oak Branch Bridge		(replaced)
GRUN40	310000.6	Medicine Creek Bridge	1- 28' 1905	steel stringer Midland Bridge Company
GRUN41	320001.5	Crooked Creek Bridge	1- 30' 1911	steel stringer county work force
*GRUN42	344000.2	Wolf Creek Bridge	1- 48' 1956	<b>2-angle Warren pony truss</b> county work force (prob.)
GRUN43	347000.9	Kemp Bridge		(replaced)
GRUN44	352000.1	Stevens Branch Bridge	1- 24' 1914	steel stringer county work force
GRUN45	357000.2	Gees Creek Bridge	1- 28' 1912	steel stringer county work force
GRUN46	360002.8	Hickory Creek Bridge		(replaced)
GRUN47	372000.4	Gees Creek Bridge	1- 26' 1909	steel stringer county work force

## EXCLUDED:

Pratt pony truss

032002.0 080001.7 165R02.0 167000.1 179000.6 209001.0

Warren pony truss

007000.7 023000.8 027000.9 034002.1 051000.1 201001.2 325000.2  
379000.5

# GRUNDY COUNTY

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## EXCLUDED (cont.):

Lattice pony truss  
245001.3

### Steel stringer

J 769	J 797	L 340	S 402	S 586	S 601	S 657
U4260001	X 153	Z 953	003R00.8	003000.2	009001.3	016000.4
020R00.4	027000.2	037000.0	041000.8	081001.0	087R01.7	087R02.2
087001.3	124001.2	155001.3	156000.6	159002.1	161003.9	162002.2
171000.2	172R01.2	183000.2	200000.4	203000.9	228000.2	230000.3
247001.2	252000.0	257000.3	278001.5	281000.7	284000.4	313000.9
315000.4	317000.4	318001.1	323001.1	353000.2	359002.0	361000.7
364001.2	364001.5	366002.4	373000.4	426000.3		

Concrete slab  
J 798

Concrete box culvert  
S 502

## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	4	31	0	0	35
Excluded	11	58	1	0	70
	15	89	1	0	105 structures

# West Medicine Creek Bridge

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GRUN02

## GENERAL DATA

<b>structure no.:</b> J 771	<b>city/town:</b> immediately east of Galt
<b>county:</b> Grundy	<b>feature inters.:</b> West Medicine Creek
	<b>cadastral grid:</b> S33, T62N, R22W
	<b>highway route:</b> Missouri State Highway 6
	<b>highway distr.:</b> 2
	<b>current owner:</b> Grundy County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, rigid-connected Warren pony truss, with steel stringer approach spans	
<b>substructure:</b> concrete abutments, wingwalls and piers	
<b>span number:</b> 1	<b>condition:</b> good
<b>span length:</b> 100.0'	<b>alterations:</b> none
<b>total length:</b> 357.0'	<b>floor/decking :</b> concrete deck over steel stringers
<b>roadway width:</b> 22.0'	<b>other features:</b> MSHD standard concrete guardrails

## HISTORICAL DATA

<b>erection date:</b> 1931-32	
<b>erection cost:</b> \$26,786.66	
<b>designer:</b> Missouri State Highway Department	
<b>fabricator :</b> unknown	
<b>contractor:</b> Ebbe Construction Company	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number J 771; files on Primary System Bridges - located at the Missouri Highway and Transportation Department, Jefferson City MO.	
<b>sign. rating:</b> 41	
<b>evaluation:</b> NRHP non-eligible (typically configured example of MSHD standard truss type)	

**inventoried by:** Clayton B. Fraser    22 April 1994

# Trenton Viaduct

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GRUN03

## GENERAL DATA

<b>structure no.:</b> K 943	<b>city/town:</b> Trenton
<b>county:</b> Grundy	<b>feature inters.:</b> Chicago, Rock Island and Pacific Railroad
	<b>cadastral grid:</b> S21, T61N, R24W
	<b>highway route:</b> Missouri State Highway 6
	<b>highway distr.:</b> 2
	<b>current owner:</b> Grundy County

## STRUCTURAL DATA

<b>superstructure:</b> steel, rigid-connected Pratt through truss, with concrete girder and slab approach spans	
<b>substructure:</b> concrete abutments and piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 160.0'	<b>alterations:</b> none
<b>total length:</b> 254.0'	<b>floor/decking :</b> concrete deck over steel stringers
<b>roadway width:</b> 28.0'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1931
<b>erection cost:</b> unknown
<b>designer:</b> Chicago, Rock Island and Pacific Railroad
<b>fabricator :</b> unknown
<b>contractor:</b> Chicago, Rock Island and Pacific Railroad

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number K 943; files on Primary System Bridges - located at the Missouri Highway and Transportation Department, Jefferson City MO.

<b>sign. rating:</b> 38
<b>evaluation:</b> NRHP non-eligible (undistinguished example of common structural type)

**inventoried by:** Clayton B. Fraser    22 April 1994

# Weldon River Bridge

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GRUN04

## GENERAL DATA

structure no.:	T 899	city/town:	Spickard
county:	Grundy	feature inters.:	Weldon River
		cadastral grid:	S21/22, T63N, R24W
		highway route:	State Supplementary Route C
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel plate through girder		
substructure:	concrete abutments, wingwalls and piers		
span number:	2; 1	condition:	good
span length:	90.0'; 105.0'	alterations:	none
total length:	289.0'	floor/decking :	concrete deck over steel stringers
roadway width:	24.0'	other features:	steel guardrails

## HISTORICAL DATA

erection date:	1936
erection cost:	\$33,972.10
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	Bushman Construction Company
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number T 899; files on Primary System Bridges - located at the Missouri Highway and Transportation Department, Jefferson City MO.
sign. rating:	57
evaluation:	NRHP possibly eligible (well-preserved example of MSHD long-span steel beam construction)

inventoried by: Clayton B. Fraser    22 April 1994

# Woods Creek Bridge

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GRUN05

## GENERAL DATA

structure no.:	022001.9	city/town:	3.9 miles southwest of Spickard
county:	Grundy	feature inters.:	Woods Creek
		cadastral grid:	S31, T63N, R24W
		highway route:	County Road 22
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	74.0'	floor/decking :	timber deck
roadway width:	13.9'	other features:	laced endposts; steel angle guardrails

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 022001.9.
sign. rating:	28
evaluation:	NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Clayton B. Fraser    22 April 1994

# Honey Creek Bridge

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GRUN06

## GENERAL DATA

<b>structure no.:</b> 042000.9	<b>city/town:</b> 4.7 miles southeast of Spickard
<b>county:</b> Grundy	<b>feature inters.:</b> Honey Creek
	<b>cadastral grid:</b> S29/32, T63N, R23W
	<b>highway route:</b> County Road 42
	<b>highway distr.:</b> 2
	<b>current owner:</b> Grundy County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 6-panel, pin-connected Pratt through truss, with steel stringer approach span at the west end	
<b>substructure:</b> steel pile bent abutments with timber wingwalls; steel pile bent pier	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 105.0'	<b>alterations:</b> truss possibly moved
<b>total length:</b> 125.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 13.8'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles (2 angles with kneebraces); floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels

## HISTORICAL DATA

<b>erection date:</b> c1915	
<b>erection cost:</b> unknown	
<b>designer:</b> Illinois Steel Bridge Company, Jacksonville IL (probable)	
<b>fabricator :</b> Illinois Steel Bridge Company, Jacksonville IL (probable); Illinois Steel Company, Chicago IL	
<b>contractor :</b> Illinois Steel Bridge Company, Jacksonville IL (probable)	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 042000.9; Court order to M.H. Mooney for bridge letting (no date) - located at the Grundy County Courthouse, Trenton MO; field inspection by Clayton Fraser, 9 December 1990.	
<b>sign. rating:</b> 27	
<b>evaluation:</b> NRHP non-eligible (typically configured, poorly documented example of common structural type)	

**inventoried by:** Clayton Fraser and Michelle Crow-Dolby    22 April 1994

# West Honey Creek Bridge

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GRUN07

## GENERAL DATA

<b>structure no.:</b>	047000.2	<b>city/town:</b>	4.8 miles east of Spickard
<b>county:</b>	Grundy	<b>feature inters.:</b>	West Honey Creek
		<b>cadastral grid:</b>	S17, T63N, R23W
		<b>highway route:</b>	County Road 47
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Grundy County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 3-panel, rigid-connected Pratt half-hip pony truss		
<b>substructure:</b>	unknown		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	40.0'	<b>alterations:</b>	unknown
<b>total length:</b>	40.0'	<b>floor/decking :</b>	timber deck
<b>roadway width:</b>	12.0'	<b>other features:</b>	steel angle guardrails

## HISTORICAL DATA

<b>erection date:</b>	c1915
<b>erection cost:</b>	unknown
<b>designer:</b>	unknown
<b>fabricator :</b>	unknown
<b>contractor:</b>	unknown
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 047000.2.
<b>sign. rating:</b>	20
<b>evaluation:</b>	NRHP non-eligible (typically configured, poorly documented example of common structural type)

inventoried by: Clayton B. Fraser 22 April 1994

# Honey Creek Bridge

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GRUN08

## GENERAL DATA

structure no.:	051000.7	city/town:	5.7 miles east of Spickard
county:	Grundy	feature inters.:	Honey Creek
		cadastral grid:	S21, T63N, R23W
		highway route:	County Road 51
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss, with steel stringer approach span		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	44.0'	alterations:	none
total length:	63.0'	floor/decking :	timber deck
roadway width:	12.0'	other features:	unknown

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 051000.7.

sign. rating:	20
evaluation:	NRHP non-eligible (typically configured, poorly documented example of common structural type)

inventoried by: Clayton B. Fraser    22 April 1994

# Honey Creek Bridge

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GRUN09

## GENERAL DATA

structure no.:	056000.2	city/town:	6.3 miles southeast of Spickard
county:	Grundy	feature inters.:	branch of Honey Creek
		cadastral grid:	S4/33, T62/63N, R23W
		highway route:	County Road 56
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	none
span number:	1	floor/decking :	unknown
span length:	30.0'	other features:	unknown
total length:	30.0'		
roadway width:	12.0'		

## HISTORICAL DATA

erection date:	1911
erection cost:	\$375.00 (engineer's estimate)
designer:	M.H. Mooney, Grundy County Engineer
fabricator :	unknown
contractor :	county work force
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 056000.2; Grundy County Court Record, Book L: page 610 (4 May 1911); letter and specifications to the county court from M.H. Mooney (3 May 1911) - all located at the Grundy County Courthouse, Trenton MO.
sign. rating:	30
evaluation:	NRHP non-eligible (typical, small-scale example of exceedingly common structural type)

Inventoried by: Clayton B. Fraser    22 April 1994

# Big No Creek Bridge

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GRUN11

## GENERAL DATA

<b>structure no.:</b>	066001.5	<b>city/town:</b>	7.2 miles northwest of Galt
<b>county:</b>	Grundy	<b>feature inters.:</b>	Big No Creek
		<b>cadastral grid:</b>	S26/35, T63N, R23W
		<b>highway route:</b>	County Road 66
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Grundy County

## STRUCTURAL DATA

<b>superstructure:</b>	steel stringer	<b>condition:</b>	fair
<b>substructure:</b>	unknown	<b>alterations:</b>	unknown
<b>span number:</b>	1	<b>floor/decking :</b>	unknown
<b>span length:</b>	30.0'	<b>other features:</b>	unknown
<b>total length:</b>	30.0'		
<b>roadway width:</b>	14.0'		

## HISTORICAL DATA

**erection date:** 1913  
**erection cost:** \$340.00 (engineer's estimate)  
**designer:** M.H. Mooney, Grundy County Engineer  
**fabricator :** unknown  
**contractor:** county work force

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 066001.5; Grundy County Court Record, Book M: page 137 (4 June 1912), page 268 (7 March 1913), page 307 (4 June 1913) - located at the Grundy County Courthouse, Trenton MO; specifications for 30-foot steel stringer - located in the bridge drawer, at the Grundy County Courthouse in Trenton MO.

**sign. rating:** 30  
**evaluation:** NRHP non-eligible (typical, small-scale example of exceedingly common structural type)

**inventoried by:** Clayton B. Fraser    22 April 1994

# Big No Creek Bridge

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GRUN12

## GENERAL DATA

structure no.:	070001.1	city/town:	9.0 miles northwest of Galt
county:	Grundy	feature inters.:	Big No Creek
		cadastral grid:	S13/24, T63N, R23W
		highway route:	County Road 70
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	22.0'	other features:	unknown
total length:	24.0'		
roadway width:	12.0'		

## HISTORICAL DATA

erection date:	1912
erection cost:	unknown
designer:	M.H. Mooney, Grundy County Engineer
fabricator :	unknown
contractor:	county work force
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 070001.1; Grundy County Court Record, Book I: page 327 (5 September 1899); Grundy County Court Record, Book M: page 137 (4 June 1912) - located at the Grundy County Courthouse, Trenton MO.
sign. rating:	30
evaluation:	NRHP non-eligible (typical, small-scale example of exceedingly common structural type)

Inventoried by: Clayton B. Fraser    22 April 1994

# Medicine Creek Bridge

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GRUN13

## GENERAL DATA

structure no.:	076001.7	city/town:	7.4 miles north of Galt
county:	Grundy	feature inters.:	West Fork of Medicine Creek
		cadastral grid:	S28, T63N, R22W
		highway route:	County Road 76
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel, 7-panel, pin-connected Pratt through truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	108.0'	alterations:	unknown
total length:	109.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 076001.7.
sign. rating:	25
evaluation:	NRHP non-eligible (typical example of common structural type, inadequately documented)

inventoried by: Clayton B. Fraser 22 April 1994

# Little No Creek Bridge

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GRUN14

## GENERAL DATA

structure no.:	079000.2	city/town:	5.8 miles north of Galt
county:	Grundy	feature inters.:	Little No Creek
		cadastral grid:	S6/31, T62N, R22W
		highway route:	County Road 79
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	28.0'	other features:	unknown
total length :	30.0'		
roadway width:	12.0'		

## HISTORICAL DATA

erection date: 1907  
erection cost: \$379.00  
designer: M.H. Mooney, Grundy County Engineer  
fabricator : unknown  
contractor: county work force

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 079000.2; Grundy County Court Record, Book K: pages 582-83 (5 June 1907); letter to county court from M.H. Mooney (4 June 1907) - all located at the Grundy County Courthouse, Trenton MO.

sign. rating: 30  
evaluation: NRHP non-eligible (typical, small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser    22 April 1994

# Medicine Creek Bridge

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GRUN15

## GENERAL DATA

<b>structure no.:</b> 082001.2	<b>city/town:</b> 5.7 miles north of Galt
<b>county:</b> Grundy	<b>feature inters.:</b> West Fork of Medicine Creek
	<b>cadastral grid:</b> S4/33, T62/63N, R22W
	<b>highway route:</b> County Road 82
	<b>highway distr.:</b> 2
	<b>current owner:</b> Grundy County

## STRUCTURAL DATA

**superstructure:** wrought iron or steel, 6-panel, pin-connected Pratt through truss, with steel stringer approach span at the east end

**substructure:** steel pile bent abutments and piers

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 100.0'	<b>alterations:</b> truss moved to this location
<b>total length:</b> 130.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 14.4'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 looped round eyerods at the hip); diagonal: 2 punched rectangular eyebars; counter: round rod with sleeve bolt; lateral bracing: looped square eyebars (bottom), round rod with sleeve bolt (top); strut: channel with angle kneebraces; portal strut: curved brackets with decorative cutouts; floor beam: I-beam, field-bolted to vertical; guard-rail: 1 channel

## HISTORICAL DATA

**erection date:** c1890  
**erection cost:** unknown  
**designer:** unknown  
**fabricator :** unknown  
**contractor:** unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 082001.2; field inspection by Clayton Fraser, 9 December 1990.

**sign. rating:** 21  
**evaluation:** NRHP non-eligible (Though apparently fabricated at an early date, the bridge is poorly documented and has subsequently been moved, diminishing its interpretive value.)

**inventoried by:** Clayton B. Fraser    22 April 1994

# Bridge

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GRUN19

## GENERAL DATA

structure no.:	089003.0	city/town:	immediately north of Galt
county:	Grundy	feature inters.:	unnamed stream
		cadastral grid:	S28/29/32/33, T62N, R22W
		highway route:	County Road 89
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel stringer		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	22.0'	alterations:	none
total length:	23.0'	floor/decking :	unknown
roadway width:	14.0'	other features:	incised in top of wingwall: M.E. Ralls 10/2/15

## HISTORICAL DATA

erection date:	1915		
erection cost:	unknown		
designer:	M.H. Mooney, Grundy County Engineer		
fabricator :	unknown		
contractor :	county work force		
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 089003.0; Grundy County Court Record, Book K: pages 582-83 (5 June 1907); Grundy County Court Record, Book L: page 15 (4 November 1907) - located at the Grundy County Courthouse, Trenton MO.		
sign. rating:	32		
evaluation:	NRHP non-eligible (typical, small-scale example of exceedingly common structural type)		

Inventoried by: Clayton B. Fraser    22 April 1994

# Little No Creek Bridge

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GRUN20

## GENERAL DATA

structure no.:	097000.3	city/town:	4.7 miles northwest of Galt
county:	Grundy	feature inters.:	Little No Creek
		cadastral grid:	S11/14, T62N, R23W
		highway route:	County Road 97
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel, rigid-connected lattice bedstead		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	35.0'	alterations:	unknown
total length:	36.0'	floor/decking :	timber deck
roadway width:	11.0'	other features:	unknown

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 097000.3.

sign. rating:	20
evaluation:	NRHP non-eligible (undistinguished small-scale pony truss)

inventoried by: Clayton B. Fraser    22 April 1994

# No Creek Bridge

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GRUN21

## GENERAL DATA

structure no.:	110000.8	city/town:	1.1 miles north of Dunlap
county:	Grundy	feature inters.:	No Creek
		cadastral grid:	S4/33, T61/62N, R23W
		highway route:	County Road 110
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss, with steel stringer approach spans		
substructure:	concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	45.0'	alterations:	unknown
total length:	76.0'	floor/decking :	timber deck
roadway width:	14.0'	other features:	unknown

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 110000.8.

sign. rating:	25
evaluation:	NRHP non-eligible (undistinguished small-scale pony truss)

inventoried by: Clayton B. Fraser 22 April 1994

# Honey Creek Bridge

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GRUN22

## GENERAL DATA

<b>structure no.:</b>	113000.5	<b>city/town:</b>	3.2 miles northwest of Dunlap
<b>county:</b>	Grundy	<b>feature inters.:</b>	Honey Creek
		<b>cadastral grid:</b>	S30, T62N, R23W
		<b>highway route:</b>	County Road 113
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Grundy County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 7-panel, rigid-connected 2-angle Parker through truss, with steel stringer approach spans		
<b>substructure:</b>	steel pile bent piers and abutments with timber wingwalls		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	120.0'	<b>alterations:</b>	none
<b>total length:</b>	179.0'	<b>floor/decking :</b>	timber deck over steel stringers
<b>roadway width:</b>	16.0'	<b>other features:</b>	upper chord and inclined end post: 2 heavy angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; lateral bracing: round rod with threaded ends; strut: angle A-frame; portal strut: angle A-frame; floor beam: I-beam; guardrail: 2 angles

## HISTORICAL DATA

<b>erection date:</b>	c1925
<b>erection cost:</b>	unknown
<b>designer:</b>	Chillicothe Iron Works, Chillicothe MO (probable)
<b>fabricator :</b>	Chillicothe Iron Works, Chillicothe MO (probable)
<b>contractor :</b>	county work force (probable)
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 113000.5; Grundy County Court Record, Book H: page 514 (2 June 1896), page 523 (7 July 1896) - located at the Grundy County Courthouse, Trenton MO; field inspection by Clayton Fraser, 9 December 1990.
<b>sign. rating:</b>	51
<b>evaluation:</b>	NRHP possibly eligible (well-preserved example of uncommon structural type)

Inventoried by: Clayton B. Fraser 22 April 1994

# Muddy Creek Bridge

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GRUN23

## GENERAL DATA

<b>structure no.:</b> 132001.0	<b>city/town:</b> 3.8 miles southeast of Spickard
<b>county:</b> Grundy	<b>feature inters.:</b> Muddy Creek
	<b>cadastral grid:</b> S2/3/10/11, T62N, R24W
	<b>highway route:</b> County Road 132
	<b>highway distr.:</b> 2
	<b>current owner:</b> Grundy County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 6-panel, rigid-connected Pratt through truss, with steel stringer approach spans	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 120.0'	<b>alterations:</b> none
<b>total length:</b> 154.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 12.3'	<b>other features:</b> steel angle guardrails

## HISTORICAL DATA

<b>erection date:</b> 1912
<b>erection cost:</b> unknown
<b>designer:</b> unknown
<b>fabricator :</b> unknown
<b>contractor:</b> county work force

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 132001.0; Grundy County Court Record, Book I: page 231 (6 February 1899); Grundy County Court Record, Book M: page 129 (8 May 1912), page 137 (4 June 1912) - located at the Grundy County Courthouse, Trenton MO.

<b>sign. rating:</b> 45
<b>evaluation:</b> NRHP possibly eligible (well-preserved, early example of mainstay structural type)

**inventoried by:** Clayton B. Fraser    22 April 1994

# Old Raccoon Creek Bridge

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GRUN29

## GENERAL DATA

<b>structure no.:</b> 179000.8	<b>city/town:</b> 2.7 miles south of Brimson
<b>county:</b> Grundy	<b>feature inters.:</b> Old Raccoon Creek
	<b>cadastral grid:</b> S8, T61N, R25W
	<b>highway route:</b> County Road 179
	<b>highway distr.:</b> 2
	<b>current owner:</b> Grundy County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt half-hip pony truss	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 48.0'	<b>alterations:</b> unknown
<b>total length:</b> 48.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.6'	<b>other features:</b> steel lattice guardrails

## HISTORICAL DATA

<b>erection date:</b> 1905
<b>erection cost:</b> unknown
<b>designer:</b> Midland Bridge Company, Kansas City MO
<b>fabricator :</b> Midland Bridge Company, Kansas City MO
<b>contractor:</b> Midland Bridge Company, Kansas City MO
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 179000.8; Grundy County Court Record, Book J: page 640 (7 June 1904); Grundy County Court Record, Book K: page 154 (5 April 1905); Court order to M.H. Mooney for bridge letting (no date) - all located at the Grundy County Courthouse, Trenton MO.
<b>sign. rating:</b> 36
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)

**inventoried by:** Clayton B. Fraser    22 April 1994

# Hickory Creek Bridge

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GRUN30

## GENERAL DATA

structure no.:	200000.4	city/town:	6.5 miles south of Brimson
county:	Grundy	feature inters.:	Hickory Creek
		cadastral grid:	S30, T61N, R25W
		highway route:	County Road 200
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	24.0'	other features:	unknown
total length:	26.0'		
roadway width:	12.0'		

## HISTORICAL DATA

erection date:	1909
erection cost:	unknown
designer:	M.H. Mooney, Grundy County Engineer
fabricator :	unknown
contractor:	county work force
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 200000.4; specifications for 24-foot steel stringer by M.H. Mooney (9 April 1909) -located in the bridge drawer at the Grundy County Courthouse in Trenton MO.
sign. rating:	32
evaluation:	NRHP non-eligible (undistinguished, small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser    22 April 1994

# Hickory Creek Bridge

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GRUN31

## GENERAL DATA

<b>structure no.:</b>	201001.2	<b>city/town:</b>	7.0 miles south of Brimson
<b>county:</b>	Grundy	<b>feature inters.:</b>	Hickory Creek
		<b>cadastral grid:</b>	S32, T61N, R25W
		<b>highway route:</b>	County Road 201
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Grundy County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 3-panel, rigid-connected Warren pony truss		
<b>substructure:</b>	concrete abutments and wingwalls		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	48.0'	<b>alterations:</b>	unknown
<b>total length:</b>	48.0'	<b>floor/decking :</b>	timber deck
<b>roadway width:</b>	11.6'	<b>other features:</b>	steel angle guardrails

## HISTORICAL DATA

<b>erection date:</b>	1917
<b>erection cost:</b>	unknown
<b>designer:</b>	Illinois Steel Bridge Company, Jacksonville IL (probable)
<b>fabricator :</b>	Illinois Steel Bridge Company, Jacksonville IL (probable)
<b>contractor:</b>	Illinois Steel Bridge Company, Jacksonville IL (probable)
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 201001.2; letter to county court from M.H. Mooney (23 July 1917) - located at the Grundy County Courthouse, Trenton MO.
<b>sign. rating:</b>	34
<b>evaluation:</b>	NRHP non-eligible (typical example of common structural type)

Inventoried by: Clayton B. Fraser    22 April 1994

# West Wolf Creek Bridge

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GRUN32

## GENERAL DATA

<b>structure no.:</b> 209000.3	<b>city/town:</b> 4.1 miles southwest of Trenton
<b>county:</b> Grundy	<b>feature inters.:</b> West Wolf Creek
	<b>cadastral grid:</b> S26/35, T61N, R25W
	<b>highway route:</b> County Road 209
	<b>highway distr.:</b> 2
	<b>current owner:</b> Grundy County

## STRUCTURAL DATA

**superstructure:** steel, 3-panel, pin-connected Pratt half-hip pony truss  
**substructure:** concrete abutments and wingwalls

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 48.0'	<b>alterations:</b> none
<b>total length:</b> 48.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 11.8'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: 1 round eyerod with turnbuckle; lateral bracing: round eyerod with turnbuckle; floor beam: I-beam, field-bolted to vertical; guardrail: 1 angle

## HISTORICAL DATA

**erection date:** 1911  
**erection cost:** \$650.00 (engineer's estimate)  
**designer:** Canton Bridge Company, Canton OH  
**fabricator :** Canton Bridge Company, Canton OH;  
Cambria Steel Company, Pittsburgh PA  
**contractor:** Canton Bridge Company, Canton OH

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 209000.3; Grundy County Court Record, Book L: page 610 (4 May 1911) - located at the Grundy County Courthouse, Trenton MO; letter to county court from M.H. Mooney (3 May 1911) - located in the bridge drawer at the Grundy County Courthouse in Trenton MO; field inspection by Clayton Fraser, 9 December 1990.

**sign. rating:** 34  
**evaluation:** NRHP non-eligible (typically configured example of common structural type)

**inventoried by:** Clayton B. Fraser    22 April 1994

# Bachelor Branch Bridge

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GRUN33

## GENERAL DATA

structure no.:	235000.3	city/town:	3.8 miles northeast of Trenton
county:	Grundy	feature inters.:	Bachelor Branch
		cadastral grid:	S2/3/34/35, T61/62N, R24W
		highway route:	County Road 235
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	24.0'	other features:	unknown
total length:	25.0'		
roadway width:	14.0'		

## HISTORICAL DATA

erection date: 1907  
erection cost: \$242.10  
designer: M.H. Mooney, Grundy County Engineer  
fabricator : unknown  
contractor: county work force

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 235000.3; Grundy County Court Record, Book K: page 600 (3 July 1907); Grundy County Court Record, Book L: page 87 (5 March 1908) - located at the Grundy County Courthouse, Trenton MO; letter to county court from M.H. Mooney (1 July 1907) - located in the bridge drawer at the Grundy County Courthouse in Trenton MO.

sign. rating: 32  
evaluation: NRHP non-eligible (undistinguished, small-scale example of common structural type)

Inventoried by: Clayton B. Fraser 22 April 1994

# Honey Creek Bridge

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GRUN34

## GENERAL DATA

<b>structure no.:</b> 239001.2	<b>city/town:</b> 2.2 miles west of Dunlap
<b>county:</b> Grundy	<b>feature inters.:</b> Honey Creek
	<b>cadastral grid:</b> S6/7, T61N, R23W
	<b>highway route:</b> County Road 239
	<b>highway distr.:</b> 2
	<b>current owner:</b> Grundy County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 8-panel, rigid-connected 2-angle Parker through truss, with steel stringer approach spans	
<b>substructure:</b> steel pile bent piers and abutments with timber wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 130.0'	<b>alterations:</b> none
<b>total length:</b> 170.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 14.0'	<b>other features:</b> upper chord and inclined end post: 2 angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; counter: 1 angle; lateral bracing: round rod with threaded ends; strut: 1 braced angle; floor beam: I-beam, field-bolted to vertical; guardrail: 2 angles

## HISTORICAL DATA

<b>erection date:</b> c1925	
<b>erection cost:</b> unknown	
<b>designer:</b> Chillicothe Iron Works, Chillicothe MO (probable)	
<b>fabricator :</b> Chillicothe Iron Works, Chillicothe MO (probable); Illinois Steel Company, Chicago IL	
<b>contractor:</b> county work force (probable)	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 239001.2; field inspection by Clayton Fraser, 9 December 1990.	
<b>sign. rating:</b> 51	
<b>evaluation:</b> NRHP possibly eligible (well-preserved example of uncommon structural type)	

**inventoried by:** Clayton B. Fraser    22 April 1994

# Honey Creek Bridge

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GRUN35

## GENERAL DATA

<b>structure no.:</b> 251001.5	<b>city/town:</b> 5.7 miles west of Laredo
<b>county:</b> Grundy	<b>feature inters.:</b> Honey Creek
	<b>cadastral grid:</b> S12/36, T60/61N, R24W
	<b>highway route:</b> County Road 251
	<b>highway distr.:</b> 2
	<b>current owner:</b> Grundy County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 8-panel, rigid-connected 2-angle Parker through truss	
<b>substructure:</b> steel pile bent abutments with timber wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 130.0'	<b>alterations:</b> none
<b>total length:</b> 130.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 13.6'	<b>other features:</b> upper chord and inclined end post: 2 heavy angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; lateral bracing: round rod with threaded ends; strut: angle A-frame; portal strut: angle A-frame; floor beam: I-beam; guardrail: 2 angles

## HISTORICAL DATA

<b>erection date:</b> 1926	
<b>erection cost:</b> unknown	
<b>designer:</b> Chillicothe Iron Works, Chillicothe MO (probable)	
<b>fabricator :</b> Chillicothe Iron Works, Chillicothe MO (probable); Illinois Steel Company, Chicago IL	
<b>contractor:</b> county work force	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 251001.5; Grundy County Court Record, Book P: page 361 (8 September 1926) - located at the Grundy County Courthouse, Trenton MO; field inspection by Clayton Fraser, 9 December 1990.	
<b>sign. rating:</b> 53	
<b>evaluation:</b> NRHP possibly eligible (well-preserved example of uncommon structural type)	

inventoried by: Clayton B. Fraser 22 April 1994

# Crooked Creek Bridge

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GRUN36

## GENERAL DATA

structure no.:	260000.2	city/town:	2.3 miles north of Laredo
county:	Grundy	feature inters.:	North Fork of Crooked Creek
		cadastral grid:	S27, T61N, R23W
		highway route:	County Road 260
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	26.0'	other features:	unknown
total length:	28.0'		
roadway width:	14.0'		

## HISTORICAL DATA

erection date: 1907  
erection cost: \$343.46  
designer: M.H. Mooney, Grundy County Engineer  
fabricator : unknown  
contractor : county work force

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 260000.2; Grundy County Court Record, Book K: pages 572-73 (10 May 1907), pages 582-83 (5 June 1907); Grundy County Court Record, Book L: page 15 (4 November 1907) - located at the Grundy County Courthouse, Trenton MO; letter to county court from M.H. Mooney (4 June 1907) - located in the bridge drawer at the Grundy County Courthouse, Trenton MO.

sign. rating: 32  
evaluation: NRHP non-eligible (typical small-scale example of common structural type)

Inventoried by: Clayton B. Fraser 22 April 1994

# Bridge

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GRUN37

## GENERAL DATA

<b>structure no.:</b> 273000.1	<b>city/town:</b> immediately south of Dunlap
<b>county:</b> Grundy	<b>feature inters.:</b> unnamed stream
	<b>cadastral grid:</b> S3/10, T61N, R23W
	<b>highway route:</b> County Road 273
	<b>highway distr.:</b> 2
	<b>current owner:</b> Grundy County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 36.0'	<b>alterations:</b> unknown
<b>total length:</b> 36.0'	<b>floor/decking :</b> unknown
<b>roadway width:</b> 14.0'	<b>other features:</b> unknown

## HISTORICAL DATA

**erection date:** 1910  
**erection cost:** \$400.00 (engineer's estimate)  
**designer:** M.H. Mooney, Grundy County Engineer  
**fabricator :** unknown  
**contractor:** county work force

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 273000.1; Grundy County Court Record, Book L: page 440 (4 May 1910) - located at the Grundy County Courthouse, Trenton MO; letter to county court from M.H. Mooney (2 May 1910) - located in the bridge drawer at the Grundy County Courthouse, Trenton MO.

**sign. rating:** 32  
**evaluation:** NRHP non-eligible (typical example of common structural type)

**Inventoried by:** Clayton B. Fraser    22 April 1994

# Medicine Creek Bridge

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GRUN38

## GENERAL DATA

<b>structure no.:</b> 280000.1	<b>city/town:</b> 2.4 miles south of Galt
<b>county:</b> Grundy	<b>feature inters.:</b> Medicine Creek
	<b>cadastral grid:</b> S9/16, T61N, R22W
	<b>highway route:</b> County Road 280
	<b>highway distr.:</b> 2
	<b>current owner:</b> Grundy County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 10-panel, rigid-connected 2-angle Parker through truss, with steel stringer approach spans	
<b>substructure:</b> steel pile bent piers and abutments with timber wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 180.0'	<b>alterations:</b> none
<b>total length:</b> 220.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 13.5'	<b>other features:</b> upper chord and inclined end post: 2 angles with cover plate; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; lateral bracing: round rod with threaded ends; strut: angle A-frame; portal strut: "M"; floor beam: I-beam; guardrail: 2 angles

## HISTORICAL DATA

<b>erection date:</b> c1925	
<b>erection cost:</b> unknown	
<b>designer:</b> Chillicothe Iron Works, Chillicothe MO (probable)	
<b>fabricator :</b> Chillicothe Iron Works, Chillicothe MO (probable); Illinois Steel Company, Chicago IL	
<b>contractor :</b> county work force (probable)	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 280000.1; field inspection by Clayton Fraser, 9 December 1990.	
<b>sign. rating:</b> 55	
<b>evaluation:</b> NRHP determined non-eligible (well-preserved example of uncommon structural type)	

Inventoried by: Clayton B. Fraser 22 April 1994

# Medicine Creek Bridge

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GRUN40

## GENERAL DATA

structure no.:	310000.6	city/town:	immediately east of Laredo
county:	Grundy	feature inters.:	Branch of Medicine Creek
		cadastral grid:	S13/14, T60N, R23W
		highway route:	County Road 310
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	25.0'	other features:	unknown
total length:	28.0'		
roadway width:	13.5'		

## HISTORICAL DATA

erection date: 1905  
erection cost: unknown  
designer: M.H. Mooney, Grundy County Engineer  
fabricator : Midland Bridge Company, Kansas City MO  
contractor: Midland Bridge Company, Kansas City MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 310000.6; Grundy County Court Record, Book K: pages 162-63 (7 April 1905), page 177 (3 May 1905), page 197 (June 1905) - located at the Grundy County Courthouse, Trenton MO.

sign. rating: 36  
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser 22 April 1994

# Crooked Creek Bridge

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GRUN41

## GENERAL DATA

structure no.:	320001.5	city/town:	1.9 miles southwest of Laredo
county:	Grundy	feature inters.:	North Fork of Crooked Creek
		cadastral grid:	S15/22, T60N, R23W
		highway route:	County Road 320
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	30.0'	other features:	unknown
total length:	30.0'		
roadway width:	12.2'		

## HISTORICAL DATA

erection date: 1911  
erection cost: \$350.00 (engineer's estimate)  
designer: M.H. Mooney, Grundy County Engineer  
fabricator : unknown  
contractor: county work force

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 320001.5; Grundy County Court Record, Book L: page 616 (6 June 1911); letter and specifications to county court from M.H. Mooney (6 June 1911) - all located at the Grundy County Courthouse, Trenton MO.

sign. rating: 32  
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser 22 April 1994

# Wolf Creek Bridge

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GRUN42

## GENERAL DATA

structure no.:	344000.2	city/town:	3.8 miles southwest of Trenton
county:	Grundy	feature inters.:	Wolf Creek
		cadastral grid:	S7, T60N, R24W
		highway route:	County Road 344
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure: steel, 3-panel, welded/bolted 2-angle Warren pony truss  
substructure: steel pile bent abutments with timber wingwalls

span number:	1	condition:	fair
span length:	48.0'	alterations:	none
total length:	48.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.8'	other features:	upper chord and inclined end post: 2 angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field- bolted to vertical; guardrail: 2 channels; welded outriders; welded beading on upper chord: "56"

## HISTORICAL DATA

erection date: 1956 (probable)  
erection cost: unknown  
designer: unknown  
fabricator : U.S. Steel Company  
contractor: county work force (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 344000.2; field inspection by Clayton Fraser, 9 December 1990.

sign. rating: 28  
evaluation: NRHP non-eligible (relatively late example of indigenous structural type)

inventoried by: Clayton B. Fraser 22 April 1994

# Stevens Branch Bridge

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GRUN44

## GENERAL DATA

structure no.:	352000.1	city/town:	7.7 miles southwest of Trenton
county:	Grundy	feature inters.:	Stevens Branch
		cadastral grid:	S26/27, T60N, R25W
		highway route:	County Road 352
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	24.0'	other features:	unknown
total length:	26.0'		
roadway width:	12.0'		

## HISTORICAL DATA

erection date:	1914
erection cost:	unknown
designer:	M.H. Mooney, Grundy County Engineer
fabricator :	unknown
contractor :	county work force
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 352000.1; Grundy County Court Record, Book M: page 419 (11 April 1914) - located at the Grundy County Courthouse, Trenton MO.
sign. rating:	30
evaluation:	NRHP non-eligible (typical, small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser    22 April 1994

# Gees Creek Bridge

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GRUN45

## GENERAL DATA

<b>structure no.:</b>	357000.2	<b>city/town:</b>	7.2 miles southwest of Trenton
<b>county:</b>	Grundy	<b>feature inters.:</b>	Gees Creek
		<b>cadastral grid:</b>	S26/27, T60N, R25W
		<b>highway route:</b>	County Road 357
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Grundy County

## STRUCTURAL DATA

<b>superstructure:</b>	steel stringer		
<b>substructure:</b>	unknown		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	28.0'	<b>alterations:</b>	unknown
<b>total length:</b>	30.0'	<b>floor/decking :</b>	unknown
<b>roadway width:</b>	12.0'	<b>other features:</b>	unknown

## HISTORICAL DATA

<b>erection date:</b>	1912
<b>erection cost:</b>	unknown
<b>designer:</b>	M.H. Mooney, Grundy County Engineer
<b>fabricator :</b>	unknown
<b>contractor :</b>	county work force
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 357000.2; Grundy County Court Record, Book M: page 129 (8 May 1912) - located at the Grundy County Courthouse, Trenton MO.
<b>sign. rating:</b>	30
<b>evaluation:</b>	NRHP non-eligible (undistinguished, small-scale example of exceedingly common structural type)

**inventoried by:** Clayton B. Fraser    22 April 1994

# Gees Creek Bridge

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GRUN47

## GENERAL DATA

structure no.:	372000.4	city/town:	9.1 miles southwest of Trenton
county:	Grundy	feature inters.:	Gees Creek
		cadastral grid:	S29/30, T60N, R25W
		highway route:	County Road 372
		highway distr.:	2
		current owner:	Grundy County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	26.0'	other features:	unknown
total length :	28.0'		
roadway width:	14.0'		

## HISTORICAL DATA

erection date:	1909
erection cost:	unknown
designer:	M.H. Mooney, Grundy County Engineer
fabricator :	unknown
contractor :	county work force
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 372000.4.
sign. rating:	32
evaluation:	NRHP non-eligible (undistinguished, small-scale example of exceedingly common structural type)

inventoried by: Clayton B. Fraser    22 April 1994

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**  
Weldon River Bridge  
MHTD: T 899

GRUN04

**DATE(S) OF CONSTRUCTION**  
1936

**LOCATION**

State Supplementary Route C over Weldon River; S21/22, T63N, R24W  
Spickard; Grundy County, Missouri

**USE (ORIGINAL / CURRENT)**

highway bridge / highway bridge

**RATING** NRHP possibly eligible (score: 57)

**CONDITION**

good

**OWNER**

Grundy County

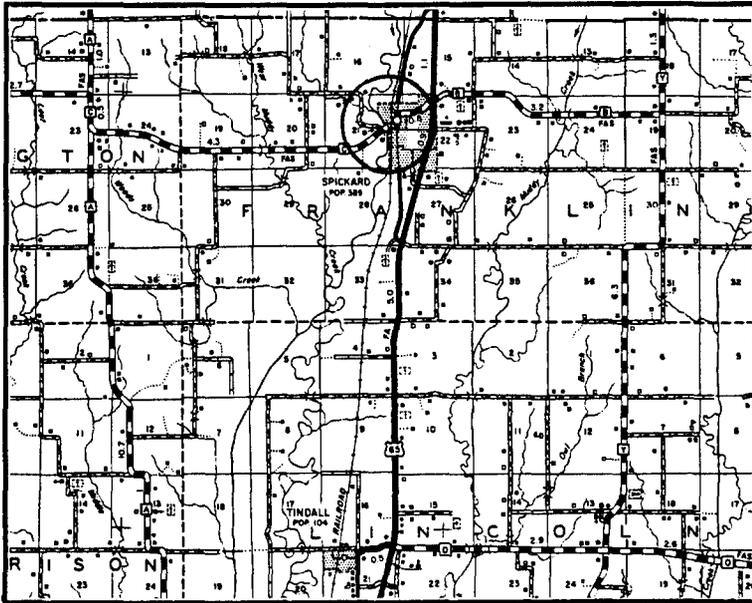
span number: 2; 1	superstructure: steel plate through girder
span length: 90.0'; 105.0'	substructure: concrete abutments, wingwalls and piers
total length: 289.0'	floor/decking: concrete deck over steel stringers
roadway wdt.: 24.0'	other features: steel guardrails

This multiple-span bridge carries State Supplementary Route C over the Weldon River in the small town of Spickard. The structure consists of a single long-span riveted plate through girder, flanked by a pair of shorter through girder approach spans; the superstructure is supported by concrete piers and abutments. The Weldon River Bridge was engineered by the Missouri State Highway Department in 1936. Designating the project as WPSS 718(A), the agency solicited competitive proposals in April. In May the state highway commission awarded a contract to build the bridge to the Bushman Construction Company. The contractors apparently completed the structure that year for \$33,972.10. Since that time the bridge has functioned in place, with only minor maintenance-related repairs.

The Weldon River Bridge is one of numerous steel plate through girders functioning in place on Missouri's highway system. Most of these bridges, including this structure in Grundy County, were built in the 1930s, and many are multiple-span examples. Although it ranks among the longest of these, the Weldon River Bridge is a typical example of a starkly utilitarian design.

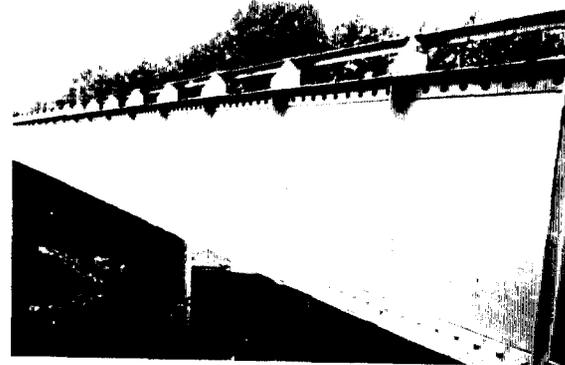
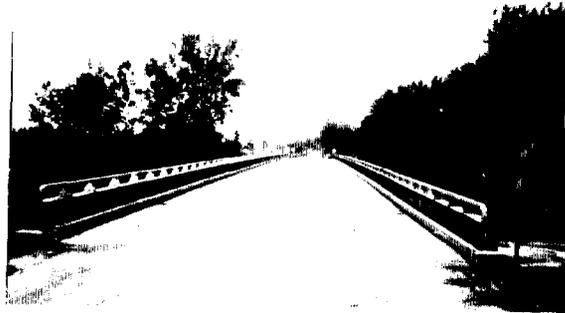
**NAME(S) OF STRUCTURE**  
Weldon River Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP



**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number T 899; files on Primary System Bridges - located at the Missouri Highway and Transportation Department, Jefferson City MO.

**INVENTORIED BY**  
Clayton B. Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
3 July 1993

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**  
 Honey Creek Bridge  
 MHTD: 042000.9

GRUN06

**DATE(S) OF CONSTRUCTION**  
 c1915

**LOCATION**  
 County Road 42 over Honey Creek; S29/32, T63N, R23W  
 4.7 miles southeast of Spickard; Grundy County, Missouri

**USE (ORIGINAL / CURRENT)**  
 roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 27)

**CONDITION**  
 fair

**OWNER**  
 Grundy County

span number: 1  
 span length: 105.0'  
 total length: 125.0'  
 roadway wdt.: 13.8'

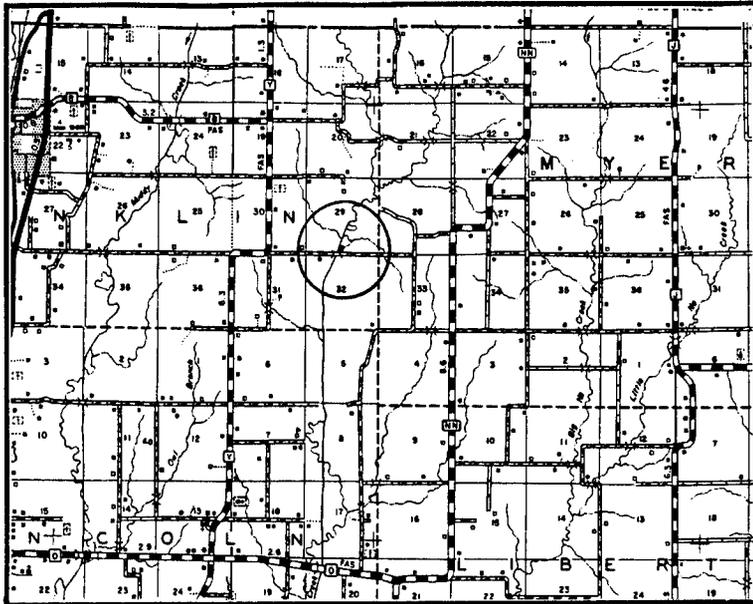
superstructure: steel, 6-panel, pin-connected Pratt through truss, with steel stringer approach span at the west end  
 substructure: steel pile bent abutments with timber wingwalls; steel pile bent pier  
 floor/decking: timber deck over steel stringers  
 other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles (2 angles with kneebraces); floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels

Included in the over sixty structures listed in Grundy County's historic bridge inventory is this pin-connected Pratt through truss located some five miles southeast of Spickard. The bridge carries County Road 42 across Honey Creek between sections 29 and 32 in northwestern Grundy County. Supported by a steel pile bent pier and abutments with timber wingwalls and approached by a steel stringer on the west, the bridge dates to circa 1915. Although the county recorder kept copious notes concerning the construction of the counties' bridges, the Honey Creek Bridge is not mentioned specifically. The unknown contractor used steel components rolled by the Chicago-based Illinois Steel Factory and erected the span approximately five years after the turn of the century. A typically configured example of a Missouri mainstay structural type, the bridge has continued to serve rural travelers, without substantial alteration, since its erection.

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**NAME(S) OF STRUCTURE**  
Honey Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**  
Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 042000.9; Court order to M.H. Mooney for bridge letting (no date) - located at the Grundy County Courthouse, Trenton MO; field inspection by Clayton Fraser, 9 December 1990.

**INVENTORIED BY**  
Clayton B. Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
22 April 1994

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Honey Creek Bridge  
MHTD: 113000.5

GRUN22

**DATE(S) OF CONSTRUCTION**

c1925

**LOCATION**

County Road 113 over Honey Creek; S30, T62N, R23W  
3.2 miles northwest of Dunlap; Grundy County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 51)

**CONDITION**

fair

**OWNER**

Grundy County

span number: 1  
span length: 120.0'  
total length: 179.0'  
roadway wdt.: 16.0'

superstructure: steel, 7-panel, rigid-connected 2-angle Parker through truss, with steel stringer approach spans  
substructure: steel pile bent piers and abutments with timber wingwalls  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 heavy angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; lateral bracing: round rod with threaded ends; strut: angle A-frame; portal strut: angle A-frame; floor beam: I-beam; guardrail: 2 angles

Located some three miles northwest of Dunlap, this medium-span steel truss carries a gravel-surfaced county road across Honey Creek in Marion Township. The structure is comprised of a single rigid-connected Parker through truss, supported by steel pile bent piers and approached by steel stringer spans. The truss employs extremely light construction, with two angles and a cover plate used in lieu of the typical box beam for the upper chords and endposts. This two-angle configuration distinguishes it as one of many such spans fabricated in the 1920s and 1930s by the Chillicothe Iron Works of Chillicothe, Missouri. Grundy County bought several truss superstructures from Chillicothe Iron, erecting the spans using county force account laborers. Although county records do not discuss this bridge over Honey Creek specifically, the structure is apparently one of the Chillicothe Iron spans built by the county in the mid-1920s. Since its completion, the Honey Creek Bridge has functioned in place at this lightly trafficked crossing, with no substantial alterations.

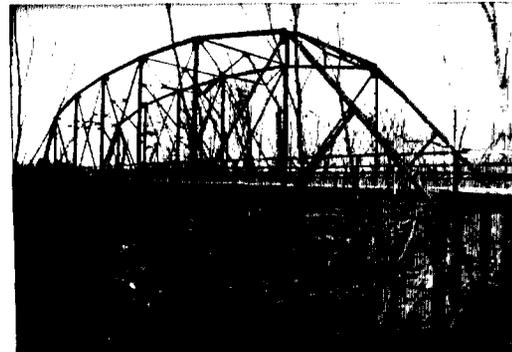
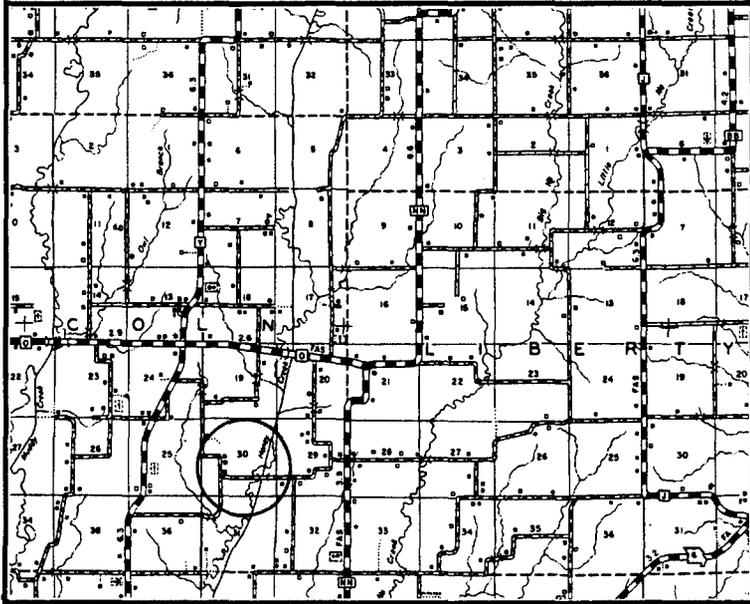
Although two-angle trusses can be found in many Midwestern states, none seem to have embraced the technology as much as Missouri in the 1920s. Numerous two-angle trusses can still be found in the state, particularly in the north-central counties near Livingston County, of which Chillicothe is the seat. The Chillicothe Iron Works fabricated its two-angle trusses using traditional web configurations: Warren, Pratt and Pratt variants such as the Parker. Like more traditional box-beam trusses, Warren and Pratt two-angle ponies were used the most widely, and such esoteric long-span configurations as the Parker were used far less often. With its 120-foot Parker configuration, the Honey Creek Bridge is thus an uncommon example—one of only a handful—of this indigenous Missouri structural type. Its well-preserved condition makes it noteworthy among Missouri's two-angle trusses.

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**NAME(S) OF STRUCTURE**

Honey Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 113000.5; Grundy County Court Record, Book H: page 514 (2 June 1896), page 523 (7 July 1896) - located at the Grundy County Courthouse, Trenton MO; field inspection by Clayton Fraser, 9 December 1990.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

22 April 1994

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Muddy Creek Bridge  
MHTD: 132001.0

GRUN23

**DATE(S) OF CONSTRUCTION**

1912

**LOCATION**

County Road 132 over Muddy Creek; S2/3/10/11, T62N, R24W  
3.8 miles southeast of Spickard; Grundy County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 45)

**CONDITION**

fair

**OWNER**

Grundy County

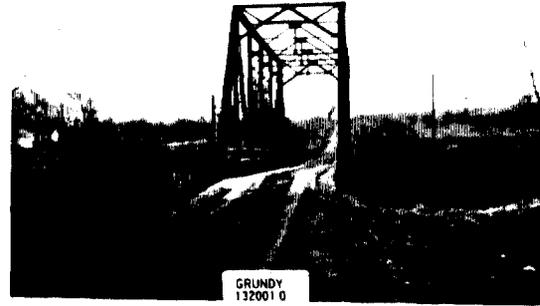
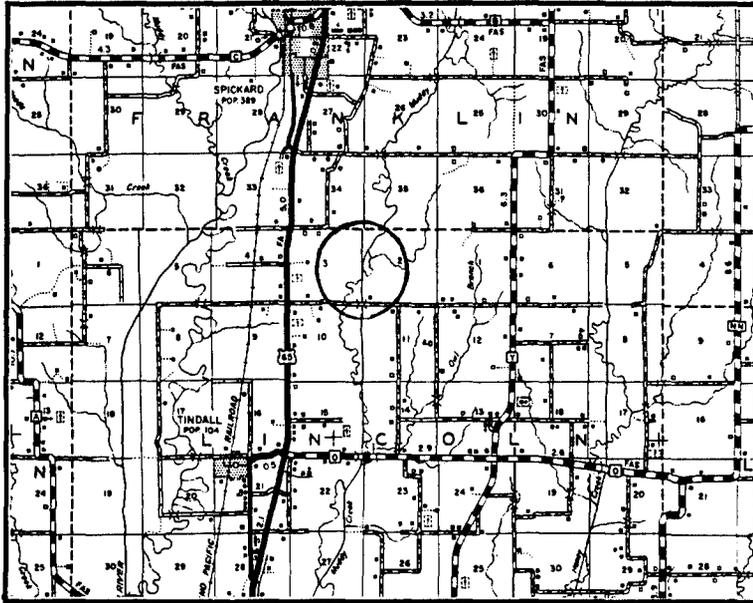
span number: 1	superstructure: steel, 6-panel, rigid-connected Pratt through truss, with steel stringer approach spans
span length: 120.0'	substructure: unknown
total length: 154.0'	floor/decking: timber deck
roadway wdt.: 12.3'	other features: steel angle guardrails

Located almost four miles southeast of Spickard, this medium-span steel truss carries an unpaved county road across Muddy Creek. The bridge is comprised of a rigid-connected Pratt through truss, with steel stringer approach spans. The Muddy Creek Bridge dates to 1912. In May of that year the Grundy County Court directed county bridge commissioner M.H. Mooney to view several crossing sites throughout the county and report regarding bridge construction at them. One of these crossings was this location over Muddy Creek in Lincoln Township. On June 4th the court ordered Mooney to purchase steel superstructures for several of the crossings and build the bridges. Presumably, he employed force account laborers to build the substructure for the Muddy Creek Bridge. Since its completion, it has functioned in place, with only maintenance-related repairs.

The Pratt truss was the steel bridge of choice for an overwhelming majority of Missouri county road crossings in the late 19th and early 20th centuries. Although virtually all of these early spans featured pinned connections, a few Pratts with bolted or field-riveted connections began to appear after 1905. In the 1920s the Missouri State Highway Department adopted the rigid-connected Pratt through truss as one of its standards, and numerous trusses remain in the state from this later period. Only a handful of pre-MSHD rigid-connected Pratts remain in place, however. The Muddy Creek Bridge, with its tall profile and attenuated members, is distinguished among these as one of the earliest dateable example in Missouri of this mainstay structural type.

**NAME(S) OF STRUCTURE**  
Muddy Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 132001.0; Grundy County Court Record, Book I: page 231 (6 February 1899); Grundy County Court Record, Book M: page 129 (8 May 1912), page 137 (4 June 1912) - located at the Grundy County Courthouse, Trenton MO.

**INVENTORIED BY**  
Clayton B. Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
22 April 1994

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**  
Honey Creek Bridge  
MHTD: 239001.2

GRUN34

**DATE(S) OF CONSTRUCTION**  
c1925

**LOCATION**

County Road 239 over Honey Creek; S6/7, T61N, R23W  
2.2 miles west of Dunlap; Grundy County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 51)

**CONDITION**  
fair

**OWNER**  
Grundy County

span number: 1  
span length: 130.0'  
total length: 170.0'  
roadway wdt.: 14.0'

superstructure: steel, 8-panel, rigid-connected 2-angle Parker through truss, with steel stringer approach spans  
substructure: steel pile bent piers and abutments with timber wingwalls  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; counter: 1 angle; lateral bracing: round rod with threaded ends; strut: 1 braced angle; floor beam: I-beam, field-bolted to vertical; guardrail: 2 angles

Located some two miles west of Dunlap, this medium-span steel truss carries a gravel-surfaced county road across Honey Creek in Marion Township. The structure is comprised of a single rigid-connected Parker through truss, supported by steel pile bent piers and approached by steel stringer spans. The truss employs extremely light construction, with two angles and a cover plate used in lieu of the typical box beam for the upper chords and endposts. This two-angle configuration distinguishes it as one of many such spans fabricated in the 1920s and 1930s by the Chillicothe Iron Works of Chillicothe, Missouri. Grundy County bought several truss superstructures from Chillicothe Iron, erecting the spans using county force account laborers. Although county records do not discuss this bridge over Honey Creek specifically, the structure is apparently one of the Chillicothe Iron spans built by the county in the mid-1920s. Since its completion, the Honey Creek Bridge has functioned in place at this lightly trafficked crossing, with no substantial alterations.

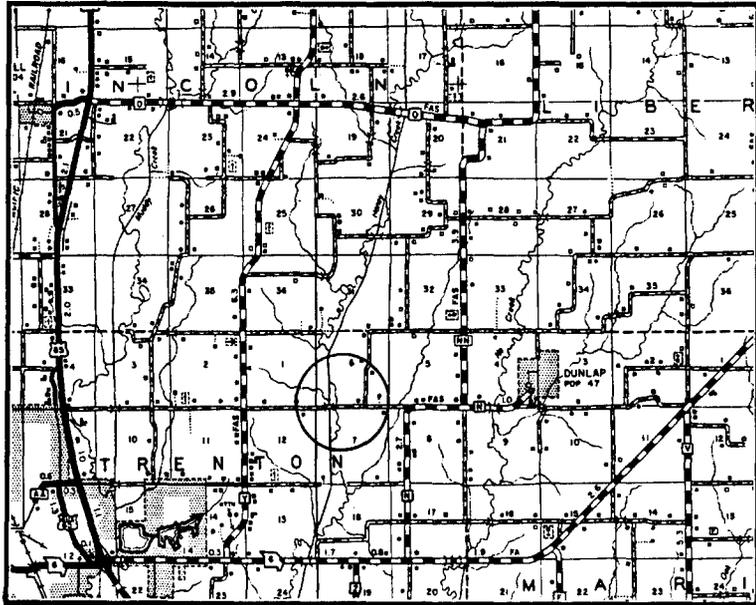
Although two-angle trusses can be found in many Midwestern states, none seem to have embraced the technology as much as Missouri in the 1920s. Numerous two-angle trusses can still be found in the state, particularly in the north-central counties near Livingston County, of which Chillicothe is the seat. The Chillicothe Iron Works fabricated its two-angle trusses using traditional web configurations: Warren, Pratt and Pratt variants such as the Parker. Like more traditional box-beam trusses, Warren and Pratt two-angle ponies were used the most widely, and such esoteric long-span configurations as the Parker were used far less often. With its 130-foot Parker configuration, the Honey Creek Bridge is thus an uncommon example—one of only a handful—of this indigenous Missouri structural type. Its well-preserved condition makes it noteworthy among Missouri's two-angle trusses.

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**NAME(S) OF STRUCTURE**

Honey Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 239001.2; field inspection by Clayton Fraser, 9 December 1990.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

22 April 1994

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**  
Honey Creek Bridge  
MHTD: 251001.5

GRUN35

**DATE(S) OF CONSTRUCTION**  
1926

**LOCATION**

County Road 251 over Honey Creek; S12/36, T60/61N, R24W  
5.7 miles west of Laredo; Grundy County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 53)

**CONDITION**  
fair

**OWNER**  
Grundy County

span number: 1  
span length: 130.0'  
total length: 130.0'  
roadway wdt.: 13.6'

superstructure: steel, 8-panel, rigid-connected 2-angle Parker through truss  
substructure: steel pile bent abutments with timber wingwalls  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 heavy angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; lateral bracing: round rod with threaded ends; strut: angle A-frame; portal strut: angle A-frame; floor beam: I-beam; guardrail: 2 angles

Located almost six miles west of Laredo, this medium-span steel truss carries a gravel-surfaced county road across Honey Creek between Jackson and Trenton Townships. The structure is comprised of a single rigid-connected Parker through truss, supported by steel pile bent abutments. The truss employs extremely light construction, with two angles and a cover plate used in lieu of the typical box beam for the upper chords and endposts. This two-angle configuration distinguishes it as one of many such spans fabricated in the 1920s and 1930s by the Chillicothe Iron Works of Chillicothe, Missouri. Grundy County bought several truss superstructures from Chillicothe Iron, erecting the spans using county force account laborers. County records indicate that this bridge over Honey Creek was fabricated by Chillicothe Iron and erected by local laborers in 1926. Since its completion, the Honey Creek Bridge has functioned in place at this lightly trafficked crossing, with no substantial alterations.

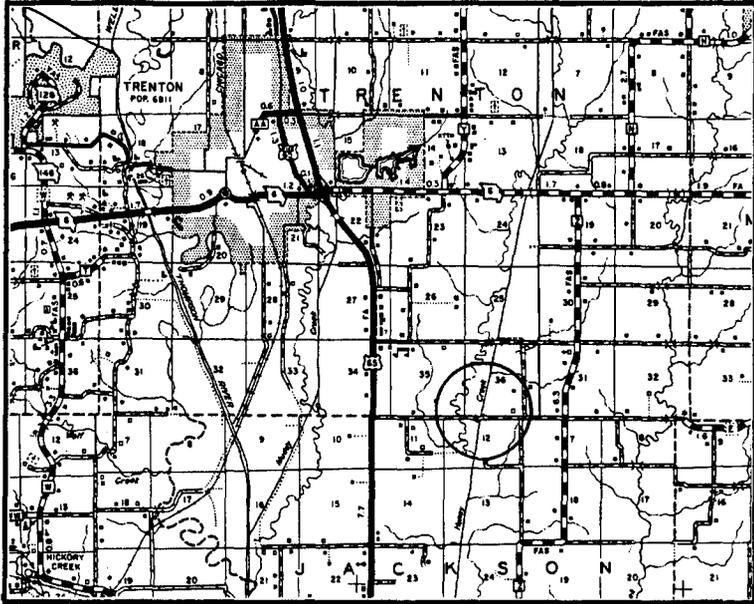
Although two-angle trusses can be found in many Midwestern states, none seem to have embraced the technology as much as Missouri in the 1920s. Numerous two-angle trusses can still be found in the state, particularly in the north-central counties near Livingston County, of which Chillicothe is the seat. The Chillicothe Iron Works fabricated its two-angle trusses using traditional web configurations: Warren, Pratt and Pratt variants such as the Parker. Like more traditional box-beam trusses, Warren and Pratt two-angle ponies were used the most widely, and such esoteric long-span configurations as the Parker were used far less often. With its 130-foot Parker configuration, the Honey Creek Bridge is thus an uncommon example—one of only a handful—of this indigenous Missouri structural type. Its well-preserved condition makes it noteworthy among Missouri's two-angle trusses.

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**NAME(S) OF STRUCTURE**

Honey Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 251001.5; Grundy County Court Record, Book P: page 361 (8 September 1926) - located at the Grundy County Courthouse, Trenton MO; field inspection by Clayton Fraser, 9 December 1990.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

22 April 1994

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Medicine Creek Bridge  
MHTD: 280000.1

GRUN38

**DATE(S) OF CONSTRUCTION**

c1925

**LOCATION**

County Road 280 over Medicine Creek; S9/16, T61N, R22W  
2.4 miles south of Galt; Grundy County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP determined non-eligible (score: 55)

**CONDITION**

fair

**OWNER**

Grundy County

span number: 1  
span length: 180.0'  
total length: 220.0'  
roadway wdt.: 13.5'

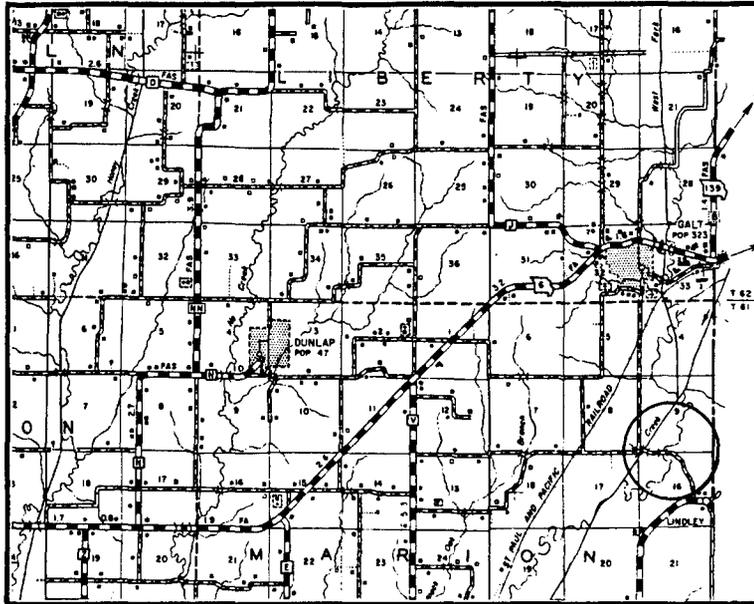
superstructure: steel, 10-panel, rigid-connected 2-angle Parker through truss, with steel stringer approach spans  
substructure: steel pile bent piers and abutments with timber wingwalls  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 angles with cover plate; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; lateral bracing: round rod with threaded ends; strut: angle A-frame; portal strut: "M"; floor beam: I-beam; guardrail: 2 angles

Located some 2½ miles south of Galt, this long-span steel truss carries a gravel-surfaced county road across Medicine Creek in Marion Township. The structure is comprised of a single rigid-connected Parker through truss, supported by steel pile bent piers and approached by steel stringer spans. The truss employs extremely light construction, with two angles and a cover plate used in lieu of the typical box beam for the upper chords and endposts. This two-angle configuration distinguishes it as one of many such spans fabricated in the 1920s and 1930s by the Chillicothe Iron Works of Chillicothe, Missouri. Grundy County bought several truss superstructures from Chillicothe Iron, erecting the spans using county force account laborers. Although county records do not discuss this bridge over Medicine Creek specifically, the structure is apparently one of the Chillicothe Iron spans built by the county in the mid-1920s. Since its completion, the Medicine Creek Bridge has functioned in place at this lightly trafficked crossing, with no substantial alterations.

Although two-angle trusses can be found in many Midwestern states, none seem to have embraced the technology as much as Missouri in the 1920s. Numerous two-angle trusses can still be found in the state, particularly in the north-central counties near Livingston County, of which Chillicothe is the seat. The Chillicothe Iron Works fabricated its two-angle trusses using traditional web configurations: Warren, Pratt and Pratt variants such as the Parker. Like more traditional box-beam trusses, Warren and Pratt two-angle ponies were used the most widely, and such esoteric long-span configurations as the Parker were used far less often. With its 180-foot Parker configuration, the Medicine Creek Bridge is thus an uncommon example—one of only a handful—of this indigenous Missouri structural type. Its well-preserved condition and long span makes it noteworthy among Missouri's two-angle trusses.

**NAME(S) OF STRUCTURE**  
Medicine Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**  
Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 280000.1; field inspection by Clayton Fraser, 9 December 1990.

**INVENTORIED BY**  
Clayton B. Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
22 April 1994

# HOWARD COUNTY

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INCLUDED: [Significant feature(s) of bridge given in boldface]  
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
*HOWA01	G 56R1	Boonville Bridge	<b>3-420'</b> riveted Pennsylv. through truss 1924 Missouri Valley B&I Company; Mt. Vernon Bridge Company
*HOWA02	K 744	New Franklin Viaduct	19-54' concrete deck girder viaduct 1940 Carrothers and Crouch
*HOWA03	K 936	MKT Railroad Overpass	<b>6-100'</b> steel plate deck girder 1941 Otto W. Knutson
HOWA04	013001.5	Batts Creek Bridge	(replaced)
*HOWA05	019002.5	Bonne Femme Creek Bridge	1- 40' pinned Pratt half-hip pony truss c1910
HOWA06	024001.1	Woods Bridge	1- 50' pinned Pratt bedstead 1913 Massillon Bridge & Struct. Co.
*HOWA07	036000.6	Tippett Bridge	(replaced)
*HOWA08	053001.5	Moniteau Creek Bridge	1- 54' pinned Pratt bedstead c1910
*HOWA09	056001.1	Moniteau Creek Bridge	1- 70' pinned Pratt bedstead 1913 Pan-American Bridge Company
*HOWA10	061001.1	Moniteau Creek Bridge	1- 60' pinned Pratt bedstead 1913 Pan-American Bridge Company
*HOWA11	061001.6	Moniteau Creek Bridge	1- 50' pinned Pratt bedstead c1910
*HOWA12	067000.7	Means Ford Bridge	(replaced)
*HOWA13	085002.0	Drake Bridge	1- 80' pinned Pratt pony truss 1899 Wrought Iron Bridge Company
*HOWA14	086001.9	Jones Bridge	(replaced)
*HOWA15	087002.0	Prairie Fork Bridge	1- 70' pinned Pratt pony truss c1900 Wrought Iron Bridge Company
HOWA16	088001.5	Prairie Fork Bridge	1- 64' pinned Pratt bedstead 1909 Pan-American Bridge Company
*HOWA17	108001.3	Bonne Femme Creek Bridge	1- 80' pinned Pratt pony truss c1905
HOWA18	113000.0	Sulphur Creek Bridge	1- 50' pinned Pratt bedstead 1910 Pan-American Bridge Company
*HOWA19	121000.0	Sulphur Creek Bridge	1- 82' 2-angle Camelback pony truss c1940
HOWA20	125000.5	Sulphur Creek Bridge	(replaced)
*HOWA21	161002.0	Adams Fork Bridge	1- 50' pinned Pratt half-hip pony truss 1896 Wrought Iron Bridge Company
*HOWA22	178003.0	Bonne Femme Creek Bridge	(replaced)
HOWA23	196000.4	Greggs Creek Bridge	(replaced)
HOWA24	200000.2	Doxies Creek Bridge	1- 54' pinned Pratt half-hip pony truss 1904
*HOWA25	211001.0	Ridgeway Bridge	1- 80' pinned Pratt pony truss 1907 Pan-American Bridge Company

# HOWARD COUNTY

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## EXCLUDED:

Warren pony truss

049001.2 066000.8 138001.0 139001.2 201001.2

Warren bedstead

040001.2

Lattice bedstead

024000.9 035000.0 048000.2 065004.8 078000.4 139000.8 163000.4  
164000.4

Steel stringer

G 544R	K 45R	S 946	T 116	X 127	X 691	013000.5
019R02.0	027002.0	029000.1	031000.9	032000.8	037000.3	065003.8
067002.3	069002.0	072001.0	078002.0	085001.0	088001.7	088001.8
088001.9	092004.0	124000.2	142000.5	143004.0	143004.0	159001.1
165003.4	175001.0	197000.5	207001.8	221000.4		

Concrete girder

H 266R H 287R H 720R J 7A J 902 K 382 K 962

Concrete slab

J 950 L 119

Concrete box culvert

J 903 J 919

## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	3	15	0	0	18
Excluded	17	40	0	0	57
	<hr/>				
	20	55	0	0	75 structures

# Boonville Bridge

HOWA01

## GENERAL DATA

structure no.: G 56R1      city/town: Boonville  
county: Cooper / Howard      feature inters.: Missouri River  
cadastral grid: Survey 2481, T48N, R16W  
highway route: U.S. Highway 40  
highway distr.: 2  
current owner: Missouri Highway and Transportation Department

## STRUCTURAL DATA

superstructure: steel, 18-panel, rigid-connected Pennsylvania through truss; steel, 11-panel, rigid-connected Parker through truss  
substructure: concrete abutments, wingwalls and piers

span number: 3; 3      condition: good  
span length: 420.0'      alterations: wood block deck replaced with steel grid total flooring and sidewalk added, 1937  
length : 2666.0'      floor/decking : open grid steel deck over steel stringers  
roadway width: 18.1'      other features: upper chord and inclined end post: 2 built-up channels with cover plate and double lacing; lower chord: two built-up channels with lacing; vertical: 2 or 4 angles with lacing; diagonal: 2 channels with lacing or batten plates; lateral bracing: 2 channels (lower), 4 angles with lacing (upper); strut: 4 angles with lacing and bracing below: floor Beam: I-beam; guardrail: steel pipe; portal plate: 1923 / BOONVILLE BRIDGE / BUILT BY THE OLD TRAILS BRIDGE CO / T.A. JOHNSTON PRESIDENT / COOPER COUNTY / HOWARD COUNTY / CITY OF BOONVILLE / FEDERAL AID APPROPRIATED BY MISSOURI STATE HIGHWAY COMMISSION / B.H. PIEPNIER CHIEF ENGINEER / DESIGNED AND SUPERVISED BY HARRINGTON HOWARD AND ASH / CONTRACTORS MT. VERNON BRIDGE CO MT VERNON, OHIO / MISSOURI VALLEY BRIDGE AND IRON CO LEAVENWORTH, KANSAS. on south approach THIS BRIDGE APPROACH WAS ERECTED AND IS MAINTAINED BY THE CITY OF BOONVILLE, MO John E. Waltz Mayor, Charles W. Journey City Counsellor, Charles E. Miller City Clerk, City Council, Rudolph E. Toennes, Dr. Charles Swap, Herman Schmidt, Herman M. Stretz,

## Boonville Bridge

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O.F. Kelly, J.C. Barnhart W.T. Gibson, Irene Schubert, HARRINGTON HOWARD AND ASH ENGINEERS, W.J. COCHRAN AND SON CONTRACTOR, 1922-23, THIS TABLET ERECTED BY A GRATEFUL PEOPLE IN HONOR OF JOHN COSGROVE AND THOMAS A JOHNSTON TO WHOSE VISION AND SERVICE THIS BRIDGE IS A TESTIMONIAL, JULY 1924

### HISTORICAL DATA

**erection date:** 1922-24  
**erection cost:** \$556,883.82  
**designer:** Harrington, Howard and Ash, Kansas City MO  
**fabricator :** Carnegie Steel Company, Pittsburgh PA  
**contractor:** Mt. Vernon Bridge Company, Mt. Vernon OH (superstructure); Missouri Valley Bridge and Iron Company, Leavenworth KS (substructure);

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number G 56R1; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; **Boonville Advertiser** July 4, 1924, **Central Missouri Republican**, 5 January 1922, 16 February 1922; Robert L. Dyer, **Boonville: An Illustrated History**, (by author, 1987), page 231; field inspection by Richard Collier, 8 June 1991.

**sign. rating:** 75  
**evaluation:** NRHP eligible (excellent early example of great river bridge construction)

**inventoried by:** Clayton B. Fraser 29 November 1994

# New Franklin Viaduct

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HOWA02

## GENERAL DATA

<b>structure no.:</b> K 744	<b>city/town:</b> New Franklin
<b>county:</b> Howard	<b>feature inters.:</b> abandoned grade of MKT Railroad
	<b>cadastral grid:</b> S29/28, T49N, R16W
	<b>highway route:</b> State Highway 5
	<b>highway distr.:</b> 2
	<b>current owner:</b> Missouri Highway Transportation Department

## STRUCTURAL DATA

<b>superstructure:</b> multiple span, concrete deck girder viaduct	
<b>substructure:</b> concrete abutments, wingwalls and piers	
<b>span number:</b> 6; 13	<b>condition:</b> good
<b>span length:</b> 54.0'; 48.0'	<b>alterations:</b> none
<b>total length:</b> 1462.0'	<b>floor/decking :</b> asphalt-covered concrete deck
<b>roadway width:</b> 24.0'	<b>other features:</b> arched concrete girders; open balustrade guardrails with flanking pedestrian stairways; modest Moderne treatment on concrete piers

## HISTORICAL DATA

<b>erection date:</b> 1939-40
<b>erection cost:</b> \$167,894.18
<b>designer:</b> Missouri State Highway Department
<b>fabricator :</b> none
<b>contractor:</b> Carrothers and Crouch
<b>references:</b> Missouri Highway Transportation Department, Structure Inventory and Appraisal: Structure Number K 744; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; field inspection by Richard Collier, 8 June 1991.
<b>sign. rating:</b> 51
<b>evaluation:</b> NRHP possibly eligible (well-preserved example of urban viaduct)

**inventoried by:** Clayton B. Fraser    29 November 1994

# MKT Railroad Overpass

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HOWA03

## GENERAL DATA

<b>structure no.:</b> K 936	<b>city/town:</b> 6.6 miles east of Franklin
<b>county:</b> Howard	<b>feature inters.:</b> abandoned grade of MKT Railroad
	<b>cadastral grid:</b> S33, T49N, R15W
	<b>highway route:</b> U.S. Highway 40
	<b>highway distr.:</b> 2
	<b>current owner:</b> Missouri Highway Transportation Department

## STRUCTURAL DATA

<b>superstructure:</b> steel plate deck girder, with continuous steel stringer approach spans	
<b>substructure:</b> concrete abutments, wingwalls and hammerhead, spill-through piers	
<b>span number:</b> 1; 1	<b>condition:</b> good
<b>span length:</b> 100.0'; 80.0'	<b>alterations:</b> none
<b>total length:</b> 578.0'	<b>floor/decking :</b> asphalt-covered concrete deck over steel stringers
<b>roadway width:</b> 26.0'	<b>other features:</b> guardrails; standard MSHD guardrails with open balustrades

## HISTORICAL DATA

<b>erection date:</b> 1941	
<b>erection cost:</b> \$108,219.47	
<b>designer:</b> Missouri State Highway Department	
<b>fabricator :</b> unknown	
<b>contractor :</b> Otto W. Knutson	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number K 936; Missouri Primary System Bridge Record, located at the Missouri Highway and Transportation Department, Jefferson City MO; field inspection by Richard Collier, 8 June 1991.
<b>sign. rating:</b> 41	
<b>evaluation:</b>	NRHP non-eligible (typical long-span example of MSHD beam bridge design)

**inventoried by:** Clayton B. Fraser    29 November 1994

# Bonne Femme Creek Bridge

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HOWA05

## GENERAL DATA

structure no.: 019002.5      city/town: 6.8 miles northeast of Fayette  
county: Howard      feature inters.: Bonne Femme Creek  
cadastral grid: S6/7, T51N, R15W  
highway route: County Road 19  
highway distr.: 2  
current owner: Howard County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss  
substructure: steel pile bent abutments with timber wingwalls faced with corrugated steel

span number: 1      condition: fair  
span length: 40.0'      alterations: unknown  
total length: 40.0'      floor/decking : timber deck over steel stringers  
roadway width: 12.3'      other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 angles with batten plates, 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical below lower chord

## HISTORICAL DATA

erection date: c1910  
erection cost: unknown  
designer: unknown  
fabricator : Jones and Laughlin Steel Company, Pittsburgh PA  
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 019002.5; field inspection by Richard Collier, 8 June 1991.

sign. rating: 20  
evaluation: NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Clayton B. Fraser      29 November 1994

# Woods Bridge

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HOWA06

## GENERAL DATA

<b>structure no.:</b> 024001.1	<b>city/town:</b> 8.7 miles northeast of Fayette
<b>county:</b> Howard	<b>feature inters.:</b> Bonne Femme Creek
	<b>cadastral grid:</b> S33, T52N, R15W
	<b>highway route:</b> County Road 24
	<b>highway distr.:</b> 2
	<b>current owner:</b> Howard County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt bedstead	
<b>substructure:</b> steel pile bent abutments with timber wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 50.0'	<b>alterations:</b> unknown
<b>total length:</b> 50.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.5'	<b>other features:</b> steel lattice guardrails

## HISTORICAL DATA

<b>erection date:</b> 1913	
<b>erection cost:</b> \$4450.00 (multiple-bridge contract)	
<b>designer:</b> Massillon Bridge and Structural Company, Massillon OH	
<b>fabricator :</b> Massillon Bridge and Structural Company, Massillon OH	
<b>contractor:</b> Massillon Bridge and Structural Company, Massillon OH	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 024001.1; Bridge Contact with the Massillon Bridge and Structural Company, dated 27 December 1912 - located at Howard County Courthouse, Fayette MO.
<b>sign. rating:</b> 40	
<b>evaluation:</b>	NRHP non-eligible (typical example of common structural type)

**inventoried by:** Clayton B. Fraser    29 November 1994

# Moniteau Creek Bridge

HOWA08

## GENERAL DATA

structure no.: 053001.5      city/town: 12.5 miles northeast of Fayette  
county: Howard      feature inters.: Moniteau Creek  
cadastral grid: S10, T51W, R14W  
highway route: County Road 53  
highway distr.: 2  
current owner: Howard County

## STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt bedstead, with steel stringer approach spans

substructure: steel bent pier abutments; concrete piers

span number: 1      condition: fair  
span length: 54.0'      alterations: truss legs possibly cut off  
total length: 90.0'      floor/decking : timber deck over steel stringers  
roadway width: 11.7'      other features: upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 channels with batten plates, 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical below lower chord; guardrail: steel lattice

## HISTORICAL DATA

erection date: c1910  
erection cost: unknown  
designer: unknown  
fabricator : Cambria Steel Company, Pittsburgh PA  
contractor: unknown

references: Missouri Highway and Transportation Department , Structure and Inventory Appraisal: Structure Number 053001.5; field inspection by Richard Collier, 8 June 1991.

sign. rating: 28  
evaluation: NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Clayton B. Fraser      29 November 1994

# Moniteau Creek Bridge

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HOWA09

## GENERAL DATA

<b>structure no.:</b> 056001.1	<b>city/town:</b> 11.5 miles northeast of Fayette
<b>county:</b> Howard	<b>feature inters.:</b> Moniteau Creek
	<b>cadastral grid:</b> S16, T51W, R14W
	<b>highway route:</b> County Road 56
	<b>highway distr.:</b> 2
	<b>current owner:</b> Howard County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt truss-leg bedstead, with steel stringer approach span on each end	
<b>substructure:</b> steel pile bent abutments with timber wingwalls, steel truss-leg piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 70.0'	<b>alterations:</b> guardrails removed; deck deteriorated
<b>total length:</b> 95.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 11.0'	<b>other features:</b> upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plates, 2 looped rectangular eyebars; vertical: 4 angles with batten plates; diagonal: 2 looped square eyebars; counter: round eyobar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted below lower chord

## HISTORICAL DATA

<b>erection date:</b> 1913	
<b>erection cost:</b> \$1670.00 (contract amount)	
<b>designer:</b> Pan-American Bridge Company, New Castle IN	
<b>fabricator :</b> Pan-American Bridge Company, New Castle IN; Cambria Steel Company, Pittsburgh PA	
<b>contractor:</b> Pan-American Bridge Company, New Castle IN	
<b>references:</b> Missouri Highway and Transportation and Department, Structure Inventory and Appraisal: Structure Number 056001.1; Bridge Contract with Pan-American Bridge Company, dated 30 May 1913 - located at Howard County Courthouse, Fayette MO; field inspection by Richard Collier, 8 June 1991.	
<b>sign. rating:</b> 44	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)	

**inventoried by:** Clayton B. Fraser    29 November 1994

# Moniteau Creek Bridge

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HOWA10

## GENERAL DATA

<b>structure no.:</b> 061001.1	<b>city/town:</b> 10.6 miles northeast of Fayette
<b>county:</b> Howard	<b>feature inters.:</b> Moniteau Creek
	<b>cadastral grid:</b> S28, T51W, R14W
	<b>highway route:</b> County Road 61
	<b>highway distr.:</b> 2
	<b>current owner:</b> Howard County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt truss-leg bedstead, with steel stringer approach spans	
<b>substructure:</b> steel pile bent abutments with timber back- and wingwalls; steel truss-leg piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 60.0'	<b>alterations:</b> none
<b>total length:</b> 80.0'	<b>floor/decking :</b> timber decking over steel stringers
<b>roadway width:</b> 11.6'	<b>other features:</b> upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plates, 2 looped rectangular eyebars; vertical: 4 angles with batten plates; diagonal: 2 looped square eyebars; counter: round eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted below lower chord; guard-rail: steel lattice

## HISTORICAL DATA

<b>erection date:</b> 1913	
<b>erection cost:</b> \$1395.00 (contract amount)	
<b>designer:</b> Pan-American Bridge Company, New Castle IN	
<b>fabricator :</b> Pan-American Bridge Company, New Castle IN; Cambria Steel Company, Pittsburgh PA	
<b>contractor :</b> Pan-American Bridge Company, New Castle IN	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 061001.1; Bridge Contract with Pan-American Bridge Company, dated 30 May 1913 - located at Howard County Courthouse, Fayette MO; field inspection by Richard Collier, 8 June 1991.	
<b>sign. rating:</b> 39	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)	

**inventoried by:** Clayton B. Fraser    29 November 1994

# Moniteau Creek Bridge

HOWA11

## GENERAL DATA

structure no.: 061001.6      city/town: 11 miles northeast of Fayette  
county: Howard      feature inters.: Moniteau Creek  
cadastral grid: S28 T51N, R14W  
highway route: County Road 61  
highway distr.: 2  
current owner: Howard County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead  
substructure: steel truss-leg abutments with timber back- and wingwalls

span number: 1      condition: fair  
span length: 50.0'      alterations:  
total length: 50.0'      floor/decking : timber deck with wear boards  
roadway width: 11.6'      other features: upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plates, 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; counter: looped square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted below lower chord; guardrail: steel lattice

## HISTORICAL DATA

erection date: c1910  
erection cost: unknown  
designer: unknown  
fabricator : Jones and Laughlin Steel Company, Pittsburgh PA  
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal; Structure Number 061001.6; field inspection by Richard Collier, 8 June 1991.

sign. rating: 28  
evaluation: NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Clayton B. Fraser      29 November 1994

# Drake Bridge

HOWA13

## GENERAL DATA

<b>structure no.:</b> 085002.0	<b>city/town:</b> 9.0 miles east of New Franklin
<b>county:</b> Howard	<b>feature inters.:</b> Moniteau Creek
	<b>cadastral grid:</b> Survey 2455, T49N, R15W
	<b>highway route:</b> County Road 85
	<b>highway distr.:</b> 2
	<b>current owner:</b> Howard County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel pin-connected Pratt pony truss, with steel stringer approach spans	
<b>substructure:</b> built-up steel piers and abutments	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 80.0'	<b>alterations:</b> none
<b>total length:</b> 119.0'	<b>floor/decking :</b> timber decking over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped square eyebars; vertical: 4 angles with double lacing; diagonal: 2 looped square eyebars; counter: looped square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted above lower chord; cast iron hip blocks and bearing shoes

## HISTORICAL DATA

<b>erection date:</b> 1899	
<b>erection cost:</b> \$1645.00	
<b>designer:</b> Wrought Iron Bridge Company, Canton OH	
<b>fabricator :</b> Wrought Iron Bridge Company, Canton OH	
<b>contractor :</b> Wrought Iron Bridge Company, Canton OH	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 085002.0; Howard County Court Record: Book 25, page 141 (9 August 1899) - located at Howard County Courthouse, Fayette MO; field inspection by Richard Collier, 8 June 1991.	
<b>sign. rating:</b> 55	
<b>evaluation:</b> NRHP possibly eligible (well-preserved example of mainstay structural type)	

inventoried by: Clayton B. Fraser 29 November 1994

# Prairie Fork Bridge

HOWA15

## GENERAL DATA

<b>structure no.:</b> 087002.0	<b>city/town:</b> 9.6 miles northeast of New Franklin
<b>county:</b> Howard	<b>feature inters.:</b> Prairie Fork Creek
	<b>cadastral grid:</b> Survey 2455, T49N, R15W
	<b>highway route:</b> County Road 87
	<b>highway distr.:</b> 2
	<b>current owner:</b> Howard County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt pony truss, with steel lattice pony girder approach span	
<b>substructure:</b> concrete-filled steel cylinder piers under pony truss; steel bent abutments with timber backwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 70.0'	<b>alterations:</b> truss possibly moved; lattice truss approach span added
<b>total length:</b> 102.0'	
<b>roadway width:</b> 13.7'	<b>floor/decking :</b> timber deck over steel stringers
	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate with lacing; lower chord: 2 looped square eyebars; vertical: 4 angles with double lacing; diagonal: 2 looped square eye-bars; counter: looped square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pins; guardrail: steel lattice

## HISTORICAL DATA

<b>erection date:</b> c1900	
<b>erection cost:</b> unknown	
<b>designer:</b> Wrought Iron Bridge Company, Canton OH	
<b>fabricator :</b> Wrought Iron Bridge Company, Canton OH	
<b>contractor:</b> Wrought Iron Bridge Company, Canton OH	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 087002.0; field inspection by Richard Collier, 8 June 1991.	
<b>sign. rating:</b> 37	
<b>evaluation:</b> NRHP non-eligible (well-preserved example of patented bridge type, poorly documented and possibly moved to this location)	

**inventoried by:** Clayton B. Fraser 29 November 1994

# Prairie Fork Bridge

HOWA16

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## GENERAL DATA

<b>structure no.:</b> 088001.5	<b>city/town:</b> 10.5 miles east of New Franklin
<b>county:</b> Howard	<b>feature inters.:</b> Prairie Fork Creek
	<b>cadastral grid:</b> S13/18, T49N, R14/15W
	<b>highway route:</b> County Road 88
	<b>highway distr.:</b> 2
	<b>current owner:</b> Howard County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt pony truss	
<b>substructure:</b> steel truss-leg abutments with timber back- and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 64.0'	<b>alterations:</b> unknown
<b>total length:</b> 64.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.9'	<b>other features:</b> timber guardrails

## HISTORICAL DATA

<b>erection date:</b> 1909	
<b>erection cost:</b> \$720.00 (contract amount)	
<b>designer:</b> Pan-American Bridge Company, New Castle IN	
<b>fabricator :</b> Pan-American Bridge Company, New Castle IN	
<b>contractor:</b> Pan-American Bridge Company, New Castle IN	
<b>references:</b> Missouri Highway and Transportation Department, Structure, Inventory, and Appraisal: Structure Number 088001.5; Howard County Court Record: Book 27, page 341 (6 July 1909), pages 346-347 (2 August 1909) - located at Howard County Courthouse, Fayette MO.	
<b>sign. rating:</b> 42	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)	

**inventoried by:** Clayton B. Fraser    29 November 1994

# Bonne Femme Creek Bridge

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HOWA17

## GENERAL DATA

structure no.:	108001.3	city/town:	3.7 miles northeast of New Franklin
county:	Howard	feature inters.:	Bonne Femme Creek
		cadastral grid:	Survey 2661, T49N, R16W
		highway route:	County Road 108
		highway distr.:	2
		current owner:	Howard County

## STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled steel cylinder piers with timber backwalls		
span number:	1	condition:	fair
span length:	80.0'	alterations:	none
total length:	84.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.5'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped square eyebars; vertical: 4 angles with lacing; diagonal: 2 square looped eyebars; counter: looped square eyebar with turnbuckle; floor beam: I-beam, field-bolted above lower chords; guardrail: 1 channel

## HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 108001.3; field inspection by Richard Collier, 8 June 1991.
sign. rating:	31
evaluation:	NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Clayton B. Fraser    24 November 1994

# Sulphur Creek Bridge

HOWA18

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## GENERAL DATA

<b>structure no.:</b> 113000.0	<b>city/town:</b> 4.5 miles northwest of New Franklin
<b>county:</b> Howard	<b>feature inters.:</b> Sulphur Creek
	<b>cadastral grid:</b> S2/3, T49N, R17W
	<b>highway route:</b> County Road 113
	<b>highway distr.:</b> 2
	<b>current owner:</b> Howard County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt pony truss	
<b>substructure:</b> steel pile bent piers with timber back- and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 50.0'	<b>alterations:</b> unknown
<b>total length:</b> 50.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 11.5'	<b>other features:</b> steel lattice guardrails

## HISTORICAL DATA

<b>erection date:</b> 1910
<b>erection cost:</b> \$725.00 (contract amount)
<b>designer:</b> Pan-American Bridge Company, New Castle IN
<b>fabricator :</b> Pan-American Bridge Company, New Castle IN
<b>contractor:</b> Pan-American Bridge Company, New Castle IN
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 113000.0; Howard County Court Record: Book 27, page 472 (6 June 1910); Bridge Contract with Pan-American Bridge Company, dated 14 September 1910 - both located at Howard County Courthouse, Fayette MO.
<b>sign. rating:</b> 37
<b>evaluation:</b> NRHP non-eligible (typically configured example of common structural type)

**inventoried by:** Clayton B. Fraser    29 November 1994

# Sulphur Creek Bridge

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HOWA19

## GENERAL DATA

<b>structure no.:</b> 121000.0	<b>city/town:</b> 1.2 miles south of New Franklin
<b>county:</b> Howard	<b>feature inters.:</b> Sulphur Creek
	<b>cadastral grid:</b> S32, T49N, R16W
	<b>highway route:</b> County Road 121
	<b>highway distr.:</b> 2
	<b>current owner:</b> Howard County

## STRUCTURAL DATA

**superstructure:** steel, 5-panel, rigid-connected, 2-angle Camelback pony truss  
**substructure:** concrete abutments and wingwalls

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 82.0'	<b>alterations:</b> none
<b>total length:</b> 82.0'	<b>floor/decking :</b> concrete deck over steel stringers
<b>roadway width:</b> 13.7'	<b>other features:</b> upper chord and inclined end post: 2 angles; lower chord: 2 angles; vertical: 2 angles with batten plates; diagonal: 2 angles; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: 2 channels

## HISTORICAL DATA

**erection date:** c1940  
**erection cost:** unknown  
**designer:** unknown  
**fabricator :** United States Steel Company, Pittsburgh PA  
**contractor:** unknown

**references:** Missouri Highway and Transportation Department Inventory and Appraisal: Structure Number 121000.0; field inspection by Richard Collier, 8 June 1991.

**sign. rating:** 34  
**evaluation:** NRHP determined non-eligible (uncommon structural type, poorly documented)

**inventoried by:** Clayton B. Fraser    29 November 1994

# Adams Fork Bridge

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HOWA21

## GENERAL DATA

<b>structure no.:</b>	161002.0	<b>city/town:</b>	1.0 mile southwest of Fayette
<b>county:</b>	Howard	<b>feature inters.:</b>	Adams Fork
		<b>cadastral grid:</b>	S10, T50N, R16W
		<b>highway route:</b>	County Road 161
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Howard County

## STRUCTURAL DATA

<b>superstructure:</b>	steel 3-panel, pin-connected Pratt half-hip pony truss		
<b>substructure:</b>	steel pile bent pier and abutments with timber backwalls		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	50.0'	<b>alterations:</b>	substructure replaced; vertical added at one hip
<b>total length:</b>	58.0'	<b>floor/decking :</b>	concrete deck over steel stringers
<b>roadway width:</b>	13.7'	<b>other features:</b>	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped square eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pins; cast iron hip blocks and bearing shoes

## HISTORICAL DATA

<b>erection date:</b>	1896
<b>erection cost:</b>	\$824.00
<b>designer:</b>	Wrought Iron Bridge Company, Canton OH
<b>fabricator :</b>	Wrought Iron Bridge Company, Canton OH
<b>contractor:</b>	Wrought Iron Bridge Company, Canton OH
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 161002.0; Howard County Court Record: Book 24, page 160 (1 June 1896), page 214 (5 November 1896) - located at Howard County Courthouse, Fayette MO; field inspection by Richard Collier, 8 June 1991.
<b>sign. rating:</b>	43
<b>evaluation:</b>	NRHP non-eligible (poorly preserved example of patented truss type)

inventoried by: Clayton B. Fraser 29 November 1994

# Doxies Creek Bridge

HOWA24

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## GENERAL DATA

<b>structure no.:</b> 200000.2	<b>city/town:</b> 4.9 miles Northeast of Glasgow
<b>county:</b> Howard	<b>feature inters.:</b> Doxies Creek
	<b>cadastral grid:</b> Survey 2449/S1, T51/52N, R17W
	<b>highway route:</b> County Road 200
	<b>highway distr.:</b> 2
	<b>current owner:</b> Howard County

## STRUCTURAL DATA

**superstructure:** steel, 4-panel, pin-connected Pratt half-hip pony truss  
**substructure:** unknown

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 54.0'	<b>alterations:</b> unknown
<b>total length:</b> 56.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 12.1'	<b>other features:</b> unknown

## HISTORICAL DATA

**erection date:** 1904  
**erection cost:** unknown  
**designer:** unknown  
**fabricator :** unknown  
**contractor :** unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 200000.2; Howard County Court Record: Book 25, page 256 (1 September 1900) - located at Howard County Courthouse, Fayette MO.

**sign. rating:** 31  
**evaluation:** NRHP non-eligible (typically configured, partially documented example of common structural type)

**inventoried by:** Clayton B. Fraser 29 November 1994

# Bonne Femme Creek Bridge

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HOWA25

## GENERAL DATA

<b>structure no.:</b> 211001.0	<b>city/town:</b> 2.5 miles southeast of Fayette
<b>county:</b> Howard	<b>feature inters.:</b> Bonne Femme Creek
	<b>cadastral grid:</b> S24, T50N, R16W
	<b>highway route:</b> County Road 211
	<b>highway distr.:</b> 2
	<b>current owner:</b> Howard County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach span at each end	
<b>substructure:</b> timber pile bent abutments with concrete-filled steel cylinder piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 80.0'	<b>alterations:</b> none
<b>total length:</b> 112.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 11.6'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with batten plates; diagonal: 2 looped rectangular eyebars; counter: looped square eyobar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical below lower chord; guard-rail: steel lattice

## HISTORICAL DATA

<b>erection date:</b> 1907
<b>erection cost:</b> \$1625.00
<b>designer:</b> Pan-American Bridge Company, New Castle IN
<b>fabricator :</b> Pan-American Bridge Company, New Castle IN
<b>contractor :</b> Pan-American Bridge Company, New Castle IN

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 211001.0; Howard County Court Record: Book 27, page 67 (5 August 1907), page 74 (3 September 1907) - located at Howard County Courthouse, Fayette MO; field inspection by Richard Collier, 8 June 1991.

<b>sign. rating:</b> 46
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)

**inventoried by:** Clayton B. Fraser    29 November 1994

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Boonville Bridge  
MHTD: G 56R1

HOWA01

**DATE(S) OF CONSTRUCTION**

1922-24

**LOCATION**

U.S. Highway 40 over Missouri River; Survey 2481, T48N, R16W  
Boonville; Cooper / Howard County, Missouri

**USE (ORIGINAL / CURRENT)**

highway bridge / highway bridge

**RATING** NRHP eligible (score: 75)

**CONDITION**

good

**OWNER**

Missouri Highway and Transportation Department

span number: 3; 3  
span length: 420.0'  
total length: 2666.0'  
roadway wdt.: 18.1'

superstructure: steel, 18-panel, rigid-connected Pennsylvania through truss; steel, 11-panel, rigid-connected Parker through truss  
substructure: concrete abutments, wingwalls and piers  
floor/decking: open grid steel deck over steel stringers  
other features: upper chord and inclined end post: 2 built-up channels with cover plate and double lacing; lower chord: two built-up channels with lacing; vertical: 2 or 4 angles with lacing; diagonal: 2 channels with lacing or batten plates; lateral bracing: 2 channels (lower), 4 angles with lacing (upper); strut: 4 angles with lacing and bracing below: floor Beam: I-beam; guardrail: steel pipe; portal plate: 1923 / BOONVILLE BRIDGE / BUILT BY THE OLD TRAILS BRIDGE CO / T.A. JOHNSTON PRESIDENT / COOPER COUNTY / HOWARD COUNTY / CITY OF BOONVILLE / FEDERAL AID APPROPRIATED BY MISSOURI STATE HIGHWAY COMMISSION / B.H. PIEPNIER CHIEF ENGINEER / DESIGNED AND SUPERVISED BY HARRINGTON HOWARD AND ASH / CONTRACTORS MT. VERNON BRIDGE CO MT VERNON, OHIO / MISSOURI VALLEY BRIDGE AND IRON CO LEAVENWORTH, KANSAS. on south approach THIS BRIDGE APPROACH WAS ERECTED AND IS MAINTAINED BY THE CITY OF BOONVILLE, MO John E. Waltz Mayor, Charles W. Journey City Counsellor, Charles E. Miller City Clerk, City Council, Rudolph E. Toennes, Dr. Charles Swap, Herman Schmidt, Herman M. Stretz, O.F. Kelly, J.C. Barnhart W.T. Gibson, Irene Schubert, HARRINGTON HOWARD AND ASH ENGINEERS, W.J. COCHRAN AND SON CONTRACTOR, 1922-23, THIS TABLET ERECTED BY A GRATEFUL PEOPLE IN HONOR OF JOHN COSGROVE AND THOMAS A JOHNSTON TO WHOSE VISION AND SERVICE THIS BRIDGE IS A TESTIMONIAL, JULY 1924

The idea of building a highway bridge across the Missouri River at Boonville was considered as early as the mid-1890s. A bridge-building corporation organized in 1896 by Captain S.W. Ravenal began planning such a structure, and even arranged for soundings to be taken for its piers. Due to a lack of funding and perhaps organization, however, the project failed to get off the ground. Bridging the Missouri at Boonville was than not seriously taken up again for more than two decades. In the interim, new impetus for Missouri River bridges—at Boonville and elsewhere—came from the National Old Trails Association, which was then promoting a transcontinental



highway. Reorganizing the potential economic benefits, Boonville's community leaders launched a spirited campaign to have the proposed highway pass through their town. A bridge over the Missouri River became a pivot point for the campaign.

Planning for the Boonville Bridge began in earnest in February 1921 when Colonel Thomas A. Johnston was appointed chairman of the bridge committee of the National Old Trails Association. A director of the Commercial Bank of Boonville, Johnston, along with Attorney John Cosgrove, became the driving force behind the bridge's planning and development. On June 20th the Old Trails Bridge Company was incorporated, with Johnston as president and Cosgrove as one of four vice-presidents. A fifteen-member board of director was elected from prominent community leaders. The Kansas City engineering firm of Harrington, Howard and Ash (HHA) began work on the structure's design in late July. In January 1922 HHA presented finalized plans and specification for the bridge to the board. Contracts to build the substructure and superstructure were subsequently awarded to the Missouri Valley Bridge Company and the Mount Vernon Bridge Company, respectively. A third contract for the south approach was let later to the W.J. Cochran and Son Construction Company, a local contractor.

With construction underway in the summer of 1922, a problem developed when the promoters learned that a recent provision in the Federal Aid Road Law forbade the charging of tolls on a federally aided bridge or highway. Because the original plan involved paying off bonds from toll receipts, the responsibility for the bridge's construction was taken over by the state. Half its cost, meanwhile, was financed locally, with the remainder coming from the federal government. Most importantly, the bridge would be toll-free. The intricate planning paid dividends on July 12, 1922, when the highway commission recommended that the main east-west highway across the state be routed through Boonville.

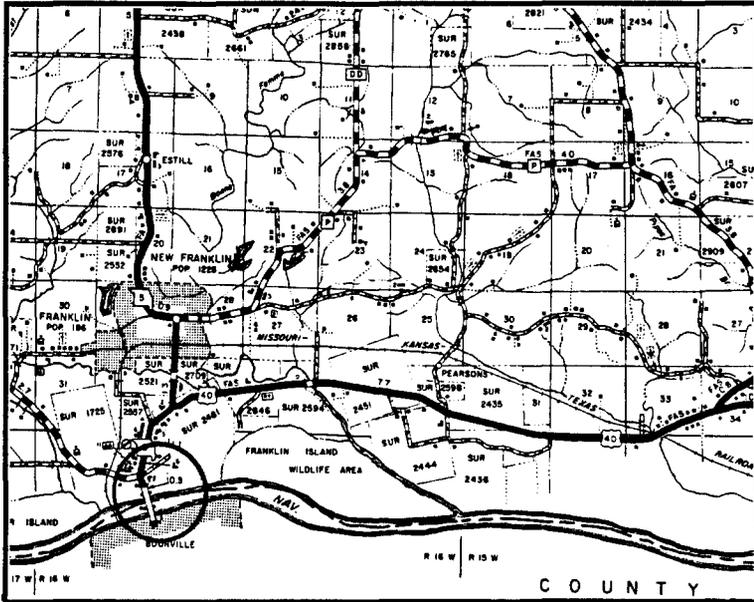
Work on the substructure began that summer and continued until the following spring. Shipments of superstructural steel began arriving in the summer of 1923. By early August, Mount Vernon's steel workers had placed the first steel members. Work on the superstructure extended into the spring of 1924. In late May the bridge was unofficially opened to traffic. Not coincidentally, that same month the operators of the Boonville Ferry failed to renew their license, shutting down operations for good. On Independence Day over 20,000 people descended on Boonville for a massive dedication ceremony. Governor Arthur M. Hyde and Colonel T.A. Johnston gave the keynote address, while the day's festivities included a concert, parade, aerobatic stunts, and a high-wire act. Built for a total cost of \$556,883.82, the Boonville Bridge was the state's first toll-free span over the Missouri. The crossing was also heralded as a final link in the National Old Trails transcontinental road.

Fourteen vehicular truss bridges over the Missouri River are included in Missouri's statewide historic bridge inventory. As a group, these spans comprise some of the state's longest and most massive vehicular structures. Each of these crossings is both historically and technologically significant. The Boonville Bridge is historically noteworthy as a pivotal link on a transcontinental highway, which had a major impact on the region's socioeconomic development. Although typically configured and detailed, the Boonville Bridge is among the state's most monumental examples of steel truss construction. With an overall length of 2666 feet, the multiple-span structure is a superlative example of its type.

**NAME(S) OF STRUCTURE**

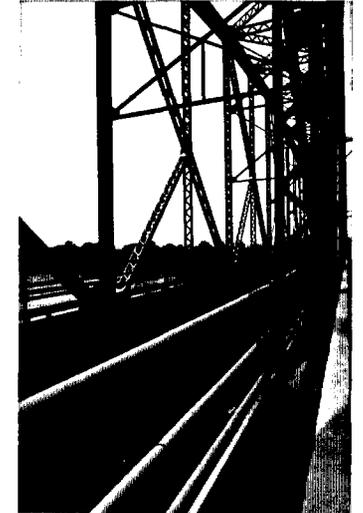
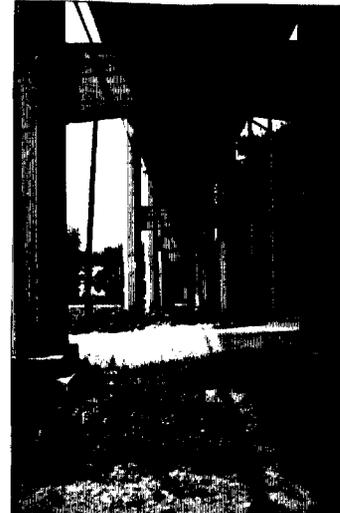
Boonville Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP



**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number G 56R1; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; Boonville Advertiser July 4, 1924, Central Missouri Republican, 5 January 1922, 16 February 1922; Robert L. Dyer, Boonville: An Illustrated History, (by author, 1987), page 231; field inspection by Richard Collier, 8 June 1991.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

29 November 1994

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

New Franklin Viaduct  
MHTD: K 744

HOWA02

**DATE(S) OF CONSTRUCTION**

1939-40

**LOCATION**

State Highway 5 over abandoned grade of MKT Railroad; S29/28, T49N, R16W  
New Franklin; Howard County, Missouri

**USE (ORIGINAL / CURRENT)**

urban viaduct / urban viaduct

**RATING** NRHP possibly eligible (score: 51)

**CONDITION**

good

**OWNER**

Missouri Highway Transportation Department

span number: 6; 13  
span length: 54.0'; 48.0'  
total length: 1462.0'  
roadway wdt.: 24.0'

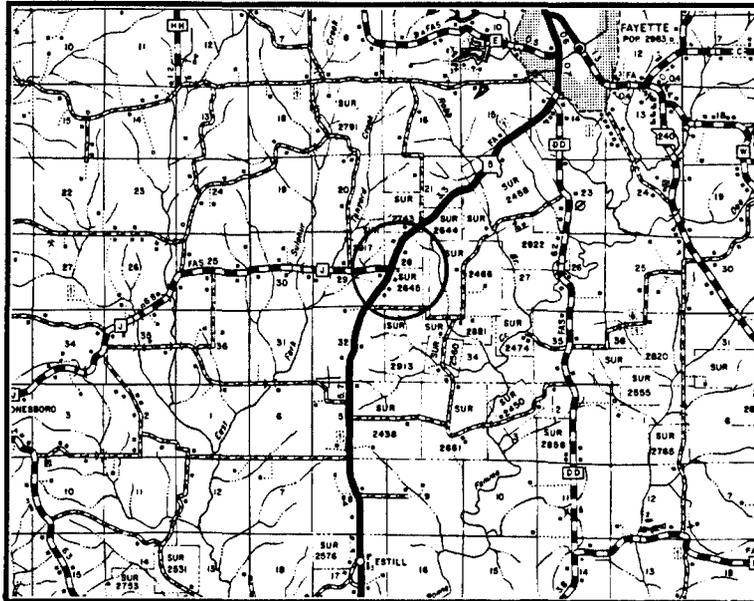
superstructure: multiple span, concrete deck girder viaduct  
substructure: concrete abutments, wingwalls and piers  
floor/decking: asphalt-covered concrete deck  
other features: arched concrete girders; open balustrade guardrails with flanking pedestrian stairways; modest Moderne treatment on concrete piers

As part of the effort to create jobs during the Depression, Congress in 1934 passed an act allowing federal monies to be used for road and bridge construction within municipalities. Taking advantage of the new legislation, the Missouri State Highway Commission undertook a number of urban road and bridge projects during the late 1930s. Located on the southern side of New Franklin on State Highway 5, the New Franklin Viaduct was one such construction project. To span the Missouri, Kansas & Texas Railroad, the Missouri State Highway Department engineered a multiple-span concrete girder structure, with slightly arched girders, open balustrade guardrails and pedestrian stairways at both ends. On September 22, 1939, a \$167,894.18 contract for the structure's construction was awarded to Carrothers and Crouch. Completed the following year, the New Franklin Viaduct has since carried increasingly heavy traffic loads on the principal highway leading into the city from the south.

As an important crossing of the MKT Railroad, the New Franklin Viaduct has formed an integral part of the city's street system. The viaduct is also important as one of the railroad separation projects funded through the New Deal's Hayden-Cartwright Act. Federal relief programs of the 1930s broke with past practice by allowing federal funds to be used for urban, as well as rural highways. Grade separation was a major focus of the highway department during this period, requiring commitment of much staff time. The New Franklin Viaduct is technologically distinguished as a multiple-span and long-span example of a MSHD mainstay structural type—the concrete deck girder. A well-preserved, regionally important example of this common bridge type, the structure is both technologically and historically significant.

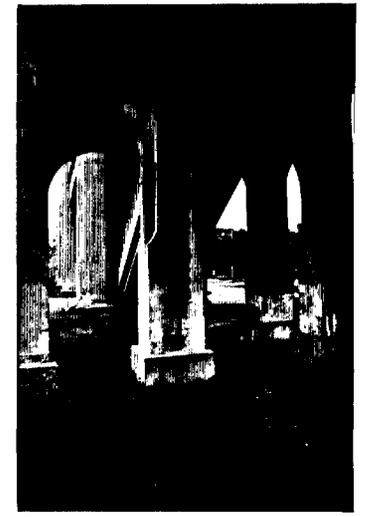
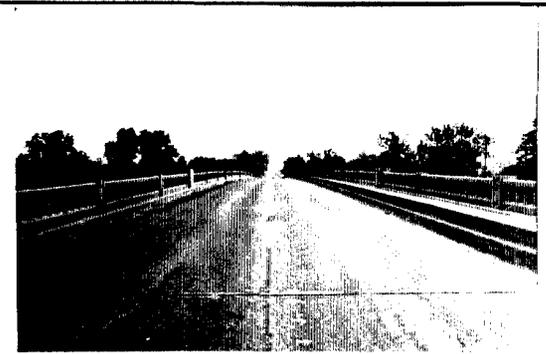
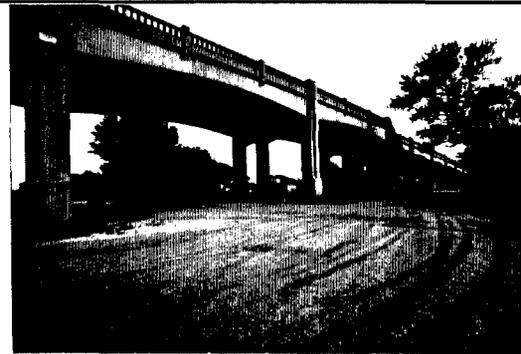
**NAME(S) OF STRUCTURE**  
New Franklin Viaduct

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP



**SOURCES**

Missouri Highway Transportation Department, Structure Inventory and Appraisal: Structure Number K 744; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; field inspection by Richard Collier, 8 June 1991.

**INVENTORIED BY**  
Clayton B. Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
29 November 1994

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Drake Bridge  
MHTD: 085002.0

HOWA13

**DATE(S) OF CONSTRUCTION**

1899

**LOCATION**

County Road 85 over Moniteau Creek; Survey 2455, T49N, R15W  
9.0 miles east of New Franklin; Howard County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 55)

**CONDITION**

fair

**OWNER**

Howard County

span number: 1  
span length: 80.0'  
total length: 119.0'  
roadway wdt.: 12.0'

superstructure: steel, 5-panel pin-connected Pratt pony truss, with steel stringer approach spans  
substructure: built-up steel piers and abutments  
floor/decking: timber decking over steel stringers  
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped square eyebars; vertical: 4 angles with double lacing; diagonal: 2 looped square eyebars; counter: looped square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted above lower chord; cast iron hip blocks and bearing shoes

In the summer of 1899, Howard County Bridge Commissioner John Wisely was ordered by the county court to prepare an estimate and solicit competitive bids for a new steel bridge over Moniteau Creek some nine miles east of New Franklin. According to Wisely, the structure would require an 80-foot pony truss span, supported by a steel substructure. That August the county awarded a contract for the construction of the Drake Bridge, as it was known locally, to the Wrought Iron Bridge Company of Canton, Ohio, for \$1645.00. The Drake Bridge has functioned in place since its completion in 1899, with only maintenance-related repairs.

In the early 1880s the pin-connected Pratt through truss superseded the bowstring arch-truss as the iron bridge of choice for medium- and long-span wagon bridges. Patented in 1844 by Thomas and Caleb Pratt, the Pratt design is distinguished by vertical members acting in compression and diagonals that act in tension. "The Pratt truss is the type most commonly used in America for spans under two hundred and fifty (250) feet in length," noted bridge engineer J.A.L. Waddell wrote in 1916. "Its advantages are simplicity, economy of metal, and suitability for connecting to the floor and lateral systems." Virtually all of the major regional bridge fabricators manufactured Pratt trusses and marketed them extensively to Missouri's counties in the late 19th and early 20th centuries. Hundreds of Pratts remain in place today. The Means Ford Bridge is distinguished among these for its patented construction and well-preserved condition. It is thus significant as an early example in Missouri of a mainstay structural type: the pin-connected Pratt pony truss.



# LINN COUNTY

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INCLUDED: [Significant feature(s) of bridge given in boldface]  
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
LINN01	U0550002	Burlington RR Overpass	1- 34' riveted Pratt pony truss 1913 Burlington Northern Railroad
*LINN02	X 480	Locust Creek Bridge	1-125' <b>pinned Pratt through truss</b> 1939 D.A. Peery
*LINN03	063000.6	Locust Creek Bridge	1- 80' pinned Pratt pony truss 1900 A.M. Blodgett
*LINN04	075001.0	Lowes Branch Bridge	(replaced)
LINN05	121002.2	East Yellow Creek Bridge	(replaced)
*LINN06	164000.1	West Yellow Creek Bridge	1- 60' pinned Pratt bedstead 1901 John Gilligan Bridge Company
*LINN07	180001.1	Bear Creek Bridge	1- 45' pinned Pratt pony truss c1910
*LINN08	201000.3	Long Branch Bridge	1- 45' pinned Pratt pony truss c1910
*LINN09	246000.7	Locust Creek Bridge	1-120' <b>pinned Pratt through truss</b> c1910
*LINN10	247000.5	Locust Creek Bridge	1- 60' pinned Pratt pony truss 1906 John Gilligan Bridge Company
*LINN11	247000.8	Locust Creek Bridge	1-160' <b>pinned Parker through truss</b> c1925
*LINN12	297001.0	Parsons Creek Bridge	1- 60' pinned Pratt pony truss 1913 Monarch Engineering Co.
*LINN13	405001.7	West Yellow Creek Bridge	1- 60' pinned Pratt pony truss c1915
*LINN14	406000.1	Long Branch Bridge	1- 60' pinned Pratt pony truss 1902 John Gilligan Bridge Company
LINN15	421000.7	East Yellow Creek Bridge	1- 90' pinned Pratt pony truss 1904 John Gilligan Bridge Company
LINN16	423001.3	East Yellow Creek Bridge	1- 58' pinned Pratt bedstead 1902 John Gilligan Bridge Company
LINN17	434001.5	East Yellow Creek Bridge	1- 55' pinned Pratt bedstead 1901 John Gilligan Br. Co. (prob.)
*LINN18	443000.5	East Yellow Creek Bridge	1- 80' <b>riveted Camelback pony truss</b> c1920
LINN19	473000.2	Burlington RR Overpass	1- 60' pinned Pratt pony truss c1915
*LINN20	none	Locust Ck. Covered Bridge	1-150' timber Howe truss (covered) 1868 Bishop and Eaton

**EXCLUDED:**

Pratt pony truss  
 G 748R U055000.2 077000.8 079000.5 167000.8 395001.0 399000.4  
 418000.3

# LINN COUNTY

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## EXCLUDED (cont.):

### Warren pony truss

X 719      265000.1    387000.9    520000.5

### Steel stringer

G 657R	G 659R	G 815R	K 403	K 665	S 31	S 489
S 747	T 377	T 806	T 807	003000.8	006001.2	010000.8
017R01.9	017000.3	019R00.3	022000.3	023001.1	027R02.2	029002.1
040000.4	041000.2	055000.2	055000.3	055500.1	069000.6	080001.2
083000.5	101000.8	104R00.6	104R01.0	104001.9	116000.5	119001.0
120001.3	145000.4	145001.1	149000.3	151000.1	154005.5	159R00.9
167R02.1	174000.8	184R00.8	187000.3	187001.6	196R03.0	196001.0
200000.9	202000.7	204000.5	205001.2	210000.3	211000.5	212000.1
213000.2	216000.1	217R01.0	217002.3	219000.4	220000.3	220000.4
221001.2	230001.3	233R00.2	239000.9	240001.8	240001.9	244000.8
244002.5	249000.1	249000.7	250000.5	255000.9	257R00.1	268000.3
269000.2	273000.5	277000.7	282001.1	286000.8	288001.8	290000.4
292R00.2	294001.0	297001.3	298001.7	305000.3	310001.9	310003.4
310004.2	311000.2	312000.1	321R00.5	332000.5	345001.5	347001.4
350000.9	353000.3	360R01.5	364000.9	364001.3	370000.9	372000.2
373000.2	376000.2	383003.4	384000.4	395R01.4	399000.5	409R00.7
412000.1	414000.5	417001.7	419R01.1	420000.3	420000.8	423002.1
424000.3	435R00.1	440002.0	442R00.1	445000.7	447000.4	448000.5
460R01.2	471R00.1	475000.4	475001.0	483000.5	493000.1	496001.4
497000.2	503R01.5	504002.3	510000.1	510002.7	519R03.5	531000.4
532R01.7	534000.2	535000.1				

### Concrete slab / girder

G 654R    H 312R    J 96R      J 97R      J 100R    J 185R    J 189R  
K 311

### Concrete box culvert

G 656R    G 661R    G 917      K 404      P 24      P 29      P 30  
S 153      S 154      T 569      T 702      X 720      X 799      X 800  
357R01.0

### Timber stringer

248001.5

## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	1	15	1	1	18
Excluded	35	143	1	0	179
	<hr/>				
	36	158	2	1	197 structures

# Burlington Northern Railroad Overpass

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LINN01

## GENERAL DATA

<b>structure no.:</b> U0550002	<b>city/town:</b> Brookfield
<b>county:</b> Linn	<b>feature inters.:</b> Burlington Northern Railroad Tracks
	<b>cadastral grid:</b> S7, T57N, R19W
	<b>highway route:</b> Pershing Street
	<b>highway distr.:</b> 2
	<b>current owner:</b> City of Brookfield

## STRUCTURAL DATA

<b>superstructure:</b> steel, rigid-connected Pratt pony truss, with timber stringer approach spans	
<b>substructure:</b> timber abutments and wingwalls; timber and steel pile bent piers	
<b>span number:</b> 1	<b>condition:</b> good
<b>span length:</b> 34.0'	<b>alterations:</b> none
<b>total length:</b> 129.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 16.0'	<b>other features:</b> timber guardrail

## HISTORICAL DATA

<b>erection date:</b> 1913	
<b>erection cost:</b> unknown	
<b>designer:</b> Burlington Northern Railroad	
<b>fabricator :</b> unknown	
<b>contractor:</b> unknown	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. U0550002.	
<b>sign. rating:</b> 33	
<b>evaluation:</b> NRHP determined non-eligible (typically configured rigid-connected Pratt pony truss; average span length)	

**inventoried by:** Michelle Crow-Dolby    24 July 1993

# Locust Creek Bridge

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LINN02

## GENERAL DATA

structure no.:	X 480	city/town:	2.3 miles west of Linneus
county:	Linn	feature inters.:	Locust Creek
		cadastral grid:	S3, T58N, R21W
		highway route:	State Secondary Route B
		highway distr.:	2
		current owner:	Linn County

## STRUCTURAL DATA

**superstructure:** steel, 5-panel, pin-connected Pratt through truss; one steel stringer approach span on the east end; two steel stringer approach spans on the west end

**substructure:** steel pile bent piers with timber backwalls; concrete piers

span number:	1	condition:	good
span length:	125.0'	alterations:	none
total length:	237.0'	floor/decking :	timber deck over steel stringers
roadway width:	20.0'	other features:	upper chord and inclined end post: 2 face-to-face channels with cover plate and double lacing; lower chord: 4 heavy punched rectangular eyebars; vertical: 2 channels with lacing (2 punched rectangular eyebars at the hip); diagonal: 4 heavy punched rectangular eyebars; lateral bracing: 1 angle; strut: 4 angles with lacing and knee braces; floor beam: plate girders; guardrail: steel pipe, with vertical pipe supports with ornamental ball caps (concrete guardrails with weep holes on the approaches); bridge plate: Erected August 30, 1939 Members of the Linn County Court Virgil Lee, Presiding Judge M.C. Post, Judge E. Dist. W.E. McKenzie Judge W. Dist. C.E. Swinney, County Clerk Wallace L. Turner, Highway Engineer D.A. Peery, Contractor

## HISTORICAL DATA

erection date:	1925; moved 1938-39
erection cost:	\$19,677.57
designer:	Missouri State Highway Department
fabricator :	Trenton Steel Company, Trenton NJ
contractor:	D.A. Peery

## Locust Creek Bridge

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**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. X 480; Linn County Court Record Y: page 206 (8 December 1938), page 237 (16 May 1939), page 241 (6 July 1939), page 245 (16 August 1939), page 249 (September 1939), page 257 (7 November 1939) - located at Linn County Courthouse, Linneus MO; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.

**sign. rating:** 41

**evaluation:** NRHP non-eligible (relatively late example of common structural type)

**inventoried by:** Michelle Crow-Dolby 24 July 1993

# Locust Creek Bridge

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LINN03

## GENERAL DATA

<b>structure no.:</b> 063000.6	<b>city/town:</b> 2.7 miles northwest of Purdin
<b>county:</b> Linn	<b>feature inters.:</b> Locust Creek
	<b>cadastral grid:</b> S36, T60N, R21W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Linn County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt pony truss; steel, 4-panel pin-connected Pratt pony truss approach span on the east end	
<b>substructure:</b> steel pile bent piers	
<b>span number:</b> 1	<b>condition:</b> poor
<b>span length:</b> 80.0'	<b>alterations:</b> approach span is not original to site; welded connections between spans
<b>total length:</b> 178.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 11.0'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates; vertical: 4 angles with batten plates; diagonal: 2 punched rectangular eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: steel angle

## HISTORICAL DATA

<b>erection date:</b> 1900	
<b>erection cost:</b> \$3000.00 (3 bridge contract)	
<b>designer:</b> unknown	
<b>fabricator :</b> Bethlehem Steel Company, Bethlehem PA	
<b>contractor:</b> A.M. Blodgett, Kansas City	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 063000.6; Linn County Court Record M: page 348 (9 November 1900), page 373 (4 December 1900) - located at Linn County Courthouse, Linneus MO; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.	
<b>sign. rating:</b> 41	
<b>evaluation:</b> NRHP non-eligible (poorly preserved example of a common structural configuration; longer-than-average span length)	

**inventoried by:** Michelle Crow-Dolby 24 July 1993

# West Yellow Creek Bridge

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LINN06

## GENERAL DATA

**structure no.:** 164000.1      **city/town:** 8.5 miles northeast of Brookfield  
**county:** Linn      **feature inters.:** West Yellow Creek  
**cadastral grid:** S26, T59N, R19W  
**highway route:** County Road 164  
**highway distr.:** 2  
**current owner:** Linn County

## STRUCTURAL DATA

**superstructure:** steel, 4-panel, pin-connected Pratt truss leg bedstead, with one steel stringer approach span at each end  
**substructure:** truss legs; steel pile bent abutment at west end; concrete abutment with wingwalls at east end

**span number:** 1      **condition:** fair  
**span length:** 60.0'      **alterations:** I-beams welded to truss legs on the west end  
**total length:** 94.0'      **floor/decking :** timber deck over steel stringers  
**roadway width:** 12.0'      **other features:** upper chord: 2 channels with cover and batten plates; end post: 2 channels with lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: none

## HISTORICAL DATA

**erection date:** 1901  
**erection cost:** \$568.00  
**designer:** John Gilligan, Falls City NE  
**fabricator :** John Gilligan, Falls City NE  
**contractor :** John Gilligan, Falls City, NE

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 164000.1; Linn County Court Record M: page 572 (7 October 1901) - located at Linn County Courthouse, Linn County MO; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.

**sign. rating:** 42  
**evaluation:** NRHP non-eligible (typically configured example of relatively common structural type)

**inventoried by:** Michelle Crow-Dolby      24 July 1993

# Bear Creek Bridge

LINN07

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## GENERAL DATA

structure no.:	180001.1	city/town:	7.8 miles north of Brookfield
county:	Linn	feature inters.:	Bear Creek
		cadastral grid:	S28/33, T59N, R19W
		highway route:	County Road 180
		highway distr.:	2
		current owner:	Linn County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with one steel stringer approach span on the west end		
substructure:	steel pile bent abutments with timber backwalls; steel pile bent pier		
span number:	1	condition:	fair
span length:	45.0'	alterations:	none
total length:	66.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.0'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; counters: round rod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field bolted to vertical; guardrail: steel angle

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	Cambria Steel Company, Pittsburgh PA
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 180001.1; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.
sign. rating:	27
evaluation:	NRHP non-eligible (undocumented example of an extremely common structural type)

inventoried by: Michelle Crow-Dolby 24 July 1993

# Long Branch Bridge

LINN08

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## GENERAL DATA

<b>structure no.:</b> 201000.3	<b>city/town:</b> 7.4 miles northeast of Linneus
<b>county:</b> Linn	<b>feature inters.:</b> Long Branch
	<b>cadastral grid:</b> S13/24, T59N, R20W
	<b>highway route:</b> County Road 201
	<b>highway distr.:</b> 2
	<b>current owner:</b> Linn County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt pony truss	
<b>substructure:</b> steel pile bent abutments with timber backwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 45.0'	<b>alterations:</b> none
<b>total length:</b> 67.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> steel angle guardrail

## HISTORICAL DATA

<b>erection date:</b> c1910
<b>erection cost:</b> unknown
<b>designer:</b> unknown
<b>fabricator :</b> unknown
<b>contractor:</b> unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 201000.3; Linn County Court Record O: page 23 (6 February 1906) - located at Linn County Courthouse, Linneus MO; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.

<b>sign. rating:</b> 25
<b>evaluation:</b> NRHP non-eligible (standard Pratt pony truss design; construction history largely undocumented)

**inventoried by:** Michelle Crow-Dolby 24 July 1993

# Locust Creek Bridge

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LINN09

## GENERAL DATA

structure no.:	246000.7	city/town:	2.7 miles northwest of Linneus
county:	Linn	feature inters.:	Locust Creek
		cadastral grid:	S27/34, T59N, R21W
		highway route:	County Road 246
		highway distr.:	2
		current owner:	Linn County

## STRUCTURAL DATA

superstructure:	steel, 6-panel, pin-connected Pratt through truss, with two steel stringer approach spans on each end		
substructure:	steel pile bent abutments with timber backwalls; steel pile bent piers		
span number:	1	condition:	fair
span length:	120.0'	alterations:	truss possibly moved
total length:	222.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.0'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles; portal strut: steel angles with bracing and curved knee braces; floor beam: I-beam, field bolted to vertical; guardrail: steel angle

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	Illinois Steel Company, Chicago
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 246000.7; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.
sign. rating:	23
evaluation:	NRHP non-eligible (typically configured Pratt through truss with slightly longer-than-average span length; construction history undocumented)

inventoried by: Michelle Crow-Dolby 24 July 1993

# Bridge

LINN10

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## GENERAL DATA

structure no.:	247000.5	city/town:	2.8 miles southwest of Linneus
county:	Linn	feature inters.:	branch of Locust Creek
		cadastral grid:	S10, T58N, R21W
		highway route:	County Road 247
		highway distr.:	2
		current owner:	Linn County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber backwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	61.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.0'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: round rod with turn-buckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field bolted to vertical; guardrail: 2 channels

## HISTORICAL DATA

erection date:	1906
erection cost:	unknown
designer:	John Gilligan Bridge Company, Falls City NE
fabricator :	John Gilligan Bridge Company, Falls City NE; Cambria Steel Company, Pittsburgh, PA
contractor:	John Gilligan Bridge Company, Falls City NE
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 247000.5; Linn County Court Record O: page 96 (6 August 1906) - located at Linn County Courthouse, Linneus MO; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.
sign. rating:	44
evaluation:	NRHP non-eligible (one of several hundred similarly configured pin-connected Pratt pony trusses)

inventoried by: Michelle Crow-Dolby 24 July 1993

# Locust Creek Bridge

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LINN11

## GENERAL DATA

<b>structure no.:</b> 247000.8	<b>city/town:</b> 2.6 miles southwest of Linneus
<b>county:</b> Linn	<b>feature inters.:</b> Locust Creek
	<b>cadastral grid:</b> S10, T58N, R21W
	<b>highway route:</b> County Road 247
	<b>highway distr.:</b> 2
	<b>current owner:</b> Linn County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 8-panel, pin-connected Parker through truss, with one steel stringer approach span at each end	
<b>substructure:</b> steel pile bent piers and abutments	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 160.0'	<b>alterations:</b> substructure replaced
<b>total length:</b> 221.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 14.0'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 angles with batten plates at the hip); diagonal: 2 punched rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; strut: steel angles, braced; floor beam: I-beam, field bolted to vertical; guardrail: steel angle

## HISTORICAL DATA

<b>erection date:</b> c1925
<b>erection cost:</b> unknown
<b>designer:</b> unknown
<b>fabricator :</b> Scullen Steel Company
<b>contractor:</b> unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 247000.8; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.

<b>sign. rating:</b> 30
<b>evaluation:</b> NRHP non-eligible (typical pinned Parker through truss, poorly documented)

**inventoried by:** Michelle Crow-Dolby 24 July 1993

# Parsons Creek Bridge

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LINN12

## GENERAL DATA

<b>structure no.:</b> 297001.0	<b>city/town:</b> 1.2 miles west of Meadville
<b>county:</b> Linn	<b>feature inters.:</b> Parsons Creek
	<b>cadastral grid:</b> S1/36, T57/58N, R22W
	<b>highway route:</b> County Road 297
	<b>highway distr.:</b> 2
	<b>current owner:</b> Linn County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt pony truss	
<b>substructure:</b> steel pile bent abutments with timber backwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 60.0'	<b>alterations:</b> none
<b>total length:</b> 61.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field bolted to vertical; guardrail: steel angle

## HISTORICAL DATA

<b>erection date:</b> 1913	
<b>erection cost:</b> \$2668.00 (multiple bridge contract)	
<b>designer:</b> Monarch Engineering Company, Fall City NE	
<b>fabricator :</b> Monarch Engineering Company, Falls City NE	
<b>contractor:</b> Monarch Engineering Company, Falls City NE	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 297001.0; Linn County Court Record O: page 103 (4 September 1906), page 289 (7 October 1907); Linn County Court Record P: page 511 (6 May 1913), page 521 (12 June 1913), page 526 (7 July 1913), page 553 (9 October 1913) - located at Linn County Courthouse, Linneus MO; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.
<b>sign. rating:</b> 38	
<b>evaluation:</b>	NRHP non-eligible (typical pinned Pratt pony truss with average span length; construction history well-documented )

**inventoried by:** Michelle Crow-Dolby 24 July 1991

# West Yellow Creek Bridge

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LINN13

## GENERAL DATA

<b>structure no.:</b> 405001.7	<b>city/town:</b> 2.1 miles northeast of Brookfield
<b>county:</b> Linn	<b>feature inters.:</b> West Yellow Creek
	<b>cadastral grid:</b> S32, T58N, R19W
	<b>highway route:</b> County Road 405
	<b>highway distr.:</b> 2
	<b>current owner:</b> Linn County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt pony truss	
<b>substructure:</b> steel pile bent abutments with timber backwalls	
<b>span number:</b> 1	<b>condition:</b> poor
<b>span length:</b> 60.0'	<b>alterations:</b> deck removed / bridge closed
<b>total length:</b> 89.0'	<b>floor/decking :</b> timber deck (removed) over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> no guardrail

## HISTORICAL DATA

<b>erection date:</b> c1915
<b>erection cost:</b> unknown
<b>designer:</b> unknown
<b>fabricator :</b> unknown
<b>contractor:</b> unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 405001.7; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.

<b>sign. rating:</b> 28
<b>evaluation:</b> NRHP non-eligible (standard pin-connected Pratt pony truss exhibiting below-average integrity; no documentation)

**inventoried by:** Michelle Crow-Dolby 24 July 1993

# Long Branch Bridge

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LINN14

## GENERAL DATA

<b>structure no.:</b> 406000.1	<b>city/town:</b> 1.2 miles northeast of Brookfield
<b>county:</b> Linn	<b>feature inters.:</b> Long Branch
	<b>cadastral grid:</b> S32, T58N, R19W
	<b>highway route:</b> County Road 406
	<b>highway distr.:</b> 2
	<b>current owner:</b> Linn County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt pony truss	
<b>substructure:</b> steel pile bent abutments with timber backwalls	
<b>span number:</b> 1	<b>condition:</b> poor
<b>span length:</b> 60.0'	<b>alterations:</b> none
<b>total length:</b> 62.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 18.0'	<b>other features:</b> steel angle guardrail

## HISTORICAL DATA

<b>erection date:</b> 1902	
<b>erection cost:</b> \$425.00	
<b>designer:</b> John Gilligan, Falls City NE	
<b>fabricator :</b> John Gilligan, Falls City NE	
<b>contractor :</b> John Gilligan, Falls City NE	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 406000.1; Linn County Court Record N: page 72 (5 March 1902), page 82 (6 May 1902) - located at Linn County Courthouse, Linneus MO; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.
<b>sign. rating:</b> 44	
<b>evaluation:</b>	NRHP non-eligible (typical pin-connected Pratt pony truss; construction history documented, but in poor physical condition)

**inventoried by:** Michelle Crow-Dolby    24 July 1993

# East Yellow Creek Bridge

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LINN15

## GENERAL DATA

<b>structure no.:</b> 421000.7	<b>city/town:</b> 6.9 miles northeast of Brookfield
<b>county:</b> Linn	<b>feature inters.:</b> East Yellow Creek
	<b>cadastral grid:</b> S18, T58N, R18W
	<b>highway route:</b> County Road 421
	<b>highway distr.:</b> 2
	<b>current owner:</b> Linn County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt pony truss	
<b>substructure:</b> steel pile bent piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 90.0'	<b>alterations:</b> none
<b>total length:</b> 121.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 14.0'	<b>other features:</b> steel angle guardrail

## HISTORICAL DATA

<b>erection date:</b> 1904
<b>erection cost:</b> unknown
<b>designer:</b> John Gilligan Bridge Company, Falls City NE (probable)
<b>fabricator :</b> John Gilligan Bridge Company, Falls City NE (probable)
<b>contractor :</b> John Gilligan Bridge Company, Falls City NE (probable)
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 421000.7; Linn County Court Record N: page 473 (7 September 1904), page 489 (3 October 1904) - located at Linn County Courthouse, Linneus MO; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.
<b>sign. rating:</b> 45
<b>evaluation:</b> NRHP non-eligible (longer-than-average pinned Pratt pony truss with standard detailing; ordinary integrity)

**inventoried by:** Michelle Crow-Dolby 24 July 1993

# East Yellow Creek Bridge

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LINN16

## GENERAL DATA

<b>structure no.:</b> 423001.3	<b>city/town:</b> 6.4 miles northeast of Brookfield
<b>county:</b> Linn	<b>feature inters.:</b> East Yellow Creek
	<b>cadastral grid:</b> S19, T58N, R18W
	<b>highway route:</b> County Road 423
	<b>highway distr.:</b> 2
	<b>current owner:</b> Linn County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt truss leg bedstead	
<b>substructure:</b> steel pile bent abutments with timber backwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 58.0'	<b>alterations:</b> substructure replaced
<b>total length:</b> 94.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> no guardrail

## HISTORICAL DATA

<b>erection date:</b> 1902	
<b>erection cost:</b> \$597.00	
<b>designer:</b> John Gilligan Bridge Company, Falls City NE	
<b>fabricator :</b> John Gilligan Bridge Company, Falls City NE	
<b>contractor :</b> John Gilligan Bridge Company, Falls City NE	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 423001.3; Linn County Court Record N: page 82 (6 May 1902) - located at Linn County Courthouse, Linneus, Missouri; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.
<b>sign. rating:</b> 42	
<b>evaluation:</b>	NRHP non-eligible (typically configured truss leg bedstead, exhibiting below-average physical integrity)

**inventoried by:** Michelle Crow-Dolby    24 July 1993

# East Yellow Creek Bridge

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LINN17

## GENERAL DATA

<b>structure no.:</b> 434001.5	<b>city/town:</b> 6.0 miles east of Brookfield
<b>county:</b> Linn	<b>feature inters.:</b> East Yellow Creek
	<b>cadastral grid:</b> S6, T57N, R18W
	<b>highway route:</b> County Road 434
	<b>highway distr.:</b> 2
	<b>current owner:</b> Linn County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt truss leg bedstead	
<b>substructure:</b> steel pile bent abutments with timber backwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 55.0'	<b>alterations:</b> none
<b>total length:</b> 86.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> steel lattice guardrail

## HISTORICAL DATA

<b>erection date:</b> 1901	
<b>erection cost:</b> unknown	
<b>designer:</b> John Gilligan Bridge Company, Falls City NE (probable)	
<b>fabricator :</b> John Gilligan Bridge Company, Falls City NE (probable)	
<b>contractor:</b> John Gilligan Bridge Company, Falls City NE	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 434001.5; Linn County Court Record M: page 535 (5 August 1901) - located at Linn County Courthouse, Linneus MO; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.	
<b>sign. rating:</b> 42	
<b>evaluation:</b> NRHP non-eligible (largely undocumented, typical Pratt truss leg bedstead)	

**inventoried by:** Michelle Crow-Dolby    24 July 1993

# East Yellow Creek Bridge

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LINN18

## GENERAL DATA

<b>structure no.:</b> 443000.5	<b>city/town:</b> 3.0 miles northwest of Marceline
<b>county:</b> Linn	<b>feature inters.:</b> East Yellow Creek
	<b>cadastral grid:</b> S26, T57N, R19W
	<b>highway route:</b> County Road 443
	<b>highway distr.:</b> 2
	<b>current owner:</b> Linn County

## STRUCTURAL DATA

**superstructure:** steel, 5-panel, rigid-connected Camelback pony truss; two steel stringer approach spans on the west end; one steel stringer approach span on the east end

**substructure:** steel pile bent abutments with timber backwalls; steel pile bent piers

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 80.0'	<b>alterations:</b> none
<b>total length:</b> 150.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 13.8'	<b>other features:</b> upper chord and end post: 2 channels with cover plate and lacing; lower chord: 4 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 angles with batten plates; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: steel angle

## HISTORICAL DATA

**erection date:** c1920

**erection cost:** unknown

**designer:** unknown

**fabricator :** Inland Steel Company, East Chicago IN

**contractor :** unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 443000.5; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.

**sign. rating:** 40

**evaluation:** NRHP non-eligible (uncommon structural configuration; construction history undocumented)

**inventoried by:** Michelle Crow-Dolby 24 July 1993

# Burlington Northern Railroad Overpass

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LINN19

## GENERAL DATA

<b>structure no.:</b> 473000.2	<b>city/town:</b> 1.2 miles southeast of Bucklin
<b>county:</b> Linn	<b>feature inters.:</b> Burlington Northern Railroad tracks
	<b>cadastral grid:</b> S12, T57N, R18W
	<b>highway route:</b> County Road 473
	<b>highway distr.:</b> 2
	<b>current owner:</b> Linn County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt pony truss, with multiple steel stringer approach spans	
<b>substructure:</b> steel pile bent abutments with timber backwalls; steel pile bent piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 60.0'	<b>alterations:</b> none
<b>total length:</b> 124.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 14.0'	<b>other features:</b> steel angle guardrail

## HISTORICAL DATA

<b>erection date:</b> c1915
<b>erection cost:</b> unknown
<b>designer:</b> unknown
<b>fabricator :</b> unknown
<b>contractor:</b> unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 473000.2; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.

<b>sign. rating:</b> 30
<b>evaluation:</b> NRHP non-eligible (undocumented example of an exceedingly common structural type)

**inventoried by:** Michelle Crow-Dolby 24 July 1993

# Locust Creek Covered Bridge

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LINN20

## GENERAL DATA

structure no.:	none	city/town:	3.5 miles west of Laclede
county:	Linn	feature inters.:	abandoned Locust Creek channel
		cadastral grid:	S3/34, T57/58N, R21W
		highway route:	none
		highway distr.:	2
		current owner:	Missouri Department of Natural Resources

## STRUCTURAL DATA

superstructure:	Timber Howe truss (covered)	condition:	under restoration
substructure:	timber (under restoration)	alterations:	none
span number:	1	floor/decking :	timber deck over heavy timber stringers
span length:	151.0'	other features:	wood shingled gable roof; upper chord, end posts, lower chord, diagonal, lateral bracing, floor beams, and guardrails: all heavy timbers; vertical: iron rods
total length:	151.0'		
roadway width:	11.0'		

## HISTORICAL DATA

erection date:	1868
erection cost:	\$5500.00
designer:	unknown
fabricator :	none
contractor:	Bishop and Eaton

references: Linn County Court Record E: page 192 (4 May 1868), page 196 (19 May 1868), page 211 (4 August 1868), page 278 (1 March 1869) - located at Linn County Courthouse, Linneus MO; National Register of Historic Places Inventory - Nomination Form: "Locust Creek Covered Bridge," 2 March 1970); "Bridges To Another Time", Sue Holst - **Missouri Resource Review** pages 14-18 (Fall 1987); "Covered Bridges", Missouri Department of Natural Resources [brochure], located at Jefferson City MO; "Engineering and Enterprise Early Metal-Truss Bridges in Ohio" by David A. Simmons - **Timeline** (The Ohio Historical Society, February - March 1985), pages 14-29; field inspection by Richard Collier, 4 and 10 June 1991.

sign. rating:	74
evaluation:	NRHP individually listed, 1970 ((outstanding early wagon truss))

inventoried by: Michelle Crow-Dolby 24 July 1993

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Locust Creek Bridge  
MHTD: X 480

LINN02

**DATE(S) OF CONSTRUCTION**

1925; moved 1938-39

**LOCATION**

State Secondary Route B over Locust Creek; S3, T58N, R21W  
2.3 miles west of Linneus; Linn County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / highway bridge

**RATING** NRHP non-eligible (score: 41)

**CONDITION**

good

**OWNER**

Linn County

span number: 1  
span length: 125.0'  
total length: 237.0'  
roadway wdt.: 20.0'

superstructure: steel, 5-panel, pin-connected Pratt through truss; 1 steel stringer approach span on the east end; 2 steel stringer approach spans on the west end  
substructure: steel pile bent piers with timber backwalls; concrete piers  
floor/decking: asphalt-covered concrete deck over steel stringers  
other features: upper chord and inclined end post: 2 channels with cover plate and double lacing; lower chord: 4 punched rectangular eyebars; vertical: 2 channels with lacing (2 punched rectangular eyebars at the hip); diagonal: 4 punched rectangular eyebars; lateral bracing: 1 angle; strut: 4 angles with lacing and knee braces; floor beam: plate girder; guardrail: steel pipe, with vertical pipe supports with ornamental ball caps (concrete guardrails with weep holes on the approaches); bridge plate: Erected August 30, 1939 / Members of the Linn County Court / Virgil Lee, Presiding Judge / M.C. Post, Judge E. Dist. / W.E. McKenzie Judge W. Dist. / C.E. Swinney, County Clerk / Wallace L. Turner, Highway Engineer / D.A. Peery, Contractor

This pin-connected Pratt through truss carries State Secondary Route B over channelized Locust Creek a little over two miles west of Linneus, in west-central Linn County. On December 16, 1938, Linn County contracted with D.A. Peery for the bridge's construction. Using plans prepared by the Missouri State Highway Department, Peery obtained an existing truss from Chariton County (Bridge No. H-18, previously re-erected in 1925 on Route 24; original location unknown) for \$420.57. Peery straightened and modified the structure, and re-erected it over Locust Creek. Peery's original contract called for payment of \$18,882.00 plus the cost of the truss, but in the spring of 1939 this amount was increased by \$375.00 when the highway department decided that "hot rivets" should be used rather than the "Dardalet rivet bolts" called for in the original contract. Rainy weather and delayed deliveries slowed construction during the summer of 1939. By the end of August, though, the project was completed, about a month behind schedule. The Linn County Court, during its November 1939 session, formally accepted the work and declared the bridge completed. Today, the structure continues to carry vehicular traffic and has not been noticeably altered.

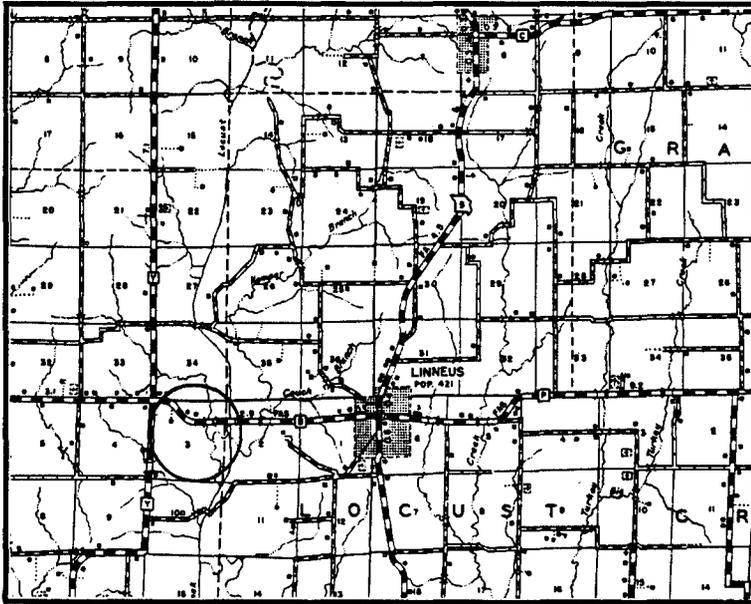
The Locust Creek Bridge is an example of a pin-connected Pratt through truss. Built originally for use on a less-traveled secondary route, the bridge was later strengthened for use on a route that carried heavier traffic loads. The bridge is of average length, and is typically configured.

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**NAME(S) OF STRUCTURE**

Locust Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP



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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. X-480; Linn County Court Record Y: page 206 (8 December 1938), page 237 (16 May 1939), page 241 (6 July 1939), page 245 (16 August 1939), page 249 (September 1939), page 257 (7 November 1939) - located at Linn County Courthouse, Linneus MO; field inspection by Carl McWilliams and Richard Collier, 4 June 1991.

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**INVENTORIED BY**

Michelle Crow-Dolby

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

24 July 1993

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

West Yellow Creek Bridge  
MHTD: 164000.1

LINN06

**DATE(S) OF CONSTRUCTION**

1901

**LOCATION**

County Road 164 over West Yellow Creek; S26, T59N, R19W  
8.5 miles northeast of Brookfield; Linn County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 42)

**CONDITION**

fair

**OWNER**

Linn County

span number: 1

span length: 60.0'

total length: 94.0'

roadway wdt.: 12.0'

superstructure: steel, 4-panel, pin-connected Pratt truss leg bedstead, with one steel stringer approach span at each end

substructure: truss legs; steel pile bent abutment at west end; concrete abutment with wingwalls at east end

floor/decking: timber deck over steel stringers

other features: upper chord: 2 channels with cover and batten plates; end post: 2 channels with lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: none

This bridge over West Yellow Creek carries traffic along a gravel-surfaced county road 8½ miles northeast of Brookfield, in east-central Linn County. Designed and built as a pin-connected Pratt truss leg bedstead, the structure's history dates to the turn of the century. From the mid-1890s to circa 1910 prolific bridge builders John Gilligan of Falls City, Nebraska, or A.M. Blodgett of Kansas City built most of Linn County's steel trusses. The West Yellow Creek Bridge was no exception. On October 7, 1901 Gilligan was awarded two contracts - \$568.00 for this bridge between sections 26 and 35, in Township 59 North, Range 19 West, and \$240.00 for another bridge (perhaps a timber structure) northwest of Purdin. No subsequent references regarding the West Yellow Creek crossing were found in county records, but it was evidently built as planned. Now in its ninth decade of service, the truss appears largely as originally built. The substructure, though, has suffered some loss of integrity by the addition of I-beams welded to the truss legs to provide additional support.

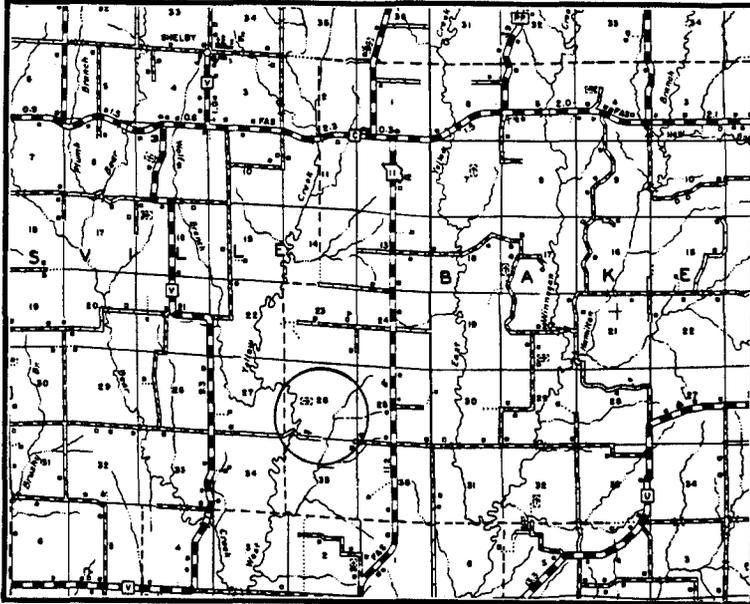
Pin-connected Pratt truss leg bedsteads were used prolifically throughout Missouri for short - to medium-span crossings, between circa 1900 and 1915. More than 200 such bridges remain, and among these, the West Yellow Creek Bridge ranks as an early and well-documented example.

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**NAME(S) OF STRUCTURE**

West Yellow Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 164000.1; Linn County Court Record, Book M: page 572 (7 October 1901) - located at Linn County Courthouse, Linneus MO; field inspection by Richard Collier, 4 June 1991.

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**INVENTORIED BY**

Michelle Crow-Dolby

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

24 July 1993

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Locust Creek Covered Bridge  
MHTD: none

LINN20

**DATE(S) OF CONSTRUCTION**

1868

**LOCATION**

abandoned Locust Creek channel; S3/34, T57/58N, R21W  
3.5 miles west of Laclede; Linn County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / under restoration

**RATING** NRHP listed, 1970 (score: 74)

**CONDITION**

under restoration

**OWNER**

Missouri Department of Natural Resources

span number: 1  
span length: 151.0'  
total length: 151.0'  
roadway wdt.: 11.0'

superstructure: Timber Howe truss (covered)  
substructure: timber (under restoration)  
floor/decking: timber deck over heavy timber stringers  
other features: wood shingled gable roof; upper chord, end posts, lower chord, diagonal, lateral bracing, floor beams, and guardrails: all heavy timbers; vertical: iron rods

During the nineteenth century the covered timber bridge was a mainstay means of spanning streams and rivers on roadways throughout rural America. During this era the ready availability and concomitant low cost of timber contributed to the popularity of wooden bridges. Moreover, because wooden structural members could not long withstand the forces of nature, such bridges were often covered to provide protection, and thus increase their longevity. After the industrial revolution, though, timber bridges, covered or otherwise, were eclipsed first by all iron and later by steel trusses. Early covered bridges were usually Kingpost or Queenpost trusses (both used for shorter crossings), a Burr Arch-Truss (patented by Theodore Burr of Pennsylvania in 1804), or a Town Lattice Truss (patented by Ithiel Town in 1820). In 1840 William Howe of Massachusetts patented a new truss design which featured diagonal wooden members acting in compression, combined with iron verticals acting in tension. Erected in 1868, the Locust Creek Bridge is an example of a Howe Truss design.

On May 19, 1868 the Linn County Court ordered the county bridge commissioner to contract for the "building of a bridge across Locust Creek on the state road from Brookfield to Collyer's Mill where it crosses Locust Creek." The court further ordered that the bridge's cost not exceed \$5500.00, and that it be built as a "150-foot span, How [Howe] Truss 50-feet of approach, 25 feet on each end, two substantial butts [abutments] under each end, that will last say from ten to fifteen years." On August 4, 1868 the court ordered that the contract of Bishop and Eaton - apparently local contractors - be accepted, and that they were to be paid in ten percent bonds rather than warrants or orders. Bishop and Eaton began construction late that summer and completed the crossing early the following year. They first built massive stone abutments, then the 151-foot main span, and finally 25-foot approaches on each end. A tribute to their craftsmanship, the bridge still stands today, having lasted more than a century beyond the ten to fifteen years originally envisioned by the Linn County Court. Located between Laclede and Meadville, the Locust Creek Bridge carried traffic along a portion of what became the country's first transcontinental highway, later designated U.S. Route 8. In later years, when U.S. Highway 36 was built to the south,



much of the old roadway including the Locust Creek crossing was no longer utilized. After World War II, a new Locust Creek channel was dredged further east, leaving the Locust Creek Covered Bridge spanning a dry creek bed. Over the years silt washing in over the site eventually covered the stone abutments, so that the bridge appeared to be resting on the ground.

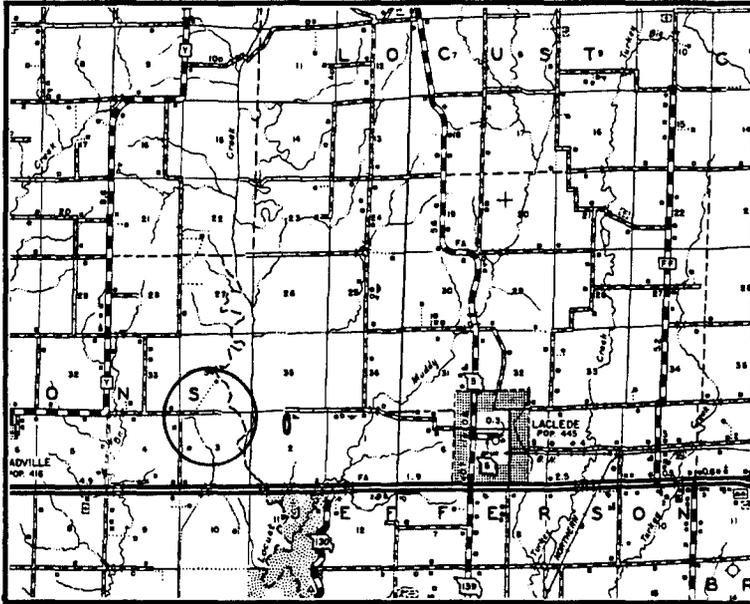
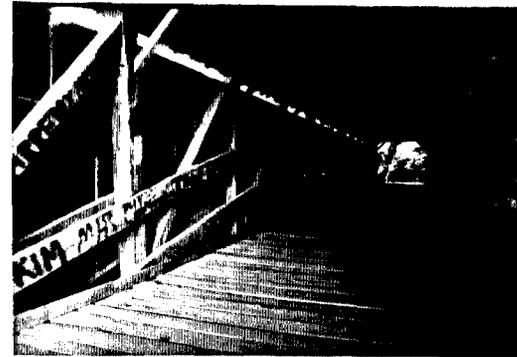
In 1967 the State of Missouri took over stewardship of its remaining covered bridges. Administered as State Historic Sites by the Department of Natural Resources, four such bridges still exist, and have each been listed in the National Register of Historic Places. In addition to the Locust Creek crossing, Missouri's three other covered bridges include the Sandy Creek Covered Bridge near Hillsboro, the Burfordville Covered Bridge at the Bollinger Mill State Historic Site, and the Union Covered Bridge near Paris in Monroe County. Efforts are currently underway (in 1991) to restore the Locust Creek crossing. Portions of the original abutments are being excavated, and the truss itself is being raised off the ground so that the bridge will more closely appear as it did during its period of significance.

The historical significance of the Locust Creek Covered Bridge can hardly be overstated. One of only four covered bridges left in the state, the crossing is a rare, well-preserved, example of covered bridge construction from the mid-19th century, prior to the advent of iron and steel trusses.

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**NAME(S) OF STRUCTURE**

Locust Creek Covered Bridge

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Linn County Court Record, Book E: page 192 (4 May 1868), page 196 (19 May 1868), page 211 (4 August 1868), page 278 (1 March 1869) - located at Linn County Courthouse, Linneus MO; National Register of Historic Places Inventory - Nomination Form: "Locust Creek Covered Bridge," 2 March 1970); "Bridges To Another Time", Sue Holst - *Missouri Resource Review* pages 14-18 (Fall 1987); "Covered Bridges", Missouri Department of Natural Resources [brochure], located at Jefferson City MO; "Engineering and Enterprise Early Metal-Truss Bridges in Ohio" by David A. Simmons - *Timeline* (The Ohio Historical Society, February - March 1985), pages 14-29; field inspection by Richard Collier, 4 and 10 June 1991.

**INVENTORIED BY**

Michelle Crow-Dolby

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**24 July 1993

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# LIVINGSTON COUNTY

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**INCLUDED:** [Significant feature(s) of bridge given in boldface]  
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
LIVI01	S 639	Shoal Creek Bridge	1-100' riveted Warren pony truss 1933 Amos Melberg
*LIVI02	035000.5	Clear Creek Bridge	1- 42' riveted Warren pony truss 1906 A.M. Blodgett, Kansas City MO
LIVI03	080500.1	Fair Avenue Bridge	(replaced)
*LIVI04	150000.7	Medicine Creek Bridge	(replaced)
*LIVI05	193000.0	Muddy Creek Bridge	1- 64' pinned Pratt bedstead 1901 Dildine Bridge and Constr. Co.
*LIVI06	207000.0	Muddy Creek Bridge	1- 50' riveted Warren pony truss 1906 W.O. Graham, Chillicothe MO
*LIVI07	322000.8	Shoal Creek Bridge	1-100' <b>pinned Pratt through truss</b> 1919 Kansas City Bridge Company
*LIVI08	337001.2	Rattlesnake Creek Bridge	1- 32' pinned Pratt bedstead c1900
*LIVI09	465002.4	Campbell Creek Bridge	1- 50' <b>riveted polyg. Warren bedstead</b> 1906 W.O. Graham, Chillicothe MO

**EXCLUDED:**

Pratt pony truss

425000.5 433000.3 434002.1 438000.3 439002.3

Warren pony truss

142000.5 144001.9 189001.1 191001.0 323000.9 324001.2 509000.1

Steel stringer

G 612R	G 614R	G 862R1	L 156R	L 158	S 472	T 378
T 379	X 148	X 956	011000.1	024000.2	036000.3	036001.3
053000.3	072R01.7	072001.3	080500.2	084000.1	090000.1	093000.2
101000.8	102000.1	105R01.2	107000.5	119000.3	123000.2	123001.0
139002.1	168000.1	176001.1	193000.1	207000.0	218000.5	253000.7
258000.6	296R02.0	296001.1	296001.5	297000.6	305R01.0	339001.3
341000.7	347001.6	358001.4	358001.8	359001.8	359002.0	361001.7
362002.3	365000.5	366000.3	370000.5	376001.8	377000.4	408000.8
411000.1	412000.3	415R00.0	429001.5	444001.2	444002.2	452000.9
452001.4	458000.5	460000.4	460001.0	462001.1	466001.8	469001.2
470000.2	473001.5	484000.3	485001.6	493000.0	507000.6	508000.6

Steel girder

080500.5 424001.3 459001.5

Concrete girder

H 844R K 518 251002.5

# LIVINGSTON COUNTY

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## EXCLUDED (cont.):

Concrete slab  
G 861R1

Concrete box culvert  
G 611R    G 613R    H 601R    H 843    L 157    T 84    T 328  
X 714

Timber stringer  
Y 570    080500.4    221000.3    229000.2    430000.9

## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	1	6	0	0	7
Excluded	22	82	3	0	107
<hr/>					
	23	88	3	0	114 structures

# Shoal Creek Bridge

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LIV101

## GENERAL DATA

<b>structure no.:</b> S 639	<b>city/town:</b> 3.7 miles northeast of Ludlow
<b>county:</b> Livingston	<b>feature inters.:</b> Shoal Creek Drainage Ditch
	<b>cadastral grid:</b> S7, T56N, R24W / S12, T56N, R25W
	<b>highway route:</b> State Secondary Route C
	<b>highway distr.:</b> 2
	<b>current owner:</b> Missouri Highway and Transportation Department

## STRUCTURAL DATA

**superstructure:** steel, 5-panel, rigid-connected Warren pony truss, skewed, with steel stringer approach spans

**substructure:** timber abutments, wingwalls and piers

<b>span number:</b> 1	<b>condition:</b> good
<b>span length:</b> 100.0'	<b>alterations:</b> none
<b>total length:</b> 178.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 20.0'	<b>other features:</b> guardrail: steel

## HISTORICAL DATA

**erection date:** 1933

**erection cost:** \$10,111.80

**designer:** Missouri State Highway Department

**fabricator :** unknown

**contractor:** Amos Melberg

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. S 639.

**sign. rating:** 63

**evaluation:** NRHP possibly eligible (typically configured example of MSHD standard highway truss design, distinguished somewhat by its skewed configuration)

**inventoried by:** Clayton B. Fraser    10 May 1991

# Clear Creek Bridge

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LIV102

## GENERAL DATA

<b>structure no.:</b> 035000.5	<b>city/town:</b> 14.8 miles northwest of Chillicothe
<b>county:</b> Livingston	<b>feature inters.:</b> Clear Creek
	<b>cadastral grid:</b> S7, T59N, R25W
	<b>highway route:</b> County Road 35
	<b>highway distr.:</b> 2
	<b>current owner:</b> Livingston County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, rigid-connected Warren pony truss	
<b>substructure:</b> steel pile abutments with timber wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 42.0'	<b>alterations:</b> unknown
<b>total length:</b> 42.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 12.0'	<b>other features:</b> steel guardrails

## HISTORICAL DATA

<b>erection date:</b> 1906
<b>erection cost:</b> unknown
<b>designer:</b> A.M. Blodgett, Kansas City MO
<b>fabricator :</b> Kansas City Bridge Company, Kansas City MO (probable)
<b>contractor:</b> A.M. Blodgett, Kansas City MO
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 035000.5; Livingston County Court Record, Book P: page 19 (2 October 1906) -located at the Livingston County Courthouse, Chillicothe MO; field inspection by Clayton Fraser, 10 December 1990.
<b>sign. rating:</b> 48
<b>evaluation:</b> NRHP possibly eligible (one of earliest documented examples in Missouri of mainstay structural type)

**inventoried by:** Clayton B. Fraser    10 May 1991

# Muddy Creek Bridge

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LIV105

## GENERAL DATA

<b>structure no.:</b> 193000.0	<b>city/town:</b> 8.2 miles northeast of Chillicothe
<b>county:</b> Livingston	<b>feature inters.:</b> Muddy Creek
	<b>cadastral grid:</b> S19/30, T58N, R22W
	<b>highway route:</b> County Road 193
	<b>highway distr.:</b> 2
	<b>current owner:</b> Livingston County

## STRUCTURAL DATA

**superstructure:** steel, 4-panel, pin-connected Pratt truss leg bedstead  
**substructure:** stone abutments and timber wingwalls

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 64.0'	<b>alterations:</b> moved to this location, 1902
<b>total length:</b> 64.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> end post: 2 channels with lacing; top chord: 2 channels with cover and batten plates; lower chord: 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: round bar with turnbuckles; lateral bracing: round bar with threaded ends; floor beam: I-beams U-bolted to vertical; guardrail: none

## HISTORICAL DATA

**erection date:** 1901  
**erection cost:** \$1500.00 (contract amount)  
**designer:** Dildine Bridge and Construction Company, Cameron MO  
**fabricator :** Dildine Bridge and Construction Company, Cameron MO  
**contractor :** Monroe Township Board

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 193000.0; Livingston County Court Record, Book N: page 92 (10 October 1901), page 95 (7 November 1901), page 246 (6 August 1902) - located at the Livingston County Courthouse, Chillicothe MO; field inspection by Clayton Fraser, 10 December 1990.

**sign. rating:** 51  
**evaluation:** NRHP possibly eligible (well-preserved example of mainstay structural type of early 20th century)

**inventoried by:** Clayton B. Fraser 10 May 1991

# Muddy Creek Bridge

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LIV106

## GENERAL DATA

<b>structure no.:</b>	207000.0	<b>city/town:</b>	2.0 miles northwest of Wheeling
<b>county:</b>	Livingston	<b>feature inters.:</b>	Muddy Creek
		<b>cadastral grid:</b>	S31, T58N, R22W
		<b>highway route:</b>	County Road 207
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Livingston County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 3-panel, rigid-connected Warren pony truss		
<b>substructure:</b>	steel pile abutments with timber wingwalls		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	50.0'	<b>alterations:</b>	none
<b>total length:</b>	115.0'	<b>floor/decking :</b>	timber deck
<b>roadway width:</b>	13.0'	<b>other features:</b>	no guardrails

## HISTORICAL DATA

<b>erection date:</b>	1906
<b>erection cost:</b>	unknown
<b>designer:</b>	unknown
<b>fabricator :</b>	unknown
<b>contractor:</b>	W.O. Graham, Chillicothe MO
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 207000.0; Livingston County Court Record, Book O: page 633 (13 September 1906) - located at the Livingston County Courthouse, Chillicothe MO; field inspection by Clayton Fraser, 10 December 1990.
<b>sign. rating:</b>	48
<b>evaluation:</b>	NRHP possibly eligible (one of earliest documented examples in Missouri of mainstay structural type)

**inventoried by:** Clayton B. Fraser    10 May 1991

# Shoal Creek Bridge

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LIVI07

## GENERAL DATA

<b>structure no.:</b> 322000.8	<b>city/town:</b> 2.2 miles northeast of Ludlow
<b>county:</b> Livingston	<b>feature inters.:</b> Shoal Creek
	<b>cadastral grid:</b> S19/20, T56N, R25W
	<b>highway route:</b> County Road 322
	<b>highway distr.:</b> 2
	<b>current owner:</b> Livingston County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 6-panel, pin-connected Pratt through truss, with steel stringer approach span at each end	
<b>substructure:</b> timber pile abutments and wingwalls with steel cylinder piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 100.0'	<b>alterations:</b> none
<b>total length:</b> 160.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> upper chord: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 punched rectangular eyebars; diagonal: 2 punched rectangular eyebars; counter: 1 round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles with continuous plates; floor beam: I-beam, field bolted to vertical; guard-rail: 2 angles; gusset plates at upper chord connection; A-frame portal

## HISTORICAL DATA

<b>erection date:</b> 1919	
<b>erection cost:</b> 4799.00	
<b>designer:</b> Kansas City Bridge Company, Kansas City MO	
<b>fabricator :</b> Kansas City Bridge Company, Kansas City MO; Illinois Steel Company, Chicago IL	
<b>contractor:</b> Kansas City Bridge Company, Kansas City MO	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 322000.8; Livingston County Court Record, Book T: page 487 (3 June 1919), page 489 (10 June 1919) - located at the Livingston County Courthouse, Chillicothe MO; field inspection by Clayton Fraser, 10 December 1990.	
<b>sign. rating:</b> 41	
<b>evaluation:</b> NRHP determined non-eligible (typically configured, relatively late example of common structural type)	

Inventoried by: Clayton B. Fraser 10 May 1991

# Rattlesnake Creek Bridge

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LIV108

## GENERAL DATA

<b>structure no.:</b> 337001.2	<b>city/town:</b> 2.5 miles northeast of Ludlow
<b>county:</b> Livingston	<b>feature inters.:</b> Rattlesnake Creek
	<b>cadastral grid:</b> S2/11, T56N, R25W
	<b>highway route:</b> County Road 337
	<b>highway distr.:</b> 2
	<b>current owner:</b> Livingston County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 2-panel, pin-connected Pratt bedstead, with steel stringer approach spans at both ends	
<b>substructure:</b> steel pile bent piers with timber pile abutments	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 32.0'	<b>alterations:</b> truss moved to this location
<b>total length:</b> 104.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> upper chord and upright end post: 2 channels with lacing; lower chord: 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: none

## HISTORICAL DATA

<b>erection date:</b> c1900
<b>erection cost:</b> unknown
<b>designer:</b> unknown
<b>fabricator :</b> unknown
<b>contractor:</b> unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 337001.2; field inspection by Clayton Fraser, 10 December 1990.

<b>sign. rating:</b> 15
<b>evaluation:</b> NRHP non-eligible (typical, small-scale example of common truss type, moved to this location)

**inventoried by:** Clayton B. Fraser    10 May 1991

# Campbell Creek Bridge

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LIVI09

## GENERAL DATA

<b>structure no.:</b>	465002.4	<b>city/town:</b>	0.6 mile west of Bedford
<b>county:</b>	Livingston	<b>feature inters.:</b>	Campbell Creek
		<b>cadastral grid:</b>	S4, T56N, R22W
		<b>highway route:</b>	County Road 465
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Livingston County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 3-panel, rigid-connected polygonal Warren bedstead truss		
<b>substructure:</b>	steel pile bents pier and abutments with timber wingwalls		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	50.0'	<b>alterations:</b>	none
<b>total length:</b>	78.0'	<b>floor/decking :</b>	timber deck
<b>roadway width:</b>	14.0'	<b>other features:</b>	no guardrails

## HISTORICAL DATA

<b>erection date:</b>	1906
<b>erection cost:</b>	unknown
<b>designer:</b>	unknown
<b>fabricator :</b>	unknown
<b>contractor:</b>	W.O. Graham, Chillicothe MO

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 465002.4; Livingston County Court Record, Book O: page 433 (13 September 1906) -located at the Livingston County Courthouse, Chillicothe MO; field inspection by Clayton Fraser, 10 December 1990.

<b>sign. rating:</b>	64
<b>evaluation:</b>	NRHP possibly eligible (uniquely configured example of what would later become a mainstay structural type)

**inventoried by:** Clayton B. Fraser    10 May 1991

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Shoal Creek Bridge  
MHTD: S 639

LIVI01

**DATE(S) OF CONSTRUCTION**

1933

**LOCATION**

State Secondary Route C over Shoal Creek Drainage Ditch; S7, T56N, R24W / S12, T56N, R25W; 3.7 miles northeast of Ludlow; Livingston County, Missouri

**USE (ORIGINAL / CURRENT)**

highway bridge / highway bridge

**RATING** NRHP possibly eligible (score: 63)

**CONDITION**

good

**OWNER**

Missouri Highway and Transportation Department

span number: 1  
span length: 100.0'  
total length: 178.0'  
roadway wdt.: 20.0'

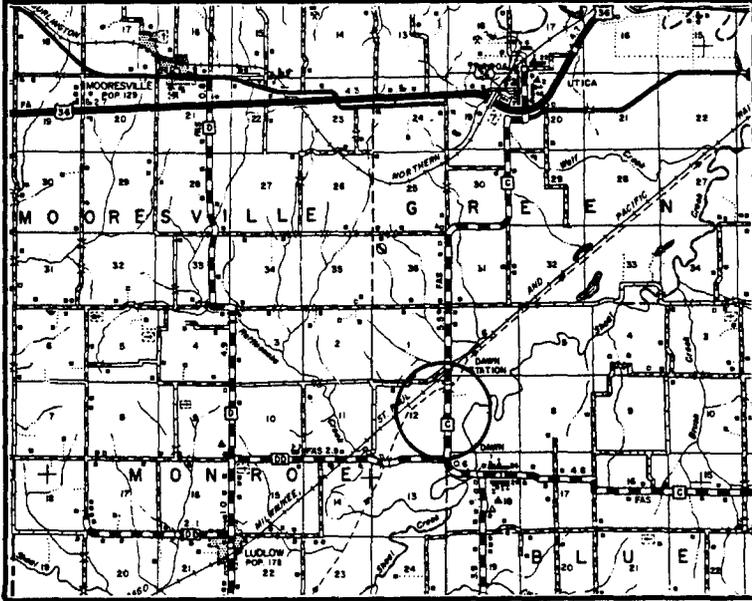
superstructure: steel, 5-panel, rigid-connected Warren pony truss, skewed, with steel stringer approach spans  
substructure: timber abutments, wingwalls and piers  
floor/decking: timber deck over steel stringers  
other features: guardrail: steel

Located northeast of Ludlow, this long-span pony truss carries State Secondary Route C over channelized Shoal Creek. The 100-foot channel span is a rigid-connected Warren pony truss, carried on a skew by a concrete substructure. The Shoal Creek Bridge was designed early in 1933 by engineers for the Missouri State Highway Department. On March 4th a contract to build the bridge was awarded to Amos Melberg. Probably completed later that year, the Shoal Creek Bridge has functioned in place with no serious alterations.

The Missouri State Highway Department used riveted Warren configurations for its pony trusses almost from the time the agency developed its first bridge standards around 1920. Structurally straightforward and versatile, these ubiquitous trusses were erected by the hundreds throughout the state in span lengths ranging from 40 to 100 feet. The Shoal Creek Bridge is distinguished among Missouri's Warren trusses as among the few skewed examples of this mainstay structural type.

**NAME(S) OF STRUCTURE**  
Shoal Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. S 639.

**INVENTORIED BY**  
Clayton B. Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
10 May 1991

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Clear Creek Bridge  
MHTD: 035000.5

LIV102

**DATE(S) OF CONSTRUCTION**

1906

**LOCATION**

County Road 35 over Clear Creek; S7, T59N, R25W  
14.8 miles northwest of Chillicothe; Livingston County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 48)

**CONDITION**

fair

**OWNER**

Livingston County

span number: 1	superstructure: steel, 3-panel, rigid-connected Warren pony truss
span length: 42.0'	substructure: steel pile abutments with timber wingwalls
total length: 42.0'	floor/decking: timber deck
roadway wdt.: 12.0'	other features: steel guardrails

Located in the extreme northwest corner of Livingston County, this small-scale steel truss carries an unpaved county road over Clear Creek. The Clear Creek Bridge is comprised of a single rigid-connected Warren pony truss, supported by steel pile bents. The bridge dates to 1906. That year the Livingston County Court contracted with bridge builder A.M. Blodgett of Kansas City to supply and erect five small-scale steel structures, including this truss in Jackson Township. At the beginning of October, all five were reported complete by the county and opened to wagon traffic. Since its completion, the Clear Creek Bridge has continued to function in place, with only maintenance-related repairs.

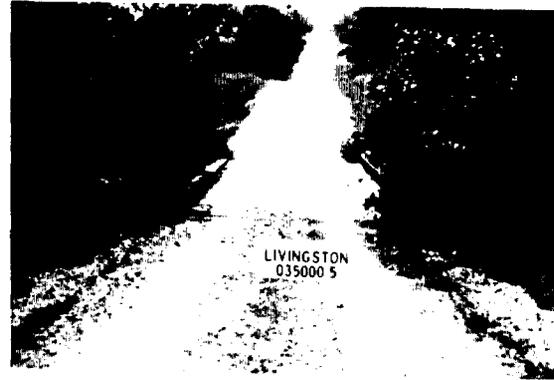
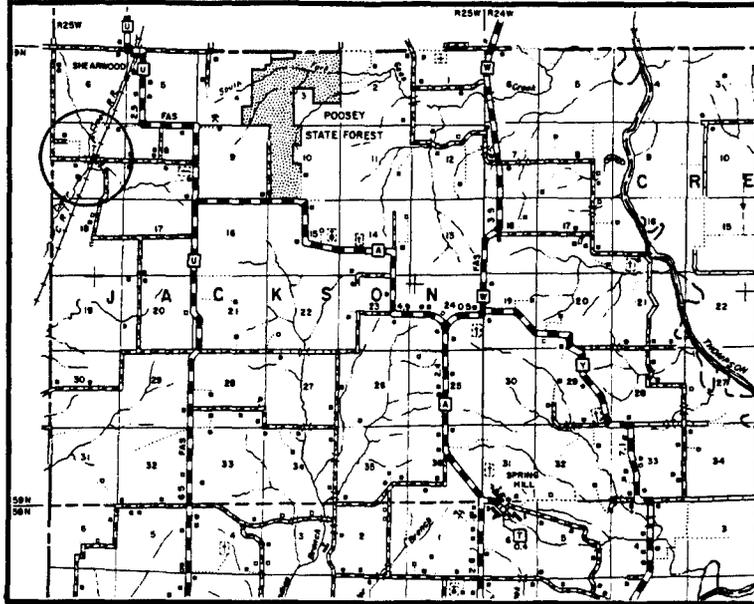
Patented in 1848 by Captain James Warren and Theobald Monzani, the Warren truss in its classic form features a web configuration that relies on simple triangulation for its rigidity. "The term Warren truss or Warren girder was originally applied only to the particular case of the Triangular truss in which the web triangles are all equilateral; but later writers generally use the name for any triangular truss" noted bridge engineer J.A.L. Waddell stated in his 1916 **Bridge Engineering**. "As there is no special advantage in making the web triangles equilateral, there does not appear to be any good *raison d'être* for the use of the true Warren type." Warrens were built sparingly in the 19th century, a period in which the pin-connected Pratt dominated the bridge industry. After the turn of the century, however, rigid-connected Warren pony trusses began to supersede earlier pinned Pratt configurations for use on short- to intermediate-span highway bridges. The Clear Creek Bridge in Livingston County is significant as the one of the earliest documented examples in Missouri of the riveted Warren truss—a small-scale example of what would later become a mainstay structural type in the state.

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**NAME(S) OF STRUCTURE**

Clear Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 035000.5; Livingston County Court Record, Book P: page 19 (2 October 1906) -located at the Livingston County Courthouse, Chillicothe MO; field inspection by Clayton Fraser, 10 December 1990.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

10 May 1991

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**  
Muddy Creek Bridge  
MHTD: 193000.0

LIV105

**DATE(S) OF CONSTRUCTION**  
1901

**LOCATION**

County Road 193 over Muddy Creek; S19/30, T58N, R22W  
8.2 miles northeast of Chillicothe; Livingston County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 51)

**CONDITION**

fair

**OWNER**

Livingston County

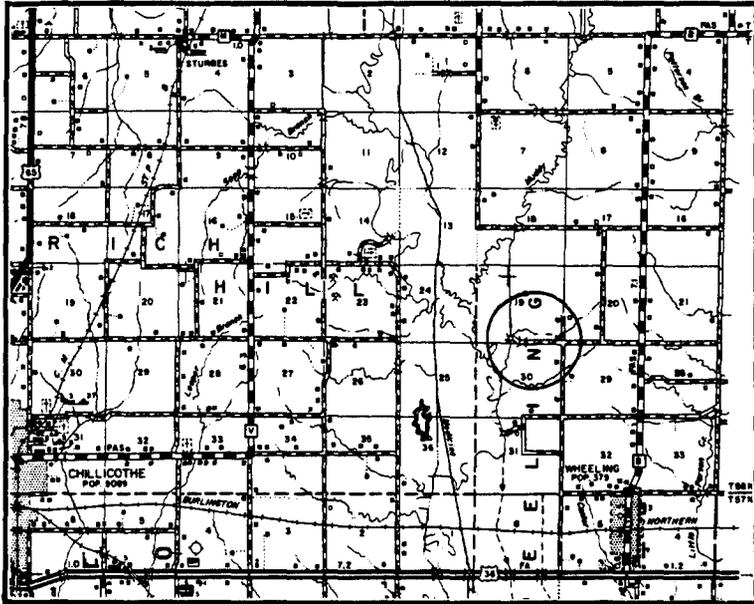
span number:	1	superstructure:	steel, 4-panel, pin-connected Pratt truss leg bedstead
span length:	64.0'	substructure:	stone abutments and timber wingwalls
total length:	64.0'	floor/decking:	timber deck over steel stringers
roadway wdt.:	12.0'	other features:	end post: 2 channels with lacing; top chord: 2 channels with cover and batten plates; lower chord: 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: round bar with turnbuckles; lateral bracing: round bar with threaded ends; floor beam: I-beams U-bolted to vertical; guardrail: none

Among the eleven structures listed in the statewide bridge inventory from Livingston County is this 64-foot steel span which crosses Muddy Creek some eight miles northeast of Chillicothe. This timber-decked structure features a pinned Pratt truss-leg bedstead, supported by bedstead legs with stone masonry backwalls. The bridge traces its history to 1901. In October of that year the county bridge commissioner J.Y. Powell was ordered by the Livingston County Court to view a crossing site here in Wheeling Township and make estimates for a new bridge. Apparently the cost of a new bridge was prohibitive, because in the summer of 1902 the county directed the Monroe Township board to move an existing bridge from its location south of Ludlow to this site. The Monroe Township bedstead had been constructed over Shoal Creek late the previous year by the Dildine Bridge and Construction Company of Cameron, Missouri. Total cost: \$1500.00. Since its move in 1902, the Muddy Creek Bridge has gained a definite sense of place while carrying County Road 193 over Muddy Creek for over ninety years, in essentially unaltered condition.

In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the late 1890s and early 1900s. Hundreds remain in place today, in fact, Missouri has probably more bedsteads than any other state. The Muddy Creek Bridge is a well-preserved, though not particularly distinguished, example of this statewide bridge construction trend.

**NAME(S) OF STRUCTURE**  
Muddy Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 193000.0; Livingston County Court Record, Book N: page 92 (10 October 1901), page 95 (7 November 1901), page 246 (6 August 1902) - located at the Livingston County Courthouse, Chillicothe MO; field inspection by Clayton Fraser, 10 December 1990.

**INVENTORIED BY**  
Clayton B. Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
10 May 1991

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**  
Muddy Creek Bridge  
MHTD: 207000.0

LIVI06

**DATE(S) OF CONSTRUCTION**  
1906

**LOCATION**

County Road 207 over Muddy Creek; S31, T58N, R22W  
2.0 miles northwest of Wheeling; Livingston County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 48)

**CONDITION**  
fair

**OWNER**  
Livingston County

span number: 1	superstructure: steel, 3-panel, rigid-connected Warren pony truss
span length: 50.0'	substructure: steel pile abutments with timber wingwalls
total length: 115.0'	floor/decking: timber deck
roadway wdt.: 13.0'	other features: no guardrails

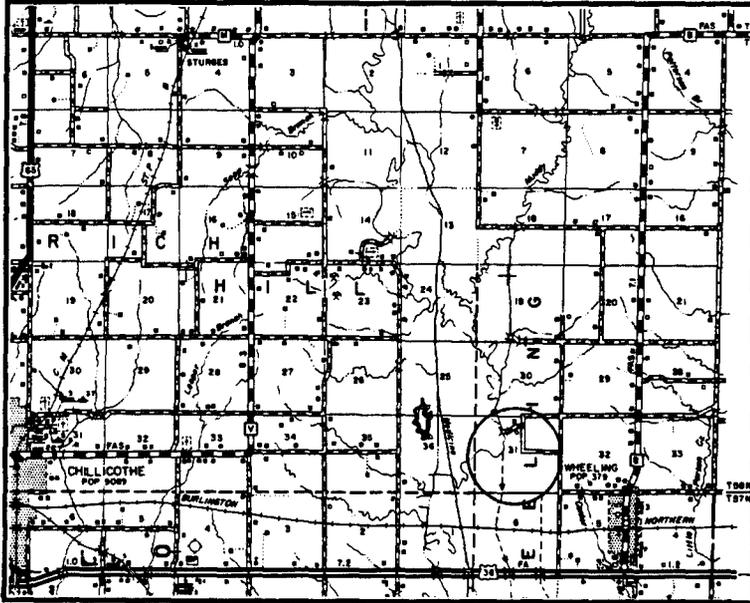
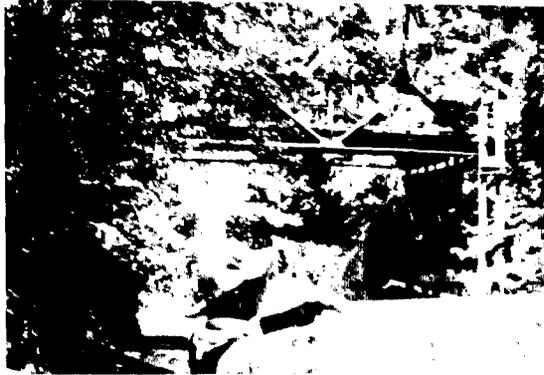
Located some two miles northwest of Wheeling, this small-scale steel truss carries an unpaved county road over Muddy Creek. The Muddy Creek Bridge is comprised of a single rigid-connected Warren pony truss, supported by steel pile bents. The bridge dates to 1906. That year the Livingston County Court contracted with bridge builder W.O. Graham of Chillicothe to supply and erect several small-scale steel structures, including this truss in Wheeling Township. By early the next year, all were reported complete by the county and opened to wagon traffic. Since its completion, the Muddy Creek Bridge has continued to function in place, with only maintenance-related repairs.

Patented in 1848 by Captain James Warren and Theobald Monzani, the Warren truss in its classic form features a web configuration that relies on simple triangulation for its rigidity. "The term Warren truss or Warren girder was originally applied only to the particular case of the Triangular truss in which the web triangles are all equilateral; but later writers generally use the name for any triangular truss" noted bridge engineer J.A.L. Waddell stated in his 1916 *Bridge Engineering*. "As there is no special advantage in making the web triangles equilateral, there does not appear to be any good *raison d'être* for the use of the true Warren type." Warrens were built sparingly in the 19th century, a period in which the pin-connected Pratt dominated the bridge industry. After the turn of the century, however, rigid-connected Warren pony trusses began to supersede earlier pinned Pratt configurations for use on short- to intermediate-span highway bridges. The Muddy Creek Bridge in Livingston County is significant as the one of the earliest documented examples in Missouri of the riveted Warren truss—a small-scale example of what would later become a mainstay structural type in the state.

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**NAME(S) OF STRUCTURE**  
Muddy Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 207000.0; Livingston County Court Record, Book O: page 633 (13 September 1906) - located at the Livingston County Courthouse, Chillicothe MO; field inspection by Clayton Fraser, 10 December 1990.

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**INVENTORIED BY**  
Clayton B. Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
10 May 1991

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Campbell Creek Bridge  
MHTD: 465002.4

LIVI09

**DATE(S) OF CONSTRUCTION**

1906

**LOCATION**

County Road 465 over Campbell Creek; S4, T56N, R22W  
0.6 mile west of Bedford; Livingston County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 64)

**CONDITION**

fair

**OWNER**

Livingston County

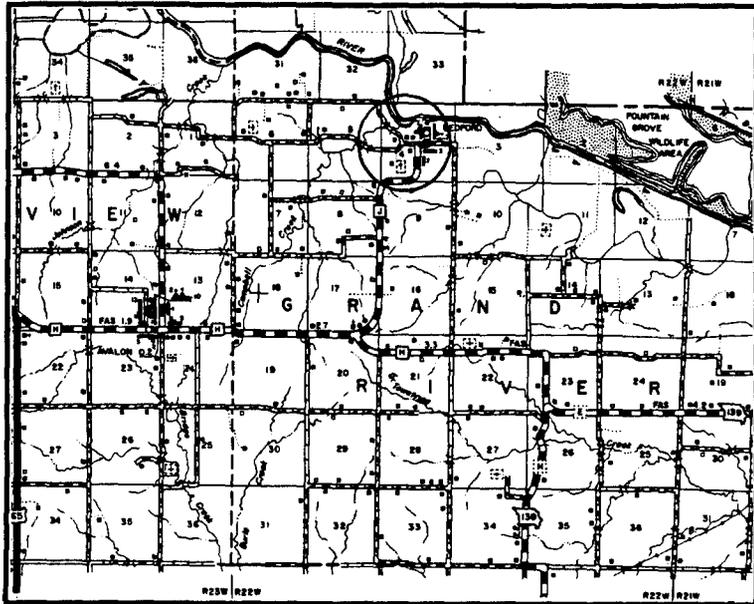
span number:	1	superstructure:	steel, 3-panel, rigid-connected polygonal Warren bedstead truss
span length:	50.0'	substructure:	steel pile bents pier and abutments with timber wingwalls
total length:	78.0'	floor/decking:	timber deck
roadway wdt.:	14.0'	other features:	no guardrails

Located just west of Bedford, this small-scale steel truss carries an unpaved county road over Campbell Creek. The Campbell Creek Bridge is comprised of a single rigid-connected Warren pony truss, supported by steel pile bents. The bridge dates to 1906. That year the Livingston County Court contracted with bridge builder W.O. Graham of Chillicothe to supply and erect several small-scale steel structures, including this truss in Grand River Township. By early the next year, all were reported complete by the county and opened to wagon traffic. Since its completion, the Campbell Creek Bridge has continued to function in place, with only maintenance-related repairs.

Patented in 1848 by Captain James Warren and Theobald Monzani, the Warren truss in its classic form features a web configuration that relies on simple triangulation for its rigidity. Warrens were built sparingly in the 19th century, a period in which the pin-connected Pratt dominated the bridge industry. After the turn of the century, however, rigid-connected Warren pony trusses began to supersede earlier pinned Pratt configurations for use on short- to intermediate-span highway bridges. Almost all of these early spans employed straight upper chords. The Campbell Creek Bridge in Livingston County, however, featured sloped upper chords, marking it as a uniquely configured example of what would later become a mainstay structural type in the state.

**NAME(S) OF STRUCTURE**  
Campbell Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**  
Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 465002.4; Livingston County Court Record, Book O: page 433 (13 September 1906) -located at the Livingston County Courthouse, Chillicothe MO; field inspection by Clayton Fraser, 10 December 1990.

**INVENTORIED BY**  
Clayton B. Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
10 May 1991

# MACON COUNTY

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INCLUDED: [Significant feature(s) of bridge given in boldface]  
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
*MACO01	029002.2	Cherry Ford Bridge	(replaced)
*MACO02	031000.8	Bridge	1- 40' c1900 pinned Pratt bedstead
MACO03	033000.1	Buck Creek Bridge	(replaced)
MACO04	044003.2	Bridge	(replaced)
MACO05	048002.4	Richland Creek Bridge	1- 40' c1900 pinned Pratt bedstead
MACO06	049000.8	Richland Creek Bridge	(replaced)
MACO07	054000.6	Middle Fork Bridge	1- 32' 1897 pinned Pratt bedstead Scoven and Haynes
MACO08	063000.9	Richland Creek Bridge	(replaced)
MACO09	067001.6	East Fork Bridge	1- 40' 1896 pinned Pratt bedstead Farnsworth and Blodgett
MACO10	067003.0	Long Branch Bridge	(replaced)
*MACO11	089001.7	Bear Creek Bridge	(replaced)
MACO12	104000.8	Bear Creek Bridge	(replaced)
MACO13	111000.2	Bear Creek Bridge	1- 50' c1910 pinned Pratt pony truss
MACO14	118000.8	Bear Creek Bridge	(replaced)
MACO15	120001.1	Bear Creek Bridge	(replaced)
MACO16	139001.9	Ely Branch Bridge	1- 40' 1902 pinned Pratt bedstead A.M. Blodgett, Kansas City
MACO17	142002.2	Middle Fork Bridge	1- 40' 1897 pinned Pratt bedstead Farnsworth and Blodgett
*MACO18	152002.2	Bridge	(replaced)
MACO19	159001.2	Middle Fork Bridge	(replaced)
MACO20	190A00.4	Ten Mile Creek Bridge	(replaced)
MACO21	297000.7	Sweezer Creek Bridge	(replaced)
MACO22	300002.0	Evans Bridge	(replaced)
*MACO23	321001.2	Chariton River Bridge	1- 50' 1917 riveted Warren pony truss Illinois Steel Bridge Company
MACO24	323000.6	Bridge	(replaced)
*MACO25	349001.2	Mussel Fork Bridge	1- 60' c1910 pinned Pratt pony truss
*MACO26	358000.5	Santa Fe RR Overpass	1-150' c1920 pinned Pratt through truss
*MACO27	382000.6	Mussel Fork Bridge	1- 70' 1897 pinned Pratt pony truss Farnsworth and Blodgett
*MACO28	384000.4	Hair's Ford Bridge	1- 80' c1880 bowstring pony arch-truss re-erected 1892 by King Br. Co
*MACO29	388001.4	Little Brush Creek Bridge	1- 36' 1905 riveted Pratt bedstead Dildine Bridge Co. Cameron, MO
*MACO30	403001.6	Bridge	1- 40 c1905 pinned Pratt bedstead

# MACON COUNTY

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## INCLUDED (cont.):

MACO31	405001.1	Bridge					(replaced)
MACO32	406000.9	Puzzle Creek Bridge	1-	30'	1904		pinned Pratt bedstead A.M. Blodgett (replaced)
*MACO33	409001.2	Hammock's Mill Bridge	1-	32'	1890		pinned Pratt bedstead Farnsworth & Blodgett (prob.)
*MACO34	454000.6	Bridge	1-	50'	c1910		pinned Pratt pony truss
*MACO35	462000.8	Middle Fork Bridge	1-	40'	c1900		pinned Pratt bedstead
*MACO36	464001.2	Bridge					(replaced)
*MACO37	480000.9	Bridge					(replaced)
MACO38	513001.6	Middle Fork Bridge	1-	70'	1907		pinned Pratt pony truss A.M. Blodgett (prob.)
*MACO39	517000.2	Middle Fork Bridge	1-	60'	c1895		pinned Pratt half-hip pony truss Farnsworth and Blodgett (prob.)
*MACO40	519000.4	Middle Fork Bridge	1-	60'	c1900		pinned Pratt bedstead
MACO41	526000.5	Middle Fork Bridge					(replaced)
MACO42	527001.2	Middle Fork Bridge					(replaced)
MACO43	533000.1	Bridge					(replaced)
MACO44	540001.3	Bridge					(replaced)
*MACO45	587000.7	East Fork Bridge					(replaced)
MACO46	606000.7	Middle Fork Bridge	1-	50'	1904		pinned Pratt bedstead A.M. Blodgett

## EXCLUDED:

### Pratt pony truss

009001.2 302001.2

### Warren pony truss

012001.1 027000.8 040000.5 226000.5 226000.7 273000.4 273003.5  
367001.5 387001.5 541000.5

### Lattice bedstead

146003.1 305000.4 307000.8 383001.4

### Steel stringer

G 750R	G 752R	G 754R	G 755R	S 84	S 338	
S 984	S 993	S 996	T 85	T 989A	W 401	X 104
X 122	003002.6	005000.2	009000.2	015001.6	054001.3	112000.4
143002.2	152001.4	163000.9	188001.2	194001.2	196000.7	198000.4
214000.8	225002.2	228002.7	232001.0	233000.9	234001.1	261002.2
277001.8	278000.8	280000.3	288R00.5	292001.7	296001.6	301000.7
323001.9	323004.7	326000.6	331001.4	336000.6	337000.5	344001.7
348000.3	352000.2	354000.5	361001.1	387001.9	403000.3	404000.8
408000.3	503002.3	503003.8	510000.9	531000.6	553000.3	559001.2
603000.6						

# MACON COUNTY

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## EXCLUDED (cont.):

### Steel girder

017001.6	020000.2	211000.6	216001.5	295001.2	373001.3	383000.7
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### Concrete slab / girder

K 516	K 674	036000.1	423R00.0	621000.3	621000.7	621000.8
627000.4						

### Concrete box culvert

G 753R	G 760R1	G 764R	S 312	S 313	S 390	T 380
T 764	T 988	X 721	X 786	X 935	X 936	X 940
X 942	X 949	629000.4				

### Timber stringer

W 404	477000.6	567002.0
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## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	0	21	0	0	21
Excluded	34	82	0	0	116
<hr/>					
	34	103	0	0	137 structures

# Bridge

MAC002

## GENERAL DATA

<b>structure no.:</b>	031000.8	<b>city/town:</b>	1.3 miles northeast of Elmer
<b>county:</b>	Macon	<b>feature inters.:</b>	unnamed stream
		<b>cadastral grid:</b>	S36, T60N, R16W
		<b>highway route:</b>	County Road 31
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Macon County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 3-panel, pin-connected Pratt bedstead, with two-span steel stringer approach spans on the east end		
<b>substructure:</b>	steel pile bent piers and abutments; timber backwalls		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	40.0'	<b>alterations:</b>	truss legs have been cut off and replaced with steel pile bents
<b>total length:</b>	76.0'	<b>floor/decking :</b>	timber deck over steel stringers
<b>roadway width:</b>	13.8'	<b>other features:</b>	upper chord and upright end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates (outer panels), and 2 punched rectangular eyebars (inner panel); vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: two channels

## HISTORICAL DATA

<b>erection date:</b>	c1900
<b>erection cost:</b>	unknown
<b>designer:</b>	unknown
<b>fabricator :</b>	unknown
<b>contractor:</b>	unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 031000.8; field inspection by Richard Collier, 11 June 1991.

<b>sign. rating:</b>	26
<b>evaluation:</b>	NRHP non-eligible (typical example of relatively common structural type)

**inventoried by:** Michelle Crow-Dolby 3 July 1993

# Richland Creek Bridge

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MAC005

## GENERAL DATA

structure no.:	048002.4	city/town:	4.6 miles southwest of La Plata
county:	Macon	feature inters.:	Richland Creek
		cadastral grid:	S21/22, T60N, R15W
		highway route:	County Road 48
		highway distr.:	2
		current owner:	Macon County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt bedstead, with steel stringer approach span

substructure: steel pile bent pier and abutments with timber backwalls

span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	51.0'	floor/decking :	timber deck
roadway width:	14.5'	other features:	unknown

## HISTORICAL DATA

erection date: c1900  
erection cost: unknown  
designer: unknown  
fabricator : unknown  
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 048002.4.

sign. rating: 26  
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 3 July 1993

# Middle Fork Bridge

MAC007

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## GENERAL DATA

structure no.:	054000.6	city/town:	3.6 miles east of Elmer
county:	Macon	feature inters.:	Middle Fork of the Chariton River
		cadastral grid:	S4, T59N, R15W
		highway route:	County Road 54
		highway distr.:	2
		current owner:	Macon County

## STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt bedstead  
substructure: steel pile bent abutments with timber backwalls

span number:	1	condition:	fair
span length:	32.0'	alterations:	unknown
total length:	32.0'	floor/decking :	timber deck
roadway width:	13.8'	other features:	no guardrails

## HISTORICAL DATA

erection date: 1897  
erection cost: \$350.00  
designer: unknown  
fabricator : unknown  
contractor: Scoven and Haynes

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 054000.6; Macon County Courthouse, Record N: page 133 (8 September 1897), page 215 (8 February 1898) - located at the Macon County Courthouse, Macon MO.

sign. rating: 39  
evaluation: NRHP non-eligible (typical, small-scale example of relatively common structural type)

inventoried by: Michelle Crow-Dolby 3 July 1993

# East Fork Bridge

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MAC009

## GENERAL DATA

structure no.:	067001.6	city/town:	5.2 miles southwest of La Plata
county:	Macon	feature inters.:	East Fork of the Chariton River
		cadastral grid:	S1, T59N, R15W
		highway route:	County Road 67
		highway distr.:	2
		current owner:	Macon County

## STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt bedstead, with steel stringer approach spans

substructure: steel truss-leg piers and abutments with timber backwalls

span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	66.0'	floor/decking :	timber deck
roadway width:	13.8'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: 1896

erection cost: \$415.00

designer: Farnsworth and Blodgett, Kansas City MO

fabricator : unknown

contractor: Farnsworth and Blodgett, Kansas City MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 067001.6; Macon County Courthouse, Record M: page 342 (6 April 1896) - located at Macon County Courthouse, Macon MO.

sign. rating: 48

evaluation: NRHP possibly eligible (typically configured, relatively early example of mainstay structural type)

inventoried by: Michelle Crow-Dolby 3 July 1993

# Bear Creek Bridge

MACO13

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## GENERAL DATA

<b>structure no.:</b> 111000.2	<b>city/town:</b> 5.4 miles southeast of La Plata
<b>county:</b> Macon	<b>feature inters.:</b> Bear Creek
	<b>cadastral grid:</b> S18/19, T60N, R13W
	<b>highway route:</b> County Road 111
	<b>highway distr.:</b> 2
	<b>current owner:</b> Macon County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 50.0'	<b>alterations:</b> unknown
<b>total length:</b> 80.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.0'	<b>other features:</b> steel angle guardrails

## HISTORICAL DATA

<b>erection date:</b> c1910	
<b>erection cost:</b> unknown	
<b>designer:</b> unknown	
<b>fabricator :</b> unknown	
<b>contractor:</b> unknown	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 111000.2.	
<b>sign. rating:</b> 26	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)	

**inventoried by:** Michelle Crow-Dolby    3 July 1993

# Ely Branch Bridge

MAC016

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## GENERAL DATA

structure no.:	139001.9	city/town:	7.8 miles southeast of La Plata
county:	Macon	feature inters.:	Ely Branch
		cadastral grid:	S6/7, T59N, R13W
		highway route:	County Road 139
		highway distr.:	2
		current owner:	Macon County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt bedstead		
substructure:	steel pile bent abutments with timber backwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	40.0'	floor/decking :	timber deck
roadway width:	14.0'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1902
erection cost:	\$340.00
designer:	A.M. Blodgett, Kansas City MO
fabricator :	unknown
contractor :	A.M. Blodgett, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 139001.9; Macon County Courthouse, Record P: pages 113-14 (13 June 1902), page 243 (11 November 1902) - located at the Macon County Courthouse, Macon MO.
sign. rating:	36
evaluation:	NRHP non-eligible (typical, small-scale example of common structural type)

Inventoried by: Michelle Crow-Dolby 3 July 1993

# Middle Fork Bridge

MACO17

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## GENERAL DATA

<b>structure no.:</b> 142002.2	<b>city/town:</b> 5.6 miles southeast of La Plata
<b>county:</b> Macon	<b>feature inters.:</b> Middle Fork of Salt River
	<b>cadastral grid:</b> S2, T59N, R14W
	<b>highway route:</b> County Road 142
	<b>highway distr.:</b> 2
	<b>current owner:</b> Macon County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt bedstead	
<b>substructure:</b> steel truss-leg abutments with timber backwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 40.0'	<b>alterations:</b> none
<b>total length:</b> 40.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 12.5'	<b>other features:</b> steel angle guardrails

## HISTORICAL DATA

<b>erection date:</b> 1897
<b>erection cost:</b> \$450.00
<b>designer:</b> Farnsworth and Blodgett, Kansas City MO
<b>fabricator :</b> unknown
<b>contractor:</b> Farnsworth and Blodgett, Kansas City MO
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 142002.2; Macon County Courthouse, Record N: page 133 (8 September 1897) - located at the Macon County Courthouse, Macon MO.
<b>sign. rating:</b> 48
<b>evaluation:</b> NRHP possibly eligible (relatively early example of mainstay structural type)

Inventoried by: Michelle Crow-Dolby 3 July 1993

# Chariton River Bridge

MACO23

## GENERAL DATA

structure no.:	321001.2	city/town:	5.6 miles northeast of New Cambria
county:	Macon	feature inters.:	Chariton River
		cadastral grid:	S15, T58N, R16W
		highway route:	County Road 321
		highway distr.:	2
		current owner:	Macon County

## STRUCTURAL DATA

**superstructure:** steel, 3-panel, rigid-connected Warren pony truss, with flanking steel stringer approach spans

**substructure:** concrete-filled steel tubular piers with concrete backwalls

span number:	1	condition:	fair
span length:	50.0'	alterations:	non-original steel tubes between main span and approaches
total length:	77.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.0'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plates; vertical: 4 angles with batten plates; diagonal: 2 angles with batten plates; lateral bracing: round rod with threaded ends; floor beam: I-beam; guard-rail: 2 channels

## HISTORICAL DATA

**erection date:** 1917

**erection cost:** unknown

**designer:** Illinois Steel Bridge Company, Jacksonville IL

**fabricator :** Illinois Steel Bridge Company, Jacksonville IL;  
Lackawanna Steel Company, Pittsburgh PA

**contractor :** Illinois Steel Bridge Company, Jacksonville IL

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 321001.2; Macon County Courthouse, Record V: page 359 (10 May 1917), page 363 (6 June 1917) - located at the Macon County Courthouse, Macon MO; field inspection by Richard Collier, 11 June 1991.

**sign. rating:** 35

**evaluation:** NRHP non-eligible (undistinguished example of common structural type)

inventoried by: Michelle Crow-Dolby 3 July 1993

# Mussel Fork Bridge

MACO25

## GENERAL DATA

<b>structure no.:</b> 349001.2	<b>city/town:</b> 5.0 miles northwest of Ethel
<b>county:</b> Macon	<b>feature inters.:</b> Mussel Fork
	<b>cadastral grid:</b> S17/20, T59N, R17W
	<b>highway route:</b> County Road 349
	<b>highway distr.:</b> 2
	<b>current owner:</b> Macon County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
<b>substructure:</b> steel pile bent abutment with timber backwall at west end, concrete-filled steel cylinder piers elsewhere	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 60.0'	<b>alterations:</b> none
<b>total length:</b> 86.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 13.0'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turn-buckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field bolted to vertical; guardrail: 2 channels

## HISTORICAL DATA

**erection date:** c1910  
**erection cost:** unknown  
**designer:** unknown  
**fabricator :** unknown  
**contractor:** unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 349001.2; field inspection by Richard Collier, 11 June 1991.

**sign. rating:** 28  
**evaluation:** NRHP non-eligible (typical example of common structural type)

**inventoried by:** Michelle Crow-Dolby 3 July 1993

# Santa Fe Railroad Overpass

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MACO26

## GENERAL DATA

<b>structure no.:</b> 358000.5	<b>city/town:</b> 1.6 miles southwest of Ethel
<b>county:</b> Macon	<b>feature inters.:</b> Atchison, Topeka and Santa Fe Railroad
	<b>cadastral grid:</b> S35, T59N, R17W
	<b>highway route:</b> County Road 358
	<b>highway distr.:</b> 2
	<b>current owner:</b> Macon County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 9-panel, pin-connected Pratt through truss, with multiple timber stringer approach spans	
<b>substructure:</b> heavy timber pile abutments and piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 150.0'	<b>alterations:</b> truss moved
<b>total length:</b> 237.0'	<b>floor/decking :</b> timber deck over timber stringers
<b>roadway width:</b> 13.0'	<b>other features:</b> upper chord: 2 channels with cover plate and lacing; inclined end post: 2 channels with 2 cover plate and angle lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with cover plate and lacing (2 channels with batten plates at the hip); diagonal: 2 punched rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round eyerod with sleeve bolt; strut: 1 channel with angle knee braces; floor beam: built up I-beam; guardrail: timber

## HISTORICAL DATA

<b>erection date:</b> c1890; moved c1920
<b>erection cost:</b> unknown
<b>designer:</b> unknown
<b>fabricator :</b> unknown
<b>contractor:</b> unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 358000.5; field inspection by Richard Collier, 11 June 1991.

<b>sign. rating:</b> 21
<b>evaluation:</b> NRHP non-eligible (typically configured example of mainstay structural type, largely undocumented)

**inventoried by:** Michelle Crow-Dolby 3 July 1993

# Mussel Fork Bridge

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MACO27

## GENERAL DATA

<b>structure no.:</b> 382000.6	<b>city/town:</b> 5.7 miles northwest of New Cambria
<b>county:</b> Macon	<b>feature inters.:</b> Mussel Fork
	<b>cadastral grid:</b> S19/30, T58N, R17W
	<b>highway route:</b> County Road 382
	<b>highway distr.:</b> 2
	<b>current owner:</b> Macon County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach span on the east end	
<b>substructure:</b> steel pile bent abutments with timber backwalls; concrete-filled steel cylinder piers, and non-original steel tubes	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 70.0'	<b>alterations:</b> none
<b>total length:</b> 111.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 14.0'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: 2 channels; builder's plate: Farnsworth and Blodgett / 1897 / Kansas City MO

## HISTORICAL DATA

<b>erection date:</b> 1897	
<b>erection cost:</b> \$1200.00 (contract amount)	
<b>designer:</b> Farnsworth and Blodgett, Kansas City MO	
<b>fabricator :</b> Jones and Laughlin Steel Company, Pittsburgh PA	
<b>contractor :</b> Farnsworth and Blodgett, Kansas City MO	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 382000.6; Macon County Court Record N: page 133 (8 September 1897) - located at the Macon County Courthouse, Macon MO; field inspection by Richard Collier, 11 June 1991.	
<b>sign. rating:</b> 50	
<b>evaluation:</b> NRHP possibly eligible (typically configured example of mainstay structural type)	

**inventoried by:** Michelle Crow-Dolby    3 July 1993

# Hair's Ford Bridge

MACO28

## GENERAL DATA

<b>structure no.:</b>	384000.4	<b>city/town:</b>	5.3 miles northwest of New Cambria
<b>county:</b>	Macon	<b>feature inters.:</b>	Mussel Fork
		<b>cadastral grid:</b>	S31, T58N, R17W
		<b>highway route:</b>	County Road 384
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Macon County

## STRUCTURAL DATA

<b>superstructure:</b>	wrought iron, bowstring pony arch-truss, with one steel stringer approach span on the west end		
<b>substructure:</b>	concrete-filled iron cylinder piers with timber backwalls		
<b>span number:</b>	1	<b>condition:</b>	poor
<b>span length:</b>	80.0'	<b>alterations:</b>	moved and re-erected, 1892; stringers and floor beams replaced
<b>total length:</b>	106.0'	<b>floor/decking :</b>	timber deck (partially intact) over steel stringers
<b>roadway width:</b>	11.0'	<b>other features:</b>	arch rib: 2 channels with 2 cover plates; lower chord: flat iron bar; vertical: star iron with star iron outriders; diagonal: round rod with threaded ends; lateral bracing: round rod with threaded ends; floor beam: steel I-beam; guardrail: none on main span, steel angle on west approach

## HISTORICAL DATA

<b>erection date:</b>	c1880; moved 1892
<b>erection cost:</b>	\$700.00 (contract amount)
<b>designer:</b>	King Iron Bridge and Manufacturing Company, Cleveland OH
<b>fabricator :</b>	King Iron Bridge and Manufacturing Company, Cleveland OH
<b>contractor:</b>	George E. King Bridge Company, Des Moines IA
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 384000.4; Macon County Courthouse, Record K: page 334 (6 August 1891), page 456 (4 February 1892), page 542 (5 July 1892) - located at the Macon County Courthouse, Macon MO; field inspection by Richard Collier, 11 June 1991.
<b>sign. rating:</b>	74
<b>evaluation:</b>	NRHP eligible (one of the last remaining examples of mainstay structural type)

inventoried by: Michelle Crow-Dolby 3 July 1993

# Little Brush Creek Bridge

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MACO29

## GENERAL DATA

<b>structure no.:</b> 388001.4	<b>city/town:</b> 2.6 miles west of New Cambria
<b>county:</b> Macon	<b>feature inters.:</b> Little Brush Creek
	<b>cadastral grid:</b> S4/9, T57N, R17W
	<b>highway route:</b> County Road 388
	<b>highway distr.:</b> 2
	<b>current owner:</b> Macon County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 2-panel, rigid-connected Pratt truss-leg bedstead, with flanking steel stringer approach spans	
<b>substructure:</b> steel pile bent piers and abutments with timber backwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 36.0'	<b>alterations:</b> non-original round tubes added to substructure between truss legs
<b>total length:</b> 68.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> upper chord: I-beam; upright end post: 2 channels; lower chord: 2 channels; vertical: 1 channel; diagonal: 2 angles; lateral bracing: 1 angle; floor beam: 1 channel; guard-rail: 1 channel; small plate on center vertical: Pat Apr 19 1904; bolted plates at intersections between verticals and top chord

## HISTORICAL DATA

<b>erection date:</b> 1905	
<b>erection cost:</b> \$490.00 (county estimate)	
<b>designer:</b> Dildine Bridge Company, Cameron MO	
<b>fabricator :</b> Dildine Bridge Company, Cameron MO	
<b>contractor:</b> Dildine Bridge Company, Cameron MO	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 388001.4; Macon County Court Record Q: pages 423-24 (2 May 1905), located at the Macon County Courthouse, Macon MO; field inspection by Richard Collier, 11 June 1991.
<b>sign. rating:</b> 41	
<b>evaluation:</b>	NRHP non-eligible (undistinguished example of relatively common truss type)

inventoried by: Michelle Crow-Dolby 3 July 1993

# Bridge

MACO30

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## GENERAL DATA

structure no.: 403001.6      city/town: 3.6 miles southwest of New Cambria  
county: Macon      feature inters.: unnamed branch  
cadastral grid: S26/27, T57N, R17W  
highway route: County Road 403  
highway distr.: 2  
current owner: Macon County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead  
substructure: truss legs with timber backwalls

span number: 1      condition: fair  
span length: 40.0'      alterations: none  
total length: 40.0'      floor/decking : timber deck over steel railroad rail stringers  
roadway width: 14.0'      other features: upper chord and upright end post: 2 channels  
with cover plate and lacing; lower chord: 2  
angles with lacing (outer panels), 2 looped  
rectangular eyebars (inner panel); vertical: 4  
angles with lacing; diagonal: 2 looped rectan-  
gular eyebars; counter: round eyerod with  
turnbuckle; lateral bracing: round bar with  
threaded ends; floor beam: I-beam, U-bolted  
to vertical; guardrail: none

## HISTORICAL DATA

erection date: c1905  
erection cost: unknown  
designer: unknown  
fabricator : unknown  
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory  
and Appraisal: Structure No. 403001.6; field inspection by Richard  
Collier, 11 June 1991.

sign. rating: 23  
evaluation: NRHP non-eligible (typical, short-span example of relatively common  
structural type)

inventoried by: Michelle Crow-Dolby      3 July 1993

# Puzzle Creek Bridge

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MACO32

## GENERAL DATA

<b>structure no.:</b>	406000.9	<b>city/town:</b>	3.4 miles south of New Cambria
<b>county:</b>	Macon	<b>feature inters.:</b>	Puzzle Creek
		<b>cadastral grid:</b>	S25, T57N, R17W
		<b>highway route:</b>	County Road 406
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Macon County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 2-panel, pin-connected Pratt truss leg bedstead		
<b>substructure:</b>	unknown		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	30.0'	<b>alterations:</b>	none
<b>total length:</b>	61.0'	<b>floor/decking :</b>	timber deck
<b>roadway width:</b>	12.0'	<b>other features:</b>	steel angle guardrails

## HISTORICAL DATA

<b>erection date:</b>	1904
<b>erection cost:</b>	\$360.00
<b>designer:</b>	A.M. Blodgett, Kansas City MO
<b>fabricator :</b>	unknown
<b>contractor :</b>	A.M. Blodgett, Kansas City MO
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 406000.9; Macon County Courthouse, Record Q: page 278 (5 October 1904); Record R: page 429 (7 August 1907) - located at the Macon County Courthouse, Macon MO.
<b>sign. rating:</b>	41
<b>evaluation:</b>	NRHP non-eligible (typical example of relatively common structural type)

inventoried by: Michelle Crow-Dolby 3 July 1993

# Bridge

MACO34

## GENERAL DATA

<b>structure no.:</b> 454000.6	<b>city/town:</b> 3.2 miles south of Bevier
<b>county:</b> Macon	<b>feature inters.:</b> unnamed branch
	<b>cadastral grid:</b> S33/34, T57N, R15W
	<b>highway route:</b> County Road 454
	<b>highway distr.:</b> 2
	<b>current owner:</b> Macon County

## STRUCTURAL DATA

**superstructure:** steel, 2-panel, pin-connected truss-leg bedstead, with steel stringer approach span on the south end

**substructure:** steel pile bent abutments with timber backwalls; truss legs; steel tubes welded to truss legs on south end for added support

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 32.0'	<b>alterations:</b> none
<b>total length:</b> 55.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> upper chord: 2 channels with cover plate and lacing; end post: 2 channels with lacing; lower chord: 2 angles with lacing; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: none; builder's plate: ....89.....

## HISTORICAL DATA

**erection date:** 1890

**erection cost:** unknown

**designer:** Farnsworth and Blodgett, Kansas City MO (probable)

**fabricator :** Jones and Laughlin Iron Company, Pittsburgh PA

**contractor:** Farnsworth and Blodgett, Kansas City MO (probable)

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 454000.6; field inspection by Richard Collier, 11 June 1991.

**sign. rating:** 50

**evaluation:** NRHP possibly eligible (earliest dateable example in state of mainstay structural type)

**inventoried by:** Michelle Crow-Dolby 3 July 1993

# Middle Fork Bridge

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MACO35

## GENERAL DATA

<b>structure no.:</b>	462000.8	<b>city/town:</b>	5.7 miles southwest of Bevier
<b>county:</b>	Macon	<b>feature inters.:</b>	Middle Fork of the Chariton River
		<b>cadastral grid:</b>	S8, T56N, R15W
		<b>highway route:</b>	County Road 462
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Macon County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach span on the west end		
<b>substructure:</b>	steel pile bent piers and abutments with timber backwalls		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	50.0'	<b>alterations:</b>	none
<b>total length:</b>	66.0'	<b>floor/decking :</b>	timber deck over steel stringers
<b>roadway width:</b>	13.0'	<b>other features:</b>	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; counter: round eyerod with turn-buckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field bolted; guardrail: none

## HISTORICAL DATA

<b>erection date:</b>	c1910
<b>erection cost:</b>	unknown
<b>designer:</b>	unknown
<b>fabricator :</b>	unknown
<b>contractor:</b>	unknown
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 462000.8; field inspection by Richard Collier, 11 June 1991.
<b>sign. rating:</b>	27
<b>evaluation:</b>	NRHP non-eligible (typically configured, inadequately documented example of common structural type)

Inventoried by: Michelle Crow-Dolby 3 July 1993

# Bridge

MACO36

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## GENERAL DATA

**structure no.:** 464001.2      **city/town:** 2.6 miles northwest of College Mound  
**county:** Macon      **feature inters.:** unnamed stream  
                                 **cadastral grid:** S17/20, T56N, R15W  
                                 **highway route:** County Road 464  
                                 **highway distr.:** 2  
                                 **current owner:** Macon County

## STRUCTURAL DATA

**superstructure:** steel, 3-panel, pin-connected Pratt truss-leg bedstead, with steel stringer approach span on the west end  
**substructure:** truss leg piers; a steel tube has been welded to one truss leg for added support

**span number:** 1      **condition:** fair  
**span length:** 40.0'      **alterations:** substructure braced  
**total length:** 56.0'      **floor/decking :** timber deck over steel stringers  
**roadway width:** 14.0'      **other features:** upper chord and upright end post: 2 channels with lacing; lower chord: 2 angles with batten plates (outer panels), 2 looped rectangular eyebars (inner panel); vertical: 4 angles with batten plates; diagonal: 2 looped rectangular eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam; guard-rail: 2 channels

## HISTORICAL DATA

**erection date:** c1900  
**erection cost:** unknown  
**designer:** unknown  
**fabricator :** Carnegie Steel Company, Pittsburgh PA  
**contractor:** unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 464001.2; field inspection by Richard Collier, 11 June 1991.

**sign. rating:** 28  
**evaluation:** NRHP non-eligible (typically configured, inadequately documented example of common structural type)

**inventoried by:** Michelle Crow-Dolby      3 July 1993

# Middle Fork Bridge

MACO39

## GENERAL DATA

<b>structure no.:</b> 517000.2	<b>city/town:</b> 10.2 miles southeast of Macon
<b>county:</b> Macon	<b>feature inters.:</b> Middle Fork of the Salt River
	<b>cadastral grid:</b> S27, T56N, R13W
	<b>highway route:</b> County Road 517
	<b>highway distr.:</b> 2
	<b>current owner:</b> Macon County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt pony truss, with 3 steel stringer approach spans on the north end and 2 steel stringer approach spans on the south end	
<b>substructure:</b> steel pile bent abutments with timber backwalls; concrete-filled steel cylinder piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 70.0'	<b>alterations:</b> none
<b>total length:</b> 159.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eye-bars; lateral bracing: round rod with threaded ends; floor beam: I-beam, field bolted below lower chord; guardrail: 2 channels

## HISTORICAL DATA

<b>erection date:</b> 1907	
<b>erection cost:</b> \$1628.00	
<b>designer:</b> A.M. Blodgett, Kansas City MO (probable)	
<b>fabricator :</b> unknown	
<b>contractor :</b> A.M. Blodgett, Kansas City MO (probable)	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 517000.2; Macon County Court Record R: page 242 (4 December 1906) - located at the Macon County Court-house, Macon MO; field inspection by Richard Collier, 11 June 1991.
<b>sign. rating:</b> 46	
<b>evaluation:</b>	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 3 July 1993

# Middle Fork Bridge

MACO40

## GENERAL DATA

structure no.:	519000.4	city/town:	9.8 miles southeast of Macon
county:	Macon	feature inters.:	Middle Fork of the Salt River
		cadastral grid:	S22/27, T56N, R13W
		highway route:	County Road 519
		highway distr.:	2
		current owner:	Macon County

## STRUCTURAL DATA

**superstructure:** steel, 4-panel, pin-connected, Pratt half-hip pony truss, with 2 steel stringer approach spans on the east end and 1 steel stringer approach span on the west end

**substructure:** steel pile bent abutments with timber backwalls; concrete-filled steel cylinder piers

span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	149.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.0'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: tapered "fishtail" plate girder; no guardrail

## HISTORICAL DATA

**erection date:** c1895

**erection cost:** unknown

**designer:** Farnsworth and Blodgett, Kansas City MO (probable)

**fabricator :** unknown

**contractor:** Farnsworth and Blodgett, Kansas City MO (probable)

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 519000.4; field inspection by Richard Collier, 11 June 1991.

**sign. rating:** 42

**evaluation:** NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Michelle Crow-Dolby 3 July 1993

# Middle Fork Bridge

MACO41

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## GENERAL DATA

structure no.:	526000.5	city/town:	8.0 miles southeast of Macon
county:	Macon	feature inters.:	Middle Fork of the Salt River
		cadastral grid:	S16, T56N, R13W
		highway route:	County Road 526
		highway distr.:	2
		current owner:	Macon County

## STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt bedstead, with steel stringer approach spans

substructure: steel pile bent piers and abutments with timber backwalls

span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	102.0'	floor/decking :	timber deck
roadway width:	13.0'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: c1900  
erection cost: unknown  
designer: unknown  
fabricator : unknown  
contractor : unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 526000.5.

sign. rating: 29  
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 3 July 1993

# Middle Fork Bridge

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MACO46

## GENERAL DATA

<b>structure no.:</b> 606000.7	<b>city/town:</b> 4.3 miles northeast of Macon
<b>county:</b> Macon	<b>feature inters.:</b> Middle Fork of the Salt River
	<b>cadastral grid:</b> S6, T57N, R13W
	<b>highway route:</b> County Road 606
	<b>highway distr.:</b> 2
	<b>current owner:</b> Macon County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach span	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 50.0'	<b>alterations:</b> unknown
<b>total length:</b> 81.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 10.0'	<b>other features:</b> steel angle guardrails

## HISTORICAL DATA

<b>erection date:</b> 1904	
<b>erection cost:</b> unknown	
<b>designer:</b> A.M. Blodgett, Kansas City MO	
<b>fabricator :</b> unknown	
<b>contractor:</b> A.M. Blodgett, Kansas City MO	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 606000.7; Macon County Courthouse, Record Q: page 278 (5 October 1904) - located at the Macon County Courthouse, Macon MO.	
<b>sign. rating:</b> 41	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 3 July 1993

# HAER INVENTORY

Missouri Historic Bridge Inventory

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**NAME(S) OF STRUCTURE**

East Fork Bridge  
MHTD: 067001.6

MAC009

**DATE(S) OF CONSTRUCTION**

1896

**LOCATION**

County Road 67 over East Fork of the Chariton River; S1, T59N, R15W  
5.2 miles southwest of La Plata; Macon County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 48)

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**CONDITION**

fair

**OWNER**

Macon County

span number:	1	superstructure:	steel, 2-panel, pin-connected Pratt bedstead, with steel stringer approach spans
span length:	40.0'	substructure:	steel truss-leg piers and abutments with timber backwalls
total length:	66.0'	floor/decking:	timber deck
roadway wdt.:	13.8'	other features:	steel angle guardrails

This short-span truss crosses the East Fork of the Chariton River some five miles southwest of La Plata. The structure consists of a single Pratt bedstead, supported by truss-leg piers and approached on one end by a stringer span. With pinned connections and a timber deck, the bridge is made up of steel components rolled in the Jones and Laughlin shops in Pittsburgh. It was erected here in 1896 by Farnsworth and Blodgett of Kansas City for \$415.00. Since that time the structure has carried vehicular traffic in relatively well-preserved condition.

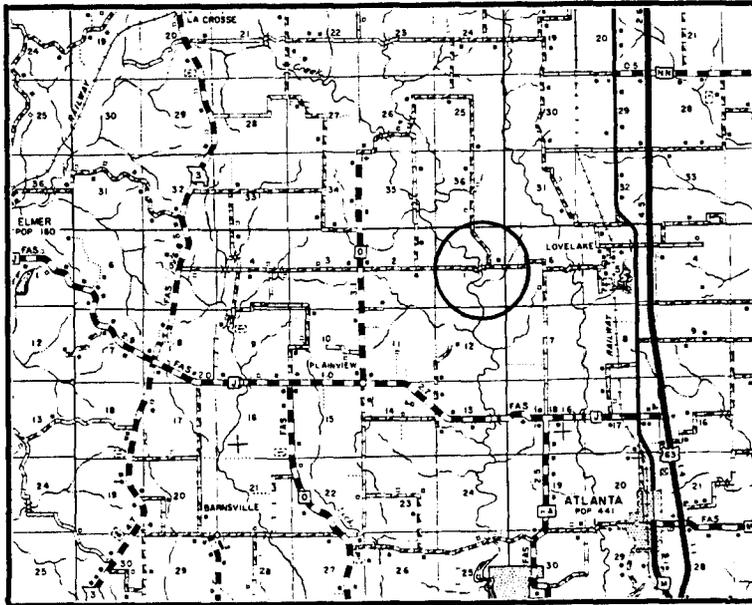
In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the late 1890s and early 1900s. Hundreds remain in place today; in fact, Missouri has probably more bedsteads than any other state. This bridge is distinguished as the earliest example remaining in Missouri of this mainstay structural type: a well-preserved, small-scale example of a prevailing statewide construction trend.

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**NAME(S) OF STRUCTURE**

East Fork Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 067001.6; Macon County Courthouse, Record M: page 342 (6 April 1896) - located at Macon County Courthouse, Macon MO.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

3 July 1993

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# HAER INVENTORY

Missouri Historic Bridge Inventory

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**NAME(S) OF STRUCTURE**

Middle Fork Bridge  
MHTD: 142002.2

MACO17

**DATE(S) OF CONSTRUCTION**

1897

**LOCATION**

County Road 142 over Middle Fork of Salt River; S2, T59N, R14W  
5.6 miles southeast of La Plata; Macon County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 48)

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**CONDITION**

fair

**OWNER**

Macon County

span number: 1

superstructure: steel, 3-panel, pin-connected Pratt bedstead

span length: 40.0'

substructure: steel truss-leg abutments with timber backwalls

total length: 40.0'

floor/decking: timber deck

roadway wdt.: 12.5'

other features: steel angle guardrails

This short-span truss crosses the Middle Fork of the Salt River some 5½ miles southeast of La Plata. The structure consists of a single Pratt bedstead, supported by truss-leg piers. With pinned connections and a timber deck, the bridge was erected here in 1897 by Farnsworth and Blodgett of Kansas City for \$450.00. Since that time the structure has carried vehicular traffic in relatively well-preserved condition.

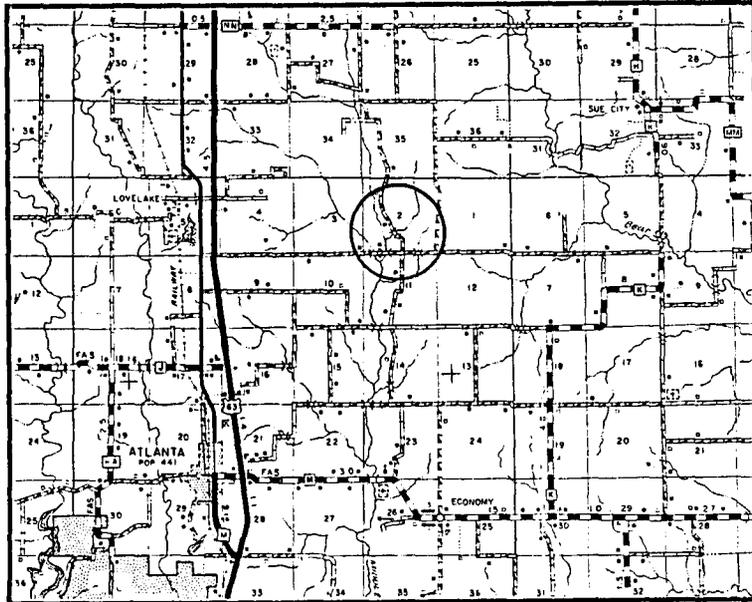
In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the late 1890s and early 1900s. Hundreds remain in place today; in fact, Missouri has probably more bedsteads than any other state. This bridge is distinguished as one of the earliest example remaining in Missouri of this mainstay structural type: a well-preserved, small-scale example of a prevailing statewide construction trend.

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**NAME(S) OF STRUCTURE**

Middle Fork Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 142002.2; Macon County Courthouse, Record N: page 133 (8 September 1897) - located at the Macon County Courthouse, Macon MO.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

3 July 1993

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Mussel Fork Bridge  
MHTD: 382000.6

MACO27

**DATE(S) OF CONSTRUCTION**

1897

**LOCATION**

County Road 382 over Mussel Fork; S19/30, T58N, R17W  
5.7 miles northwest of New Cambria; Macon County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 50)

**CONDITION**

fair

**OWNER**

Macon County

span number: 1  
span length: 70.0'  
total length: 111.0'  
roadway wdt.: 14.0'

superstructure: steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach span on the east end  
substructure: steel pile bent abutments with timber backwalls; concrete-filled steel cylinder piers, and non-original steel tubes  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: 2 channels; builder's plate: Farnsworth and Blodgett / 1897 / Kansas City MO

Linking the towns of Hart and Bucklin, between Linn and Macon Counties, the Mussel Fork Bridge was constructed in 1897. On September 8th of that year, the Macon County Court awarded Kansas City based builders Farnsworth and Blodgett a \$1200.00 contract to build this pin-connected Pratt pony truss. One of numerous such crossings built over Mussel Fork around the turn of the century, the bridge continues to carry local traffic in this rural setting.

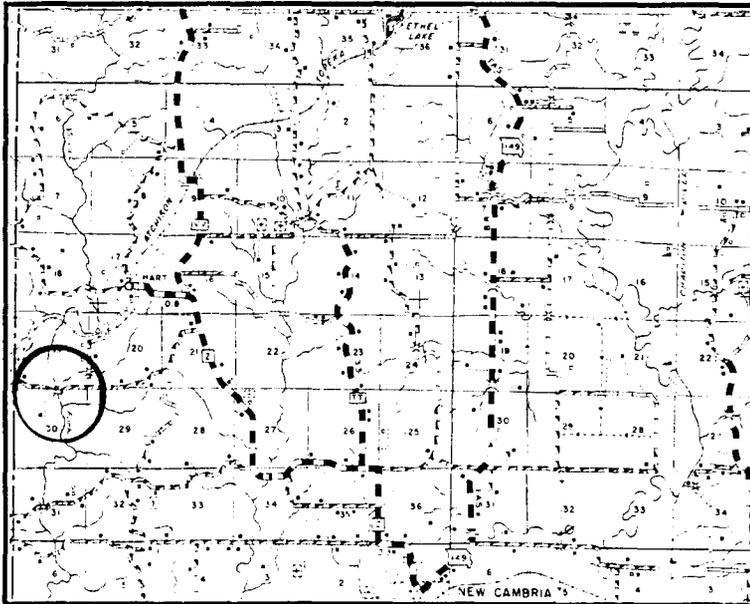
With industry giants such as the King Bridge Company and the Wrought Iron Bridge Company marketing it extensively, the Pratt truss was the bridge of choice among Missouri's counties for medium-span crossings in the late 19th century. Thousands of pin-connected Pratts were erected throughout the state during this period of intense bridge construction, and hundreds remain in place today. The Mussel Fork Bridge is distinguished among these for its relatively early erection date and its well-preserved condition.

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**NAME(S) OF STRUCTURE**

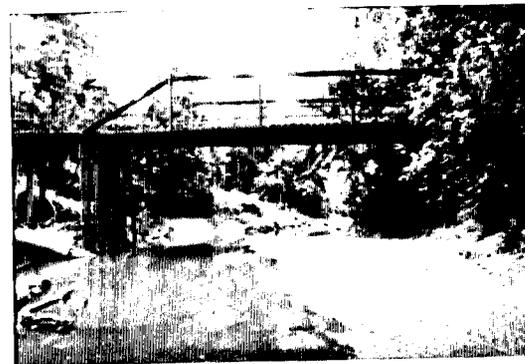
Mussel Fork Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP



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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 382000.6; Macon County Court Record N: page 133 (8 September 1897) - located at the Macon County Courthouse, Macon MO; field inspection by Richard Collier, 11 June 1991.

**INVENTORIED BY**  
Michelle Crow-Dolby

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
3 July 1993

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Hair's Ford Bridge  
MHTD: 384000.4

MACO28

**DATE(S) OF CONSTRUCTION**

c1880; moved 1892

**LOCATION**

County Road 384 over Mussel Fork; S31, T58N, R17W  
5.3 miles northwest of New Cambria; Macon County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / closed

**RATING** NRHP eligible (score: 74)

**CONDITION**

poor

**OWNER**

Macon County

span number: 1  
span length: 80.0'  
total length: 106.0'  
roadway wdt.: 11.0'

superstructure: wrought iron, bowstring pony arch-truss, with one steel stringer approach span on the west end  
substructure: concrete-filled iron cylinder piers with timber backwalls  
floor/decking: timber deck (partially intact) over steel stringers  
other features: arch rib: 2 channels with 2 cover plates; lower chord: flat iron bar; vertical: star iron with star iron outriders; diagonal: round rod with threaded ends; lateral bracing: round rod with threaded ends; floor beam: steel I-beam; guardrail: none on main span, steel angle on west approach

Known locally as the Hair's Ford Bridge, this long-span bowstring crosses the Mussel Fork northwest of New Cambria. The structure is comprised of a single wrought iron pony arch-truss, supported by iron tubular piers and approached on one side by a stringer span. The initial erection date for this span is not known, but physical characteristics of the truss indicate that it was fabricated around 1880 using the patented tubular arch design of the King Iron Bridge and Manufacturing Company. The Cleveland giant was active throughout Missouri during the period and was, in fact, responsible for the bowstring's re-erection in 1892. On August 6, 1891, the Macon County Court ordered that the bridge over Mussel Fork on the Old State Road in Russell Township, be moved 3/8 mile and put up on iron piers. John E. Wright, county road and bridge commissioner, reported that "I have made a strict examination of bridge across Muscle[sic] Fork on public road leading east from Bucklin...." The bridge should be "moved downstream and rebuilt according to contract", Wright concluded. On February 5, 1892, the contract for the structure's re-erection was let to the George E. King Bridge Company of Des Moines. George King was Zenas King's nephew, and the Iowa firm was actually a subsidiary of the Cleveland firm. King completed the work early that summer and in July was paid the remaining \$100.00 due on a \$700.00 contract. Now located on a vacated county road, the Hair's Ford Bridge is now permanently closed to vehicular traffic. Having been allowed to molder, much of the bridge's deck is missing, and the web on the north side is leaning inward.

The bowstring arch-truss was the iron span of choice for Missouri counties in the late 1860 and 1870s. Marketed extensively throughout the Midwest by such industry giants as the King Iron Bridge Manufacturing Company and the Wrought Iron Bridge Company, these often-patented bridges featured a wide range of span lengths and tube or box arch configurations. The proliferation of the bowstring corresponded with the initial development of Missouri's road system; as a result, perhaps thousands of these prototypical iron spans were

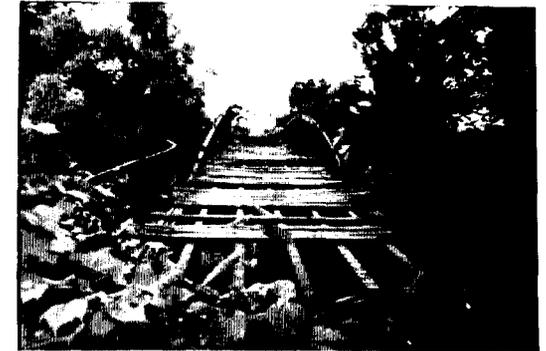
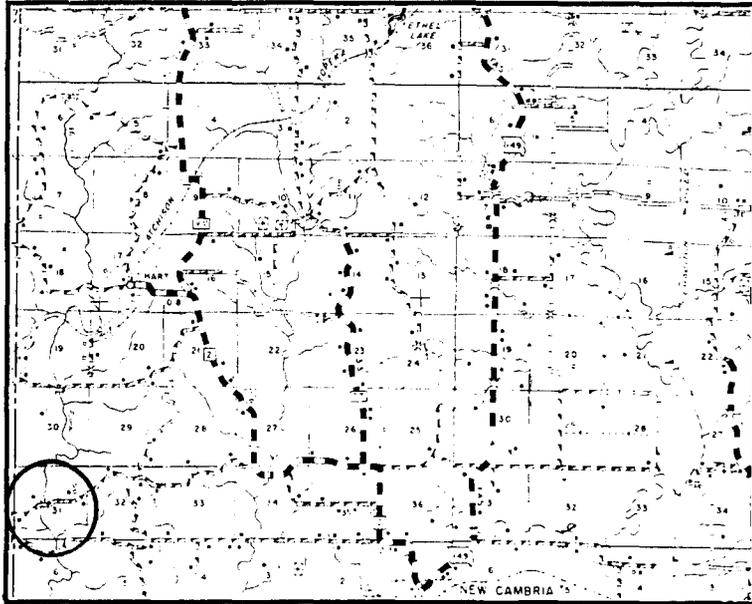
erected throughout the state. The bowstring had some rather severe structural flaws, however, relating primarily to lateral stability of the arches, and it was largely superseded by the pin-connected truss in the early 1880s. Through subsequent attrition, almost all of Missouri's bowstrings have since been demolished and replaced. Although it no longer carries traffic, the Hair's Ford Bridge is historically and technologically significant as one of the last remaining examples in the state of what was once a mainstay structural type.

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**NAME(S) OF STRUCTURE**

Hair's Ford Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 384000.4; Macon County Courthouse, Record K: page 334 (6 August 1891), page 456 (4 February 1892), page 542 (5 July 1892) - located at the Macon County Courthouse, Macon MO; field inspection by Richard Collier, 11 June 1991.

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**INVENTORIED BY**

Michelle Crow-Dolby

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

3 July 1993

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Little Brush Creek Bridge  
MHTD: 388001.4

MACO29

**DATE(S) OF CONSTRUCTION**

1905

**LOCATION**

County Road 388 over Little Brush Creek; S4/9, T57N, R17W  
2.6 miles west of New Cambria; Macon County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 41)

**CONDITION**

fair

**OWNER**

Macon County

span number: 1  
span length: 36.0'  
total length: 68.0'  
roadway wdt.: 12.0'

superstructure: steel, 2-panel, rigid-connected Pratt truss-leg bedstead, with flanking steel stringer approach spans  
substructure: steel pile bent piers and abutments with timber backwalls  
floor/decking: timber deck over steel stringers  
other features: upper chord: I-beam; upright end post: 2 channels; lower chord: 2 channels; vertical: 1 channel; diagonal: 2 angles; lateral bracing: 1 angle; floor beam: 1 channel; guardrail: 1 channel; small plate on center vertical: Pat Apr 19 1904; bolted plates at intersections between verticals and top chord

Carrying a gravel-surfaced county road over the Little Brush Creek, this short-span pony truss is located about 2½ miles west of New Cambria in southwestern Macon County. Minutes of the Macon County Court reveal that the contract for the single-span bridge was awarded on May 2, 1905, to the Dildine Bridge Company of Cameron, Missouri, for \$490.00. A rigid-connected truss leg bedstead, the bridge has since carried intermittent local traffic in its rural setting. Concrete-filled steel cylinder piers have been added to shore up the truss legs at the west end, but the bridge's integrity remains otherwise intact.

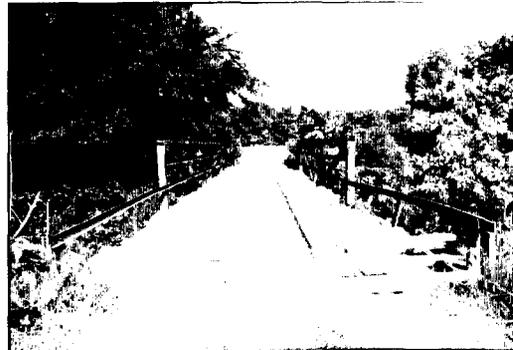
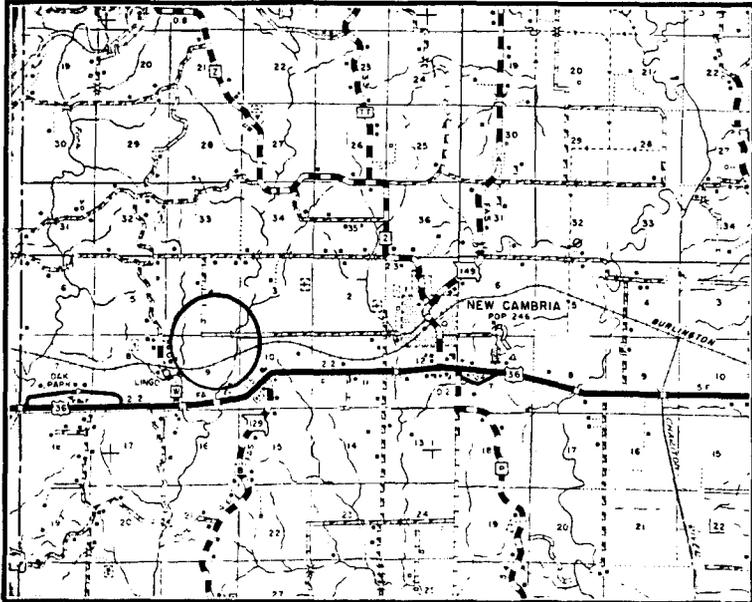
In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the late 1890s and early 1900s. Hundreds remain in place today -in fact, Missouri has probably more bedsteads than any other state. The Little Brush Creek Bridge is a fairly well-preserved, though not particularly distinguished, example of this statewide bridge construction trend.

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**NAME(S) OF STRUCTURE**

Little Brush Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 388001.4; Macon County Court Record Q: pages 423-24 (2 May 1905), located at the Macon County Courthouse, Macon MO; field inspection by Richard Collier, 11 June 1991.

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**INVENTORIED BY**

Michelle Crow-Dolby

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

3 July 1993

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Bridge  
MHTD: 454000.6

MACO34

**DATE(S) OF CONSTRUCTION**

1890

**LOCATION**

County Road 454 over unnamed branch; S33/34, T57N, R15W  
3.2 miles south of Bevier; Macon County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 50)

**CONDITION**

fair

**OWNER**

Macon County

span number: 1  
span length: 32.0'  
total length: 55.0'  
roadway wdt.: 12.0'

superstructure: steel, 2-panel, pin-connected truss-leg bedstead, with steel stringer approach span on the south end  
substructure: steel pile bent abutments with timber backwalls; truss legs; steel tubes welded to truss legs on south end for added support  
floor/decking: timber deck over steel stringers  
other features: upper chord: 2 channels with cover plate and lacing; end post: 2 channels with lacing; lower chord: 2 angles with lacing; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: none; builder's plate: ....89.....

This short-span truss crosses a small stream south of Bevier in south-central Macon County. The structure consists of a single Pratt bedstead, supported by truss-leg piers and approached on one end by a stringer span. With pinned connections and a timber deck, the bridge is made up of wrought iron components rolled in the Jones and Laughlin shops in Pittsburgh. Macon County SI&A records indicate that the bridge was erected here in 1890, probably by Kansas City contractors Farnsworth and Blodgett. Today, more than 100 years later, the bridge remains in use, with only maintenance-related repairs.

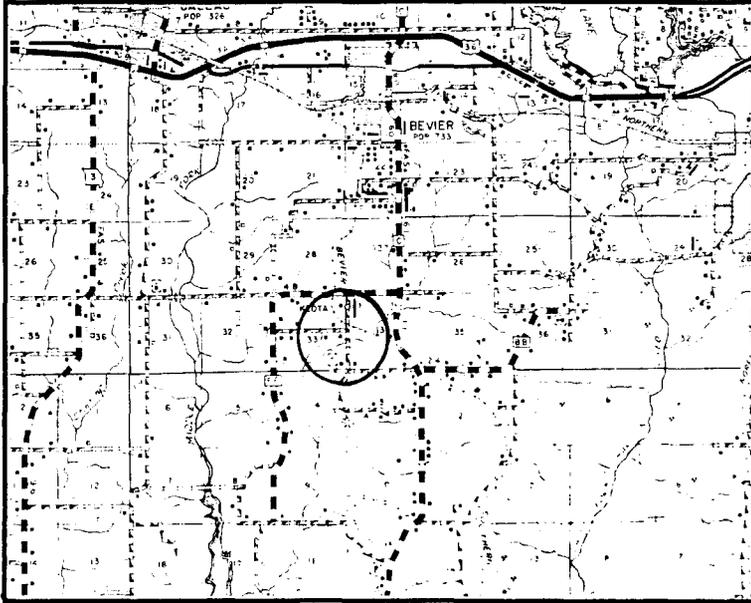
In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the late 1890s and early 1900s. Hundreds remain in place today; in fact, Missouri has probably more bedsteads than any other state. This bridge is distinguished as the earliest example remaining in Missouri of this mainstay structural type: a well-preserved, small-scale example of a prevailing statewide construction trend.

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**NAME(S) OF STRUCTURE**

Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 454000.6; field inspection by Richard Collier, 11 June 1991.

**INVENTORIED BY**

Michelle Crow-Dolby

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

3 July 1993

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Bridge  
MHTD: 480000.9

MACO37

**DATE(S) OF CONSTRUCTION**

1897

**LOCATION**

County Road 480 over unnamed branch; S25/30, T57N, R15/14W  
3.0 miles southwest of Macon; Macon County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 45)

**CONDITION**

fair

**OWNER**

Macon County

span number: 1  
span length: 40.0'  
total length: 40.0'  
roadway wdt.: 13.0'

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead  
substructure: steel truss legs with timber backwalls  
floor/decking: timber deck over steel stringers  
other features: upper chord and upright end post: 2 channels with lacing; lower chord: 2 angles with lacing (outer panels), 2 looped rectangular eyebars (inner panel); vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: none; builder's plate: ..189...

This steel Pratt truss-leg bedstead spans a small stream southwest of Macon in south-central Macon County. Supported by a timber substructure and featuring pinned connections throughout, this structure dates to 1897. A fragment of the original builder's plate indicates ...189... as the construction date which helps to corroborate the county engineer's construction date of 1897. Additionally, the county engineer's office records Farnsworth and Blodgett of Kansas City, Missouri, as the bridge's contractor. The small-scale Macon County bridge continues to carry local traffic in its rural setting, while retaining a high degree of structural integrity.

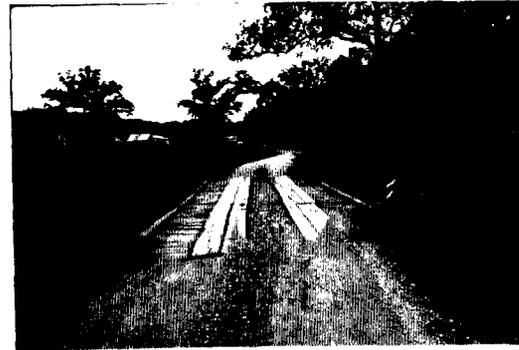
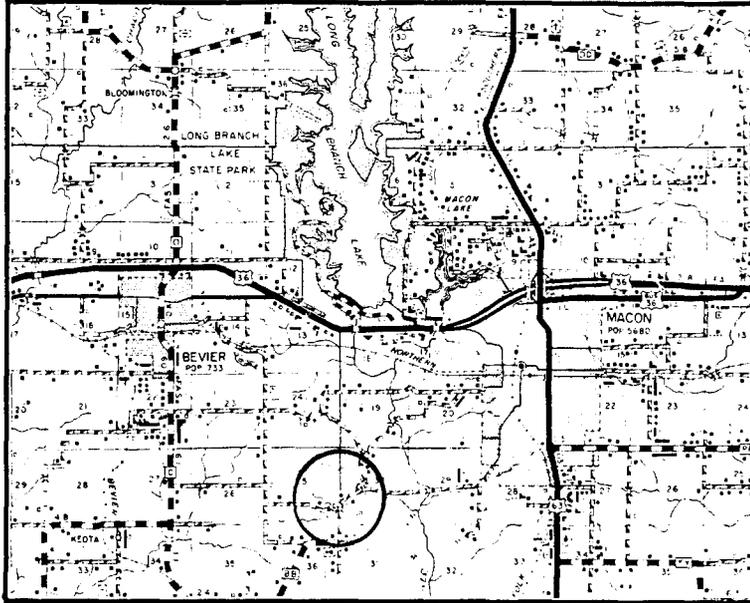
In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the late 1890s and early 1900s. Hundreds remain in place today -in fact, Missouri has probably more bedsteads than any other state. This bridge is a early, well-preserved example of this statewide bridge construction trend.

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**NAME(S) OF STRUCTURE**

Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 480000.9; field inspection by Richard Collier, 11 June 1991.

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**INVENTORIED BY**

Michelle Crow-Dolby

**AFFILIATION**

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**DATE**

3 July 1993

---

# MERCER COUNTY

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INCLUDED: [Significant feature(s) of bridge given in boldface]  
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
MERC01	L 73	Princeton Overpass	10-27' timber stringer 1942 CRI&P Railroad
*MERC02	025001.7	Brush Creek Bridge	1- 65' pinned Pratt pony truss c1905 Dildine Bridge Co. (prob.)
*MERC03	033000.1	West Fork Bridge	1-100' <b>pinned Pratt through truss</b> c1895
*MERC04	044002.3	West Fork Bridge	1-108' <b>pinned Pratt through truss</b> 1899 Dildine Bridge Company
*MERC05	051R00.7	Weldon Fork Bridge	1-120' <b>pinned Pratt through truss</b> c1895
*MERC06	052000.0	West Fork Bridge	1- 75' <b>pinned Pratt through truss</b> c1910
MERC07	073001.4	Duncan Bridge	1- 24' steel stringer 1907 Dildine Bridge Company
*MERC08	079001.6	Bridge	(replaced)
MERC09	086000.1	Little Muddy Creek Bridge	1- 30' steel stringer 1908 Dildine Bridge Company
MERC10	086R01.8	Irwin Creek Bridge	1- 30' steel stringer 1902 Dildine Bridge Company
MERC11	092R00.3	Stone Bridge	1- 30' steel stringer 1907 Dildine Bridge Company
MERC12	122002.5	W. Medicine Creek Bridge	1- 40' pinned Pratt bedstead 1899 Dildine Bridge Company
*MERC13	145000.3	W. Medicine Creek Bridge	(replaced)
MERC14	153001.2	Muddy Creek Bridge	(replaced)
*MERC15	160002.2	Muddy Creek Bridge	1- 70' pinned Pratt half-hip pony truss 1903 Dildine Bridge Company
MERC16	162000.0	C&NW Railroad Overpass	1- 66' steel plate through girder 1901 American Bridge Company
MERC17	163001.5	Wildcat Creek Bridge	1- 86' <b>pinned Pratt through truss</b> c1900
MERC18	222000.3	W. Muddy Creek Bridge	1- 30' steel stringer 1909 Dildine Bridge Company
*MERC19	244000.7	Muddy Creek Bridge	1- 70' pinned Pratt pony truss 1908 Dildine Bridge Company
MERC20	246001.2	Muddy Creek Bridge	1- 70' pinned Pratt pony truss c1905 Dildine Bridge Co. (prob.)
MERC21	262001.7	East Honey Creek Bridge	1- 40' pinned Pratt pony truss 1903 Dildine Bridge Company
MERC22	282003.0	Medicine Creek Bridge	1- 80' pinned Pratt pony truss c1915
MERC23	284000.7	Medicine Creek Bridge	1- 80' <b>pinned Pratt through truss</b> c1900
MERC24	301000.1	Bridge	1- 26' steel stringer 1902 Dildine Bridge Company

# MERCER COUNTY

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## EXCLUDED:

Pratt pony truss  
164000.1 221000.7 318000.0

Warren pony truss  
083002.1

Pratt bedstead  
232002.2

### Steel stringer / girder

G 353R	J 657	S 371	T 87	T 89	X 123	X 124
006001.0	009002.1	013R00.6	015R01.2	015001.3	016000.9	018000.9
019001.3	024001.5	026000.5	027000.7	033001.4	033002.1	039000.2
044000.1	060R01.4	066R00.2	067000.4	067001.8	070000.4	071R02.0
071001.1	073000.7	075002.1	079002.2	088001.7	091000.6	093R01.1
099001.5	103001.4	105001.8	110001.1	112000.8	121000.4	122R00.9
122002.8	123000.3	124000.3	124002.5	130000.6	130002.0	131000.8
135000.6	137001.4	138002.0	141R01.1	144000.8	146000.0	147000.6
150001.1	157R01.4	164002.3	165001.7	167000.7	168000.3	171000.4
175R00.7	179001.6	179001.8	184000.5	187000.2	189001.6	192R00.5
196R00.3	200000.4	200003.0	200004.0	202001.1	205000.5	206001.4
210R02.3	212002.0	214002.1	218R01.1	220000.7	223000.3	224000.9
224001.3	226000.1	231000.7	236R01.5	238001.9	252R00.7	253000.4
255000.2	261000.7	262000.2	264001.7	270R02.2	270000.7	

### Concrete girder

H 953 H 954 H 956 J 27

### Concrete box culvert

H 955 J 640 S 155 S 156 T 86 T 88

### Timber stringer

084000.0 153000.3 154000.8 230001.8 270001.6 275002.9 285001.9  
299000.3 324000.5

## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	1	20	0	0	21
Excluded	17	104	0	0	121
	18	124	0	0	142 structures

# Princeton Overpass

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MERC01

## GENERAL DATA

structure no.:	L 73	city/town:	eastern edge of Princeton
county:	Mercer	feature inters.:	Chicago, Rock Island & Pacific Railroad
		cadastral grid:	S26, T65N, R24W
		highway route:	U.S. Highway 136
		highway distr.:	2
		current owner:	Missouri Highway and Transportation Department

## STRUCTURAL DATA

superstructure:	timber stringer		
substructure:	timber pile bent abutments, wingwalls and piers		
span number:	10	condition:	good
span length:	27.0'	alterations:	none
total length:	167.0'	floor/decking :	timber decking
roadway width:	26.0'	other features:	guardrail: timber

## HISTORICAL DATA

erection date:	1942		
erection cost:	\$18,000.00 (estimate)		
designer:	Chicago, Rock Island & Pacific Railroad		
fabricator :	none		
contractor :	Chicago, Rock Island & Pacific Railroad		
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. L 73; Missouri Highway and Transportation Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO.		
sign. rating:	33		
evaluation:	NRHP non-eligible (undistinguished example of common overpass structural type, built during the war using non-strategic materials)		

inventoried by: Clayton B. Fraser 1 April 1991

# Brush Creek Bridge

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MERC02

## GENERAL DATA

<b>structure no.:</b>	025001.7	<b>city/town:</b>	3.2 miles southeast of Saline
<b>county:</b>	Mercer	<b>feature inters.:</b>	Brush Creek
		<b>cadastral grid:</b>	S26, T66N, R25W
		<b>highway route:</b>	county road
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Mercer County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 4-panel, pin-connected Pratt pony truss		
<b>substructure:</b>	timber pile bent abutments with timber back- and wingwalls		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	65.0'	<b>alterations:</b>	truss perhaps moved; one vertical replaced
<b>total length:</b>	65.0'	<b>floor/decking :</b>	timber decking over steel stringers
<b>roadway width:</b>	11.9'	<b>other features:</b>	upper chord: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: none

## HISTORICAL DATA

<b>erection date:</b>	c1905
<b>erection cost:</b>	unknown
<b>designer:</b>	Dildine Bridge Company, Cameron MO (probable)
<b>fabricator :</b>	Carnegie Steel Company, Pittsburgh PA
<b>contractor:</b>	Dildine Bridge Company, Cameron MO (probable)
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 025001.7; field inspection by Clayton Fraser, 8 December 1990.
<b>sign. rating:</b>	24
<b>evaluation:</b>	NRHP non-eligible (typical, undocumented example of common structural type)

Inventoried by: Clayton B. Fraser 1 April 1991

# West Fork Little River Bridge

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MERC03

## GENERAL DATA

<b>structure no.:</b> 033000.1	<b>city/town:</b> 4.4 miles northeast of Saline
<b>county:</b> Mercer	<b>feature inters.:</b> West Fork of Little River
	<b>cadastral grid:</b> S25, T67N, R25W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Mercer County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 6-panel, pin-connected Pratt through truss with steel stringer approach spans	
<b>substructure:</b> steel pile bent abutments and piers; timber backwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 100.0'	<b>alterations:</b> truss perhaps moved; struts and upper lateral bracing replaced
<b>total length:</b> 158.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 16.0'	<b>other features:</b> upper chord: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 square eyebars at hip); diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: 2 angles (new); strut: 2 angles (new); floor beam: I-beam, U-bolted to vertical; guardrail: none

## HISTORICAL DATA

**erection date:** c1895  
**erection cost:** unknown  
**designer:** unknown  
**fabricator :** unknown  
**contractor:** unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 033000.1; field inspection by Clayton Fraser, 8 December 1990.

**sign. rating:** 15  
**evaluation:** NRHP non-eligible (typical example of common structural type, altered and perhaps moved)

**inventoried by:** Clayton B. Fraser 1 April 1991

# West Fork Bridge

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MERC04

## GENERAL DATA

<b>structure no.:</b> 044002.3	<b>city/town:</b> 4.9 miles west of Mercer
<b>county:</b> Mercer	<b>feature inters.:</b> West Fork of Little River
	<b>cadastral grid:</b> S20/21, T66N, R24W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Mercer County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 6-panel, pin-connected Pratt through truss with steel stringer approach spans	
<b>substructure:</b> steel pile bent abutments and pier, with timber wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 108.0'	<b>alterations:</b> truss moved; one vertical replaced
<b>total length:</b> 148.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 14.3'	<b>other features:</b> upper chord: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 angles with lacing (2 looped square eyebars at hip); diagonal: 2 punched rectangular eyebars; counter: round eyerod with unslotted turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; portal strut: lattice; floor beam: riveted plate girders, U-bolted to vertical; guardrail: none

## HISTORICAL DATA

<b>erection date:</b> 1899	
<b>erection cost:</b> \$1604.00	
<b>designer:</b> Dildine Bridge Company, Cameron MO	
<b>fabricator :</b> Carnegie Steel Company, Pittsburgh PA	
<b>contractor:</b> Dildine Bridge Company, Cameron MO	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 044002.3; Mercer County Court Minute Book J: page 137 (21 February 1899); field inspection by Clayton Fraser, 8 December 1990.
<b>sign. rating:</b> 38	
<b>evaluation:</b>	NRHP determined non-eligible (typically configured example of common structural type, altered and moved)

inventoried by: Clayton B. Fraser 1 April 1991

# Weldon Fork Bridge

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MERC05

## GENERAL DATA

<b>structure no.:</b>	051R00.7	<b>city/town:</b>	4.6 miles northwest of Mercer
<b>county:</b>	Mercer	<b>feature inters.:</b>	Weldon Fork of Grand River
		<b>cadastral grid:</b>	S3/4, T66N, R24W
		<b>highway route:</b>	county road
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Mercer County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 7-panel, pin-connected Pratt through truss with rigid-connected Pratt pony truss and steel stringer approach spans		
<b>substructure:</b>	timber abutments and wingwalls with steel pile bent piers		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	120.0'	<b>alterations:</b>	through truss moved; pony truss added
<b>total length:</b>	240.0'	<b>floor/decking :</b>	timber deck over steel stringers
<b>roadway width:</b>	13.0'	<b>other features:</b>	upper chord: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing and knee braces; floor beam: I-beam, U-bolted to vertical; guardrail: 1 angle

## HISTORICAL DATA

<b>erection date:</b>	c1895
<b>erection cost:</b>	unknown
<b>designer:</b>	unknown
<b>fabricator :</b>	Jones and Laughlin Steel Company, Pittsburgh PA
<b>contractor:</b>	unknown
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 051R00.7; field inspection by Clayton Fraser, 8 December 1990.
<b>sign. rating:</b>	22
<b>evaluation:</b>	NRHP non-eligible (typically configured example of common structural type, apparently moved to this location)

inventoried by: Clayton B. Fraser 1 April 1991

# West Fork Bridge

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MERC06

## GENERAL DATA

<b>structure no.:</b> 052000.0	<b>city/town:</b> 4.9 miles northwest of Mercer
<b>county:</b> Mercer	<b>feature inters.:</b> West Fork of Little River
	<b>cadastral grid:</b> S16/17, T66N, R24W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Mercer County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt through truss with steel stringer approach spans	
<b>substructure:</b> steel pile bent abutments (dated 1976) and timber wingwalls at north end; concrete abutment with steel cylinder pier at south end	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 75.0'	<b>alterations:</b> truss evidently moved to this location
<b>total length:</b> 138.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 13.5'	<b>other features:</b> upper chord: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 looped round eyerods at hip); diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels

## HISTORICAL DATA

<b>erection date:</b> c1910	
<b>erection cost:</b> unknown	
<b>designer:</b> unknown	
<b>fabricator :</b> unknown	
<b>contractor :</b> unknown	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 052000.0; field inspection by Clayton Fraser, 8 December 1990.	
<b>sign. rating:</b> 20	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type, moved to this location)	

Inventoried by: Clayton B. Fraser 1 April 1991

# Duncan Bridge

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MERC07

## GENERAL DATA

structure no.:	073001.4	city/town:	1.4 miles east of South Lineville
county:	Mercer	feature inters.:	branch of Little Muddy Creek
		cadastral grid:	S28, T67N, R23W
		highway route:	county road
		highway distr.:	2
		current owner:	Mercer County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	timber deck
span length:	24.0'	other features:	unknown
total length:	24.0'		
roadway width:	13.0'		

## HISTORICAL DATA

erection date:	1907
erection cost:	\$220.00
designer:	Dildine Bridge Company, Cameron MO
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 073001.4; Mercer County Court Minute Book L: pages 196-97 (1 July 1907), located at Mercer County Court-house, Princeton MO.
sign. rating:	36
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser 1 April 1991

# Little Muddy Creek Bridge

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MERC09

## GENERAL DATA

structure no.:	086000.1	city/town:	2.2 miles east of South Lineville
county:	Mercer	feature inters.:	Little Muddy Creek
		cadastral grid:	S27/34, T67N, R23W
		highway route:	county road
		highway distr.:	2
		current owner:	Mercer County

## STRUCTURAL DATA

superstructure:	steel stringer		
substructure:	timber pile bent abutments and wingwalls		
span number:	1	condition:	fair
span length:	30.0'	alterations:	unknown
total length:	30.0'	floor/decking :	timber deck
roadway width:	15.0'	other features:	unknown

## HISTORICAL DATA

erection date:	1908
erection cost:	\$340.00
designer:	Dildine Bridge Company, Cameron MO
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 086000.1; Mercer County Court Minute Book L: page 345 (10 June 1908), page 422 (4 November 1908), located at Mercer County Courthouse, Princeton MO.
sign. rating:	41
evaluation:	NRHP non-eligible (typical example of exceedingly common structural type)

inventoried by: Clayton B. Fraser 1 April 1991

# Irwin Creek Bridge

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MERC10

## GENERAL DATA

structure no.:	086R01.8	city/town:	3.7 miles east of South Lineville
county:	Mercer	feature inters.:	Irwin Creek
		cadastral grid:	S26/35, T67N, R23W
		highway route:	county road
		highway distr.:	2
		current owner:	Mercer County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	timber pile bent abutments and wingwalls	alterations:	unknown
span number:	1	floor/decking :	timber deck
span length:	30.0'	other features:	unknown
total length:	30.0'		
roadway width:	12.0'		

## HISTORICAL DATA

erection date:	1902
erection cost:	\$230.00
designer:	Dildine Bridge Company, Cameron MO
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 086R01.8; Mercer County Court Minute Book J: page 421 (18 July 1902) - located at Mercer County Court-house, Princeton, Missouri.
sign. rating:	41
evaluation:	NRHP non-eligible (typical example of exceedingly common structural type)

inventoried by: Clayton B. Fraser 1 April 1991

# Stone Bridge (Irwin Creek Bridge)

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MERC11

## GENERAL DATA

<b>structure no.:</b> 092R00.3	<b>city/town:</b> 3.1 miles northeast of Mercer
<b>county:</b> Mercer	<b>feature inters.:</b> Irwin Creek
	<b>cadastral grid:</b> S10/15, T66N, R23W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Mercer County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> timber pile bent abutments and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 30.0'	<b>alterations:</b> unknown
<b>total length:</b> 30.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 12.0'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1907	
<b>erection cost:</b> \$300.00	
<b>designer:</b> Dildine Bridge Company, Cameron MO	
<b>fabricator :</b> unknown	
<b>contractor :</b> Dildine Bridge Company, Cameron MO	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 092R00.3; Mercer County Court Minute Book L: pages 196 and 197 (1 July 1907), located at Mercer County Courthouse, Princeton MO.
<b>sign. rating:</b> 41	
<b>evaluation:</b>	NRHP non-eligible (typical example of exceedingly common structural type)

Inventoried by: Clayton B. Fraser    1 April 1991

# West Medicine Creek Bridge

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MERC12

## GENERAL DATA

structure no.:	122002.5	city/town:	7.1 miles southeast of Mercer
county:	Mercer	feature inters.:	West Medicine Creek
		cadastral grid:	S6, T65N, R22W / S32, T66N, R22W
		highway route:	county road
		highway distr.:	2
		current owner:	Mercer County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt truss-leg bedstead		
substructure:	truss-leg abutments and timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	40.0'	floor/decking :	timber deck
roadway width:	12.0'	other features:	unknown

## HISTORICAL DATA

erection date:	1899
erection cost:	\$425.00
designer:	Dildine Bridge Company, Cameron MO
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 122002.5; Mercer County Court Minute Book J: page 189 (7 August 1899), pages 200-01 (5 September 1899), located at Mercer County Courthouse, Princeton MO.
sign. rating:	48
evaluation:	NRHP possibly eligible (relatively early example of mainstay structural type)

inventoried by: Clayton B. Fraser    1 April 1991

# Muddy Creek Bridge

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MERC15

## GENERAL DATA

<b>structure no.:</b> 160002.2	<b>city/town:</b> 3.4 miles southeast of Princeton
<b>county:</b> Mercer	<b>feature inters.:</b> Muddy Creek
	<b>cadastral grid:</b> S31, T65N, R23W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Mercer County

## STRUCTURAL DATA

**superstructure:** steel, 4-panel, pin-connected Pratt half-hip pony truss  
**substructure:** timber abutments and wingwalls; steel pile bent piers

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 70.0'	<b>alterations:</b> none
<b>total length:</b> 70.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 13.3'	<b>other features:</b> upper chord: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: 1 angle

## HISTORICAL DATA

**erection date:** 1903  
**erection cost:** \$472.00  
**designer:** Dildine Bridge Company, Cameron MO  
**fabricator :** Inland Steel Company, East Chicago IN  
**contractor:** Dildine Bridge Company, Cameron MO

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 160002.2; Mercer County Court Minute Book K: page 16 (3 February 1903), located in Mercer County Courthouse, Princeton MO; field inspection by Clayton Fraser, 8 December 1990.

**sign. rating:** 46  
**evaluation:** NRHP non-eligible (well-documented, but undistinguished example of common structural type)

**inventoried by:** Clayton B. Fraser    1 April 1991

# Chicago & North Western Railroad Overpass

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MERC16

## GENERAL DATA

structure no.:	162000.0	city/town:	southern edge of Princeton
county:	Mercer	feature inters.:	Chicago & North Western Railroad
		cadastral grid:	S34, T65N, R24W
		highway route:	county road
		highway distr.:	2
		current owner:	Mercer County

## STRUCTURAL DATA

superstructure:	steel plate through girder	condition:	good
substructure:	timber pile bent abutments and piers	alterations:	old railroad deck girder moved and turned upside down to form through girder
span number:	1	floor/decking :	timber deck over steel stringers
span length:	66.0'	other features:	builder's plate: 1910 / BUILT BY AMERICAN BRIDGE COMPANY / LASSIG BRANCH / CHICAGO, ILL.
total length:	210.0'		
roadway width:	21.1'		

## HISTORICAL DATA

erection date:	1901
erection cost:	unknown
designer:	unknown
fabricator :	American Bridge Company, Chicago IL
contractor :	Chicago and North Western Railroad
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 162000.0.
sign. rating:	36
evaluation:	NRHP non-eligible (typical railroad structure, removed from original context)

inventoried by: Clayton B. Fraser 1 April 1991

# Wildcat Creek Bridge

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MERC17

## GENERAL DATA

<b>structure no.:</b> 163001.5	<b>city/town:</b> 1.8 miles southwest of Princeton
<b>county:</b> Mercer	<b>feature inters.:</b> Wildcat Creek
	<b>cadastral grid:</b> S4, T64N, R24W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Mercer County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel pin-connected Pratt through truss, with steel stringer approach span	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 86.0'	<b>alterations:</b> unknown
<b>total length:</b> 104.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 13.6'	<b>other features:</b> portal bracing: lattice

## HISTORICAL DATA

<b>erection date:</b> c1900	
<b>erection cost:</b> unknown	
<b>designer:</b> unknown	
<b>fabricator :</b> unknown	
<b>contractor:</b> unknown	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 163001.5.	
<b>sign. rating:</b> 26	
<b>evaluation:</b> NRHP non-eligible (typical, undocumented example of common structural type)	

inventoried by: Clayton B. Fraser 1 April 1991

# West Muddy Creek Bridge

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MERC18

## GENERAL DATA

<b>structure no.:</b> 222000.3	<b>city/town:</b> 4.8 miles southwest of Princeton
<b>county:</b> Mercer	<b>feature inters.:</b> West Muddy Creek
	<b>cadastral grid.:</b> S18, T64N, R24W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Mercer County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 30.0'	<b>alterations:</b> unknown
<b>total length:</b> 30.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 14.5'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1909	
<b>erection cost:</b> \$280.00	
<b>designer:</b> Dildine Bridge Company, Cameron MO	
<b>fabricator :</b> unknown	
<b>contractor :</b> Dildine Bridge Company, Cameron MO	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 222000.3; Mercer County Court Minute Book L: page 541 (19 May 1909), 632 (27 December 1909), located at Mercer County Courthouse, Princeton MO.
<b>sign. rating:</b> 41	
<b>evaluation:</b>	NRHP non-eligible (typical example of exceedingly common structural type)

inventoried by: Clayton B. Fraser 1 April 1991

# Muddy Creek Bridge

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MERC19

## GENERAL DATA

<b>structure no.:</b> 244000.7	<b>city/town:</b> 4.8 miles southeast of Princeton
<b>county:</b> Mercer	<b>feature inters.:</b> Muddy Creek
	<b>cadastral grid:</b> S24, T64N, R24W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Mercer County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt pony truss with steel stringer approach spans	
<b>substructure:</b> steel pile bent abutments with timber backwalls; steel pile bent piers at north end; steel cylinder piers at south end (one steel cylinder pier lying in water)	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 70.0'	<b>alterations:</b> none
<b>total length:</b> 108.0'	<b>floor/decking :</b> timber decking over steel stringers
<b>roadway width:</b> 13.8'	<b>other features:</b> upper chord/ inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with double lacing; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 1 angle

## HISTORICAL DATA

<b>erection date:</b> 1908	
<b>erection cost:</b> \$600.00	
<b>designer:</b> Dildine Bridge Company, Cameron MO	
<b>fabricator :</b> unknown	
<b>contractor:</b> Dildine Bridge Company, Cameron MO	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 244000.7; Mercer County Court Minute Book L: page 345 (10 June 1908), page 422 (4 November 1908), located at Mercer County Courthouse, Princeton MO; field inspection by Clayton Fraser, 8 December 1990.	
<b>sign. rating:</b> 46	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)	

**inventoried by:** Clayton B. Fraser    1 April 1991

# Muddy Creek Bridge

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MERC20

## GENERAL DATA

structure no.:	246001.2	city/town:	4.0 miles southeast of Princeton
county:	Mercer	feature inters.:	Muddy Creek
		cadastral grid:	S12, T64N, R24W
		highway route:	county road
		highway distr.:	2
		current owner:	Mercer County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments and timber wingwalls		
span number:	1	condition:	fair
span length:	70.0'	alterations:	unknown
total length:	126.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	guardrail: steel angle

## HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor :	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 246001.2.
sign. rating:	29
evaluation:	NRHP non-eligible (typical, poorly documented example of common structural type)

inventoried by: Clayton B. Fraser 1 April 1991

# East Honey Creek Bridge

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MERC21

## GENERAL DATA

<b>structure no.:</b> 262001.7	<b>city/town:</b> 9.7 miles southeast of Princeton
<b>county:</b> Mercer	<b>feature inters.:</b> East Honey Creek
	<b>cadastral grid:</b> S26/35, T64N, R23W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Mercer County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt pony truss	
<b>substructure:</b> steel pile bent abutments and timber wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 40.0'	<b>alterations:</b> unknown
<b>total length:</b> 40.0'	<b>floor/decking :</b> timber decking
<b>roadway width:</b> 11.8'	<b>other features:</b> guardrail: steel angle

## HISTORICAL DATA

<b>erection date:</b> 1903	
<b>erection cost:</b> \$246.00	
<b>designer:</b> Dildine Bridge Company, Cameron MO	
<b>fabricator :</b> unknown	
<b>contractor:</b> Dildine Bridge Company, Cameron MO	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 262001.7; Mercer County Court Minute Book K: page 16 (3 February 1903), located at Mercer County Court-house, Princeton MO.	
<b>sign. rating:</b> 41	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)	

**inventoried by:** Clayton B. Fraser    1 April 1991

# Medicine Creek Bridge

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MERC22

## GENERAL DATA

structure no.:	282003.0	city/town:	13.6 miles southeast of Princeton
county:	Mercer	feature inters.:	Medicine Creek
		cadastral grid:	S4, T63N, R22W
		highway route:	county road
		highway distr.:	2
		current owner:	Mercer County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	80.0'	alterations:	unknown
total length:	145.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	guardrail: steel angle

## HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 282003.0.

sign. rating: 30  
evaluation: NRHP non-eligible (undistinguished, poorly documented example of common structural type)

inventoried by: Clayton B. Fraser 1 April 1991

# West Medicine Creek Bridge

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MERC23

## GENERAL DATA

structure no.:	284000.7	city/town:	12.8 miles southeast of Princeton
county:	Mercer	feature inters.:	West Medicine Creek
		cadastral grid:	S33, T64N, R22W
		highway route:	county road
		highway distr.:	2
		current owner:	Mercer County

## STRUCTURAL DATA

superstructure:	steel, 6-panel, pin-connected Pratt through truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	80.0'	alterations:	truss perhaps moved
total length:	128.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.8'	other features:	guardrail: none

## HISTORICAL DATA

erection date:	c1900
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 284000.7.
sign. rating:	21
evaluation:	NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Clayton B. Fraser 1 April 1991

# Bridge

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MERC24

## GENERAL DATA

<b>structure no.:</b> 301000.1	<b>city/town:</b> 8.3 miles southwest of Princeton
<b>county:</b> Mercer	<b>feature inters.:</b> branch of Sandy Creek
	<b>cadastral grid:</b> S8, T64N, R25W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Mercer County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 26.0'	<b>alterations:</b> unknown
<b>total length:</b> 26.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 12.0'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1902	
<b>erection cost:</b> \$171.00	
<b>designer:</b> Dildine Bridge Company, Cameron MO	
<b>fabricator :</b> unknown	
<b>contractor:</b> Dildine Bridge Company, Cameron MO	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 301000.1; Mercer County Court Minute Book J: pages 421-22 (18 April 1902), located at Mercer County Court-house, Princeton MO.	
<b>sign. rating:</b> 36	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)	

**inventoried by:** Clayton B. Fraser    1 April 1991

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

West Medicine Creek Bridge

MHTD:

122002.5

**LOCATION**county road over West Medicine Creek; S6, T65N, R22W / S32, T66N, R22W  
7.1 miles southeast of Mercer; Mercer County, Missouri

MERC12

**DATE(S) OF CONSTRUCTION**

1899

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 48)**CONDITION**

fair

**OWNER**

Mercer County

span number:	1	superstructure:	steel, 3-panel, pin-connected Pratt truss-leg bedstead
span length:	40.0'	substructure:	truss-leg abutments and timber wingwalls
total length:	40.0'	floor/decking:	timber deck
roadway wdt.:	12.0'	other features:	unknown

This short-span truss crosses the West Medicine Creek some seven miles southeast of Mercer. The structure consists of a single Pratt bedstead, supported by truss-leg piers and approached on one end by a stringer span. With pinned connections and a timber deck, the bridge was erected here in 1899 by the Dildine Bridge Company of Cameron, Missouri, for \$425.00. Since that time the structure has carried vehicular traffic in relatively well-preserved condition.

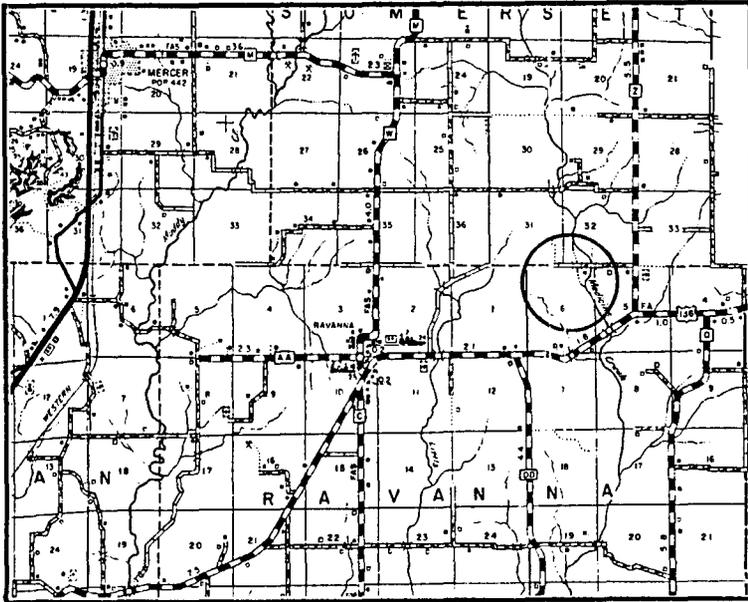
In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the late 1890s and early 1900s. Hundreds remain in place today; in fact, Missouri has probably more bedsteads than any other state. This bridge in Mercer County is distinguished as the earliest example remaining in Missouri of this mainstay structural type: a well-preserved, small-scale example of a prevailing statewide construction trend.

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**NAME(S) OF STRUCTURE**

West Medicine Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 122002.5; Mercer County Court Minute Book J: page 189 (7 August 1899), pages 200-01 (5 September 1899), located at Mercer County Courthouse, Princeton MO.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

3 July 1993

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# HAER INVENTORY

Missouri Historic Bridge Inventory

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**NAME(S) OF STRUCTURE**

Muddy Creek Bridge  
MHTD: 244000.7

MERC19

**DATE(S) OF CONSTRUCTION**

1908

**LOCATION**

county road over Muddy Creek; S24, T64N, R24W  
4.8 miles southeast of Princeton; Mercer County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 46)

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**CONDITION**

fair

**OWNER**

Mercer County

span number: 1  
span length: 70.0'  
total length: 108.0'  
roadway wdt.: 13.8'

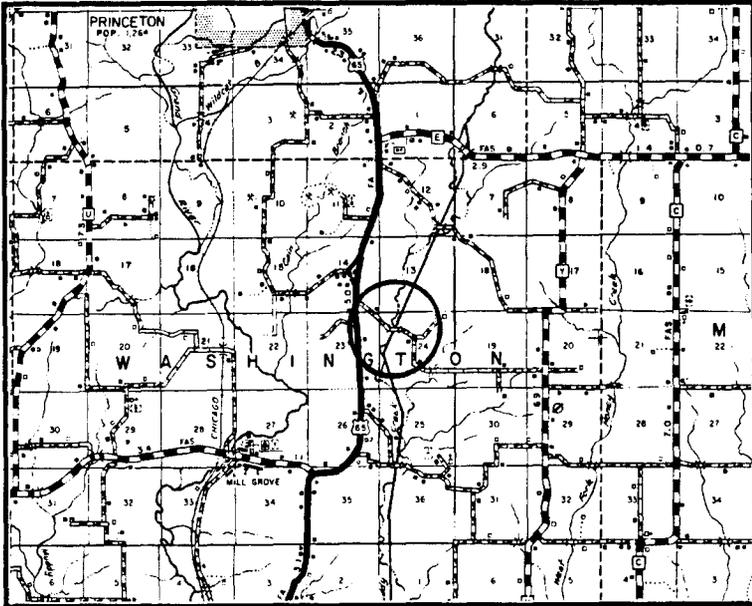
superstructure: steel, 4-panel, pin-connected Pratt pony truss with steel stringer approach spans  
substructure: steel pile bent abutments with timber backwalls; steel pile bent piers at north end; steel cylinder piers at south end (one steel cylinder pier lying in water)  
floor/decking: timber decking over steel stringers  
other features: upper chord/ inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with double lacing; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 1 angle

This bridge carries a gravel-surfaced county road across Muddy Creek about five miles southeast of Princeton, in south-central Mercer County. Resting on a substructure of steel pile bents and concrete-filled steel cylinder piers, the truss is configured as a four-panel pin-connected Pratt pony truss. County records show that the structure was built in 1908 by the Dildine Bridge Company of Cameron, Missouri. Contracted for a price of \$600.00, the span was included in a group of eight bridges that Dildine built for Mercer County that summer. Most of the county's bridges dating from this period - essentially all of which were erected by Dildine - have been moved to other locations, or dismantled entirely. This modest crossing, however, still carries traffic in its original location. The bridge has not been altered significantly, and as such it retains much of its historical integrity.

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**NAME(S) OF STRUCTURE**  
Muddy Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 244000.7; Mercer County Court Minute Book L: page 345 (10 June 1908), page 422 (4 November 1908), located at Mercer County Courthouse, Princeton MO; field inspection by Clayton Fraser and Carl McWilliams, 8 December 1990.

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**INVENTORIED BY**

Clayton Fraser and Carl McWilliams

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

1 April 1991

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# PUTNAM COUNTY

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INCLUDED: [Significant feature(s) of bridge given in boldface]  
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
PUTN01	003002.2	Medicine Creek Bridge	(replaced)
*PUTN02	060000.5	Berry Bridge	(replaced)
*PUTN03	070000.4	Bridge	1- 34' steel stringer 1914 L.H. Elson
*PUTN04	070000.5	Demery Bridge	1- 48' pinned Pratt half-hip pony truss 1909 Canton Bridge Company
*PUTN05	082001.6	East Locust Creek Bridge	1- 48' pinned Pratt pony truss c1910
*PUTN06	085002.0	Montgomery Bridge	1- 48' pinned Pratt half-hip pony truss 1908 Canton Bridge Company
PUTN07	087000.2	Brown Bridge	1- 30' steel stringer 1909 Canton Bridge Company
PUTN08	127003.2	Drury Bridge	1- 42' pinned Pratt half-hip pony truss 1909 Midland Bridge Company
*PUTN09	147000.9	Bridge	1- 48' pinned Pratt pony truss c1915 Illinois Steel Bridge Company
*PUTN10	181001.0	Shadden Bridge	1- 60' pinned Pratt pony truss 1909 Illinois Steel Bridge Company
PUTN11	186000.8	Petty Bridge	1- 80' pinned Pratt pony truss 1908 Canton Bridge Company
PUTN12	198001.1	Husted Bridge	1- 80' pinned Pratt pony truss c1915 Illinois Steel Bridge Company
*PUTN13	227000.1	Margrove Bridge	1- 60' pinned Pratt pony truss 1909 Midland Bridge Company
*PUTN14	253001.5	Bradshaw Bridge	1- 48' pin Pratt half-hip pony truss 1909 Midland Bridge Company
*PUTN15	278000.3	Lick Creek Bridge	(replaced)
PUTN16	279003.0	Buster Bridge	1- 60' riveted Warren pony truss 1913 Illinois Steel Bridge Company
*PUTN17	291000.4	Wildcat Creek Bridge	1- 48' pinned Pratt half-hip pony truss c1910 Canton Bridge Company
*PUTN18	300000.5	Wildcat Creek Bridge	1- 48' pinned Pratt pony truss c1905 Canton Bridge Company
PUTN19	332000.2	Herndon Bridge	(replaced)
PUTN20	412000.4	Dickson Bridge	(replaced)
PUTN21	413000.3	East Walnut Creek Bridge	1- 50' riveted Pratt half-hip pony truss 1917 A.L. Greenberg Iron Company
PUTN22	432001.7	Bridge	1- 48' pinned Pratt half-hip pony truss c1910 Canton Bridge Company

EXCLUDED:

Pratt pony truss  
 084002.3 196000.6 222000.1 256000.3 259000.5 269000.8 403000.8  
 411004.8 412000.5

# PUTNAM COUNTY

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## EXCLUDED (cont.):

### Warren pony truss

043000.1	181000.4	261001.8	279003.0	304002.0	346001.8	380001.6
418000.5	445000.7	445000.8				

### Lattice bedstead

019000.3	024002.4	036002.9	039002.4	050001.2	073002.3	084001.7
095000.1	132001.4	156000.3	165000.9	210000.4	215000.1	216000.6
257002.0	273000.5	293001.0	305000.5	307000.2	352001.1	386000.4
393000.7	452001.2					

### Steel stringer

H 495R	J 400	J 406	J 410	J 689	K 711	L 256
S 584	T 490	X 239	001001.1	011000.5	015001.4	020000.4
020000.6	022002.2	025000.9	027R02.9	029000.7	031000.3	042001.2
044000.5	044000.7	045000.2	045000.5	061000.3	067000.3	068000.2
069000.2	073000.1	079000.5	082000.9	084001.1	087000.2	088002.5
094000.1	100000.5	101001.1	101001.2	107000.0	112000.2	114000.3
127002.1	128000.4	138000.4	156000.2	158001.0	159R01.0	161R02.0
161000.5	171001.0	172000.8	173000.7	211001.3	224001.0	241001.0
255000.3	275001.1	281001.3	282000.3	286002.8	293000.5	301000.2
328000.3	351001.5	351002.1	351002.6	355000.7	358R00.4	359000.1
400000.4	411000.3	413001.0	417000.5	454000.9	455001.6	458000.2
459000.3	459001.0	461000.8				

### Concrete girder

J 101	J 179	095002.7	104001.7	118001.7	141000.5	159002.0
352500.1						

### Concrete slab

J 954	036003.5	047002.2	070000.4			
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### Concrete box culvert

J 180	J 401	J 901	X 161			
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### Timber stringer

021001.5

## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	0	17	0	0	17
Excluded	17	122	0	0	139
	17	139	0	0	156 structures

# Bridge

PUTN03

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## GENERAL DATA

structure no.:	070000.4	city/town:	9.6 miles northwest of Unionville
county:	Putnam	feature inters.:	branch of West Locust Creek
		cadastral grid:	S30, T66N, R20W
		highway route:	County Road 70
		highway distr.:	2
		current owner:	Putnam County

## STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	concrete abutments and wingwalls	alterations:	unknown
span number:	1	floor/decking :	concrete deck
span length:	34.0'	other features:	concrete guardrails with recessed rectangular panels; builder's name impressed in concrete guardrail: L.H. ELSON / MAKER / UNIONVILLE MO
total length:	35.0'		
roadway width:	12.3'		

## HISTORICAL DATA

erection date:	1914
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	L.H. Elson, Unionville MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 070000.4; field inspection by Clayton Fraser, 11 July 1992.
sign. rating:	32
evaluation:	NRHP non-eligible (undistinguished, small-scale structure, lacking in technological significance)

inventoried by: Clayton B. Fraser 28 July 1992

# Demery Bridge

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PUTN04

## GENERAL DATA

<b>structure no.:</b> 070000.5	<b>city/town:</b> 9.5 miles northwest of Unionville
<b>county:</b> Putnam	<b>feature inters.:</b> West Locust Creek
	<b>cadastral grid:</b> S29, T66N, R20W
	<b>highway route:</b> County Road 70
	<b>highway distr.:</b> 2
	<b>current owner:</b> Putnam County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt half-hip pony truss, with steel stringer approach span	
<b>substructure:</b> welded steel pile bent piers and abutments with timber back- and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 48.0'	<b>alterations:</b> substructure replaced
<b>total length:</b> 62.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 12.0'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; no guardrails

## HISTORICAL DATA

<b>erection date:</b> 1909	
<b>erection cost:</b> unknown	
<b>designer:</b> Canton Bridge Company, Canton OH	
<b>fabricator :</b> Canton Bridge Company, Canton OH	
<b>contractor:</b> Canton Bridge Company, Canton OH	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 070000.5; Putnam County Court Record, Book N: pages 111-12 (22 May 1909), located at Putnam County Courthouse, Unionville MO; field inspection by Clayton Fraser, 11 July 1992.	
<b>sign. rating:</b> 37	
<b>evaluation:</b> NRHP non-eligible (typical example of pinned Pratt pony truss construction)	

**inventoried by:** Clayton B. Fraser    28 July 1992

# East Locust Creek Bridge

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PUTN05

## GENERAL DATA

structure no.:	082001.6	city/town:	7.7 miles northwest of Unionville
county:	Putnam	feature inters.:	East Locust Creek
		cadastral grid:	S11, T66N, R20W
		highway route:	County Road 82
		highway distr.:	2
		current owner:	Putnam County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	steel truss leg piers		
span number:	1	condition:	fair
span length:	48.0'	alterations:	none
total length:	71.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.9'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal (counter): square eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; no guardrails

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 082001.6; field inspection by Clayton Fraser, 11 July 1992.

sign. rating:	27
evaluation:	NRHP non-eligible (undistinguished, undocumented example of a common structural configuration)

inventoried by: Clayton B. Fraser 28 July 1992

# Montgomery Bridge

PUTN06

## GENERAL DATA

<b>structure no.:</b> 085002.0	<b>city/town:</b> 9.0 miles northwest of Unionville
<b>county:</b> Putnam	<b>feature inters.:</b> East Locust Creek
	<b>cadastral grid:</b> S3, T66/67N, R20W
	<b>highway route:</b> County Road 85
	<b>highway distr.:</b> 2
	<b>current owner:</b> Putnam County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt half-hip pony truss	
<b>substructure:</b> steel pile bent abutments and piers, with timber back- and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 48.0'	<b>alterations:</b> substructure replaced
<b>total length:</b> 63.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 13.9'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 round eyerods, 2 square eyebars; vertical: 4 angles with double lacing; diagonal: 2 looped square eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; hip-mounted builder's plate: THE CANTON BRIDGE COMPANY, CANTON OHIO

## HISTORICAL DATA

<b>erection date:</b> 1908	
<b>erection cost:</b> \$624.00 (superstructure cost)	
<b>designer:</b> Canton Bridge Company, Canton OH	
<b>fabricator :</b> Canton Bridge Company, Canton OH	
<b>contractor:</b> Canton Bridge Company, Canton OH	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 085002.0; Putnam County Court Record, Book N: page 305 (31 December 1906), page 523 (10 April 1908), page 553 (3 August 1908) - located at Putnam County Court-house, Unionville MO; field inspection by Clayton Fraser, 11 July 1992.	
<b>sign. rating:</b> 39	
<b>evaluation:</b> NRHP non-eligible (although well-documented, a typically configured example of a standard truss type)	

**inventoried by:** Clayton B. Fraser 28 July 1992

# Brown Bridge

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PUTN07

## GENERAL DATA

<b>structure no.:</b> 087000.2	<b>city/town:</b> 9.8 miles northwest of Unionville
<b>county:</b> Putnam	<b>feature inters.:</b> branch of Locust Creek
	<b>cadastral grid:</b> S33/34, T67N, R20W
	<b>highway route:</b> County Road 87
	<b>highway distr.:</b> 2
	<b>current owner:</b> Putnam County

## STRUCTURAL DATA

**superstructure:** steel stringer  
**substructure:** unknown

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 30.0'	<b>alterations:</b> unknown
<b>total length:</b> 30.0'	<b>floor/decking :</b> concrete deck
<b>roadway width:</b> 10.3'	<b>other features:</b> unknown

## HISTORICAL DATA

**erection date:** 1909  
**erection cost:** unknown  
**designer:** Canton Bridge Company, Canton OH  
**fabricator :** Canton Bridge Company, Canton OH  
**contractor:** Canton Bridge Company, Canton OH

**references:** Missouri Highway and Transportation Department, Structure and Inventory and Appraisal: Structure Number 087000.2; Putnam County Court Record, Book O; pages 111-12 (22 May 1909), located at Putnam County Courthouse, Unionville MO.

**sign. rating:** 34  
**evaluation:** NRHP non-eligible (a relatively early example of a common structural type, used for short-span crossings)

**inventoried by:** Clayton B. Fraser    13 August 1992

# Drury Bridge

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PUTN08

## GENERAL DATA

<b>structure no.:</b> 127003.2	<b>city/town:</b> 2.5 miles north of Unionville
<b>county:</b> Putnam	<b>feature inters.:</b> tributary of North Blackbird Creek
	<b>cadastral grid:</b> S23/24, T66N, R19W
	<b>highway route:</b> County Road 127
	<b>highway distr.:</b> 2
	<b>current owner:</b> Putnam County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt half-hip pony truss	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 42.0'	<b>alterations:</b> unknown
<b>total length:</b> 42.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.6'	<b>other features:</b> steel angle guardrails

## HISTORICAL DATA

<b>erection date:</b> 1909
<b>erection cost:</b> unknown
<b>designer:</b> Midland Bridge Company, Kansas City MO
<b>fabricator :</b> Midland Bridge Company, Kansas City MO
<b>contractor:</b> Midland Bridge Company, Kansas City MO
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 127003.2; Putnam County Court Record, Book O, pages 111-12 (22 May 1909). located at Putnam County Courthouse, Unionville MO.
<b>sign. rating:</b> 36
<b>evaluation:</b> NRHP non-eligible (technologically undistinguished example of pinned Pratt pony truss construction)

**inventoried by:** Clayton B. Fraser    28 July 1992

# Bridge

PUTN09

## GENERAL DATA

<b>structure no.:</b> 147000.9	<b>city/town:</b> 6.8 miles northeast of Unionville
<b>county:</b> Putnam	<b>feature inters.:</b> tributary of North Blackbird Creek
	<b>cadastral grid:</b> S5, T66N, R18W
	<b>highway route:</b> County Road 147
	<b>highway distr.:</b> 2
	<b>current owner:</b> Putnam County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt pony truss	
<b>substructure:</b> steel pile bent piers with concrete back- and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 48.0'	<b>alterations:</b> none
<b>total length:</b> 48.0'	<b>floor/decking :</b> concrete deck over steel stringers
<b>roadway width:</b> 11.7'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal (counter): square eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 angles

## HISTORICAL DATA

<b>erection date:</b> c1915	
<b>erection cost:</b> unknown	
<b>designer:</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>fabricator :</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>contractor:</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 147000.9; field inspection by Clayton Fraser, 11 July 1992.	
<b>sign. rating:</b> 30	
<b>evaluation:</b> NRHP non-eligible (typically configured example of a common structural type)	

inventoried by: Clayton B. Fraser 28 July 1992

# Shadden Bridge

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PUTN10

## GENERAL DATA

structure no.:	181001.0	city/town:	10.1 miles northeast of Unionville
county:	Putnam	feature inters.:	Shoal Creek
		cadastral grid:	S32, T66/67N, R17W
		highway route:	County Road 181
		highway distr.:	2
		current owner:	Putnam County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers, with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	60.0'	floor/decking :	concrete deck over steel stringers
roadway width:	11.8'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 square eyebars; counter: eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice

## HISTORICAL DATA

erection date:	1909
erection cost:	\$952.00
designer:	Illinois Steel Bridge Company, Jacksonville IL
fabricator :	Illinois Steel Bridge Company, Jacksonville IL
contractor:	Illinois Steel Bridge Company, Jacksonville IL
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 181001.0; Putnam County Court Record, Book N: page 430 (9 August 1907), page 43 (2 March 1909), page 67 (9 April 1909), page 153 (3 November 1909) - located at Putnam County Courthouse, Unionville MO; field inspection by Clayton Fraser, 11 July 1992.
sign. rating:	44
evaluation:	NRHP non-eligible (although well-documented, a typically configured example of a standard truss configuration)

inventoried by: Clayton B. Fraser 28 July 1992

# Petty Bridge

PUTN11

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## GENERAL DATA

structure no.:	186000.8	city/town:	9.5 miles northeast of Unionville
county:	Putnam	feature inters.:	Shoal Creek
		cadastral grid:	S17, T66N, R17W
		highway route:	County Road 186
		highway distr.:	2
		current owner:	Putnam County

## STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	80.0'	alterations:	none
total length:	255.0'	floor/decking :	timber deck
roadway width:	11.4'	other features:	steel lattice guardrails

## HISTORICAL DATA

erection date:	1908
erection cost:	\$3500.00
designer:	Canton Bridge Company, Canton OH
fabricator :	Canton Bridge Company, Canton OH
contractor:	Canton Bridge Company, Canton OH
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 186000.8; Putnam County Court Record, Book N: page 524 (11 April 1908), page 553 (3 August 1908), located at Putnam County Courthouse, Unionville MO.
sign. rating:	44
evaluation:	NRHP non-eligible (typically configured pinned Pratt pony truss)

inventoried by: Clayton B. Fraser 28 July 1992

# Husted Bridge

PUTN12

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## GENERAL DATA

structure no.:	198001.1	city/town:	5.7 miles west of Livonia
county:	Putnam	feature inters.:	Shoal Creek
		cadastral grid:	S27, T66N, R17W
		highway route:	County Road 198
		highway distr.:	2
		current owner:	Putnam County

## STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	unknown		
span number:	1	condition:	fair
span length:	80.0'	alterations:	unknown
total length:	109.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel lattice guardrails

## HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	Illinois Steel Bridge Company, Jacksonville IL
fabricator :	Illinois Steel Bridge Company, Jacksonville IL
contractor :	Illinois Steel Bridge Company, Jacksonville IL
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 198001.1.
sign. rating:	38
evaluation:	NRHP non-eligible (typically configured example of common structural type)

Inventoried by: Clayton B. Fraser 28 July 1992

# Margrove Bridge

PUTN13

## GENERAL DATA

structure no.:	227000.1	city/town:	2.5 miles northwest of Livonia
county:	Putnam	feature inters.:	Brush Creek
		cadastral grid:	S24, T66N, R17W
		highway route:	County Road 227
		highway distr.:	2
		current owner:	Putnam County

## STRUCTURAL DATA

superstructure:	steel, 4-panel pin-connected Pratt pony truss		
substructure:	steel pile bent abutments, with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	60.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.8'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to lower chord pin; guardrail: 2 channels

## HISTORICAL DATA

erection date:	1909
erection cost:	unknown
designer:	Midland Bridge Company, Kansas City MO
fabricator :	Midland Bridge Company, Kansas City MO
contractor:	Midland Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 227000.1; Putnam County Court Record, Book N: page 76 (4 May 1909), pages 111-12 (22 May 1909) - located at Putnam County Courthouse, Unionville MO; field inspection by Clayton Fraser, 11 July 1992.
sign. rating:	44
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser 28 July 1992

# Bradshaw Bridge

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PUTN14

## GENERAL DATA

structure no.:	253001.5	city/town:	9.2 miles southeast of Unionville
county:	Putnam	feature inters.:	North Blackbird Creek
		cadastral grid:	S18, T65N, R17W
		highway route:	County Road 253
		highway distr.:	2
		current owner:	Putnam County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss, with steel stringer approach spans		
substructure:	steel pile bent abutments and piers, with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	48.0'	alterations:	none
total length:	111.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.6'	other features:	upper chord and inclined end post: 2 channels with cover plates and lacing; lower chord: 2 round eyerods, 2 square eyebars; vertical: 4 angles with lacing; diagonal: 2 square eyerods; counter: square eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice

## HISTORICAL DATA

erection date:	1909
erection cost:	unknown
designer:	Midland Bridge Company, Kansas City MO
fabricator :	Midland Bridge Company, Kansas City MO
contractor :	Midland Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 253001.5; Putnam County Court Record, Book N: page 68 (9 April 1909), pages 111-112 (22 May 1909) - located at Putnam County Courthouse, Unionville MO; field inspection by Clayton Fraser, 11 July 1992.
sign. rating:	43
evaluation:	NRHP non-eligible (typical example of a common truss configuration, with standard detailing, and unremarkable dimensions)

inventoried by: Clayton B. Fraser 28 July 1992

# Buster Bridge

PUTN16

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## GENERAL DATA

structure no.:	279003.0	city/town:	6.2 miles northwest of Worthington
county:	Putnam	feature inters.:	Blackbird Creek
		cadastral grid:	S21, T65N, R17W
		highway route:	County Road 279
		highway distr.:	2
		current owner:	Putnam County

## STRUCTURAL DATA

superstructure: steel, 4-panel, rigid-connected Warren pony truss  
substructure: unknown

span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	75.0'	floor/decking :	timber deck
roadway width:	11.7'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: 1913  
erection cost: \$1934.00  
designer: Illinois Steel Bridge Company, Jacksonville IL  
fabricator : Illinois Steel Bridge Company, Jacksonville IL  
contractor: Illinois Steel Bridge Company, Jacksonville IL

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 279003.0; Putnam County Court Record, Book Q: page 68 (3 February 1914) - located at Putnam County Courthouse, Unionville MO.

sign. rating: 37  
evaluation: NRHP non-eligible (modestly scaled and detailed example of Warren pony truss construction from the 1910s)

inventoried by: Clayton B. Fraser 13 August 1992

# Wildcat Creek Bridge

PUTN17

## GENERAL DATA

<b>structure no.:</b> 291000.4	<b>city/town:</b> 3.2 miles northwest of Worthington
<b>county:</b> Putnam	<b>feature inters.:</b> Wildcat Creek
	<b>cadastral grid:</b> S24, T65N, R17W
	<b>highway route:</b> County Road 291
	<b>highway distr.:</b> 2
	<b>current owner:</b> Putnam County

## STRUCTURAL DATA

**superstructure:** steel, 3-panel, pin-connected Pratt half-hip pony truss  
**substructure:** timber pile bent abutments and wingwalls

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 48.0'	<b>alterations:</b> none
<b>total length:</b> 48.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 11.7'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 round eyerods, 2 square eyebars; vertical: 4 angles with double lacing; diagonal: 2 looped square eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels

## HISTORICAL DATA

**erection date:** c1910  
**erection cost:** unknown  
**designer:** Canton Bridge Company, Canton OH  
**fabricator :** Canton Bridge Company, Canton OH  
**contractor:** Canton Bridge Company, Canton OH

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 291000.4; field inspection by Clayton Fraser, 11 July 1992.

**sign. rating:** 29  
**evaluation:** NRHP non-eligible (typical example of pinned Pratt pony truss construction)

**inventoried by:** Clayton B. Fraser 28 July 1992

# Wildcat Creek Bridge

PUTN18

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## GENERAL DATA

structure no.:	300000.5	city/town:	3.1 miles southwest of Worthington
county:	Putnam	feature inters.:	Wildcat Creek
		cadastral grid:	S6, T64N, R16W
		highway route:	County Road 300
		highway distr.:	2
		current owner:	Putnam County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	steel pile bent piers, with timber back- and wingwalls		
span number:	1	condition:	poor
span length:	48.0'	alterations:	substructure replaced, outriders added to verticals, diagonals modified
total length:	63.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.2'	other features:	I-beam floor beams, U-bolted to lower chord pins; no guardrails

## HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	Canton Bridge Company, Canton OH
fabricator :	Canton Bridge Company, Canton OH
contractor :	Canton Bridge Company, Canton OH
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 300000.5; field inspection by Clayton Fraser, 11 July 1992.
sign. rating:	29
evaluation:	NRHP non-eligible (typical example of common structural type, substantially altered)

inventoried by: Clayton B. Fraser 28 July 1992

# East Walnut Creek Bridge

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PUTN21

## GENERAL DATA

structure no.:	413000.3	city/town:	9.4 miles southwest of Unionville
county:	Putnam	feature inters.:	East Walnut Creek
		cadastral grid:	S29, T65N, R20W
		highway route:	County Road 413
		highway distr.:	2
		current owner:	Putnam County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, rigid-connected Pratt half-hip pony truss, with steel stringer approach spans		
substructure:	timber pile bent abutments, wingwalls and piers		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	84.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.3'	other features:	no guardrails

## HISTORICAL DATA

erection date:	1917
erection cost:	unknown
designer:	A.L. Greenberg Iron Company, Terre Haute IN
fabricator :	A.L. Greenberg Iron Company, Terre Haute IN
contractor:	A.L. Greenberg Iron Company, Terre Haute IN
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 413000.3.
sign. rating:	33
evaluation:	NRHP non-eligible (typically built example of a common structural configuration)

inventoried by: Clayton B. Fraser 13 August 1992

# Bridge

PUTN22

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## GENERAL DATA

structure no.:	432001.7	city/town:	2.6 miles southwest of Lucerne
county:	Putnam	feature inters.:	tributary of Medicine Creek
		cadastral grid:	S11, T65N, R22W
		highway route:	County Road 432
		highway distr.:	2
		current owner:	Putnam County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss  
substructure: unknown

span number:	1	condition:	fair
span length:	48.0'	alterations:	unknown
total length:	48.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.0'	other features:	steel angle guardrails; hip-mounted builder's plate: THE CANTON BRIDGE COMPANY, CANTON OHIO

## HISTORICAL DATA

erection date: c1910  
erection cost: unknown  
designer: Canton Bridge Company, Canton OH  
fabricator : Canton Bridge Company, Canton OH  
contractor: Canton Bridge Company, Canton OH

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 432001.7.

sign. rating: 29  
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser 28 July 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Demery Bridge (West Locust Creek Bridge)  
MHTD: 070000.5

PUTN04

**DATE(S) OF CONSTRUCTION**

1909

**LOCATION**

County Road 70 over West Locust Creek; S29, T66N, R20W  
9.5 miles northwest of Unionville; Putnam County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 37)

**CONDITION**

fair

**OWNER**

Putnam County

span number: 1  
span length: 48.0'  
total length: 62.0'  
roadway wdt.: 12.0'

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss, with steel stringer approach span  
substructure: welded steel pile bent piers and abutments with timber back- and wingwalls  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; no guardrails

The Demery Bridge carries an unpaved county road over West Locust Creek on the boundary line between Jackson and Sherman Counties. Comprised of a pinned Pratt half-hip pony truss, the bridge dates to 1909. In May of that year the Putnam County Court authorized its construction and contracted with the Canton Bridge Company of Ohio to build the small-scale truss. Completed the same year, the bridge today features a timber deck and fairly new steel pile bent piers with timber wingwalls. The Demery Bridge has carried intermittent vehicular traffic in essentially unaltered condition since its construction.

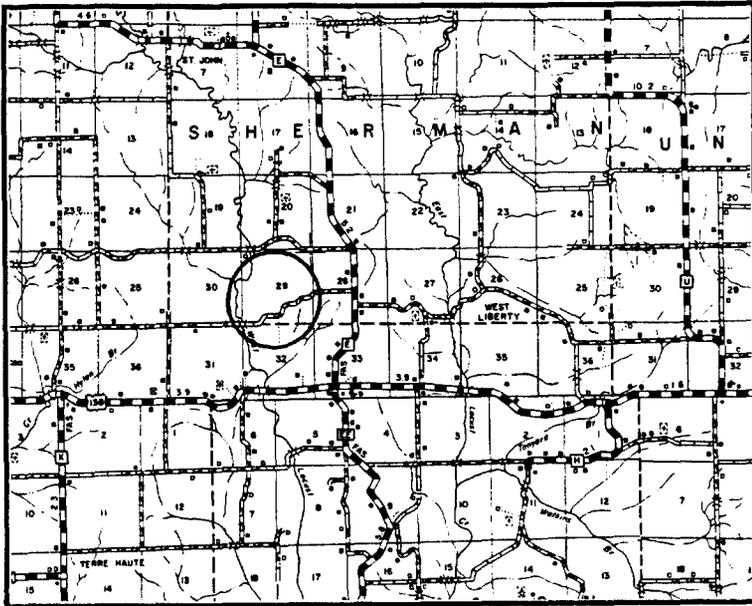
The Demery Bridge typifies the type of small-scale pinned truss that was widely built throughout Missouri in the early 20th century. With modest dimensions, typical design and detailing and substructural replacement, it lacks any technological or historical significance.

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**NAME(S) OF STRUCTURE**

Demery Bridge (West Locust Creek Bridge)

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 070000.5; Putnam County Court Record, Book N: pages 111-12 (22 May 1909), located at Putnam County Courthouse, Unionville MO; field inspection by Clayton Fraser, 11 July 1992.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

28 July 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Montgomery Bridge (East Locust Creek Bridge)  
MHTD: 085002.0

PUTN06

**DATE(S) OF CONSTRUCTION**

1908

**LOCATION**

County Road 85 over East Locust Creek; S3, T66/67N, R20W  
9.0 miles northwest of Unionville; Putnam County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 39)

**CONDITION**

fair

**OWNER**

Putnam County

span number:	1	superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss
span length:	48.0'	substructure:	steel pile bent abutments and piers, with timber back- and wingwalls
total length:	63.0'	floor/decking:	timber deck over steel stringers
roadway wdt.:	13.9'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 round eyerods, 2 square eyebars; vertical: 4 angles with double lacing; diagonal: 2 looped square eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; hip-mounted builder's plate: THE CANTON BRIDGE COMPANY, CANTON OHIO

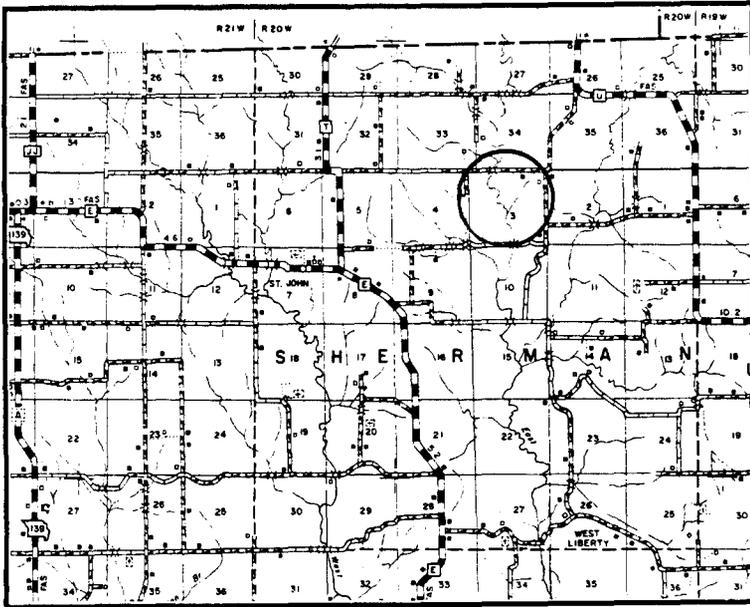
This bridge carries a county road over the East Locust Creek about 9 miles northwest of Unionville. The timber-decked structure is comprised of a pinned Pratt half-hip pony truss, with a steel stringer approach span on one end. Named after adjacent landowner Gail Montgomery, the Montgomery Bridge dates to late 1906. That December the Putnam County Bridge Commissioner was directed by the county court to advertise for competitive bids to build this structure and another small-scale truss in Sherman Township. Local contractor George Nulton was hired to build the Montgomery Bridge for \$546.00 in February 1907. For some reason, the bridge remained unbuilt for more than a year, however, until the county purchased a pinned truss superstructure in April 1908 from the Canton Bridge Company for \$624.00. The Montgomery Bridge was completed later that year. It has carried local traffic since, in essentially unaltered condition.

The pinned Pratt half-hip pony truss was used extensively on Missouri's county roads in the late 19th and early 20th centuries. Thousands of small-scale examples of this ubiquitous truss type were built during this period, and numerous examples remain in service today. The Montgomery Bridge is a typically configured, technologically undistinguished example of this widespread bridge building trend.

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**NAME(S) OF STRUCTURE**

Montgomery Bridge (East Locust Creek Bridge)

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 085002.0; Putnam County Court Record, Book N: page 305 (31 December 1906), page 523 (10 April 1908), page 553 (3 August 1908) - located at Putnam County Courthouse, Unionville MO; field inspection by Clayton Fraser, 11 July 1992.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

28 July 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

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**NAME(S) OF STRUCTURE**

Shadden Bridge (Shoal Creek Bridge)  
MHTD: 181001.0

PUTN10

**DATE(S) OF CONSTRUCTION**

1909

**LOCATION**

County Road 181 over Shoal Creek; S32, T66/67N, R17W  
10.1 miles northeast of Unionville; Putnam County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 44)

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**CONDITION**

fair

**OWNER**

Putnam County

span number: 1  
span length: 60.0'  
total length: 60.0'  
roadway wdt.: 11.8'

superstructure: steel, 4-panel, pin-connected Pratt pony truss  
substructure: steel pile bent piers, with timber back- and wingwalls  
floor/decking: concrete deck over steel stringers  
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 square eyebars; counter: eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice

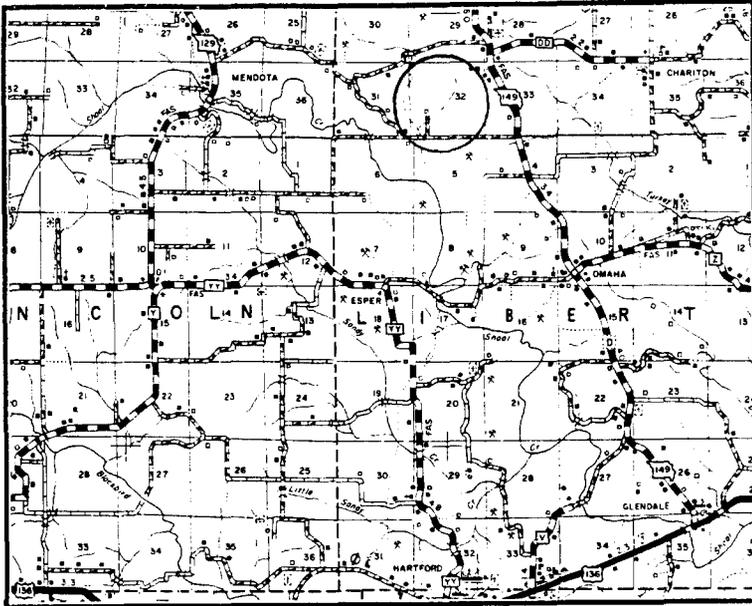
In February 1909 the Putnam County Court ordered the construction of the Shadden Bridge, named after adjacent landowner Jonas Shadden. That April the county purchased seven steel superstructures from the Jacksonville-based Illinois Steel Bridge Company for an aggregate cost of \$4675.00. Included in the acquisition was the pin-connected Pratt pony truss Shadden Bridge. Spanning Shoal Creek some ten miles northeast of Unionville, the timber/steel structure has lattice guardrails and relatively new steel pile bent abutments. This bridge today appears largely unaltered and possesses good integrity: a typically configured example of a common structural type.

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**NAME(S) OF STRUCTURE**

Shadden Bridge (Shoal Creek Bridge)

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 181001.0; Putnam County Court Record, Book N: page 430 (9 August 1907), page 43 (2 March 1909), page 67 (9 April 1909), page 153 (3 November 1909) - located at Putnam County Courthouse, Unionville MO; field inspection by Clayton Fraser, 11 July 1992.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

28 July 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Bradshaw Bridge (North Blackbird Creek Bridge)  
MHTD: 253001.5

PUTN14

**DATE(S) OF CONSTRUCTION**

1909

**LOCATION**

County Road 253 over North Blackbird Creek; S18, T65N, R17W  
9.2 miles southeast of Unionville; Putnam County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 43)

**CONDITION**

fair

**OWNER**

Putnam County

span number: 1	superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss, with steel stringer approach spans
span length: 48.0'	substructure: steel pile bent abutments and piers, with timber back- and wingwalls
total length: 111.0'	floor/decking: timber deck over steel stringers
roadway wdt.: 11.6'	other features: upper chord and inclined end post: 2 channels with cover plates and lacing; lower chord: 2 round eyerods, 2 square eyebars; vertical: 4 angles with lacing; diagonal: 2 square eyerods; counter: square eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice

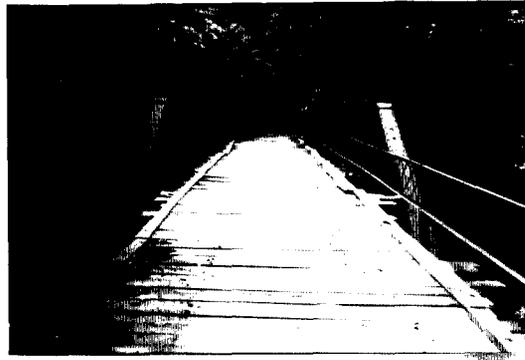
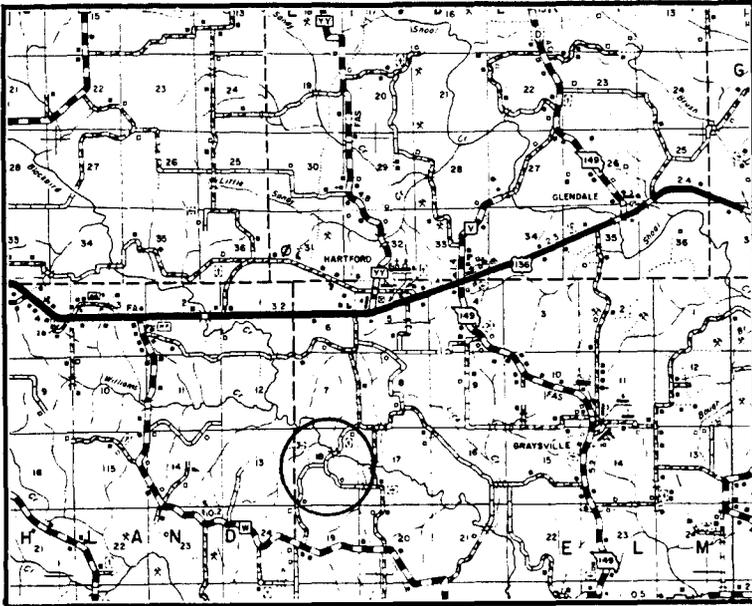
Located a little more than nine miles southeast of Unionville over North Blackbird Creek, the Bradshaw Bridge was ordered built by the Putnam County Court in April 1909. A month later the county contracted with Midland Bridge Company of Kansas City to fabricate and erect the single-span truss; construction was completed later that year. The structure is a pinned Pratt half-hip pony truss, approached on both sides by steel stringer spans and supported by original steel pile bent piers. Named after W.T. Bradshaw, who owned an adjacent farm, the Bradshaw Bridge has carried traffic since its completion, in essentially unaltered condition. Although well preserved, it is a technologically undistinguished example of a relatively common structural type.

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**NAME(S) OF STRUCTURE**

Bradshaw Bridge (North Blackbird Creek Bridge)

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 253001.5; Putnam County Court Record, Book N: page 68 (9 April 1909), pages 111-112 (22 May 1909) - located at Putnam County Courthouse, Unionville MO; field inspection by Clayton Fraser, 11 July 1992.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

28 July 1992

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# RANDOLPH COUNTY

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**INCLUDED:** [Significant feature(s) of bridge given in boldface]  
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
*RAND01	057001.8	Hoover Creek Bridge	1- 40' pinned Pratt bedstead c1905 Canton Bridge Company
*RAND02	064002.1	Mud Creek Bridge	1- 54' pinned Pratt half hip pony truss c1905 Canton Bridge Company
*RAND03	065000.4	Bond Bridge	1- 60' pinned Pratt bedstead 1910 Pan American Bridge Company
*RAND04	090000.9	Mud Creek Bridge	1- 42' pinned Pratt bedstead c1910 Pan American Br. Co. (prob.)
*RAND05	149000.7	Elk Fork Bridge	1- 60' pinned Pratt bedstead c1910 Pan American Br. Co. (prob.)
RAND06	194001.2	Dark Creek Bridge	(replaced)
RAND07	206000.4	Chariton River Bridge	1- 60' pinned Pratt bedstead c1905 Canton Bridge Co. (prob.)
RAND08	237000.2	Jacoby Bridge	(replaced)
RAND09	343001.0	Sugar Creek Bridge	(replaced)
RAND10	363001.3	Renick Bridge	(replaced)

**EXCLUDED:**

Warren pony truss

X 447 057000.4 083001.5 213001.9

Warren bedstead

116002.2 382000.8

Lattice bedstead

085000.4 176000.8 265000.2 403000.2

Steel stringer

F 403R1 F 529R1 J 402 J 727 P 23 S 588 X 105

Concrete slab / girder

K 477A K 517 L 118R 162002.4 299000.5

Concrete box culvert

F 12R H 833R P 22 S 589 T 388 T 487 T 488  
 X 107 X 754 054001.2

Timber stringer

357001.0 367002.0 379000.1

# RANDOLPH COUNTY

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## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	0	6	0	0	6
Excluded	20	15	0	0	35
<hr/>					
	20	21	0	0	41 structures

# Hoover Creek Bridge

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RAND01

## GENERAL DATA

<b>structure no.:</b> 057001.8	<b>city/town:</b> 11.5 miles northeast of Huntsville
<b>county:</b> Randolph	<b>feature inters.:</b> Hoover Creek
	<b>cadastral grid:</b> S1, T55N, R14W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Randolph County

## STRUCTURAL DATA

**superstructure:** steel, 3-panel, pin-connected Pratt truss leg bedstead  
**substructure:** truss legs with timber back- and wingwalls

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 40.0'	<b>alterations:</b> none
<b>total length:</b> 41.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 11.5'	<b>other features:</b> upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plates / 2 square, looped rectangular eyebars; vertical: 4 angles with double lacing; diagonal: 2 looped square eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: steel lattice

## HISTORICAL DATA

**erection date:** c1905  
**erection cost:** unknown  
**designer:** Canton Bridge Company, Canton OH  
**fabricator :** Canton Bridge Company, Canton OH;  
Cambria Steel Company, Pittsburgh PA  
**contractor:** Canton Bridge Company, Canton OH  
**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 057001.8; field inspection by Richard Collier, 7 June 1991.  
**sign. rating:** 28  
**evaluation:** NRHP non-eligible (typically configured, inadequately documented example of common structural type)

**inventoried by:** Michelle Crow-Dolby 30 July 1993

# Mud Creek Bridge

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RAND02

## GENERAL DATA

structure no.:	064002.1	city/town:	11.7 miles northeast of Huntsville
county:	Randolph	feature inters.:	Mud Creek
		cadastral grid:	S19/20, T55N, R13W
		highway route:	County Road 64
		highway distr.:	2
		current owner:	Randolph County

## STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach span

substructure: concrete abutments and wingwalls, with steel pile bent pier

span number:	1	condition:	fair
span length:	54.0'	alterations:	none
total length:	70.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.6'	other features:	upper chord and inclined end post: 2 channels with cover plates and lacing; lower chord: 2 looped square eyebars; vertical: 4 angles with double lacing; diagonal: 2 looped square eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: steel lattice; end-post-mounted builder's plate: THE CANTON BRIDGE COMPANY, CANTON OHIO

## HISTORICAL DATA

erection date: c1905

erection cost: unknown

designer: Canton Bridge Company, Canton OH

fabricator : Canton Bridge Company, Canton OH;  
Cambria Steel Company, Pittsburgh PA

contractor: Canton Bridge Company, Canton OH

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 064002.1; field inspection by Richard Collier, 7 June 1991.

sign. rating: 30

evaluation: NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby 30 July 1993

# Bond Bridge

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RAND03

## GENERAL DATA

<b>structure no.:</b> 065000.4	<b>city/town:</b> 12.3 miles northeast of Huntsville
<b>county:</b> Randolph	<b>feature inters.:</b> Mud Creek
	<b>cadastral grid:</b> S17, T55N, R15W
	<b>highway route:</b> County Road 65
	<b>highway distr.:</b> 2
	<b>current owner:</b> Randolph County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected truss leg bedstead, with steel stringer approach spans	
<b>substructure:</b> concrete abutments and wingwalls; steel-pile bent piers; truss legs	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 60.0'	<b>alterations:</b> none
<b>total length:</b> 107.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 11.5'	<b>other features:</b> upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plates / 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: looped square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: steel lattice

## HISTORICAL DATA

<b>erection date:</b> 1910	
<b>erection cost:</b> \$1080.00	
<b>designer:</b> Pan American Bridge Company	
<b>fabricator :</b> Lackawanna Steel Company, Pittsburgh PA	
<b>contractor:</b> Pan American Bridge Company	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 065000.4; Randolph County Court Record, Book M, page 530 (12 September 1910), page 588 (22 May 1911), located at the Randolph County Courthouse, Huntsville MO; field inspection by Richard Collier, 7 June 1991.
<b>sign. rating:</b> 38	
<b>evaluation:</b>	NRHP non-eligible (typical example of common structural type)

**inventoried by:** Michelle Crow-Dolby 30 July 1993

# Mud Creek Bridge

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RAND04

## GENERAL DATA

structure no.:	090000.9	city/town:	12.0 miles northeast of Huntsville
county:	Randolph	feature inters.:	Mud Creek
		cadastral grid:	S17, T55N, R13W
		highway route:	County Road 90
		highway distr.:	2
		current owner:	Randolph County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected truss leg bedstead, with steel stringer approach span		
substructure:	steel truss legs with stone abutments		
span number:	1	condition:	fair
span length:	42.0'	alterations:	none
total length:	55.0'	floor/decking :	timber deck over steel stringers
roadway width:	10.5'	other features:	upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plates / 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guard-rail: steel lattice

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Pan American Bridge Company (probable)
fabricator :	unknown
contractor :	Pan American Bridge Company (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 090000.9; field inspection by Richard Collier, 7 June 1991.
sign. rating:	27
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 30 July 1993

# Elk Fork Bridge

RAND05

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## GENERAL DATA

structure no.:	149000.7	city/town:	10.1 miles northeast of Huntsville
county:	Randolph	feature inters.:	Elk Fork
		cadastral grid:	S15, T54N, R13W
		highway route:	County Road 149
		highway distr.:	2
		current owner:	Randolph County

## STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected truss leg bedstead, with steel stringer approach spans

substructure: steel truss legs with timber pile bent abutments and wingwalls

span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	81.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.6'	other features:	upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 looped square eye-bars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: steel lattice

## HISTORICAL DATA

erection date: c1910

erection cost: unknown

designer: Pan American Bridge Company (probable)

fabricator : Lackawanna Steel Company, Pittsburgh PA

contractor: Pan American Bridge Company (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 149000.7; field inspection by Richard Collier, 7 June 1991.

sign. rating: 30

evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 30 July 1993

# Chariton River Bridge

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RAND07

## GENERAL DATA

<b>structure no.:</b> 206000.4	<b>city/town:</b> 8.5 miles northwest of Huntsville
<b>county:</b> Randolph	<b>feature inters.:</b> Chariton River
	<b>cadastral grid:</b> S15, T54N, R16W
	<b>highway route:</b> County Road 206
	<b>highway distr.:</b> 2
	<b>current owner:</b> Randolph County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt bedstead with steel stringer approach spans	
<b>substructure:</b> steel truss leg piers with timber pile bent abutments	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 60.0'	<b>alterations:</b> unknown
<b>total length:</b> 90.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.6'	<b>other features:</b> steel lattice guardrails

## HISTORICAL DATA

<b>erection date:</b> c1905	
<b>erection cost:</b> unknown	
<b>designer:</b> Canton Bridge Company, Canton OH (probable)	
<b>fabricator :</b> Canton Bridge Company, Canton OH (probable)	
<b>contractor:</b> Canton Bridge Company, Canton OH (probable)	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 206000.4.	
<b>sign. rating:</b> 29	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)	

**inventoried by:** Michelle Crow-Dolby    30 July 1993

# SALINE COUNTY

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INCLUDED: [Significant feature(s) of bridge given in boldface]  
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
*SALI01	G 69R	Glasgow Bridge	5-343' <b>riveted Pennsylv. through truss</b> 1925 Mt. Vernon Bridge Company
SALI02	G 518R2	Blackwater River Bridge	3-115' continuous steel plate deck girder 1948 C.E. Maxwell and Son
*SALI03	K 999R	McDaniel Memorial Bridge	7-475' <b>riveted cantilever through truss</b> 1939 Massman Construction Company
SALI04	025000.9	Muddy Creek Bridge	1- 35' steel plate deck girder 1910 county work force
SALI05	027001.6	Muddy Creek Bridge	1- 35' steel plate deck girder 1907 county work force
SALI06	040001.8	Salt Fork Bridge	1- 80' <b>riveted Pratt through truss</b> 1932 Kansas City Bridge Co.
*SALI07	041000.0	Muddy Creek Bridge	1- 36' pinned Pratt pony truss c1915
*SALI08	065000.7	Salt Branch Bridge	1- 25' pinned Pratt bedstead 1895 Massillon Bridge Company
*SALI09	084000.4	Pass Branch Bridge	1- 60' pinned Pratt half-hip pony truss 1913 county work force (prob.)
SALI10	129001.5	Finney Creek Bridge	1- 40' pinned Pratt pony truss c1900
SALI11	142001.0	Finney Creek Bridge	1- 30' steel plate deck girder 1911 Illinois Steel Bridge Co. (prob.)
SALI12	146000.3	Finney Creek Bridge	1- 45' pinned Pratt bedstead 1901 A.M. Blodgett, Kansas City
SALI13	150000.5	Dicks Branch Bridge	1- 30' steel plate deck girder 1912 Kansas City Bridge Company
SALI14	151001.5	Dicks Branch Bridge	1- 30' pinned Pratt bedstead 1900 Stupp Brothers B&I Company
SALI15	152000.8	Dicks Branch Bridge	1- 80' riveted Pratt pony truss 1911 Kansas City Bridge Company
*SALI16	165002.3	Salt Fork Bridge	1-120' <b>pinned Pratt through truss</b> 1899 Central Bridge Co., St. Louis
SALI17	172000.5	Rock Creek Bridge	1- 27' steel stringer 1919 Kansas City Bridge Company
SALI18	178001.0	Rock Creek Bridge	(replaced)
*SALI19	189000.3	West Cow Creek Bridge	1- 45' pinned Pratt bedstead 1903 Kansas City Bridge Company
*SALI20	217001.1	Edmonson Creek Bridge	1- 45' pinned Pratt half-hip pony truss 1908 Illinois Steel Bridge Company
*SALI21	218000.5	Salt Branch Bridge	(replaced)
*SALI22	245000.4	Rock Creek Bridge	1- 30' <b>pin/rivet Kingpost pony truss</b> 1901 Stupp Brothers B&I Company
*SALI23	263000.8	Bear Creek Bridge	1- 68' pinned Pratt half-hip pony truss 1907 county work force
SALI24	281001.0	Fish Creek Bridge	(replaced)

# SALINE COUNTY

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## INCLUDED (cont.):

SALI25	299000.7	Fish Creek Bridge	1- 35' 1910	steel plate through girder county work force (replaced)
*SALI26	309001.0	Camp Creek Bridge		
SALI27	311001.0	Camp Creek Bridge	1- 60' c1910	pinned Pratt pony truss  (replaced)
*SALI28	322000.3	Pierre Fleche Creek Bridge		
*SALI29	325002.1	Pierre Fleche Creek Bridge	1- 70' 1891	pinned Pratt pony truss Massillon Bridge Company
*SALI30	327000.4	Pierre Fleche Creek Bridge	1- 60' 1915	pinned Pratt half-hip pony truss county work force
*SALI31	351000.5	Steel's Fish Trap Bridge	1-100' 1875 1882	bowstring through arch-truss W.L. Smiley Missouri Valley B&I Company
SALI32	353001.0	Napton Bridge	1-100' 1912	riveted Pratt pony truss Kansas City Bridge Company
*SALI33	366000.4	Scott & Cooney Ford Bridge	1-120' 1910	pinned Pratt through truss Illinois Steel Bridge Company
*SALI34	368002.5	Boat Yard Ford Bridge	1-150' 1888	pinned Pratt through truss Kansas City B&I Company
*SALI35	369001.0	Nelson Bridge	1-150' 1929	riveted Pratt through truss Kansas City Bridge Company (replaced)
*SALI36	388000.2	Blackwater River Bridge		
*SALI37	390000.8	Finney Creek Bridge	1- 80' 1895	pinned Pratt through truss Wrought Iron Bridge Company
SALI38	391001.0	Buck Branch Bridge	1- 30' c1900	pinned Pratt bedstead
*SALI39	408001.0	Blackwater River Bridge	1-100' c1900	pinned Pratt through truss
*SALI40	408001.5	Blackwater River Bridge	1- 80' c1930	riveted Pratt through truss
SALI41	439001.5	Sweet Springs Bridge	1-140' 1929	riveted Pratt through truss Kansas City Bridge Company
*SALI42	476001.9	Blackwater River Bridge	2-120' 1924	riveted Pratt through truss C.T. Fogle
SALI43	477000.1	Eastwood Road Overpass	3- 35' 1923	concrete through girder Missouri Pacific Railroad
SALI44	477000.2	Salt Fork Bridge	3- 90' 1923	concrete filled spandrel arch M.E. Gillioz

## EXCLUDED:

Pratt pony truss

034000.3 064000.4 072000.7 184000.8 192000.2

Warren pony truss

103001.6 276001.0 277001.5 278001.5

# SALINE COUNTY

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## EXCLUDED (cont.):

### Steel stringer

F 105R3	G 517R2	S 803	T 68	T 69	T 70	T 576
T 708	T 975	X 426	050000.4	060001.4	064R00.6	076001.5
078001.5	078003.0	079000.1	084001.8	087001.5	095003.0	103R01.4
105002.0	182000.5	199001.3	249000.1	253001.0	257R00.6	264R00.5
268001.0	276000.2	294000.1	297R01.8	328001.6	407000.7	419000.8
420000.5	429R00.6	432000.5	436000.4	442R00.9	448000.1	451001.2
463001.0	474001.8					

### Steel girder

187001.2	208001.2	248000.8	253001.5	294000.1	343001.0	349001.3
419500.1						

### Concrete girder

G 524R	G 525R	H 920R	J 41	U267500.1	166000.8	
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### Concrete slab

G 131R1	J 120	087001.0	153000.8	163000.9	475000.8	476001.5
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### Concrete box culvert

G 560	G 747	J 42	L 315	S 67R	X 645	203R01.5
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### Timber stringer

368002.9

## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	3	34	0	0	36
Excluded	22	59	1	0	82
	25	93	1	0	119 structures

# Glasgow Bridge

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SALI01

## GENERAL DATA

structure no.:	G 69R	city/town:	Glasgow
county:	Saline/Howard	feature inters.:	Missouri River
		cadastral grid:	S2/3, T51N, R18W
		highway route:	State Highway 240
		highway distr.:	2
		current owner:	Missouri Highway and Transportation Department

## STRUCTURAL DATA

**superstructure:** steel, 10-, 12- and 14-panel, rigid-connected Pennsylvania through trusses; rigid-connected Pratt deck truss approach spans

**substructure:** concrete abutments; concrete bullnose and spill-through piers

span number:	5	condition:	good
span length:	343.0'	alterations:	deck replaced, 1986
total length:	2244.0'	floor/decking :	concrete over metal plate deck
roadway width:	20.3'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates and lacing; vertical: 4 angles with batten plates and lacing; diagonal: 2 channels with batten plates and lacing; lateral bracing: 1 angle - lower, 4 angles with lacing - upper; strut: 4 angles with lattice and lacing; portal strut; 4 angles with lacing (W-frame); floor beam: I-beam; bridge plate: Glasgow Bridge / Financed By / Howard County, Saline County / Federal Aid / Appropriated By / Missouri State Highway Commission / Designed By F.W. Adgate / Consulting Engineer / Chicago, Ill. / Supervised By / Missouri State Highway Department / B.H. Piepmeier, Chief Engineer / L.J. Sverdrup, Bridge Engineer / Contractors / Substructure / Union Bridge & Const. Co. / Kansas City, Mo / Superstructure / Mt. Vernon Bridge Co. / Mt. Vernon, Ohio / Completed 1925

## HISTORICAL DATA

erection date:	1922-25
erection cost:	\$607,970.72
designer:	F.W. Adgate, Chicago IL
fabricator :	Carnegie Steel Company, Pittsburgh PA
contractor :	Mt. Vernon Bridge Company, Mt. Vernon OH (superstructure); Union Bridge and Construction Company, Kansas City MO (substructure)

## Glasgow Bridge

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**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. G 69R; Missouri Highway and Transportation Department, Primary System Bridge Record, Book 2 (see entries for Saline County), on file at MHTD, Jefferson City, MO.; Saline County Court, Book 1: page 146 (2 June 1919) - located at Saline County Courthouse, Marshall MO; H.J. Kesner, "Some Recent Missouri Bridges," *The Nebraska Blue Print*, May 1929, pages 9-10; "Photo Flashback," *The Slater News-Ruslter*, 2 July 1985; field inspection by Clayton Fraser, 2 October 1991.

**sign. rating:** 70

**evaluation:** NRHP eligible (excellent, large-scale highway truss)

**Inventoried by:** Clayton B. Fraser 24 January 1992

# Blackwater River Bridge

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SALI02

## GENERAL DATA

structure no.:	G 518R2	city/town:	10.8 miles south of Marshall
county:	Saline	feature inters.:	Blackwater River
		cadastral grid:	S4, T48N, R21W
		highway route:	U.S. Highway 40
		highway distr.:	2
		current owner:	Missouri Highway and Transportation Department

## STRUCTURAL DATA

superstructure:	continuous steel plate deck girder		
substructure:	concrete abutments and wingwalls		
span number:	1; 2	condition:	good
span length:	115.0'; 92.0'	alterations:	superstructure and substructure widened, deck and guardrails replaced, 1976
total length:	723.0'	floor/decking :	concrete deck
roadway width:	38.0'	other features:	concrete guardrails with steel pipe

## HISTORICAL DATA

erection date:	1948
erection cost:	\$247,065.05
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	C.E. Maxwell and Son
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. G 518R2; files on Primary System Bridges - located at the Missouri Highway and Transportation Department, Jefferson City MO.
sign. rating:	36
evaluation:	NRHP non-eligible (long-span example of beam bridge construction, substantially altered)

inventoried by: Clayton B. Fraser 24 January 1992

# McDaniel Memorial Bridge

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SALI03

## GENERAL DATA

structure no.:	K 999R	city/town:	Miami
county:	Saline / Carroll	feature inters.:	Missouri River
		cadastral grid:	S11, T52N, R21W
		highway route:	State Highway 41
		highway distr.:	2
		current owner:	Missouri Highway and Transportation Department

## STRUCTURAL DATA

**superstructure:** steel, rigid-connected cantilever through truss spans; five steel stringer approach spans; three deck truss approach spans

**substructure:** concrete abutments and piers

span number:	7	condition:	good
span length:	475.0'	alterations:	deck replaced, 1983
total length:	2072.0'	floor/decking :	concrete deck over steel stringers
roadway width:	23.0'	other features:	upper chord, lower chord, and inclined end posts: 2 built-up channels with cover plate and double lacing; vertical: 2 channels with lacing, 4 angles with batten plates; diagonal: 2 channels with double lacing, 4 angles with batten plates; lateral bracing: 2 angles with lacing; strut: 4 angles with lacing and braces; floor beam: steel I-beam; guardrail: 2 channels; upper builder's plate text: Federal Emergency Administration of Public Works; Franklin D. Roosevelt, President of the United States; Harold L. Ickes, Administrator of Public Works; McDaniel Memorial Bridge; 1939; lower builder's plate text: McDaniel Memorial Bridge; built by Saline County Missouri with the cooperation of Missouri State Highway Commission; county court of Saline County, Robert L. Hyatt - presiding judge, T. M. Smith - judge, O. Boyd Davis - judge; Sverdrup and Parcel, consulting engineers; Massman Construction Co., general contractors; 1939

## HISTORICAL DATA

erection date:	1939
erection cost:	\$771,517.00
designer:	Sverdrup and Parcel, Consulting Engineers, St. Louis MO
fabricator :	Inland Steel Company, East Chicago IN
contractor:	Massman Construction Company, Kansas City MO

## McDaniel Memorial Bridge

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**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal (Structure Number K-999R); Carroll County Court Record, Book 5: pages 527-539, 595 (5 July 1938 - 20 October 1938); Book 6: pages 1-16 (December 1938) - located at Carroll County Courthouse, Carrollton MO; **History of Carroll County** (Carrollton MO: Carroll County Historical Society, 1968), page 29; **Bridge Over Missouri River at Miami, Missouri**, P.W.A. Docket Missouri 1075-F, Contract No. 2, Saline County, Missouri (St. Louis MO: Sverdrup and Parcel, Consulting Engineers, no date); **Bridges, Missouri River: Data, History and Laws**, U.S. Engineer Office, Kansas City, Missouri, 1933; field inspection by Clayton Fraser, 9 October 1991.

**sign. rating:** 72

**evaluation:** NRHP eligible (outstanding, large-scale example of steel truss construction, located on important great river crossing)

**inventoried by:** Clayton B. Fraser 14 October 1992

# Muddy Creek Bridge

SALI04

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## GENERAL DATA

structure no.:	025000.9	city/town:	3.8 miles south of Miami
county:	Saline	feature inters.:	Muddy Creek
		cadastral grid:	S20/29, T52N, R21W
		highway route:	County Road 25
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel plate deck girder		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	35.0'	alterations:	none
total length:	36.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.9'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1910
erection cost:	unknown
designer:	Saline County Engineer
fabricator :	Kansas City Bridge Company, Kansas City MO (probable)
contractor:	county work force
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 025000.9; Saline County Court Record, Book X: page 233 (6 July 1910) - located at Saline County Courthouse, Marshall MO.
sign. rating:	47
evaluation:	NRHP possibly eligible (well-preserved example of little-used beam bridge type)

inventoried by: Clayton B. Fraser

24 January 1992

# Muddy Creek Bridge

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SALIO5

## GENERAL DATA

<b>structure no.:</b> 027001.6	<b>city/town:</b> 7.4 miles northwest of Marshall
<b>county:</b> Saline	<b>feature inters.:</b> branch of Muddy Creek
	<b>cadastral grid:</b> S7/12, T51N, R21/22W
	<b>highway route:</b> County Road 27
	<b>highway distr.:</b> 2
	<b>current owner:</b> Saline County

## STRUCTURAL DATA

<b>superstructure:</b> steel plate deck girder	
<b>substructure:</b> concrete abutments and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 35.0'	<b>alterations:</b> none
<b>total length:</b> 36.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 13.8'	<b>other features:</b> steel angle guardrails

## HISTORICAL DATA

<b>erection date:</b> 1907	
<b>erection cost:</b> unknown	
<b>designer:</b> Saline County Engineer	
<b>fabricator :</b> Kansas City Bridge Company, Kansas City MO (probable)	
<b>contractor:</b> county work force (probable)	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 027001.6; Saline County Court Record, Book W: page 298 (4 September 1907) -located at Saline County Courthouse, Marshall MO.	
<b>sign. rating:</b> 47	
<b>evaluation:</b> NRHP possibly eligible (well-preserved example of little-used beam bridge type)	

inventoried by: Clayton B. Fraser    24 January 1992

# Salt Fork Bridge

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SALI06

## GENERAL DATA

structure no.:	040001.8	city/town:	6.5 miles northwest of Marshall
county:	Saline	feature inters.:	Salt Fork
		cadastral grid:	S23, T51N, R22W
		highway route:	County Road 40
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, rigid-connected Pratt through truss, with steel stringer approach spans		
substructure:	concrete abutments, wingwalls and piers		
span number:	1	condition:	fair
span length:	80.0'	alterations:	unknown
total length:	114.0'	floor/decking :	concrete deck over steel stringers
roadway width:	16.5'	other features:	steel guardrails

## HISTORICAL DATA

erection date:	1932
erection cost:	unknown
designer:	Kansas City Bridge Company, Kansas City MO
fabricator :	Kansas City Bridge Company, Kansas City MO
contractor :	Kansas City Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 040001.8; Saline County Court contract with Kansas City Bridge Company, "Specifications for Steel Bridge" (December 1931) - located at the Saline County Courthouse, Marshall MO.
sign. rating:	35
evaluation:	NRHP non-eligible (typically configured, relatively late example of common structural type)

Inventoried by: Clayton B. Fraser    24 January 1992

# Muddy Creek Bridge

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SALIO7

## GENERAL DATA

structure no.:	041000.0	city/town:	6.6 miles northwest of Marshall
county:	Saline	feature inters.:	Muddy Creek
		cadastral grid:	S23/24, T51N, R22W
		highway route:	County Road 41
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach span		
substructure:	concrete abutments, wingwalls and piers		
span number:	1	condition:	poor
span length:	36.0'	alterations:	truss moved to site; deck removed and bridge closed
total length:	59.0'		
roadway width:	14.5'	floor/decking :	removed
		other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 041000.0; field inspection by Clayton Fraser, 2 October 1991.

sign. rating:	20
evaluation:	NRHP non-eligible (poorly preserved example of common structural type)

inventoried by: Clayton B. Fraser    24 January 1992

# Salt Branch Bridge

SAL108

## GENERAL DATA

structure no.:	065000.7	city/town:	7.2 miles west of Marshall
county:	Saline	feature inters.:	Salt Branch
		cadastral grid:	S9/16, T50N, R22W
		highway route:	County Road 65
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

**superstructure:** steel, 2-panel, pin-connected Pratt truss-leg bedstead, with steel stringer approach spans

**substructure:** timber abutments with steel truss-leg piers

span number:	1	condition:	fair
span length:	50.0'	alterations:	none
total length:	50.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.5'	other features:	upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 channels with batten plates; vertical: 2 channels with lacing; diagonal: 2 square eyebars; lateral bracing: 2 round rods with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles; builder's plate: THE MASSILLON BRIDGE COMPANY / OHIO / 1895

## HISTORICAL DATA

**erection date:** 1895

**erection cost:** \$427.00

**designer:** Massillon Bridge Company, Massillon OH

**fabricator :** Massillon Bridge Company, Massillon OH

**contractor :** Massillon Bridge Company, Massillon OH

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 065000.7; Saline County Court Record, Book T: page 408 (3 March 1896) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.

**sign. rating:** 47

**evaluation:** NRHP possibly eligible (well-preserved, small-scale example of a relatively common structural type)

inventoried by: Clayton B. Fraser 24 January 1992

# Pass Branch Bridge

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SALI09

## GENERAL DATA

structure no.:	084000.4	city/town:	5.7 miles northeast of Blackburn
county:	Saline	feature inters.:	Pass Branch
		cadastral grid:	S3, T50N, R23W
		highway route:	County Road 84
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt half-hip pony truss  
substructure: timber pile bent abutments

span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	60.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.8'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars or 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: 1 round eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to the vertical; guardrail: 2 angles

## HISTORICAL DATA

erection date: 1913  
erection cost: \$1850.00 (engineer's estimate)  
designer: Kansas City Bridge Company, Kansas City MO (probable)  
fabricator : Kansas City Bridge Company, Kansas City MO (probable);  
Illinois Steel Company, Chicago IL  
contractor : county work force (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 084000.4; Saline County Court Record, Book T: page 447 (7 May 1899) - located at the Saline County Courthouse, Marshall Mo; field inspection by Clayton Fraser, 2 October 1991.

sign. rating: 44  
evaluation: NRHP non-eligible (well-preserved, structurally unremarkable example of common structural type)

inventoried by: Clayton B. Fraser 24 January 1992

# Finney Creek Bridge

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SALI10

## GENERAL DATA

structure no.:	129001.5	city/town:	5.9 miles southwest of Marshall
county:	Saline	feature inters.:	West Fork of Finney Creek
		cadastral grid:	S1, T49N, R21/22W
		highway route:	County Road 129
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	stone masonry abutments		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	40.0'	floor/decking :	timber deck
roadway width:	12.3'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	c1900
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 129001.5.

sign. rating:	26
evaluation:	NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Clayton B. Fraser 24 January 1992

# Finney Creek Bridge

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SALI11

## GENERAL DATA

structure no.:	142001.0	city/town:	4.6 miles south of Marshall
county:	Saline	feature inters.:	North Fork of Finney Creek
		cadastral grid:	S4, T49N, R21W
		highway route:	County Road 142
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel plate deck girder		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	30.0'	alterations:	unknown
total length:	31.0'	floor/decking :	timber deck
roadway width:	14.1'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1911
erection cost:	unknown
designer:	Saline County Engineer
fabricator :	Illinois Steel Bridge Company, Jacksonville IL (probable)
contractor:	Illinois Steel Bridge Company, Jacksonville IL (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 142001.0; Index to Saline County Proceedings X: page 419 (4 May 1911); Index to Saline County Proceedings Y: page 603 (20 April 1915) - located at Saline County Courthouse, Marshall MO.
sign. rating:	47
evaluation:	NRHP possibly eligible (relatively early example of little-used beam bridge type)

inventoried by: Clayton B. Fraser    24 January 1992

# Finney Creek Bridge

SALI12

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## GENERAL DATA

structure no.:	146000.3	city/town:	2.6 miles south of Marshall
county:	Saline	feature inters.:	North Fork of Finney Creek
		cadastral grid:	S28, T50N, R21W
		highway route:	County Road 146
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt truss-leg bedstead		
substructure:	steel truss-leg piers with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	45.0'	alterations:	unknown
total length:	45.0'	floor/decking :	timber deck
roadway width:	12.0'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1901
erection cost:	unknown
designer:	A.M. Blodgett, Kansas City MO
fabricator :	unknown
contractor:	A.M. Blodgett, Kansas City MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 146000.3; Index to Saline County Proceedings U: pages 158 and 169 (7 January 1901), pages 204, 244 and 302 (6 May 1901) - located at Saline County Courthouse, Marshall MO.

sign. rating:	41
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Clayton B. Fraser    24 January 1992

# Dicks Branch Bridge

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SALI13

## GENERAL DATA

structure no.:	150000.5	city/town:	4.6 miles south of Marshall
county:	Saline	feature inters.:	Dicks Branch
		cadastral grid:	S1/2, T49N, R21W
		highway route:	County Road 150
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel plate deck girder		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	30.0'	alterations:	none
total length:	31.0'	floor/decking :	timber deck
roadway width:	12.2'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: 1912  
erection cost: \$675.00 (engineer's estimate)  
designer: Saline County Engineer  
fabricator : Kansas City Bridge Company, Kansas City MO  
contractor: Kansas City Bridge Company, Kansas City MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 150000.5; Saline County Court Record, Book Y: page 10 (4 April 1912); Index to Saline County Proceedings U: page 568 (8 April 1903) - located at Saline County Courthouse, Marshall MO.

sign. rating: 47  
evaluation: NRHP possibly eligible (relatively early example of little-used beam bridge type)

inventoried by: Clayton B. Fraser 24 January 1992

# Dicks Branch Bridge

SALI14

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## GENERAL DATA

structure no.:	151001.5	city/town:	4.7 miles south of Marshall
county:	Saline	feature inters.:	Dicks Branch
		cadastral grid:	S1, T49N, R21W
		highway route:	County Road 151
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt truss-leg bedstead, with steel stringer approach span		
substructure:	steel truss-leg piers, with timber pile abutments and backwalls		
span number:	1	condition:	fair
span length:	30.0'	alterations:	unknown
total length:	47.0'	floor/decking :	timber deck
roadway width:	10.9'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1900
erection cost:	unknown
designer:	Stupp Brothers Bridge and Iron Company, St. Louis MO
fabricator :	Stupp Brothers Bridge and Iron Company, St. Louis MO
contractor:	Stupp Brothers Bridge and Iron Company, St. Louis MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 151001.5; Saline County Court Record, Book U: page 63 (7 August 1900), page 82 (4 September 1900); Index to Saline County Proceedings U: page 63 (7 August 1900) - located at Saline County Courthouse, Marshall MO.
sign. rating:	41
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Clayton B. Fraser    24 January 1992

# Dicks Branch Bridge

SALI15

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## GENERAL DATA

structure no.:	152000.8	city/town:	4.8 miles southeast of Marshall
county:	Saline	feature inters.:	Dicks Branch
		cadastral grid:	S31, T50N, R20W
		highway route:	County Road 152
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, 5-panel, rigid-connected Pratt pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	80.0'	alterations:	none
total length:	82.0'	floor/decking :	concrete deck over steel stringers
roadway width:	13.3'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1911
erection cost:	\$1995.00 (superstructure cost)
designer:	Kansas City Bridge Company, Kansas City MO
fabricator :	Kansas City Bridge Company, Kansas City MO
contractor :	Kansas City Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 152000.8; Saline County Court Record, Book T: page 456 (7 June 1899); Saline County Court Record, Book X: page 419 (4 May 1911), page 422 (5 June 1911), page 479 (5 September 1911); Index to Saline County Proceedings X: pages 419-422 (4 May 1911) - located at Saline County Courthouse, Marshall MO.
sign. rating:	44
evaluation:	NRHP non-eligible (typical example of mainstay structural type)

inventoried by: Clayton B. Fraser 24 January 1992

# Salt Fork Bridge

SALI16

## GENERAL DATA

structure no.:	165002.3	city/town:	5.3 miles southeast of Marshall
county:	Saline	feature inters.:	Salt Fork
		cadastral grid:	S32/33, T50N, R20W
		highway route:	County Road 165
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

**superstructure:** steel, 8-panel, pin-connected Pratt through truss; 2 steel stringer approach spans

**substructure:** timber abutments and timber pile bent piers; concrete-filled steel cylinder piers

span number:	1	condition:	fair
span length:	120.0'	alterations:	none
total length:	165.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.5'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with double lacing (2 looped square eye-rods at the hip); diagonal: 2 looped rectangular eyebars; counter: 1 looped round eye-rod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beam, U-bolted to vertical; guardrail: 2 channels

## HISTORICAL DATA

**erection date:** 1899

**erection cost:** \$7600.00 (two-bridge contract)

**designer:** unknown

**fabricator :** Carnegie Steel Company, Pittsburgh PA

**contractor:** Central Bridge Company, St. Louis MO

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 165002.3; Saline County Court Record, Book T: page 470 (3 July 1899), page 506 (6 September 1899) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.

**sign. rating:** 51

**evaluation:** NRHP possibly eligible (typically configured, medium-span example of mainstay structural type)

**inventoried by:** Clayton B. Fraser 24 January 1992

# Rock Creek Bridge

SALI17

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## GENERAL DATA

<b>structure no.:</b> 172000.5	<b>city/town:</b> 4.5 miles east of Marshall
<b>county:</b> Saline	<b>feature inters.:</b> branch of Rock Creek
	<b>cadastral grid:</b> S4/5, T50N, R20W
	<b>highway route:</b> County Road 172
	<b>highway distr.:</b> 2
	<b>current owner:</b> Saline County

## STRUCTURAL DATA

<b>superstructure:</b> steel stringer	
<b>substructure:</b> concrete abutments and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 27.0'	<b>alterations:</b> unknown
<b>total length:</b> 28.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 14.0'	<b>other features:</b> steel guardrails

## HISTORICAL DATA

**erection date:** 1919  
**erection cost:** \$175.00 (engineer's estimate)  
**designer:** Saline County Engineer  
**fabricator :** Kansas City Bridge Company, Kansas City MO  
**contractor:** county work force

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 172000.5; Saline County Court Record, Book 1: page 147 (2 June 1919); Index to Saline County Proceedings U: pages 557 and 563 (10 March 1903) - located at Saline County Courthouse, Marshall MO.

**sign. rating:** 30  
**evaluation:** NRHP non-eligible (structurally undistinguished, small-scale example of exceedingly common structural type)

**inventoried by:** Clayton B. Fraser    24 January 1992

# West Cow Creek Bridge

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SALI19

## GENERAL DATA

structure no.:	189000.3	city/town:	5.9 miles north of Marshall
county:	Saline	feature inters.:	West Cow Creek
		cadastral grid:	S14, T51N, R21W
		highway route:	County Road 189
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt bedstead  
substructure: cut-off truss legs set in concrete abutments

span number:	1	condition:	poor
span length:	45.0'	alterations:	collision damage
total length:	53.0'	floor/decking :	timber deck over steel stringers
roadway width:	10.5'	other features:	upper chord: 2 channels with cover and batten plates; upright end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars, or 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 1 or 2 looped square eyebars; counter: 1 looped square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 1 angle; gusset plate at the hip; builder's plate: Kansas City Bridge Co Kansas City Mo

## HISTORICAL DATA

erection date: 1903  
erection cost: unknown  
designer: Kansas City Bridge Company, Kansas City MO  
fabricator : Kansas City Bridge Company, Kansas City MO; Cambria Steel Company, Pittsburgh PA  
contractor: Kansas City Bridge Company, Kansas City MO  
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 189000.3; Index to Saline County Proceedings V: page 4 (3 August 1903) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.  
sign. rating: 41  
evaluation: NRHP non-eligible (poorly preserved example of common structural type)

inventoried by: Clayton B. Fraser 24 January 1992

# Edmonson Creek Bridge

SALI20

## GENERAL DATA

structure no.:	217001.1	city/town:	5.1 miles east of Miami
county:	Saline	feature inters.:	Edmonson Creek
		cadastral grid:	S6/7, T52N, R20W
		highway route:	County Road 217
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss  
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	45.0'	alterations:	none
total length:	47.0'	floor/decking :	concrete deck over steel stringers
roadway width:	13.9'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: 1 round eyerod with turnbuckle; lateral bracing: round rod with turnbuckle; floor beam: I-beam, field-bolted to vertical; guardrail: 2 angles

## HISTORICAL DATA

erection date: 1908  
erection cost: unknown  
designer: Illinois Steel Bridge Company, Jacksonville IL (probable)  
fabricator : Illinois Steel Bridge Company, Jacksonville IL (probable); Cambria Steel Company, Pittsburgh PA  
contractor : county work force  
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 217001.1; Saline County Court Record, Book W: page 461 (7 July 1908); Index to Saline County Proceedings W: page 461 (7 July 1908) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.  
sign. rating: 39  
evaluation: NRHP non-eligible (well-preserved, structurally unremarkable example of common structural type)

inventoried by: Clayton B. Fraser 24 January 1992

# Rock Creek Bridge

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SALI22

## GENERAL DATA

structure no.:	245000.4	city/town:	4.7 miles north of Gilliam
county:	Saline	feature inters.:	Rock Creek
		cadastral grid:	S8, T52N, R19W
		highway route:	County Road 245
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected kingpost pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	30.0'	alterations:	none
total length:	31.0'	floor/decking :	timber deck over steel stringers
roadway width:	10.7'	other features:	inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with batten plates and lacing; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical

## HISTORICAL DATA

erection date:	1900-01
erection cost:	unknown
designer:	Stupp Brothers Bridge and Iron Company, St. Louis MO
fabricator :	Stupp Brothers Bridge and Iron Company, St. Louis MO; Carnegie Steel Company, Pittsburgh PA
contractor:	Stupp Brothers Bridge and Iron Company, St. Louis MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 245000.4; Saline County Court Record, Book U: page 62 (7 August 1900), page 82 (9 April 1900); Index to Saline County Proceedings Book U: page 62 (7 August 1900) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.
sign. rating:	57
evaluation:	NRHP possibly eligible (well-preserved example of now-rare structural type)

inventoried by: Clayton B. Fraser    24 January 1992

# Bear Creek Bridge

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SALI23

## GENERAL DATA

structure no.:	263000.8	city/town:	2.9 miles north of Gilliam
county:	Saline	feature inters.:	Bear Creek
		cadastral grid:	S21, T52N, R19W
		highway route:	County Road 263
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt half-hip pony truss  
substructure: concrete on masonry abutments

span number:	1	condition:	fair
span length:	68.0'	alterations:	none
total length:	68.0'	floor/decking :	timber deck over steel stringers
roadway width:	10.0'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars, or 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: looped round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 angles

## HISTORICAL DATA

erection date: 1907  
erection cost: unknown  
designer: Kansas City Bridge Company, Kansas City MO  
fabricator : Kansas City Bridge Company, Kansas City MO;  
Illinois Steel Company, Chicago IL  
contractor: county work force

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 263000.8; Saline County Court Record, Book Q: page 103 (5 August 1889), page 273 (8 July 1907) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.

sign. rating: 42  
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser 24 January 1992

# Fish Creek Bridge

SALI25

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## GENERAL DATA

<b>structure no.:</b> 299000.7	<b>city/town:</b> 2.7 miles south of Gilliam
<b>county:</b> Saline	<b>feature inters.:</b> Fish Creek
	<b>cadastral grid:</b> S17, T51N, R19W
	<b>highway route:</b> County Road 299
	<b>highway distr.:</b> 2
	<b>current owner:</b> Saline County

## STRUCTURAL DATA

<b>superstructure:</b> steel plate through girder	
<b>substructure:</b> concrete abutments and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 35.0'	<b>alterations:</b> none
<b>total length:</b> 36.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 13.0'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> 1910	
<b>erection cost:</b> unknown	
<b>designer:</b> Saline County Engineer	
<b>fabricator :</b> Kansas City Bridge Company, Kansas City MO (probable)	
<b>contractor:</b> county work force	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 299000.7; Saline County Court Record, Book V: page 157 (10 February 1910) - located Saline County Courthouse, Marshall MO.	
<b>sign. rating:</b> 55	
<b>evaluation:</b> NRHP possibly eligible (well-preserved, early example of uncommon structural type)	

**inventoried by:** Clayton B. Fraser    24 January 1992

# Camp Creek Bridge

SALI27

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## GENERAL DATA

structure no.:	311001.0	city/town:	7.9 miles east of Marshall
county:	Saline	feature inters.:	Camp Creek
		cadastral grid:	S12/13, T50N, R20W
		highway route:	County Road 311
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss  
substructure: unknown

span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	60.0'	floor/decking :	unknown
roadway width:	11.8'	other features:	unknown

## HISTORICAL DATA

erection date: c1910  
erection cost: unknown  
designer: unknown  
fabricator : unknown  
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 311001.0.

sign. rating: 28  
evaluation: NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Clayton B. Fraser 24 January 1992

# Pierre Fleche Creek Bridge

SALI29

## GENERAL DATA

structure no.:	325002.1	city/town:	8.0 miles south of Gilliam
county:	Saline	feature inters.:	Pierre Fleche Creek
		cadastral grid:	S11, T50N, R19W
		highway route:	County Road 325
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure: wrought iron, 4-panel, pin-connected Pratt pony truss  
substructure: stone masonry abutments

span number:	1	condition:	fair
span length:	70.0'	alterations:	none
total length:	70.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.3'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing, with 2-angle outrider; diagonal: 2 square eyebars; counter: 1 round eyerod with unslotted turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical

## HISTORICAL DATA

erection date: 1890-91  
erection cost: \$1600.00  
designer: Massillon Bridge Company, Massillon OH  
fabricator : Massillon Bridge Company, Massillon OH  
contractor : Massillon Bridge Company, Massillon OH

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No 325002.1; Saline County Court Record, Book Q: page 359 ( 8 November 1890), page 439 (10 April 1891) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.

sign. rating: 51  
evaluation: NRHP possibly eligible (well-preserved, early example of mainstay structural type)

inventoried by: Clayton B. Fraser 24 January 1992

# Pierre Fleche Creek Bridge

SALI30

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## GENERAL DATA

structure no.:	327000.4	city/town:	8.5 miles south of Gilliam
county:	Saline	feature inters.:	Pierre Fleche Creek
		cadastral grid:	S15, T50N, R19W
		highway route:	County Road 327
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt half-hip pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	62.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.0'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1915
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	county work force

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 327000.4; Saline County Court Record, Book W: page 289 (7 August 1907); Saline County Court Record Y: page 189 (9 January 1913); Saline County Court Record X: page 157 (10 February 1910); Saline County Court Record Z: page 40 (8 September 1915); Index to Saline County Proceedings Y: page 189 (9 January 1913) - located at Saline County Courthouse, Marshall MO.

sign. rating:	32
evaluation:	NRHP determined non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser    24 January 1992

# Steel's Fish Trap Bridge

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SALI31

## GENERAL DATA

structure no.:	351000.5	city/town:	9.9 miles southeast of Marshall
county:	Saline	feature inters.:	Salt Fork
		cadastral grid:	S13, T49N, R20W
		highway route:	County Road 351
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

**superstructure:** wrought iron, 10-panel, bolt-connected bowstring through arch-truss  
**substructure:** stone masonry abutments and wingwalls

span number:	1	condition:	fair
span length:	100.0'	alterations:	none
total length:	100.0'	floor/decking :	corrugated steel plate deck
roadway width:	14.0'	other features:	upper chord: 2 channels with cover plate and double lacing; lower chord: 2 rectangular eyebars; vertical: star iron; diagonal: round rods with threaded ends; lateral bracing: round rod with threaded ends; strut: iron tube; floor beam: 2 channels with I-beams extended to star iron outriders; guardrail: 1 channel; chiseled into stone abutment: H. Murphy; chiseled into other stone abutment: 1875

## HISTORICAL DATA

**erection date:** 1875 / 1882  
**erection cost:** \$1507.50 (substructure); \$880.00 (superstructure)  
**designer:** Missouri Valley Bridge and Iron Company, Leavenworth KS  
**fabricator :** Missouri Valley Bridge and Iron Company, Leavenworth KS  
**contractor :** W.L. Smiley (substructure); Missouri Valley Bridge and Iron Company, Leavenworth KS (superstructure)  
**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 351000.5; Saline County Court Record, Book K: page 194 (5 February 1875), page 216 (2 March 1875), page 303 (9 August 1875), page 335 (2 November 1875), page 83 (10 February 1882), page 219 (10 October 1882) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.  
**sign. rating:** 76  
**evaluation:** NRHP eligible (well-preserved example of early all-metal vehicular structure)

inventoried by: Clayton B. Fraser 24 January 1992

# Napton Bridge

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SALI32

## GENERAL DATA

structure no.:	353001.0	city/town:	7.5 miles southeast of Marshall
county:	Saline	feature inters.:	Salt Fork
		cadastral grid:	S3, T49N, R20W
		highway route:	County Road 353
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, 7-panel, rigid-connected Pratt pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	100.0'	alterations:	unknown
total length:	102.0'	floor/decking :	unknown
roadway width:	14.5'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1912
erection cost:	unknown
designer:	Kansas City Bridge Company, Kansas City MO
fabricator :	Kansas City Bridge Company, Kansas City MO
contractor :	Kansas City Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 353001.0; Saline County Court Record, Book Y: page 32 (8 May 1912), page 114 (4 September 1912); Saline County Court Record, Book X: page 479 (5 September 1911) - located Saline County Courthouse, Marshall MO.
sign. rating:	44
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser    24 January 1992

# Scott & Cooney Ford Bridge

SALI33

## GENERAL DATA

structure no.:	366000.4	city/town:	10.2 miles southeast of Marshall
county:	Saline	feature inters.:	Blackwater River
		cadastral grid:	S22, T49N, R20W
		highway route:	County Road 366
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

**superstructure:** steel, 7-panel, pin-connected Pratt through truss; 3 pin-connected Pratt pony approach spans

**substructure:** concrete abutments; concrete-filled steel cylinder piers

span number:	1	condition:	fair
span length:	120.0'	alterations:	minor floor system repairs
total length:	374.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.8'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 looped square eyebars at the hip); diagonal: 2 punched rectangular eyebars; counter: 1 square eyebar with turn-buckle; lateral bracing: round rod with threaded ends; strut: 2 angles with continuous plate; floor beam: plate girder, U-bolted to vertical; guardrail: 2 channels; portal plate: Built by the Illinois Steel Bridge Co., Jacksonville Illinois

## HISTORICAL DATA

**erection date:** 1910  
**erection cost:** unknown  
**designer:** Illinois Steel Bridge Company, Jacksonville IL  
**fabricator :** Illinois Steel Bridge Company, Jacksonville IL  
**contractor :** Illinois Steel Bridge Company, Jacksonville IL

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 366000.4; Saline County Court Record, Book P: page 193 (3 May 1886), page 427 (10 November 1887), page 429 (5 December 1887); Saline County Court Record, Book X: page 236 (7 July 1910), page 323 (10 November 1910) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.

**sign. rating:** 40  
**evaluation:** NRHP determined non-eligible (typical example of common structural type, with average degree of physical integrity)

inventoried by: Clayton B. Fraser 24 January 1992

# Boat Yard Ford Bridge

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SALI34

## GENERAL DATA

structure no.:	368002.5	city/town:	11.7 miles southeast of Marshall
county:	Saline	feature inters.:	unnamed stream
		cadastral grid:	S19, T49N, R19W
		highway route:	County Road 368
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	wrought iron, 9-panel, pin-connected Pratt through truss, with 3-panel, pin-connected Pratt pony truss approach span		
substructure:	stone masonry abutments; concrete-filled wrought iron cylinder piers		
span number:	1	condition:	fair
span length:	150.0'	alterations:	deck removed and bridge closed
total length:	200.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.0'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 punched rectangular eyebars, or 2 channels with lacing; diagonal: 2 punched rectangular eyebars; lateral bracing: round rod with threaded ends; strut: I-beam; floor beam: riveted plate girder, U-bolted to vertical

## HISTORICAL DATA

erection date:	1888
erection cost:	\$4800.00 (contract amount)
designer:	Kansas City Bridge and Iron Company, Kansas City MO
fabricator :	Kansas City Bridge and Iron Company, Kansas City MO; Carnegie Steel Company, Pittsburgh PA
contractor:	Kansas City Bridge and Iron Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 368002.5; Saline County Court Record, Book P: page 486 (7 May 1888), page 531 (7 August 1888), page 598 (8 January 1889); Saline County Court Record, Book Q: page 177 (9 January 1890); Saline County History, Volume One (1983) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.
sign. rating:	56
evaluation:	NRHP possibly eligible (relatively early, long-span example of mainstay structural type)

inventoried by: Clayton B. Fraser 24 January 1992

# Nelson Bridge

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SALI35

## GENERAL DATA

structure no.:	369001.0	city/town:	0.9 mile south of Nelson
county:	Saline	feature inters.:	Blackwater River
		cadastral grid:	S31, T49N, R19W
		highway route:	County Road 369
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, 6-panel, rigid-connected Pratt through truss; rigid-connected Pratt pony and Warren pony truss approach spans		
substructure:	concrete-filled steel cylinder piers; concrete abutments		
span number:	1	condition:	fair
span length:	150.0'	alterations:	none
total length:	243.0'	floor/decking :	concrete deck over steel stringers
roadway width:	15.1'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 4 angles; vertical: 4 angles with batten plates and gussets; diagonal: 4 angles with batten plates and gussets; lateral bracing: round rod with threaded ends; strut: 2 angles with knee braces; portal strut: A-frame; floor beam: I-beam, riveted to vertical; guardrail: 2 channels

## HISTORICAL DATA

erection date:	1929
erection cost:	\$24,400.00
designer:	unknown
fabricator :	Kansas City Bridge Company, Kansas City MO; Inland Steel Company, East Chicago IN
contractor :	Kansas City Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 369001.0; Saline County Court Record, Book 3: page 280 (21 January 1929), page 283 (28 January 1929); Kansas City Bridge Company, Kansas City, Missouri, proposal to build Nelson Bridge, dated 21 January 1929 - located at the Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.
sign. rating:	42
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser 24 January 1992

# Finney Creek Bridge

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SALI37

## GENERAL DATA

<b>structure no.:</b> 390000.8	<b>city/town:</b> 9.5 miles south of Marshall
<b>county:</b> Saline	<b>feature inters.:</b> Finney Creek
	<b>cadastral grid:</b> S28, T49N, R21W
	<b>highway route:</b> County Road 390
	<b>highway distr.:</b> 2
	<b>current owner:</b> Saline County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt through truss, with steel stringer approach spans	
<b>substructure:</b> stone masonry abutments; concrete-filled steel cylinder piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 80.0'	<b>alterations:</b> none
<b>total length:</b> 109.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 14.0'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 2 looped square eyebars, or 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: 2 round eyerods with turnbuckles; lateral bracing: round rod with turnbuckle; strut: 4 angles with lacing; floor beam: riveted plate girder, U-bolted to vertical; guardrail: 1 angle

## HISTORICAL DATA

<b>erection date:</b> 1895	
<b>erection cost:</b> \$1477.00 (two-bridge contract)	
<b>designer:</b> Wrought Iron Bridge Company, Canton OH	
<b>fabricator :</b> Wrought Iron Bridge Company, Canton OH	
<b>contractor:</b> Wrought Iron Bridge Company, Canton OH	
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 390000.8; Saline County Court Record, Book S: page 87 (9 November 1894), page 186 (5 March 1895); Index to Saline County Proceedings V: page 143 (7 May 1904) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.
<b>sign. rating:</b> 46	
<b>evaluation:</b>	NRHP non-eligible (typically configured, structurally undistinguished example of common structural type)

**inventoried by:** Clayton B. Fraser    24 January 1992

# Buck Branch Bridge

SALI38

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## GENERAL DATA

structure no.:	391001.0	city/town:	9.5 miles south of Marshall
county:	Saline	feature inters.:	Buck Branch
		cadastral grid:	S32, T49N, R21W
		highway route:	County Road 391
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt bedstead		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	30.0'	alterations:	unknown
total length:	44.0'	floor/decking :	timber deck over steel stringers
roadway width:	10.8'	other features:	unknown

## HISTORICAL DATA

erection date:	c1900
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 391001.0.
sign. rating:	21
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser    24 January 1992

# Blackwater River Bridge

SALI39

## GENERAL DATA

structure no.:	408001.0	city/town:	8.1 miles east of Sweet Springs
county:	Saline	feature inters.:	Blackwater River
		cadastral grid:	S7/12, T48N, R21/22W
		highway route:	County Road 408
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, 6-panel, pin-connected Pratt through truss; rigid-connected Pratt pony truss approach span		
substructure:	stone masonry abutments; stone piers with concrete caps		
span number:	1	condition:	fair
span length:	100.0'	alterations:	pony truss probably added
total length:	149.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.1'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 punched rectangular eyebars at the hip); diagonal: 2 punched rectangular eyebars; counter: 1 round eyerod with unslotted turnbuckle; lateral bracing: round eyerod - top, round rod with threaded ends - bottom; strut: I-beam; floor beam: I-beam, U-bolted to vertical; guardrail: 2 channels

## HISTORICAL DATA

erection date:	c1900
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 408001.0; field inspection by Clayton Fraser, 2 October 1991.

sign. rating:	26
evaluation:	NRHP non-eligible (typically configured, inadequately documented example of common structural type)

inventoried by: Clayton B. Fraser 24 January 1992

# Blackwater River Bridge

SALI40

## GENERAL DATA

structure no.:	408001.5	city/town:	8.1 miles east of Sweet Springs
county:	Saline	feature inters.:	Blackwater River
		cadastral grid:	S21/22, T48N, R21/22W
		highway route:	County Road 408
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, 5-panel, rigid-connected Pratt through truss; steel stringer approach span at each end		
substructure:	concrete abutments; concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	80.0'	alterations:	collision damage to portal strut
total length:	129.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.3'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 angles with batten plates; vertical: 4 angles with batten plates; diagonal: 2 angles with batten plates; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels

## HISTORICAL DATA

erection date:	c1930
erection cost:	unknown
designer:	unknown
fabricator :	Inland Steel Company, East Chicago IN
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 408001.5; field inspection by Clayton Fraser, 2 October 1991.
sign. rating:	23
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Clayton B. Fraser 24 January 1992

# Sweet Springs Bridge

SALI41

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## GENERAL DATA

structure no.:	439001.5	city/town:	2.0 miles southeast of Sweet Springs
county:	Saline	feature inters.:	Blackwater River
		cadastral grid:	S13, T48N, R23W
		highway route:	County Road 439
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	steel, 8-panel, rigid-connected Pratt pony truss; rigid-connected Warren pony truss approach span		
substructure:	concrete abutments and wingwalls; concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	140.0'	alterations:	unknown
total length:	241.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.0'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1929
erection cost:	\$19,752.00
designer:	unknown
fabricator :	Kansas City Bridge Company, Kansas City MO
contractor :	Kansas City Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 439001.5; Saline County Court Record, Book Q: page 219 (3 March 1890), page 226 (7 April 1890), page 333 (6 October 1890); Index to Saline County Proceedings U: pages 401, 419 (7 May 1902); Index to Saline County Proceedings V: page 133 (5 April 1904); Kansas City Bridge Company, proposal to build Sweet Springs Bridge, dated 21 January 1929 - located at Saline County Courthouse, Marshall MO.
sign. rating:	38
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser    24 January 1992

# Blackwater River Bridge

SALI42

## GENERAL DATA

structure no.:	476001.9	city/town:	9.0 miles south of Marshall
county:	Saline	feature inters.:	Blackwater River
		cadastral grid:	S34, T49N, R21W
		highway route:	County Road 476
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

**superstructure:** steel, 6-panel, rigid-connected Pratt through truss; 1 steel stringer approach span at the south end; 6 steel stringer approach spans at the north end

**substructure:** concrete abutments, wingwalls and piers

span number:	2	condition:	fair
span length:	120.0'	alterations:	none
total length:	376.0'	floor/decking :	asphalt-covered concrete deck over steel road-stringers
way width:	20.1'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates; vertical: 4 angles with lacing; diagonal: 2 angles with batten plates; lateral bracing: 2 angles; strut: 4 latticed angles; floor beam: I-beam, riveted to vertical; guardrail: 2 steel pipes

## HISTORICAL DATA

**erection date:** 1924  
**erection cost:** \$45,685.14  
**designer:** Missouri State Highway Department  
**fabricator :** Inland Steel Company, East Chicago IN  
**contractor :** C.T. Fogle

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 476001.9; Primary System Bridge Records, located at Missouri Highway and Transportation Department, Jefferson City MO; Saline County Court Record, Book W: page 107 (2 October 1906); Saline County Court Record, Book X: page 426 (13 April 1917); Index to Saline County Proceedings W: page 107 (2 October 1906) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.

**sign. rating:** 43  
**evaluation:** NRHP non-eligible (typically configured example of MSHD highway truss design)

inventoried by: Clayton B. Fraser 24 January 1992

# Eastwood Road Overpass

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SALI43

## GENERAL DATA

structure no.:	477000.1	city/town:	Marshall
county:	Saline	feature inters.:	Missouri Pacific Railroad
		cadastral grid:	S12, T50N, R21W
		highway route:	Eastwood Road
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	concrete through girder, skewed		
substructure:	concrete abutments, wingwalls and piers		
span number:	3	condition:	good
span length:	35.0'	alterations:	none
total length:	105.0'	floor/decking :	concrete deck
roadway width:	19.0'	other features:	solid concrete girders with recessed rectangular panels

## HISTORICAL DATA

erection date:	1923
erection cost:	unknown
designer:	Missouri State Highway Department
fabricator :	none
contractor:	Missouri Pacific Railroad
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 477000.1; Missouri State Highway Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; Missouri State Highway Board, <b>First Biennial Report</b> : 1917-18, pages 100-101; Missouri State Highway Board, <b>Second Biennial Report</b> : 1919-20, pages 129, 133, 148-49.
sign. rating:	59
evaluation:	NRHP possibly eligible (relatively early example of MSHD concrete beam bridge design)

inventoried by: Clayton B. Fraser    24 January 1992

# Eastwood Road Bridge

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SALI44

## GENERAL DATA

structure no.:	477000.2	city/town:	Marshall
county:	Saline	feature inters.:	Salt Fork Creek
		cadastral grid:	S12, T50N, R21W
		highway route:	Eastwood Road
		highway distr.:	2
		current owner:	Saline County

## STRUCTURAL DATA

superstructure:	concrete filled spandrel arch		
substructure:	concrete abutments, wingwalls and piers		
span number:	1; 2	condition:	good
span length:	90.0'; 60.0'	alterations:	none
total length:	281.0'	floor/decking :	concrete deck over earth fill
roadway width:	18.8'	other features:	MSHD standard concrete guardrails

## HISTORICAL DATA

erection date:	1923
erection cost:	\$46,276.20
designer:	Missouri State Highway Department
fabricator :	none
contractor:	M.E. Gillioz, Monett MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 477000.2; Missouri State Highway Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; Missouri State Highway Board, <b>First Biennial Report</b> : 1917-18, pages 100-101; Missouri State Highway Board, <b>Second Biennial Report</b> : 1919-20, pages 129, 133, 148-49.
sign. rating:	59
evaluation:	NRHP possibly eligible (relatively early example of MSHD concrete arch bridge design)

inventoried by: Clayton B. Fraser    24 January 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Glasgow Bridge  
MHTD: G 69R

SALI01

**DATE(S) OF CONSTRUCTION**

1922-25

**LOCATION**

State Highway 240 over Missouri River; S2/3, T51N, R18W  
Glasgow; Saline/Howard County, Missouri

**USE (ORIGINAL / CURRENT)**

highway bridge / highway bridge

**RATING** NRHP eligible (score: 70)

**CONDITION**

good

**OWNER**

Missouri Highway and Transportation Department

span number: 5  
span length: 343.0'  
total length: 2244.0'  
roadway wdt.: 20.3'

superstructure: steel, 10-, 12- and 14-panel, rigid-connected Pennsylvania through trusses; rigid-connected Pratt deck truss approach spans  
substructure: concrete abutments; concrete bullnose and spill-through piers  
floor/decking: concrete over metal plate deck  
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates and lacing; vertical: 4 angles with batten plates and lacing; diagonal: 2 channels with batten plates and lacing; lateral bracing: 1 angle - lower, 4 angles with lacing - upper; strut: 4 angles with lattice and lacing; portal strut; 4 angles with lacing (W-frame); floor beam: I-beam; bridge plate: Glasgow Bridge / Financed By / Howard County, Saline County / Federal Aid / Appropriated By / Missouri State Highway Commission / Designed By F.W. Adgate / Consulting Engineer / Chicago, Ill. / Supervised By / Missouri State Highway Department / B.H. Piepmeier, Chief Engineer / L.J. Sverdrup, Bridge Engineer / Contractors / Substructure / Union Bridge & Const. Co. / Kansas City, Mo / Superstructure / Mt. Vernon Bridge Co. / Mt. Vernon, Ohio / Completed 1925

This multiple-span highway bridge carries State Highway 240 over the Missouri River at Glasgow, between Saline and Howard counties. Located immediately downriver from the Illinois Central Gulf Railroad's bridge, this immense structure consists of five truss spans varying between 224 feet to 343 feet in length. The first bridge built over the Missouri River at Glasgow was the Glasgow Steel Bridge—the first all-steel bridge in the world—built in 1879 by the Chicago & Alton Railroad Company. Positioned alongside the existing railroad bridge, the highway bridge was designed in 1922 with the objective of linking vehicular traffic between Saline and Howard counties. Comprised of five riveted Pennsylvania through trusses and series of deck truss and deck girder approach spans, the structure cost \$619,00.00–\$278.00 per lineal foot—and was financed in part by the two counties. Federal aid was also appropriated by the Missouri State Highway Commission. Designed by Consulting Engineer F. Adgate and supervised by the Missouri State Highway Department, the bridge extends some 2,244 feet in length. The contract to build the superstructure was awarded to the Mt. Vernon Bridge Company of Mt. Vernon, Ohio. The concrete substructure was built by the Union Bridge and Construction Company of Kansas City, Missouri. Throughout the years, rehabilitation efforts have taken place, including resurfacing the floor with asphalt, general painting and installation of a concrete-filled grid deck. Since its completion, the Glasgow Bridge has functioned as a strategically important state highway while

retaining its structural integrity.

The Glasgow Bridge, significant as one of the first over the Missouri River in this area of the state, has continuously provided a reliable year-round route to local and interstate vehicular traffic over the Missouri River since its construction. The structure is also significant as a well-preserved example of large-scale bridge construction in Missouri. Like several bridges built over the Missouri River in the 1920s, it incorporated long, fixed-span, simply supported trusses on concrete piers. The Pennsylvania trusses of the Glasgow Bridge fit well within the mainstream of great river bridge construction in the 1920s. Just as the pin-connected Whipple truss had been the industry standard for Missouri River railroad bridges in the 1880s and 1890s, the riveted Pennsylvania (or Petit) truss was the standard configuration for long-span highway bridge applications in the 1910s, 1920s and 1930s.

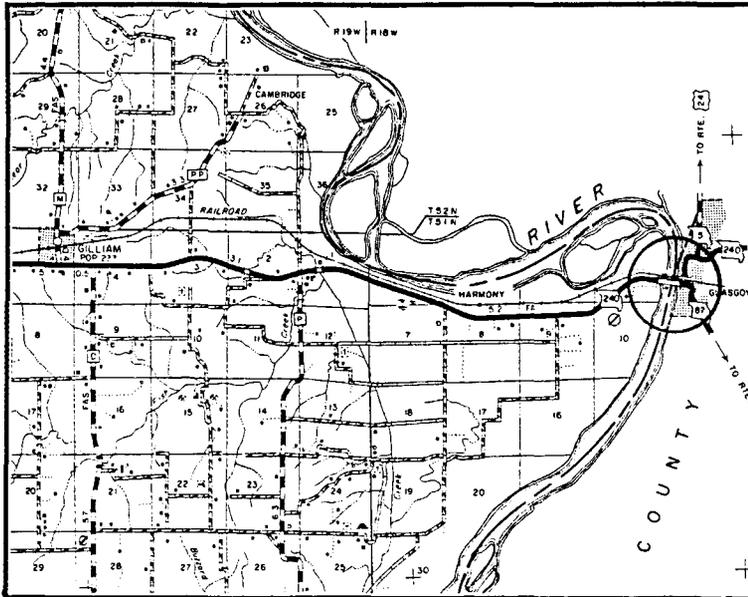
Its name derived from extensive use in the 19th century by the Pennsylvania Railroad, the Pennsylvania truss was patented in 1875 as a refinement of the standard Pratt truss, from which it was derived. Pennsylvania trusses featured vertical compression members and two-panel diagonals in tension, like the Whipple trusses they superseded. The primary differences lay in the substitution of a polygonal top chords for economy of materials and the addition of sub-struts or sub-ties. These relatively short diagonal members severed to provide lateral support for the diagonals and stiffen the truss under heavy moving loads. Like most truss types in America, Pennsylvanias were pioneered by the railroads and were later used to carry vehicular traffic.

The first polygonal-chorded bridge over the Mississippi River - the double-track Merchants' Bridge at St. Louis, was a Pennsylvania truss. During the bridge boom along the Missouri River in the 1920s, fifteen of the eighteen fixed-span bridges featured Pennsylvania trusses. Almost identical, they varied only in span lengths, which were determined largely by adjacent railroad bridges.

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**NAME(S) OF STRUCTURE**

Glasgow Bridge

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. G 69R; Missouri Highway and Transportation Department, Primary System Bridge Record, Book 2 (see entries for Saline County), on file at MHTD, Jefferson City, MO.; Saline County Court, Book 1: page 146 (2 June 1919) - located at Saline County Courthouse, Marshall MO; H.J. Kesner, "Some Recent Missouri Bridges," *The Nebraska Blue Print*, May 1929, pages 9-10; "Photo Flashback," *The Slater News-Ruslter*, 2 July 1985; field inspection by Clayton Fraser, 2 October 1991.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**24 January 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**  
Muddy Creek Bridge  
MHTD: 025000.9

SALI04

**DATE(S) OF CONSTRUCTION**  
1910

**LOCATION**

County Road 25 over Muddy Creek; S20/29, T52N, R21W  
3.8 miles south of Miami; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 47)

**CONDITION**

fair

**OWNER**

Saline County

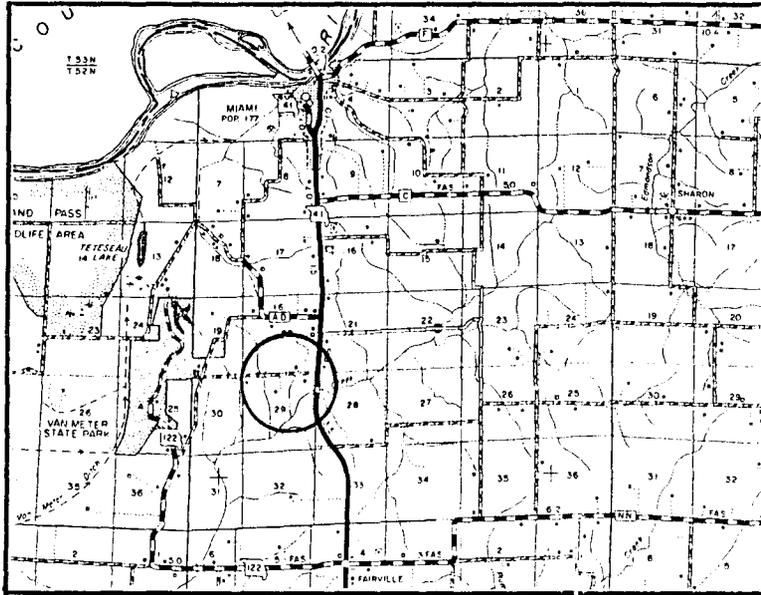
span number: 1	superstructure: steel plate deck girder
span length: 35.0'	substructure: concrete abutments and wingwalls
total length: 36.0'	floor/decking: timber deck over steel stringers
roadway wdt.: 12.9'	other features: steel angle guardrails

This small-scale steel bridge carries an unpaved county road over Muddy Creek almost four miles south of Miami, in northern Saline County. Built as a riveted plate deck girder, the timber-floored bridge is supported by concrete abutments. The Muddy Creek Bridge was conceived by the Saline County Court in July 1910, when the judges directed the county engineer to build a bridge at this crossing of Muddy Creek. The engineer evidently ordered the superstructural steel from the Kansas City Bridge Company—which at the time supplied most of the county's bridge steel—and built the concrete substructure and erected the steel girders using local force account labor. Probably completed later that year, the Muddy Creek Bridge has since functioned in place, without substantial alteration.

In the 1930s and 1940s, the state highway department experimented with steel plate girders for its long-span crossings. Before that, however, this relatively simple beam bridge type had received only sparing use in Missouri, as the counties had eschewed girders for short- and medium-span trusses in the 19th and early 20th centuries. As a result comparably few deck girder bridges have been identified from the state's formative bridge building period. Most of the standing county-built girders are found in Saline County, which used several plate deck structures for short-span crossings between 1907 and 1915. The Muddy Creek Bridge is one of these: a well-preserved, relatively early example of this little-used beam bridge type.

**NAME(S) OF STRUCTURE**  
Muddy Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 025000.9; Saline County Court Record, Book X: page 233 (6 July 1910) - located at Saline County Courthouse, Marshall MO.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

24 January 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

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**NAME(S) OF STRUCTURE**

Muddy Creek Bridge  
MHTD: 027001.6

SALI05

**DATE(S) OF CONSTRUCTION**

1907

**LOCATION**

County Road 27 over branch of Muddy Creek; S7/12, T51N, R21/22W  
7.4 miles northwest of Marshall; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 47)

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**CONDITION**

fair

**OWNER**

Saline County

span number:	1	superstructure:	steel plate deck girder
span length:	35.0'	substructure:	concrete abutments and wingwalls
total length:	36.0'	floor/decking:	timber deck over steel stringers
roadway wdt.:	13.8'	other features:	steel angle guardrails

This small-scale steel bridge carries an unpaved county road over Muddy Creek some 7½ miles northwest of Marshall, the seat of Saline County. Built as a riveted plate deck girder, the timber-floored bridge is supported by concrete abutments. The Muddy Creek Bridge was conceived by the Saline County Court in September 1907, when the judges directed the county engineer to build a bridge at this crossing of Muddy Creek. The engineer evidently ordered the superstructural steel from the Kansas City Bridge Company—which at the time supplied most of the county's bridge steel—and built the concrete substructure and erected the steel girders using local force account labor. Probably completed later that year, the Muddy Creek Bridge has since functioned in place, without substantial alteration.

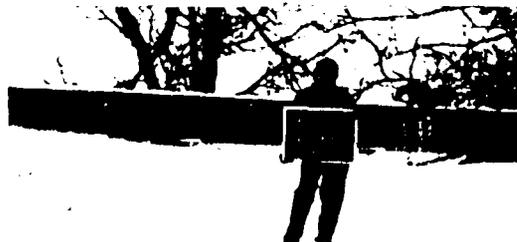
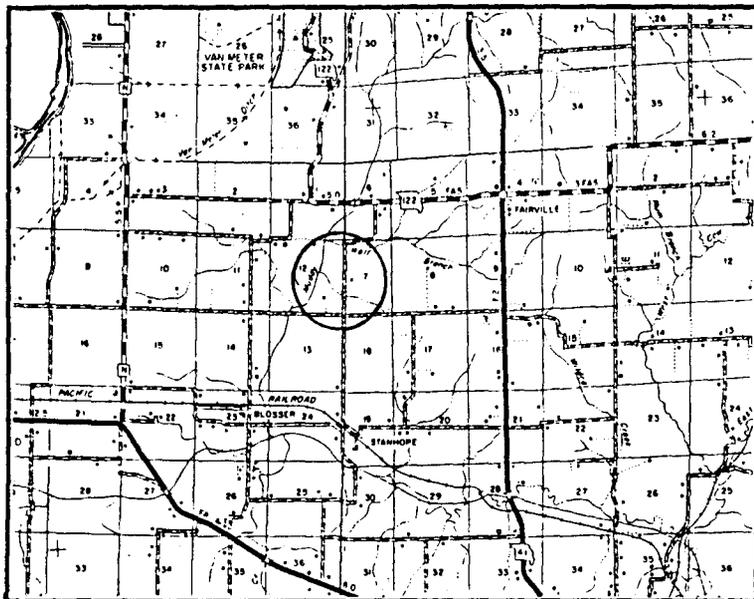
In the 1930s and 1940s, the state highway department experimented with steel plate girders for its long-span crossings. Before that, however, this relatively simple beam bridge type had received only sparing use in Missouri, as the counties had eschewed girders for short- and medium-span trusses in the 19th and early 20th centuries. As a result comparably few deck girder bridges have been identified from the state's formative bridge building period. Most of the standing county-built girders are found in Saline County, which used several plate deck structures for short-span crossings between 1907 and 1915. The Muddy Creek Bridge is one of these: a well-preserved, relatively early example of this little-used beam bridge type.

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**NAME(S) OF STRUCTURE**

Muddy Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 027001.6; Saline County Court Record, Book W: page 298 (4 September 1907) -located at Saline County Courthouse, Marshall MO.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

24 January 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**  
Finney Creek Bridge  
MHTD: 142001.0

SALI11

**DATE(S) OF CONSTRUCTION**  
1911

**LOCATION**

County Road 142 over North Fork of Finney Creek; S4, T49N, R21W  
4.6 miles south of Marshall; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 47)

**CONDITION**

fair

**OWNER**

Saline County

span number: 1  
span length: 30.0'  
total length: 31.0'  
roadway wdt.: 14.1'

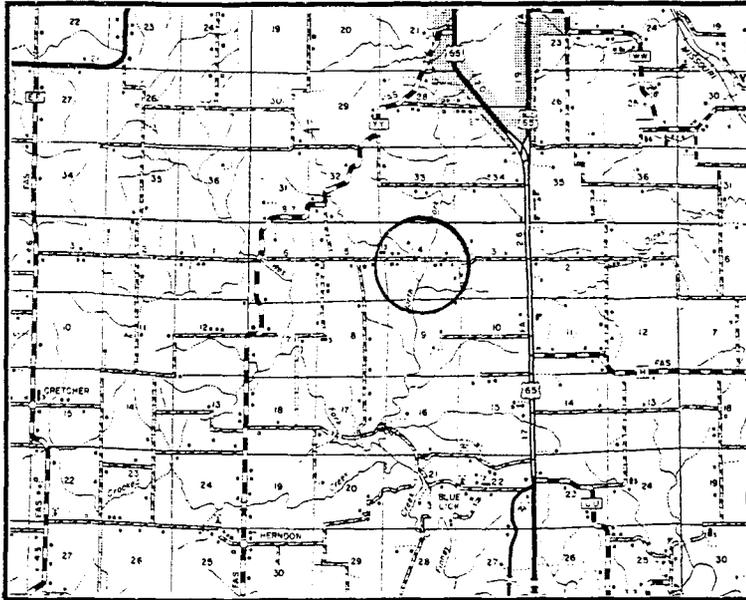
superstructure: steel plate deck girder  
substructure: concrete abutments and wingwalls  
floor/decking: timber deck  
other features: steel angle guardrails

Located on an ungraded county road south of Marshall, this small-scale steel bridge spans the North Fork of Finney Creek. The structure is comprised of a single plate girder span that carries the roadway in a deck position, supported by concrete abutments. The Finney Creek Bridge was fabricated and erected in 1911 by the Illinois Steel Bridge Company of Jacksonville, Illinois, a perennial bridge contractor for Saline County. Since its completion, the Finney Creek Bridge has functioned in place, with only maintenance-related repairs.

Although steel plate girders were used commonly by railroads throughout the late 19th and early 20th centuries, they never received widespread use for vehicular spans by Missouri's counties. For some reason, Saline County alone differed from this statewide trend, building several small-scale plate girder spans between 1907 and 1915. Eight of these bridges remain in place, all modestly scaled and simply detailed. The Finney Creek Bridge is one of these. With a 30-foot span, it is technologically unremarkable; nevertheless, it is a noteworthy example of a little-used beam bridge type in Missouri.

**NAME(S) OF STRUCTURE**  
Finney Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 142001.0; Index to Saline County Proceedings X: page 419 (4 May 1911); Index to Saline County Proceedings Y: page 603 (20 April 1915) - located at Saline County Courthouse, Marshall MO.

**INVENTORIED BY**  
Clayton B. Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
24 January 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Salt Branch Bridge  
MHTD: 065000.7

SALI08

**DATE(S) OF CONSTRUCTION**

1895

**LOCATION**

County Road 65 over Salt Branch; S9/16, T50N, R22W  
7.2 miles west of Marshall; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 47)

**CONDITION**

fair

**OWNER**

Saline County

span number: 1  
span length: 50.0'  
total length: 50.0'  
roadway wdt.: 12.5'

superstructure: steel, 2-panel, pin-connected Pratt truss-leg bedstead, with steel stringer approach spans  
substructure: timber abutments with steel truss-leg piers  
floor/decking: timber deck over steel stringers  
other features: upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 channels with batten plates; vertical: 2 channels with lacing; diagonal: 2 square eyebars; lateral bracing: 2 round rods with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles; builder's plate: **THE MASSILLON BRIDGE COMPANY / OHIO / 1895**

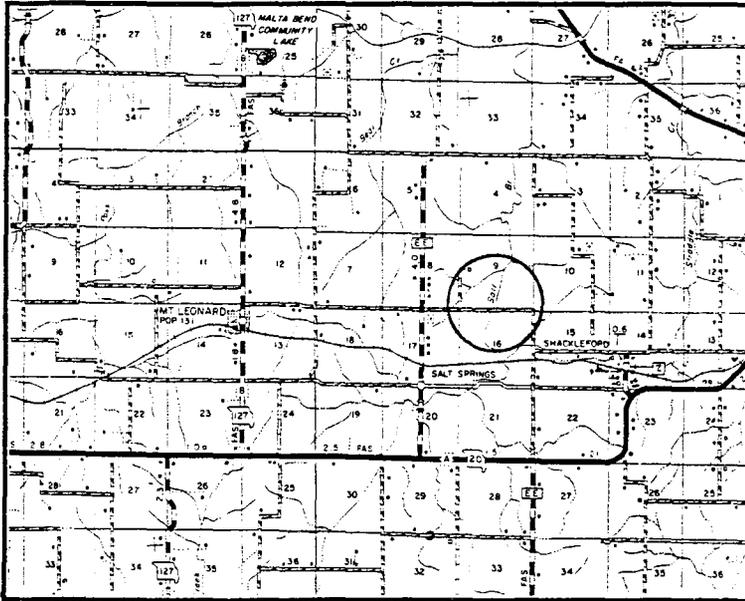
Located some seven miles west of Marshall, this small-scale steel bridge carries a gravel-surfaced county road over Salt Branch. The structure is configured as a single-span, pinned Pratt bedstead with a timber deck over steel stringers. The truss is supported by steel truss-leg piers; the stringers bear on timber pile abutments. The Salt Branch Bridge dates to 1895. That year the Saline County Court contracted with Massillon Bridge Company of Massillon, Ohio, to fabricate and erect the 25-foot bridge for \$427.00. The structure was completed by the following March. Since that time the Salt Branch Bridge has functioned in place, without serious alteration.

The bedstead truss enjoyed widespread popularity in Missouri in the late 1890s and early 1900s. A single "leg" functioned as both end post and support at each corner. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Because of this weakness, bedsteads were typically limited to short-span applications. One of the oldest bedsteads remaining in Missouri, the Salt Branch Bridge is a well-preserved, small-scale example of a relatively common structural type.

**NAME(S) OF STRUCTURE**

Salt Branch Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCE**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 065000.7; Saline County Court Record, Book T: page 408 (3 March 1896) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

24 January 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**  
Dicks Branch Bridge  
MHTD: 150000.5

SALI13

**DATE(S) OF CONSTRUCTION**  
1912

**LOCATION**

County Road 150 over Dicks Branch; S1/2, T49N, R21W  
4.6 miles south of Marshall; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 47)

**CONDITION**

fair

**OWNER**

Saline County

span number:	1	superstructure:	steel plate deck girder
span length:	30.0'	substructure:	concrete abutments and wingwalls
total length:	31.0'	floor/decking:	timber deck
roadway wdt.:	12.2'	other features:	steel angle guardrails

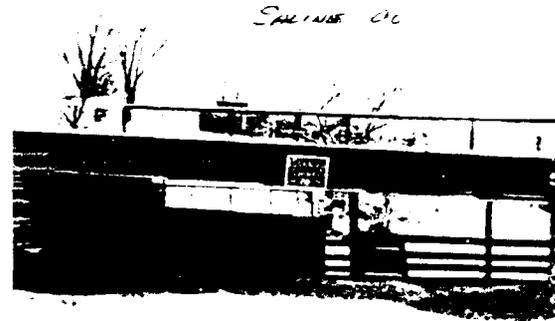
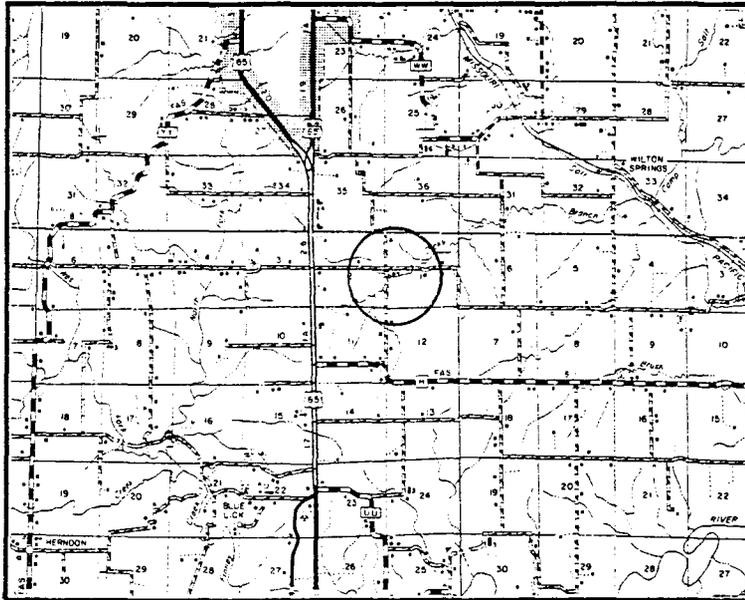
Located some 4½ miles south of Marshall, this small-scale steel bridge carries a gravel-surfaced county road over Dicks Branch. The structure is comprised of a single plate girder span that carries the roadway in a deck position, supported by concrete abutments and framed by steel guardrails. The Dicks Branch Bridge dates to 1912. In April of that year, the Saline County Court directed the county engineer to design and build seven short-span bridges, including this span south of Marshall, estimated to cost \$675. The county hired the Kansas City Bridge Company to fabricate and erect the bridge. Since its completion, the Dicks Branch Bridge has carried vehicular traffic, in essentially unaltered condition.

Although steel plate girders were used commonly by railroads throughout the late 19<sup>th</sup> and early 20<sup>th</sup> centuries, they never received widespread use for vehicular spans by Missouri's counties. For some reason, Saline County alone differed from this statewide trend, building several small-scale plate girder spans between 1907 and 1915. Eight of these bridges remain in place, all modestly scaled and simply detailed. The Finney Creek Bridge is one of these. With a 30-foot span, it is technologically unremarkable; nevertheless, it is a noteworthy example of a little-used beam bridge type in Missouri.

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**NAME(S) OF STRUCTURE**  
Dicks Branch Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**  
Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 150000.5; Saline County Court Record, Book Y: page 10 (4 April 1912); Index to Saline County Proceedings U: page 568 (8 April 1903) - located at Saline County Courthouse, Marshall MO.

**INVENTORIED BY**  
Clayton B. Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
24 January 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

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**NAME(S) OF STRUCTURE**

Salt Fork Bridge  
MHTD: 165002.3

SALI16

**DATE(S) OF CONSTRUCTION**

1899

**LOCATION**

County Road 165 over Salt Fork; S32/33, T50N, R20W  
5.3 miles southeast of Marshall; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 51)

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**CONDITION**

fair

**OWNER**

Saline County

span number: 1

span length: 120.0'

total length: 165.0'

roadway wdt.: 14.5'

superstructure: steel, 8-panel, pin-connected Pratt through truss; 2 steel stringer approach spans  
substructure: timber abutments and timber pile bent piers; concrete-filled steel cylinder piers  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with double lacing (2 looped square eyerods at the hip); diagonal: 2 looped rectangular eyebars; counter: 1 looped round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beam, U-bolted to vertical; guardrail: 2 channels

Located about five miles southeast of Marshall, this Pratt through truss carries an unpaved county road over Salt Fork Creek. This road follows the line between Sections 32 and 33, of Township 50, Range 20 in east-central Saline County. Supported by concrete-filled steel cylinder piers, the bridge's superstructure consists of a single 120-foot, pinned Pratt truss, flanked by two steel stringer approach spans. In July 1899 the Saline County Court ordered the construction of the truss, and that September the county engineer contracted with the Central Bridge Company of St. Louis to fabricate and erect this bridge and another 150-foot span over the Blackwater River [SALI36]. (Contract cost for the two structures: \$7600.00.) Completed the same year, the Salt Fork Bridge today appears largely unaltered and possesses average integrity.

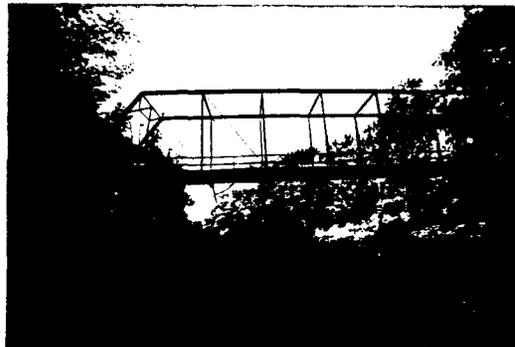
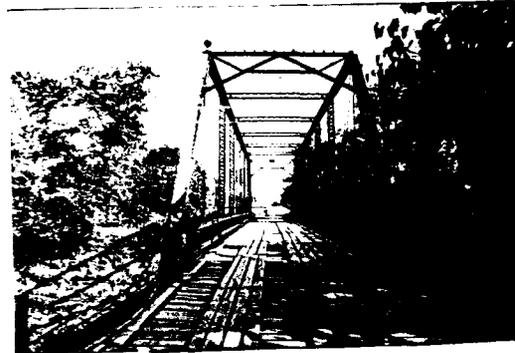
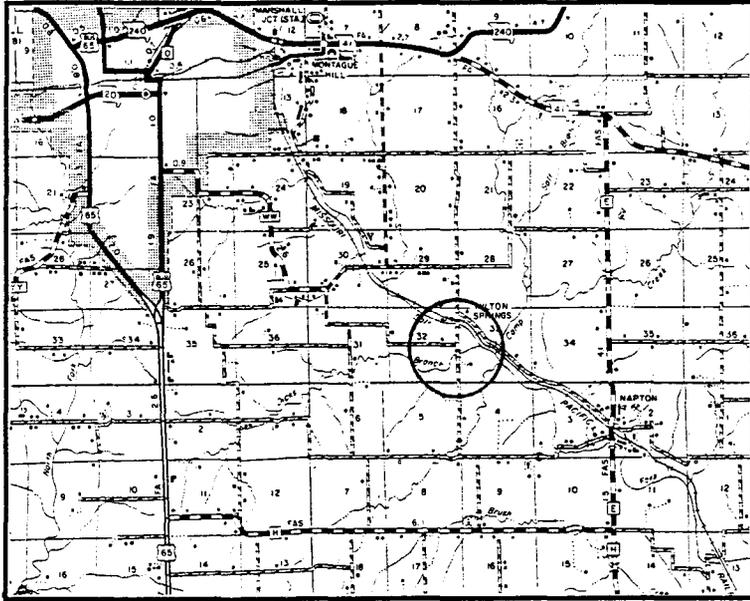
In Missouri the pinned Pratt through truss was the bridge of choice for short- and medium-span applications in the late 19th and early 20th centuries. As a result, thousands of Pratts were built across the state, and today Pratts constitute the most populous group of through trusses. Though it retains a relatively high degree of physical integrity, the Salt Fork Bridge is unremarkable in its design and dimensions. The truss is distinguished somewhat by the decorative cast iron finials attached to its hips and the cresting across its portal struts.

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**NAME(S) OF STRUCTURE**

Salt Fork Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 165002.3; Saline County Court Record, Book T: page 470 (3 July 1899), page 506 (6 September 1899) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

24 January 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

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**NAME(S) OF STRUCTURE**

Rock Creek Bridge  
MHTD: 245000.4

SALI22

**DATE(S) OF CONSTRUCTION**

1900-01

**LOCATION**

County Road 245 over Rock Creek; S8, T52N, R19W  
4.7 miles north of Gilliam; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 57)

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**CONDITION**

fair

**OWNER**

Saline County

span number: 1	superstructure: steel, 2-panel, pin-connected kingpost pony truss
span length: 30.0'	substructure: concrete abutments and wingwalls
total length: 31.0'	floor/decking: timber deck over steel stringers
roadway wdt.: 10.7'	other features: inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with batten plates and lacing; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical

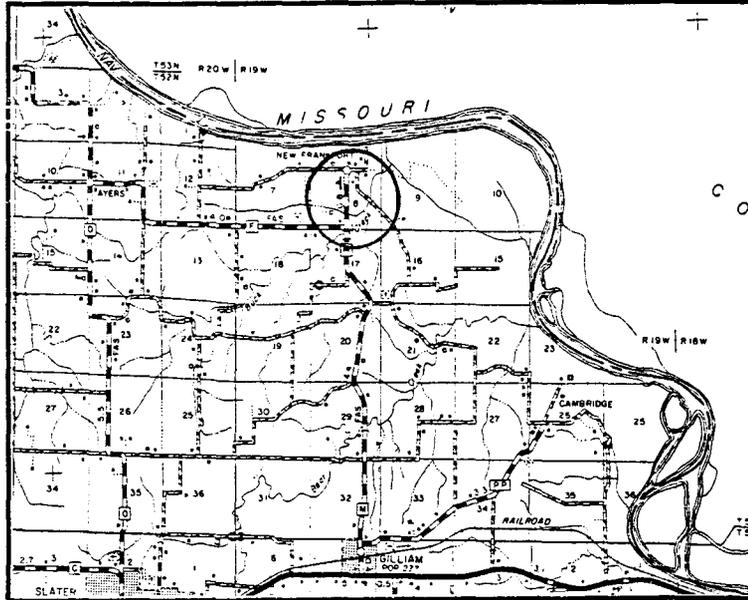
Ordered built in August 1900 by the Saline County Court, the Rock Creek Bridge is located in Section 8, Township 52, Range 19, in the northeastern corner of Saline County. One month later the Stupp Brothers Bridge and Iron Company signed a contract to fabricate and erect this and five other small-scale bridges in the county. County road and bridge commissioner G.W. Latimer agreed to the price of \$2018.00, which included the construction cost of the Rock Creek Bridge. The contract stipulated that the bridge be ready for inspection by March 1, 1901. Stupp Brothers used steel components rolled by the Carnegie Steel Company of Pittsburgh, Pennsylvania, to rivet the bridge parts in its St. Louis shops and shipped them by rail to Saline County. As built, the Rock Creek Bridge consisted of a single kingpost pony truss-pinned lower chord connections and a riveted upper connection-supported by a concrete substructure. Since its completion early in 1901, the Rock Creek Bridge has functioned in place, with only maintenance-related repairs.

The simplest of truss types, the kingpost pony truss was utilized throughout Missouri during the state's settlement and early development periods in the 19th century. Earlier examples were simple wood trusses, while later kingposts were built of iron or steel. By the turn of the century, though, Pratt truss configurations - including truss-leg bedsteads and half-hip pony trusses - had become the designs of choice for medium-span crossings, and steel stringer structures were used for short-span applications. Consequently, few kingposts were built after 1900, and only a dozen are now known to remain in use on Missouri's roadways. The only kingpost in Missouri with both pinned and riveted connections, the Rock Creek Bridge is technologically representative of kingpost pony truss construction: a once popular but now rare truss design.

**NAME(S) OF STRUCTURE**

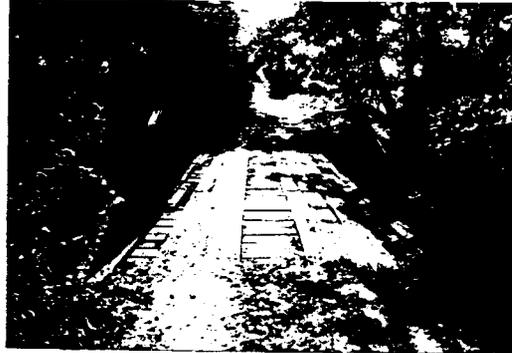
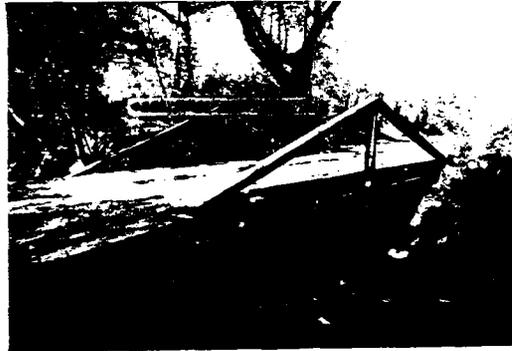
Rock Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP



**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 245000.4; Saline County Court Record, Book U: page 62 (7 August 1900), page 82 (9 April 1900); Index to Saline County Proceedings Book U: page 62 (7 August 1900) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

24 January 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Fish Creek Bridge  
MHTD: 299000.7

SALI25

**DATE(S) OF CONSTRUCTION**

1910

**LOCATION**

County Road 299 over Fish Creek; S17, T51N, R19W  
2.7 miles south of Gilliam; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 55)

**CONDITION**

fair

**OWNER**

Saline County

span number: 1	superstructure: steel plate through girder
span length: 35.0'	substructure: concrete abutments and wingwalls
total length: 36.0'	floor/decking: timber deck over steel stringers
roadway wdt.: 13.0'	other features: unknown

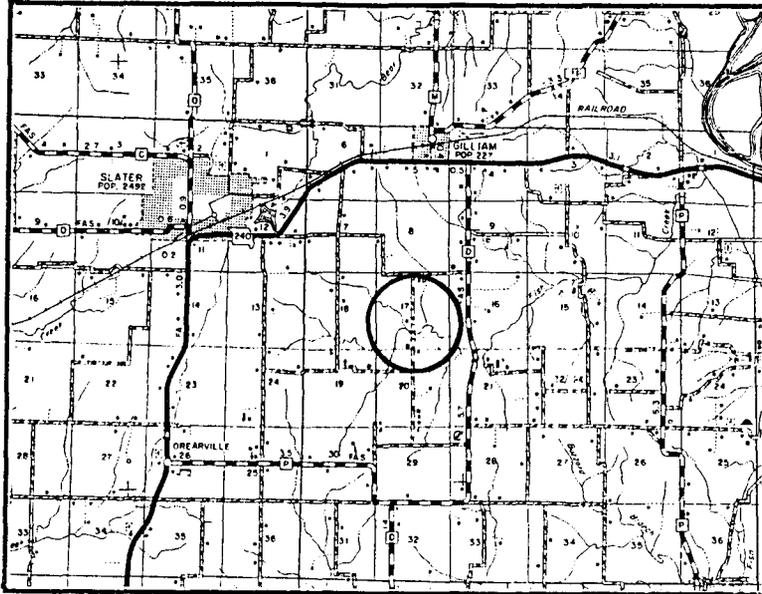
Located south of Gilliam, this short-span steel beam bridge carries a gravel-surfaced county road across Fish Creek in eastern Saline County. The structure is comprised of a single plate girder span, with the girders in the through position, supported by a concrete substructure. The structure dates to 1910. That February the Saline County Court resolved to build 13 small-scale structures, including this bridge over Fish Creek. The county apparently ordered the superstructure steel from the Kansas City Bridge Company and used force account local labor to place the abutments and erect the girders. Probably completed in 1910, the Fish Creek Bridge has since functioned in place, with only maintenance-related repairs.

In the 1930s and 1940s, the state highway department experimented with steel plate girders for its long-span crossings. Before that, however, this relatively simple beam bridge type had received only sparing use in Missouri, as the counties had eschewed girders for short- and medium-span trusses in the 19th and early 20th centuries. As a result comparably few through girder bridges have been identified from the state's formative bridge building period. Most of the standing county-built girders are found in Saline County, which used several plate deck and through structures for short-span crossings between 1907 and 1915. The Fish Creek Bridge is one of these: a well-preserved, relatively early example of this little-used beam bridge type.

**NAME(S) OF STRUCTURE**

Fish Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 299000.7; Saline County Court Record, Book V: page 157 (10 February 1910) - located Saline County Courthouse, Marshall MO.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

24 September 1994

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Pierre Fleche Creek Bridge  
MHTD: 325002.1

SALI29

**DATE(S) OF CONSTRUCTION**

1890-91

**LOCATION**

County Road 325 over Pierre Fleche Creek; S11, T50N, R19W  
8.0 miles south of Gilliam; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 51)

**CONDITION**

fair

**OWNER**

Saline County

span number:	1	superstructure:	wrought iron, 4-panel, pin-connected Pratt pony truss
span length:	70.0'	substructure:	stone masonry abutments
total length:	70.0'	floor/decking:	timber deck over steel stringers
roadway wdt.:	12.3'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing, with 2-angle outrider; diagonal: 2 square eyebars; counter: 1 round eyerod with unslotted turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical

Saline County was formed in 1820. Situated south of a broad sweep of the Missouri River, about two-thirds of the way between St. Louis and Independence, the county was strategically located to capture both waterborne and overland traffic. Its development has been directed and influenced from its inception by interstate commerce, both from the boats that plied the great river and from the wagon traffic that paralleled it. In 1820 the Missouri State Legislature sought to facilitate this overland traffic by authorizing the survey and construction of an east-west state road through the region. Known as the Franklin-to-Fort Osage Post Road, the route crossed the Missouri River at Arrow Rock and extended westward through Saline County via Grand Pass to the county's western border. The routing of this road, Saline County's first, was one of the first items of business considered by the newly formed county court in July 1821.

The state legislature designated other state roads in the region, but organized road and bridge building was, by and large, the responsibility of Saline County itself. Under the supervision of the county court, a network of roads and trails gradually developed to link such developing settlements as Marshall, Jonesboro, Miami and Arrow Rock. The court ordered short-span timber stringer bridges built over the myriad small streams and sloughs that crisscrossed the region. Though inexpensive to erect, most of these early spans tended to be structurally suspect and required frequent maintenance to prevent their collapse. Moreover, they were limited to short-span crossings.

Throughout the 1850s and 1860s, Saline County built only timber and timber/iron combination bridges at the major crossings. In the early 1870s, however, the court began contracting for all-iron structures as a more durable alternative to wood construction. This trend continued through the end of the century, the county contracting with several state and regional fabricators for pin-connected trusses. The Pierre Fleche Creek Bridge is one of these later structures: a pinned Pratt pony truss supported by massive stone masonry abutments. The bridge dates to late 1890. That November the Saline County Court ordered F.H. Boyd, the county road and bridge commissioner, to solicit bids for the construction of this structure across Pierre Fleche Creek in the far eastern region of the county. Proposals were

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received from several firms later that year and a contract to fabricate and erect the truss let to the Massillon Bridge Company of Massillon, Ohio. Massillon fabricated the built-up iron components of the truss in its Ohio shops, shipped them by rail to the county, and erected the bridge over traditional timber falseworks. Completed in 1891, the Pierre Fleche Creek Bridge has carried traffic since, with no substantial alterations.

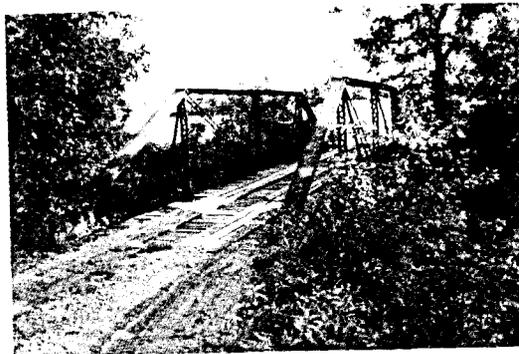
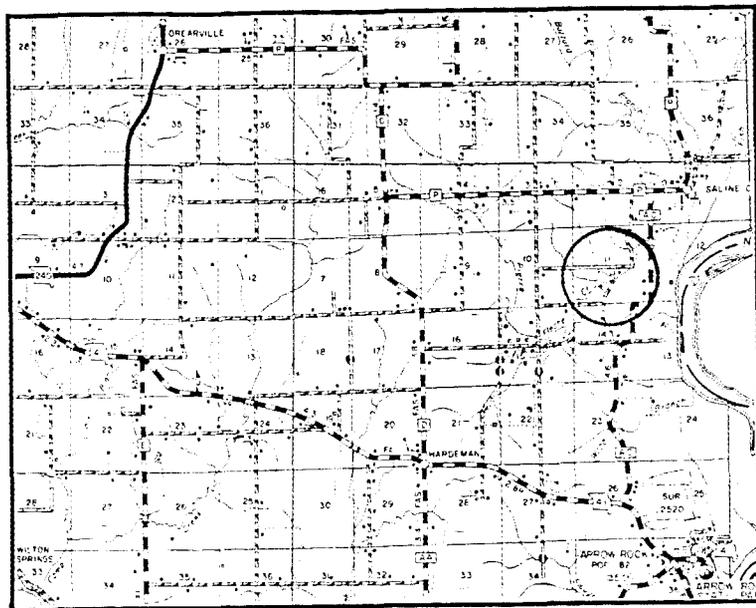
Like virtually all of Missouri's counties, Saline County followed a definite progression in its bridge construction in the 19th century, in response to evolving transportation needs and technological development in the bridge industry. The first simple spans, built as the county was undergoing its initial settlement, were rudimentary timber structures. These were cheap and easy to build but lacking in durability and limited in span length. With greater revenues from increased settlement, the county could undertake more ambitious timber trusses in the 1840s. Iron was used in the tension members in the combination spans erected between the 1850s and early 1880s. These, in turn, were superseded in the 1880s and 1890s by all-iron spans such as the Pierre Fleche Creek Bridge, made readily available by mass production. Featuring typical 19th century pin-connected detailing, this structure is one of the more noteworthy examples in Missouri of what was once a mainstay structural type—the pinned Pratt pony truss. Numerous such Pratts were built on the state's early roads in the 19th century, executed first as all-timber or timber/iron combination structures and later as all-metal configurations. Iron or steel Pratt trusses were marketed by the state's bridge companies as an inexpensive bridge for short-span and medium-span applications. With its early erection date and high degree of structural integrity, the Pierre Fleche Creek Bridge is a significant example of this bridge construction trend in Missouri.

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**NAME(S) OF STRUCTURE**

Pierre Fleche Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No 325002.1; Saline County Court Record, Book Q: page 359 ( 8 November 1890), page 439 (10 April 1891) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

24 January 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Steel's Fish Trap Bridge  
MHTD: 351000.5

SALI31

**DATE(S) OF CONSTRUCTION**

1875 / 1882

**LOCATION**

County Road 351 over Salt Fork; S13, T49N, R20W  
9.9 miles southeast of Marshall; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP eligible (score: 76)

**CONDITION**

fair

**OWNER**

Saline County

span number: 1  
span length: 100.0'  
total length: 100.0'  
roadway wdt.: 14.0'

superstructure: wrought iron, 10-panel, bolt-connected bowstring through arch-truss  
substructure: stone masonry abutments and wingwalls  
floor/decking: corrugated steel plate deck  
other features: upper chord: 2 channels with cover plate and double lacing; lower chord: 2 rectangular eyebars; vertical: star iron; diagonal: round rods with threaded ends; lateral bracing: round rod with threaded ends; strut: iron tube; floor beam: 2 channels with I-beams extended to star iron outriders; guardrail: 1 channel; chiseled into stone abutment: H. Murphy; chiseled into other stone abutment: 1875

Lacking the funds to build permanent bridges over larger rivers such as the Salt Fork and Blackwater, Saline County in its formative years licensed the operation of toll ferries during the 1820s, 1830s and 1840s. The Arrow Rock ferry across the Missouri River was clearly the region's earliest. Established in 1817 as the first ferry across the Missouri west of Franklin, it proved pivotal in the routing of the Santa Fe Trail through the county. In 1833 the county court authorized Richard Marshall to operate a ferry across the Blackwater at the mouth of Salt Fork. Other ferries and fords, notably the Hunt's Ferry across the Blackwater and the Jonesboro Ferry across Salt Fork, developed with need.

By the 1840s, however, it had become apparent that the ferries made poor river crossings and that permanent bridges were necessary for the developing road system. In May 1843 the court authorized construction of the county's first major bridge over the Salt Fork at Jonesboro. Completed two years later, the timber span lacked a wooden sheathing to protect the structural members. Leaving the superstructure exposed did not pose a serious threat to the small-scale timber pile spans, which rarely lasted long enough to suffer damage by rain and snow. But more complex and expensive timber trusses, left open to the weather, proved vulnerable to deterioration in their chord connections. The Jonesboro Bridge lasted only nine years before it needed replacement. For this reason, the county began covering its wooden trusses with shingle roofs and plank siding in the early 1850s. Structures such as the new Jonesboro Bridge [1854] and Kiser Bridge [1854] over Salt Fork and the Napton Bridge [1855] and Marshall Bridge [1856] over the Blackwater featured timber/iron Howe truss superstructures with wood housings.

Throughout the 1850s and 1860s, Saline County built only timber and timber/iron combination bridges at the major crossings. In the early 1870s, however, the court began contracting for all-iron structures as a more durable alternative to wood construction. Two of

these earliest iron trusses were an 80-foot span across Salt Fork at Walker's Ford and a 60-foot span across Salt Fork on the Marshall-Miami Road, both erected in 1875 by Farnsworth and Eaves of Kansas City. In the early 1880s the county began replacing some of its earliest covered bridges with iron spans. The Steel's Fish Trap Bridge is one of these replacement structures. The original bridge at this crossing southeast of Marshall was built in 1875. That February the Saline County Court appropriated funds, in the amount of \$1600.00, to build a bridge across the Salt Fork Creek at Steel's Trap, as petitioned by G.A. Munel and others. The court at that time appointed the county road commissioner to prepare the plans and specifications for the structure. The bridge contract was awarded a month later to W.L. Smiley for \$1550.00. After construction was underway in August, George W. Latimer, the Saline County Bridge Commissioner, reported to the road and bridge commission that "a portion of the masonry (fell) short of specs in some unimportant particulars." Nonetheless, the court accepted the bridge as complete, paying Smiley \$1507.50 for the masonry work and \$1550.00 for the combination truss superstructure.

It is not known whether the original bridge here was sheathed with siding. Seven years after its completion, the county began planning for its replacement—an indicator of either calamity or poor construction. The Missouri Valley Bridge and Iron Company of Leavenworth, Kansas, placed an \$880.00 bid in October 1882 to erect the replacement superstructure: a 100-foot wrought iron bowstring arch-truss using the firm's patented configuration. Since its 1882 completion, the Steel's Fish Trap Bridge continues to carry traffic in essentially unaltered condition.

The bowstring arch-truss was the iron span of choice for Missouri counties in the late 1860s and 1870s. Marketed extensively throughout the Midwest by such industry giants as the King Iron Bridge and Manufacturing Company, the Wrought Iron Bridge Company and the Missouri Valley Bridge and Iron Company, these often-patented bridge forms featured a wide range of span lengths, economical fabrication cost and relatively quick erection. The proliferation of the bowstring corresponded with the initial development of Missouri's road system. As a result, perhaps thousands of these prototypical iron spans were erected throughout the state.

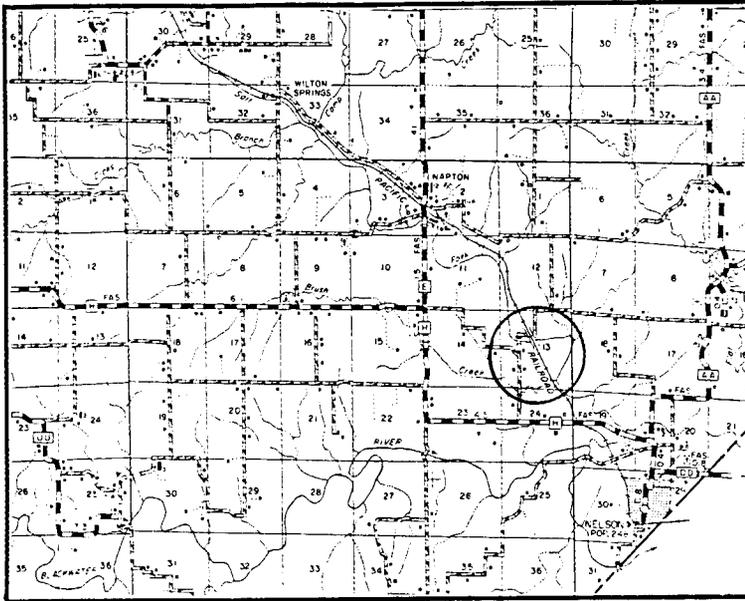
The bowstring had some rather severe structural flaws, however, relating primarily to lateral stability of the arches, and it was largely superseded by the pin-connected truss in the early 1880s. Despite this, some bowstrings were still erected in Missouri in the 1880s, although the number dwindled precipitously by the decade's end. Through subsequent attrition, almost all of Missouri's bowstrings have since been replaced and demolished. Now only a half-dozen remain in place. With its excellent degree of physical integrity, the Steel's Fish Creek Bridge is both historically and technologically significant as one of the last remaining examples in the state of what was once a mainstay structural type.

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**NAME(S) OF STRUCTURE**

Steel's Fish Trap Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP



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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 351000.5; Saline County Court Record, Book K: page 194 (5 February 1875), page 216 (2 March 1875), page 303 (9 August 1875), page 335 (2 November 1875), page 83 (10 February 1882), page 219 (10 October 1882) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

24 January 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Boat Yard Ford Bridge  
MHTD: 368002.5

SALI34

**DATE(S) OF CONSTRUCTION**

1888

**LOCATION**

County Road 368 over unnamed stream; S19, T49N, R19W  
11.7 miles southeast of Marshall; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / abandoned

**RATING** NRHP possibly eligible (score: 56)

**CONDITION**

fair

**OWNER**

Saline County

span number: 1  
span length: 150.0'  
total length: 200.0'  
roadway wdt.: 12.0'

superstructure: wrought iron, 9-panel, pin-connected Pratt through truss, with 3-panel, pin-connected Pratt pony truss approach span  
substructure: stone masonry abutments; concrete-filled wrought iron cylinder piers  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 punched rectangular eyebars, or 2 channels with lacing; diagonal: 2 punched rectangular eyebars; lateral bracing: round rod with threaded ends; strut: I-beam; floor beam: riveted plate girder, U-bolted to vertical

Now abandoned with its timber deck burned and badly decomposed, this long-span truss crosses the Blackwater River at the Boat Yard Ford some eleven miles southeast of Marshall. This pinned Pratt truss dates to 1888. The original bridge constructed on this site was replaced in 1856, when the Marshall Covered Bridge was built on the earlier bridge's stone abutments and center pier. Irreparably damaged in a flood and ensuing ice jam, only the wooden bridge's stone abutments and pier remained. The Boat Yard Ford Bridge was constructed using the original abutments. Bridge commissioner George W. Latimer was instructed in May 1888 by the Saline County Court to obtain construction bids for a replacement structure here. That August Latimer presented to the court a proposal from the Kansas City Bridge and Iron Company to fabricate and build the bridge for \$4800.00. Completed later that year, the bridge features concrete-filled wrought iron cylinder piers and pony truss approach spans. The court issued a warrant in 1890 to pay a Mr. A. Olson \$1261.00 for masonry work he completed on the bridge's original stone abutments, presumably for minor repairs. Although this bridge is no longer carrying traffic, the truss remains remarkably intact.

Like virtually all of Missouri's counties, Saline County followed a definite progression in its bridge construction in the 19th century, in response to evolving transportation needs and technological development in the bridge industry. The first simple spans, built as the county was undergoing its initial settlement, were rudimentary timber structures. These were cheap and easy to build but lacking in durability and limited in span length. With greater revenues from increased settlement, the county could undertake more ambitious timber trusses in the 1840s. Iron was used in the tension members in the combination spans erected between the 1850s and early 1880s. These, in turn, were superseded in the 1880s and 1890s by all-iron spans such as the Boat Yard Ford Bridge, made readily available by mass production. Featuring typical 19th century pin-connected detailing, this structure is one of the more noteworthy examples in Missouri of what was once a mainstay structural type—the pinned Pratt pony truss. Numerous such Pratts were built on the state's early

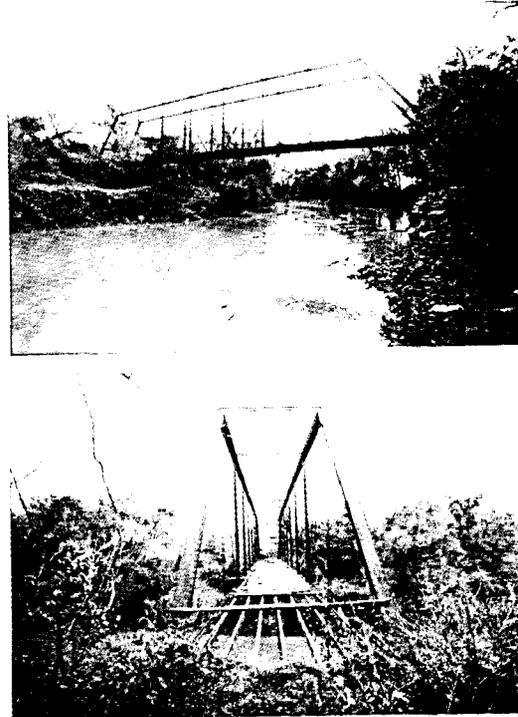
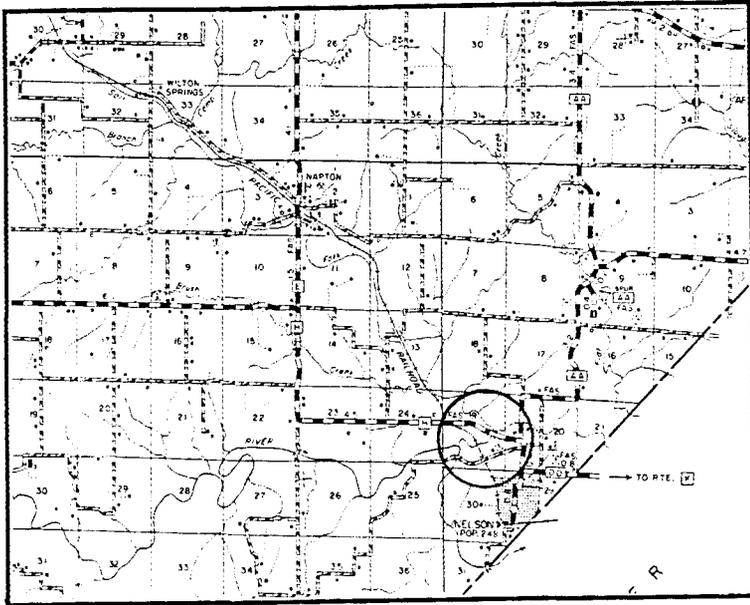
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roads in the 19th century, executed first as all-timber or timber/iron combination structures and later as all-metal configurations. Iron or steel Pratt trusses were marketed by the state's bridge companies as an inexpensive bridge for short-span and medium-span applications. With its early erection date and high degree of structural integrity, the boat Yard Ford Bridge is a significant example of this bridge construction trend in Missouri.

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**NAME(S) OF STRUCTURE**

Boat Yard Ford Bridge

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 368002.5; Saline County Court Record, Book P: page 486 (7 May 1888), page 531 (7 August 1888), page 598 (8 January 1889); Saline County Court Record, Book Q: page 177 (9 January 1890); Saline County History, Volume One (1983) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

24 January 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**  
Finney Creek Bridge  
MHTD: 390000.8

SALI37

**DATE(S) OF CONSTRUCTION**  
1895

**LOCATION**

County Road 390 over Finney Creek; S28, T49N, R21W  
9.5 miles south of Marshall; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 46)

**CONDITION**

fair

**OWNER**

Saline County

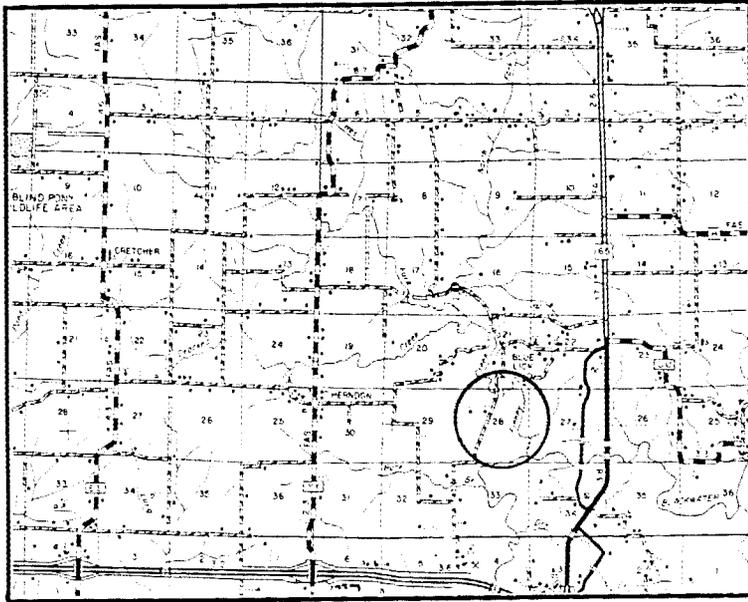
span number: 1	superstructure: steel, 5-panel, pin-connected Pratt through truss, with steel stringer approach spans
span length: 80.0'	substructure: stone masonry abutments; concrete-filled steel cylinder piers
total length: 109.0'	floor/decking: timber deck over steel stringers
roadway wdt.: 14.0'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 2 looped square eyebars, or 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: 2 round eyerods with turnbuckles; lateral bracing: round rod with turnbuckle; strut: 4 angles with lacing; floor beam: riveted plate girder, U-bolted to vertical; guardrail: 1 angle

This bridge carries a county road over Finney Creek near Blue Lick Spring, a small crossroads community some nine miles south of Marshall. Consisting of a single-span, pinned Pratt through truss, the bridge features steel angle guardrails and stone abutments, with tubular steel piers. First ordered built in November 1894 by the Saline County Commissioners, the structure was completed in March the following year. The county paid Wrought Iron Bridge Company of Canton, Ohio, \$1477.00 for the construction of two bridges, including the Finney Creek Bridge. Only two bridges were built by the Wrought Iron Bridge Company in Saline County during this period, while most of the bridges were assembled at that time by the Kansas City Bridge Company of Missouri. Since its completion, the structure has carried traffic in essentially unaltered condition.

In Missouri, the pinned Pratt through truss was the bridge of choice for short- and medium-span applications in the late 19th and 20th centuries. As a result, thousands of Pratts were built across the state, and today Pratts constitute the most populous group of through trusses. Though it retains a relatively high degree of physical integrity, the Finney Creek Bridge is unremarkable in its design, dimensions, and detailing.

**NAME(S) OF STRUCTURE**  
Finney Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 390000.8; Saline County Court Record, Book S: page 87 (9 November 1894), page 186 (5 March 1895); Index to Saline County Proceedings V: page 143 (7 May 1904) - located at Saline County Courthouse, Marshall MO; field inspection by Clayton Fraser, 2 October 1991.

**INVENTORIED BY**  
Clayton B. Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
24 January 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Eastwood Road Overpass  
MHTD: 477000.1

SALI43

**DATE(S) OF CONSTRUCTION**

1923

**LOCATION**

Eastwood Road over Missouri Pacific Railroad; S12, T50N, R21W  
Marshall; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

highway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 59)

**CONDITION**

good

**OWNER**

Saline County

span number: 3	superstructure: concrete through girder, skewed
span length: 35.0'	substructure: concrete abutments, wingwalls and piers
total length: 105.0'	floor/decking: concrete deck
roadway wdt.: 19.0'	other features: solid concrete girders with recessed rectangular panels

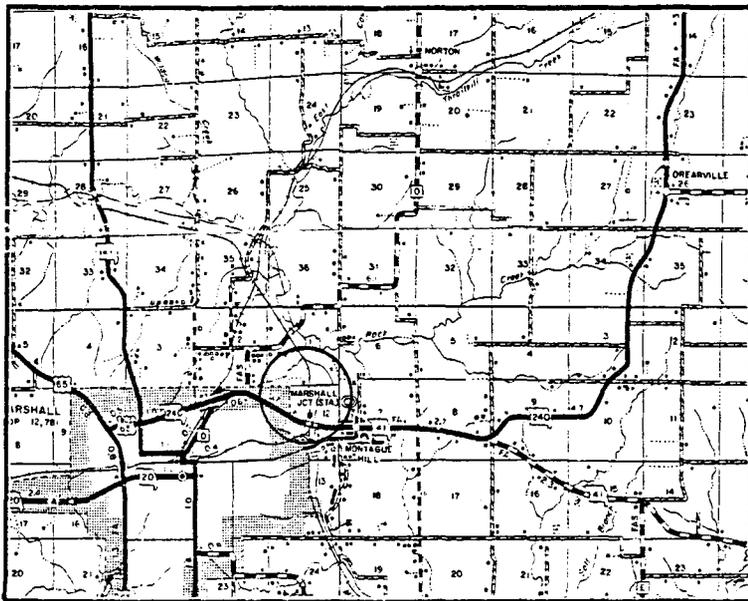
One of the provisions of the Hawes Road Law establishing the Missouri State Highway Department was that the newly formed agency develop plans and specifications for bridges and culverts. "The Highway Department has maintained a drafting room which has been called upon for many kinds of service," the department reported in 1918, "but the especial function of which has been the preparation of bridge and culvert designs." By 1920, the department had developed several standards for short- and medium-span bridges, including 13 designs for steel superstructures with spans ranging up to 100 feet. In addition, the department delineated some 185 special bridge designs during the 1919-20 biennium. In 1922 the department designed some 293 spans with a total length of over 20,000 feet. One of these was a structure over the tracks of the Missouri Pacific Railroad in Marshall County, a three-span concrete through girder, supported on a skew by concrete piers and abutments. One of two concrete structures designed by MSHD as part of work on State Route 20 through Marshall, the Saline County seat, it was built as a joint venture between the highway department and the railroad. Monett, Missouri, contractor M.E. Gillioz filled and graded the approaches under contract with the highway department, and the railroad built the structure itself. Completed in 1923, the Eastwood Road Overpass has functioned in place to the present. It is no longer located on a state route, but the overpass remains in unaltered condition. The Eastwood Road Overpass is historically significant as one of the few structures left intact from MSHD's formative years—a small-scale concrete beam bridge that represents early highway bridge building in Missouri.

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**NAME(S) OF STRUCTURE**

Eastwood Road Overpass

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 477000.1; Missouri State Highway Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; Missouri State Highway Board, First Biennial Report: 1917-18, pages 100-101; Missouri State Highway Board, Second Biennial Report: 1919-20, pages 129, 133, 148-49.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

24 January 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Eastwood Road Bridge  
MHTD: 477000.2

SALI44

**DATE(S) OF CONSTRUCTION**

1923

**LOCATION**

Eastwood Road over Salt Fork Creek; S12, T50N, R21W  
Marshall; Saline County, Missouri

**USE (ORIGINAL / CURRENT)**

highway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 59)

**CONDITION**

good

**OWNER**

Saline County

span number: 1; 2	superstructure: concrete filled spandrel arch
span length: 90.0'; 60.0'	substructure: concrete abutments, wingwalls and piers
total length: 281.0'	floor/decking: concrete deck over earth fill
roadway wdt.: 18.8'	other features: MSHD standard concrete guardrails

Located on the eastern periphery of Marshall, the Saline County seat, this three-span concrete bridge was built in 1923 by M.E. Gillioz of Monett, Missouri. The Eastwood Road Bridge is comprised of a 90-foot filled spandrel arch, flanked by two 60-foot arches for an overall length of 281 feet. The bridge was designed in late 1922 by the Missouri State Highway Commission, and on December 23rd, Gillioz won the contract to build it. Completed the following year for over \$46,000, the Eastwood Road Bridge has carried Missouri State Highway 20 (later State Highway 41, and more recently County Road 477) over Salt Fork Creek, without serious alteration.

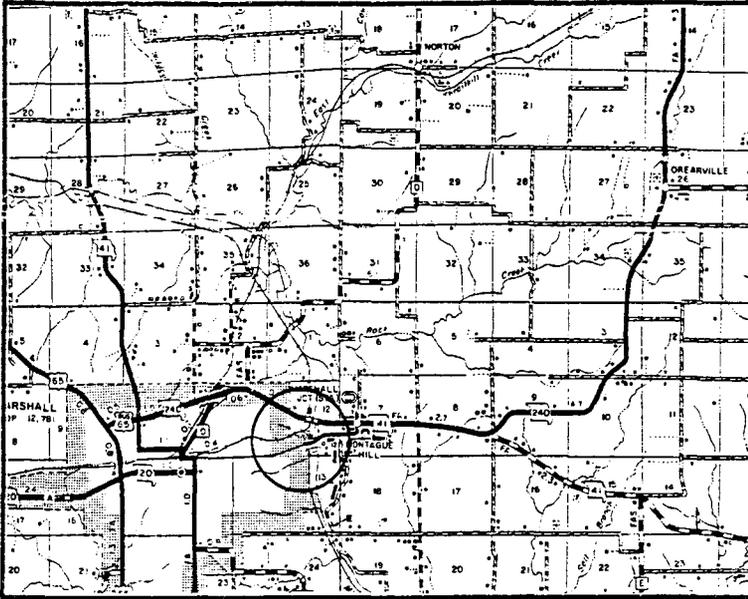
The Missouri State Highway Department typically used filled spandrel designs for its concrete arches with 80 feet or less of span; open spandrel arches were employed for longer-span applications. MSHD engineers designed numerous single-span examples of the former configuration in the 1920s, but few filled spandrel arches with multiple spans. A handful of these large-scale bridges have been identified by the statewide bridge inventory. The Eastwood Road Bridge in Saline County stands out among those remaining for its three-span configuration. It is, therefore, technologically significant as a well-preserved, multiple-span representative of Missouri State Highway Department concrete design during its formative years in the early 1920s.

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**NAME(S) OF STRUCTURE**

Eastwood Road Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 477000.2; Missouri State Highway Department Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO; Missouri State Highway Board, First Biennial Report: 1917-18, pages 100-101; Missouri State Highway Board, Second Biennial Report: 1919-20, pages 129, 133, 148-49.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

24 January 1992

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# SCHUYLER COUNTY

**INCLUDED:** [Significant feature(s) of bridge given in boldface]  
[Field inventoried bridge indicated by asterisk]

Inv.No.	MHTD	Bridge Name	Description
SCHU01	K 25	Chariton River Bridge	14-25' steel stringer 1932 Pittsburgh-Des Moines Steel Co.
SCHU02	K 26	Chariton River Bridge	1-120' <b>riveted Pratt through truss</b> 1932 Pittsburgh-Des Moines Steel Co.
*SCHU03	002000.1	Mock Bridge	1-108' <b>pinned Pratt through truss</b> 1904 William Irelan
*SCHU04	006000.7	Thief Creek Bridge	1- 60' pinned Pratt pony truss c1910
*SCHU05	012001.4	Chariton River Bridge	1-112' <b>pinned Pratt through truss</b> 1909 Illinois Steel Bridge Company
SCHU06	022000.9	Bridge	1- 40' pinned Pratt pony truss c1910 Illinois Steel Bridge Company
SCHU07	040001.3	North Fabius River Bridge	1- 60' pinned Pratt pony truss c1910 Illinois Steel Bridge Company
SCHU08	068000.5	Bridge	(replaced)
*SCHU09	092001.1	Middle Fabius River Bridge	1- 60' pinned Pratt pony truss c1915 Illinois Steel Bridge Company
*SCHU10	097001.1	North Fabius River Bridge	(replaced)
SCHU11	131001.2	North Fabius Branch Bridge	1- 40' pinned Pratt pony truss c1910 Illinois Steel Bridge Company
SCHU12	155001.5	Middle Fabius River Bridge	(replaced)
SCHU13	158001.1	Middle Fabius River Bridge	(replaced)
*SCHU14	172000.6	Elm Creek Bridge	1- 50' pinned Pratt pony truss c1910 Illinois Steel Bridge Company
*SCHU15	175000.5	Bridge	1- 40' pinned Pratt pony truss 1910 Illinois Steel Bridge Company
SCHU16	238000.3	South Fork Bridge	1- 60' pinned Pratt pony truss c1910 Illinois Steel Bridge Company
*SCHU17	239002.8	South Fork Bridge	1- 80' pinned Pratt pony truss c1910
*SCHU18	240000.7	Brush Creek Bridge	1- 60' pinned Pratt pony truss 1910 Illinois Steel Bridge Co.
*SCHU19	256001.9	North Fork Bridge	1- 60' pinned Pratt pony truss c1910 Illinois Steel Bridge Company
SCHU20	260001.0	Brush Creek Bridge	1- 60' pinned Pratt pony truss c1910
*SCHU21	270003.4	Brushy Creek Bridge	1- 48' pinned Pratt pony truss c1910 Illinois Steel Bridge Company
*SCHU22	274000.1	Waldo Bridge	1-112' <b>pinned Pratt through truss</b> 1903 Dildine Bridge Company
*SCHU23	274000.9	Chariton River Bridge	1- 80' pinned Pratt pony truss c1910 Illinois Steel Bridge Company

# SCHUYLER COUNTY

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## EXCLUDED:

Pratt pony truss

115000.1 116000.6 149000.2 172000.6 198000.1

Warren pony truss

100000.8 153000.6 230001.8

Lattice bedstead

034001.0 251001.0

Steel stringer

J 690	K 60	S 23	S 166	S 388	S 911	S 912
T 891	T 892	X 97	X 798	004000.8	028001.9	
035000.8	037002.6	051000.3	081001.5	091000.7	097001.2	113000.4
122000.6	130000.9	135000.3	136000.3	147000.8	151000.5	155001.3
160001.1	162000.1	176000.5	182001.4	201001.3	216000.2	221000.2
221000.7	224000.8	227R01.8	228R00.1	232001.1	234002.4	237000.1
240000.7	247000.5	248001.0	252000.2	255001.2	266000.5	

Concrete girder

G 162 H 384R J 34 K 111 L 136 L 140 280000.3

Concrete slab

077000.6

Concrete box culvert

H 383R J 26 K 880 L 137 L 138 L 139 T 890  
 X 797 X 971 X 972

## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	2	17	0	0	19
Excluded	28	46	0	0	74
	30	63	0	0	93 structures

# Chariton River Bridge

SCHU01

## GENERAL DATA

<b>structure no.:</b>	K 25	<b>city/town:</b>	8.5 miles northwest of Queen City
<b>county:</b>	Schuyler	<b>feature inters.:</b>	Chariton River
		<b>cadastral grid:</b>	S33/34, T66N, R16W
		<b>highway route:</b>	U.S. Highway 136
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Missouri Highway and Transportation Department

## STRUCTURAL DATA

<b>superstructure:</b>	steel stringer	<b>condition:</b>	good
<b>substructure:</b>	timber pile bent abutments, wingwalls and piers	<b>alterations:</b>	none
<b>span number:</b>	14	<b>floor/decking :</b>	concrete deck
<b>span length:</b>	25.0'	<b>other features:</b>	MSHD standard concrete guardrails with square balusters
<b>total length :</b>	383.0'		
<b>roadway width:</b>	22.0'		

## HISTORICAL DATA

<b>erection date:</b>	1932
<b>erection cost:</b>	\$13,449.93
<b>designer:</b>	Missouri State Highway Department
<b>fabricator :</b>	Pittsburgh-Des Moines Steel Company, Des Moines IA
<b>contractor:</b>	Pittsburgh-Des Moines Steel Company, Des Moines IA
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. K 25; Missouri Highway and Transportation Department, Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO.
<b>sign. rating:</b>	42
<b>evaluation:</b>	NRHP non-eligible (Other than its curved alignment and multiple spans, this bridge is technologically undistinguished.)

Inventoried by: Clayton B. Fraser    4 February 1992

# Chariton River Bridge

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SCHU02

## GENERAL DATA

<b>structure no.:</b> K 26	<b>city/town:</b> 8.3 miles northwest of Queen City
<b>county:</b> Schuyler	<b>feature inters.:</b> Chariton River
	<b>cadastral grid:</b> S34, T66N, R16W
	<b>highway route:</b> U.S. Highway 136
	<b>highway distr.:</b> 2
	<b>current owner:</b> Missouri Highway and Transportation Department

## STRUCTURAL DATA

**superstructure:** steel, 6-panel, rigid-connected Pratt through truss with steel stringer approach spans

**substructure:** concrete abutments, wingwalls and piers

<b>span number:</b> 1	<b>condition:</b> good
<b>span length:</b> 120.0'	<b>alterations:</b> none
<b>total length:</b> 227.0'	<b>floor/decking :</b> concrete deck over steel stringers
<b>roadway width:</b> 22.0'	<b>other features:</b> steel guardrails

## HISTORICAL DATA

**erection date:** 1932

**erection cost:** \$18,443.13

**designer:** Missouri State Highway Department

**fabricator :** Pittsburgh-Des Moines Steel Company, Des Moines IA

**contractor:** Pittsburgh-Des Moines Steel Company, Des Moines IA

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. K 26; Missouri Highway and Transportation Department, Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City MO.

**sign. rating:** 43

**evaluation:** NRHP non-eligible (typically configured example of MSHD truss design of the 1930s)

**Inventoried by:** Clayton B. Fraser    4 February 1992

# Mock Bridge

SCHU03

## GENERAL DATA

structure no.: 002000.1      city/town: 8.9 miles northwest of Lancaster  
county: Schuyler      feature inters.: Chariton River  
cadastral grid: S34, T67N, R16W  
highway route: county road  
highway distr.: 2  
current owner: Schuyler County

## STRUCTURAL DATA

superstructure: steel, 7-panel, pin-connected Pratt through truss; 1 steel stringer approach span at the north end; 2 steel stringer approach spans at the south end  
substructure: concrete-filled steel cylinder piers (smaller steel cylinder piers under south approach span)

span number: 1      condition: fair/poor  
span length: 108.0'      alterations: deck partially removed  
total length: 156.0'      floor/decking : timber deck over steel stringers  
roadway width: 11.4'      other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (2 looped rectangular eyebars at the hip); diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round looped and bolted rod with turnbuckle; strut: 2 angles; portal strut: A-frame; floor beam: I-beam, field-bolted to vertical

## HISTORICAL DATA

erection date: 1904  
erection cost: \$1595.00 (contract amount)  
designer: G.W. Dickson, Schuyler County Bridge Commissioner  
fabricator : Jones and Laughlin Steel Company, Pittsburgh PA  
contractor : William A. Irelan

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 002000.1; Schuyler County Court Record 14: page 500 (19 May 1903); Schuyler County Court Record 15: page 29 (5 April 1904), located at Schuyler County Courthouse, Lancaster MO; Putnam County Court Record M: page 226 (2 February 1903), page 270 (27 March 1903), located at Putnam County Courthouse, Unionville MO; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 7 October 1991.

sign. rating: 40  
evaluation: NRHP non-eligible (typically configured example of early wagon truss type)

inventoried by: Clayton B. Fraser      4 February 1992

# Thief Creek Bridge

SCHU04

## GENERAL DATA

<b>structure no.:</b> 006000.7	<b>city/town:</b> 7.2 miles west of Lancaster
<b>county:</b> Schuyler	<b>feature inters.:</b> Thief Creek
	<b>cadastral grid:</b> S11, T66N, R16W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Schuyler County

## STRUCTURAL DATA

**superstructure:** steel, 4-panel pin-connected Pratt pony truss  
**substructure:** timber pile bent abutments

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 60.0'	<b>alterations:</b> none
<b>total length:</b> 60.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 15.0'	<b>other features:</b> upper chord / inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical

## HISTORICAL DATA

**erection date:** c1910  
**erection cost:** unknown  
**designer:** unknown  
**fabricator :** Lackawanna Steel Company, Pittsburgh PA  
**contractor:** unknown

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 006000.7; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 7 October 1991.

**sign. rating:** 30  
**evaluation:** NRHP non-eligible (typically configured, inadequately documented example of common structural type)

**inventoried by:** Clayton B. Fraser 4 February 1992

# Chariton River Bridge

SCHU05

## GENERAL DATA

structure no.:	012001.4	city/town:	7.7 miles west of Lancaster
county:	Schuyler	feature inters.:	Chariton River
		cadastral grid:	S15, T66N, R16W
		highway route:	county road
		highway distr.:	2
		current owner:	Schuyler County

## STRUCTURAL DATA

**superstructure:** steel, 7-panel, pin-connected Pratt through truss, with steel stringer approach spans  
**substructure:** timber pile bent abutments

span number:	1	condition:	fair
span length:	112.0'	alterations:	truss moved, c1932
total length:	144.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.5'	other features:	upper chord/ inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (looped square eyebars at the hip); diagonal: 2 punched rectangular eyebars; counter: 2 looped square eyebars with turnbuckles; lateral bracing: round rod with threaded ends; strut: 2 angles with continuous plate; portal strut: A-frame; floor beam: I-beam, field-bolted to vertical; guard-rail: 2 latticed angles; bridge plate: Built by the Illinois Steel Bridge Co. Jacksonville Ill.

## HISTORICAL DATA

**erection date:** 1909; moved c1932  
**erection cost:** unknown  
**designer:** George Grist, Schulyer County Road and Bridge Commissioner  
**fabricator :** Illinois Steel Company, Chicago IL  
**contractor :** Illinois Steel Bridge Company, Jacksonville IL

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 012001.4; Schuyler County Court Record 16: page 152 (20 May 1908), page 168 (30 June 1908), page 184 (6 August 1908), page 437 (7 October 1909), located at Schuyler County Courthouse, Lancaster MO; Putnam County Court Record N: page 540 (9 May 1908), Putnam County Court Record O: page 29 (8 February 1909), pages 111-12 (22 May 1909), page 153 (3 November 1909), located at Putnam County Courthouse, Unionville MO; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 7 October 1991.

**sign. rating:** 42  
**evaluation:** NRHP non-eligible (typically configured example of common structural type, moved to this location)

**inventoried by:** Clayton B. Fraser 4 February 1992

# Bridge

SCHU06

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## GENERAL DATA

structure no.:	022000.9	city/town:	4.0 miles west of Lancaster
county:	Schuyler	feature inters.:	tributary of Elm Creek
		cadastral grid:	S17/20, T66N, R15W
		highway route:	county road
		highway distr.:	2
		current owner:	Schuyler County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss  
substructure: timber pile bent abutments and wingwalls

span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	40.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel lattice guardrails

## HISTORICAL DATA

erection date: c1910  
erection cost: unknown  
designer: Illinois Steel Bridge Company, Jacksonville IL  
fabricator : Illinois Steel Bridge Company, Jacksonville IL  
contractor: Illinois Steel Bridge Company, Jacksonville IL

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 022000.9.

sign. rating: 33  
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser    4 February 1992

# North Fabius River Bridge

SCHU07

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## GENERAL DATA

<b>structure no.:</b> 040001.3	<b>city/town:</b> 3.9 miles north of Lancaster
<b>county:</b> Schuyler	<b>feature inters.:</b> North Fabius River
	<b>cadastral grid:</b> S34/35, T67N, R15W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Schuyler County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt pony truss	
<b>substructure:</b> timber pile bent abutments and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 60.0'	<b>alterations:</b> unknown
<b>total length:</b> 60.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.8'	<b>other features:</b> steel lattice guardrails

## HISTORICAL DATA

<b>erection date:</b> c1910
<b>erection cost:</b> unknown
<b>designer:</b> Illinois Steel Bridge Company, Jacksonville IL
<b>fabricator :</b> Illinois Steel Bridge Company, Jacksonville IL
<b>contractor :</b> Illinois Steel Bridge Company, Jacksonville IL
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 040001.3.
<b>sign. rating:</b> 36
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)

**inventoried by:** Clayton B. Fraser    4 February 1992

# Middle Fabius River Bridge

SCHU09

## GENERAL DATA

<b>structure no.:</b> 092001.1	<b>city/town:</b> 4.5 miles east of Lancaster
<b>county:</b> Schuyler	<b>feature inters.:</b> Middle Fabius River
	<b>cadastral grid:</b> S22/23, T66N, R14W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Schuyler County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach span	
<b>substructure:</b> timber pile bent abutments and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 60.0'	<b>alterations:</b> truss perhaps moved
<b>total length:</b> 80.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 13.8'	<b>other features:</b> upper chord / inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing and gusset plates; diagonal: 2 looped square eyebars; counter: looped round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 latticed angles

## HISTORICAL DATA

<b>erection date:</b> c1915	
<b>erection cost:</b> unknown	
<b>designer:</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>fabricator :</b> Illinois Steel Bridge Company, Jacksonville IL; Illinois Steel Company, Chicago IL	
<b>contractor:</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 092001.1; field inspection by Ron Ashbach-Sladek and Clayton Fraser, 7 October 1991.	
<b>sign. rating:</b> 31	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type, perhaps moved)	

Inventoried by: Clayton B. Fraser 4 February 1992

# North Fabius Branch Bridge

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SCHU11

## GENERAL DATA

<b>structure no.:</b> 131001.2	<b>city/town:</b> 9.5 miles east of Lancaster
<b>county:</b> Schuyler	<b>feature inters.:</b> branch of North Fabius River
	<b>cadastral grid:</b> S27/28, T66N, R13W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Schuyler County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach span	
<b>substructure:</b> timber pile bent abutments, wingwalls and piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 40.0'	<b>alterations:</b> unknown
<b>total length:</b> 55.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.3'	<b>other features:</b> unknown

## HISTORICAL DATA

<b>erection date:</b> c1910	
<b>erection cost:</b> unknown	
<b>designer:</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>fabricator :</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>contractor :</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 131001.2.	
<b>sign. rating:</b> 33	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)	

**inventoried by:** Clayton B. Fraser    4 February 1992

# Elm Creek Bridge

SCHU14

## GENERAL DATA

structure no.: 172000.6      city/town: 7.9 miles southwest of Lancaster  
county: Schuyler      feature inters.: Elm Creek  
cadastral grid: S2, T65N, R16W  
highway route: county road  
highway distr.: 2  
current owner: Schuyler County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach span  
substructure: concrete abutments and wingwalls with timber pile bent pier

span number: 1      condition: fair/poor  
span length: 50.0'      alterations: none  
total length: 66.0'      floor/decking : timber deck over steel stringers  
roadway width: 11.6'      other features: upper chord / inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing and gusset plates; diagonal: 2 square eyebars; counter: 2 square eyebars with turnbuckles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 small channels

## HISTORICAL DATA

erection date: c1910  
erection cost: unknown  
designer: Illinois Steel Bridge Company, Jacksonville IL  
fabricator : Illinois Steel Bridge Company, Jacksonville IL;  
Cambria Steel Company, Pittsburgh PA  
contractor: Illinois Steel Bridge Company, Jacksonville IL

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 172000.6; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 7 October 1991.

sign. rating: 33  
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser      4 February 1992

# Bridge

SCHU15

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## GENERAL DATA

<b>structure no.:</b> 175000.5	<b>city/town:</b> 7.4 miles southwest of Lancaster
<b>county:</b> Schuyler	<b>feature inters.:</b> branch of Elm Creek
	<b>cadastral grid:</b> S7, T65N, R15W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Schuyler County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected Pratt pony truss	
<b>substructure:</b> concrete-filled steel cylinder piers with timber back- and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 40.0'	<b>alterations:</b> none
<b>total length:</b> 50.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 11.9'	<b>other features:</b> upper chord / inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched square eyebars; counter: 2 looped square eyerods with turnbuckles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 angles with lacing; bridge plate: 1910 Illinois Steel Bridge Co. Jacksonville Ill.

## HISTORICAL DATA

<b>erection date:</b> 1910	
<b>erection cost:</b> \$3779.00 (multiple-bridge contract)	
<b>designer:</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>fabricator :</b> Illinois Steel Bridge Company, Jacksonville IL; Cambria Steel Company, Pittsburgh PA	
<b>contractor:</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 175000.5; Schuyler County Court Record 16, page 563 (18 May 1910), located at Schuyler County Courthouse, Lancaster MO; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 7 October 1991.	
<b>sign. rating:</b> 41	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)	

inventoried by: Clayton B. Fraser 4 February 1992

# South Fork Bridge

SCHU16

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## GENERAL DATA

structure no.:	238000.3	city/town:	7.7 miles southeast of Lancaster
county:	Schuyler	feature inters.:	South Fork of Middle Fabius River
		cadastral grid:	S14, T65N, R14W
		highway route:	county road
		highway distr.:	2
		current owner:	Schuyler County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	concrete/timber pile bent abutments and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	75.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel lattice guardrails

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Illinois Steel Bridge Company, Jacksonville IL
fabricator :	Illinois Steel Bridge Company, Jacksonville IL
contractor :	Illinois Steel Bridge Company, Jacksonville IL
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 238000.3.
sign. rating:	36
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Clayton B. Fraser    4 February 1992

# South Fork Bridge

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SCHU17

## GENERAL DATA

structure no.: 239002.8      city/town: 8.4 miles east of Queen City  
county: Schuyler      feature inters.: South Fork of Middle Fabius River  
cadastral grid: S24/25, T65N, R14W  
highway route: county road  
highway distr.: 2  
current owner: Schuyler County

## STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss, with  
substructure: timber abutments, wingwalls and piers

span number:	1	condition:	fair
span length:	80.0'	alterations:	none
total length:	95.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.0'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: 2 looped round eyerods with turnbuckles; lateral bracing: round eyerods with threaded ends; floor beam: I-beam, field-bolted to vertical

## HISTORICAL DATA

erection date: c1910  
erection cost: unknown  
designer: unknown  
fabricator : Inland Steel Company, East Chicago IN  
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 239002.8; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 7 October 1991.

sign. rating: 30  
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser      4 February 1992

# Brush Creek Bridge

SCHU18

## GENERAL DATA

structure no.: 240000.7      city/town: 7.1 miles east of Queen City  
county: Schuyler      feature inters.: Brush Creek  
cadastral grid: S26, T65N, R14W  
highway route: county road  
highway distr.: 2  
current owner: Schuyler County

## STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach span

substructure: timber pile bent abutments and pier

span number: 1      condition: fair  
span length: 60.0'      alterations: none  
total length: 67.0'      floor/decking : timber deck over steel stringers  
roadway width: 13.6'      other features: upper chord / inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing and gusset plates; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 latticed angles; end post-mounted bridge plate: Illinois Steel Bridge Co. Jacksonville, Ill. 1910

## HISTORICAL DATA

erection date: 1910  
erection cost: \$3779.00 (multiple-bridge contract)  
designer: Illinois Steel Bridge Company, Jacksonville IL  
fabricator : Illinois Steel Bridge Company, Jacksonville IL;  
Cambria Steel Company, Pittsburgh PA  
contractor: Illinois Steel Bridge Company, Jacksonville IL  
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 240000.7; Schuyler County Court Record 16, page 563 (18 May 1910), located at Schuyler County Courthouse, Lancaster MO; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 7 October 1991.  
sign. rating: 44  
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser      4 February 1992

# North Fork Bridge

SCHU19

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## GENERAL DATA

structure no.: 256001.9      city/town: 5.2 miles southeast of Queen City  
county: Schuyler      feature inters.: North Fork of South Fabius River  
cadastral grid: S4, T64N, R14W  
highway route: county road  
highway distr.: 2  
current owner: Schuyler County

## STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss  
substructure: concrete abutments and wingwalls

span number: 1      condition: fair  
span length: 60.0'      alterations: none  
total length: 60.0'      floor/decking : timber deck over steel stringers  
roadway width: 11.8'      other features: upper chord / inclined end post: 2 channels  
with cover and batten plates; lower chord: 2  
punched rectangular eyebars; vertical: 4 angles  
with lacing and gusset plates; diagonal: looped  
round eyerods with turnbuckles; lateral bracing:  
round rod with threaded ends; floor beam: I-  
beam, field-bolted to vertical; guardrail: 2  
latticed angles

## HISTORICAL DATA

erection date: c1910  
erection cost: unknown  
designer: Illinois Steel Bridge Company, Jacksonville IL  
fabricator : Illinois Steel Bridge Company, Jacksonville IL;  
Illinois Steel Company, Chicago IL  
contractor: Illinois Steel Bridge Company, Jacksonville IL  
references: Missouri Highway and Transportation Department, Structure Inventory  
and Appraisal: Structure No. 256001.9; field inspection by Clayton Fraser  
and Ron Ashbach-Sladek, 7 October 1991.

sign. rating: 36  
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser      4 February 1992

# Brush Creek Bridge

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SCHU20

## GENERAL DATA

<b>structure no.:</b> 260001.0	<b>city/town:</b> 7.8 miles south of Downing
<b>county:</b> Schuyler	<b>feature inters.:</b> Brush Creek
	<b>cadastral grid:</b> S9, T64N, R13W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Schuyler County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt pony truss	
<b>substructure:</b> concrete abutments and wingwalls	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 60.0'	<b>alterations:</b> unknown
<b>total length:</b> 60.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.7'	<b>other features:</b> floor beam field-bolted to vertical

## HISTORICAL DATA

<b>erection date:</b> c1910	
<b>erection cost:</b> unknown	
<b>designer:</b> unknown	
<b>fabricator :</b> unknown	
<b>contractor :</b> unknown	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 260001.0.	
<b>sign. rating:</b> 28	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)	

inventoried by: Clayton B. Fraser    4 February 1992

# Brushy Creek Bridge

SCHU21

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## GENERAL DATA

structure no.:	270003.4	city/town:	4.0 miles south of Downing
county:	Schuyler	feature inters.:	Brushy Creek
		cadastral grid:	S21, T65N, R13W
		highway route:	county road
		highway distr.:	2
		current owner:	Schuyler County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss  
substructure: timber pile bent abutments

span number:	1	condition:	fair
span length:	48.0'	alterations:	truss perhaps moved
total length:	48.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.2'	other features:	upper chord / inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing and gusset plates; diagonal: 2 square eyebars with turnbuckles; lateral bracing: round rods with threaded ends; floor beam: I-beam, field-bolted to vertical

## HISTORICAL DATA

erection date: c1910  
erection cost: unknown  
designer: Illinois Steel Bridge Company, Jacksonville IL  
fabricator : Illinois Steel Bridge Company, Jacksonville IL  
contractor : Illinois Steel Bridge Company, Jacksonville IL

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 270003.4; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 7 October 1991.

sign. rating: 28  
evaluation: NRHP non-eligible (typical example of common structural type, moved)

inventoried by: Clayton B. Fraser 4 February 1992

# Waldo Bridge

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SCHU22

## GENERAL DATA

<b>structure no.:</b>	274000.1	<b>city/town:</b>	8.0 miles northwest of Queen City
<b>county:</b>	Schuyler	<b>feature inters.:</b>	Chariton River
		<b>cadastral grid:</b>	S8/17, T65N, R16W
		<b>highway route:</b>	county road
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Schuyler County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 7-panel, pin-connected Pratt through truss		
<b>substructure:</b>	concrete-filled steel cylinder piers		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	112.0'	<b>alterations:</b>	none
<b>total length:</b>	117.0'	<b>floor/decking :</b>	timber deck over steel stringers
<b>roadway width:</b>	12.8'	<b>other features:</b>	upper chord / inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles with continuous plate and knee braces; portal strut: latticed angle; floor beam: I-beam, U-bolted to vertical; lattice guardrails

## HISTORICAL DATA

<b>erection date:</b>	1903
<b>erection cost:</b>	\$2760.00 (contract amount)
<b>designer:</b>	Dildine Bridge Company, Cameron MO
<b>fabricator :</b>	Cambria Steel Company, Pittsburgh PA
<b>contractor:</b>	Dildine Bridge Company, Cameron MO
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 274000.1; Schuyler County Court Record 14: page 501 (19 May 1903), page 519 (18 June 1903); Schuyler County Court Record 15: page 119 (3 September 1904) - located at Schuyler County Courthouse, Lancaster MO; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 7 October 1991.
<b>sign. rating:</b>	48
<b>evaluation:</b>	NRHP possibly eligible (typically configured, well-preserved example of mainstay structural type)

**inventoried by:** Clayton B. Fraser    4 February 1992

# Chariton River Bridge

SCHU23

## GENERAL DATA

<b>structure no.:</b> 274000.9	<b>city/town:</b> 7.1 miles northwest of Queen City
<b>county:</b> Schuyler	<b>feature inters.:</b> Chariton River
	<b>cadastral grid:</b> S9/16, T65N, R16W
	<b>highway route:</b> county road
	<b>highway distr.:</b> 2
	<b>current owner:</b> Schuyler County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
<b>substructure:</b> timber / welded steel pile bent abutments and piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 80.0'	<b>alterations:</b> unknown
<b>total length:</b> 140.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 11.7'	<b>other features:</b> upper chord / inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: looped round eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; lattice guardrails

## HISTORICAL DATA

<b>erection date:</b> c1910	
<b>erection cost:</b> unknown	
<b>designer:</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>fabricator :</b> Illinois Steel Bridge Company, Jacksonville IL; Illinois Steel Company, Chicago IL	
<b>contractor:</b> Illinois Steel Bridge Company, Jacksonville IL	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 274000.9; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 7 October 1991.	
<b>sign. rating:</b> 38	
<b>evaluation:</b> NRHP non-eligible (typical example of common structural type)	

**inventoried by:** Clayton B. Fraser    4 February 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Mock Bridge (Hargrove Bridge; Chariton River Bridge)  
MHTD: 002000.1

SCHU03

**DATE(S) OF CONSTRUCTION**

1904

**LOCATION**

county road over Chariton River; S34, T67N, R16W  
8.9 miles northwest of Lancaster; Schuyler County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / abandoned

**RATING** NRHP non-eligible (score: 40)

**CONDITION**

fair/poor

**OWNER**

Schuyler County

span number: 1  
span length: 108.0'  
total length: 156.0'  
roadway wdt.: 11.4'

superstructure: steel, 7-panel, pin-connected Pratt through truss; 1 steel stringer approach span at the north end; 2 steel stringer approach spans at the south end  
substructure: concrete-filled steel cylinder piers (smaller steel cylinder piers under south approach span)  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (2 looped rectangular eyebars at the hip); diagonal: 2 looped rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round looped and bolted rod with turnbuckle; strut: 2 angles; portal strut: A-frame; floor beam: I-beam, field-bolted to vertical

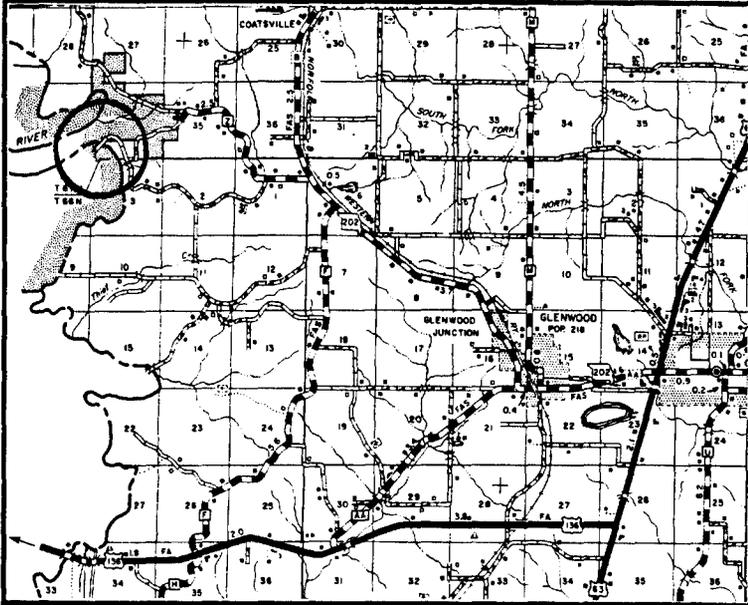
Separated by the Chariton River, Putnam and Schuyler counties joined in cooperative ventures to bridge the river at several places. One of these was the Mock - also known as the Hargrove - Bridge northwest of Lancaster. After discussing the proposition of a permanent wagon bridge at this point, the two counties finally agreed early in 1903 to build the structure. In March they hired bridge contractor William Irelan to provide and erect a medium-span pinned Pratt through truss at the Mock crossing. The cost was to be \$1595.00, of which Putnam County paid 7/12ths and Schuyler County paid the remainder. Using steel components rolled in Pittsburgh by Jones and Laughlin, Irelan's crew completed the Mock Bridge later that year. It carried traffic as a regionally important crossing of the Chariton River for decades. The bridge has since been allowed to deteriorate beyond the point of utility, and the river's channel has been ditched. The road is now closed, the bridge uncrossable.

In Missouri, the pinned Pratt through truss was the bridge of choice for short- and medium-span applications in the late 19th and early 20th centuries. As a result, thousands of Pratts were built across the state, and today Pratts constitute the most populous group of through trusses. Though it remains more-or-less intact, the Mock Bridge is unremarkable in its design, dimensions and detailing.

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**NAME(S) OF STRUCTURE**

Mock Bridge (Hargrove Bridge; Chariton River Bridge)

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 002000.1; Schuyler County Court Record 14: page 500 (19 May 1903); Schuyler County Court Record 15: page 29 (5 April 1904), located at Schuyler County Courthouse, Lancaster MO; Putnam County Court Record M: page 226 (2 February 1903), page 270 (27 March 1903), located at Putnam County Courthouse, Unionville MO; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 7 October 1991.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

4 February 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Waldo Bridge  
MHTD: 274000.1

SCHU22

**DATE(S) OF CONSTRUCTION**

1903

**LOCATION**

county road over Chariton River; S8/17, T65N, R16W  
8.0 miles northwest of Queen City; Schuyler County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 48)

**CONDITION**

fair

**OWNER**

Schuyler County

span number: 1  
span length: 112.0'  
total length: 117.0'  
roadway wdt.: 12.8'

superstructure: steel, 7-panel, pin-connected Pratt through truss  
substructure: concrete-filled steel cylinder piers  
floor/decking: timber deck over steel stringers  
other features: upper chord / inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles with continuous plate and knee braces; portal strut: latticed angle; floor beam: I-beam, U-bolted to vertical; lattice guardrails

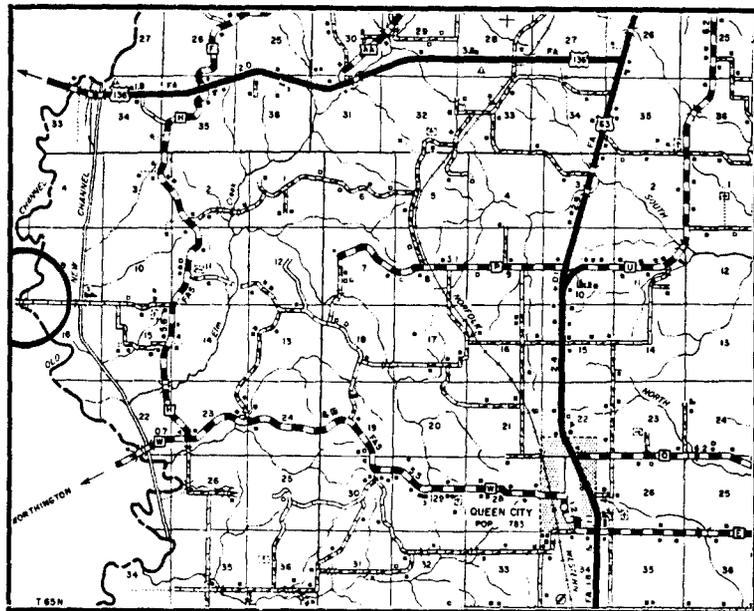
One of the series of medium-span trusses erected over the Chariton River by Schuyler and Putnam counties is the Waldo Ford Bridge, located northwest of Queen City. The two counties agreed early in 1903 to build the structure. In June they hired Cameron, Missouri, bridge contractor John Dildine to provide and erect a medium-span pinned Pratt through truss at the Waldo crossing. The cost was to be \$2760.00, of which Putnam County paid 7/12ths and Schuyler County paid the remainder. Using steel components rolled in Pittsburgh by the Cambria Steel Works, Dildine completed the bridge later that year. The channel of the Chariton River has since been moved, but the Waldo Bridge continues to carry intermittent local traffic.

The Waldo Bridge is distinguished historically as the oldest truss remaining from what was once a succession of early 20th century spans built cooperatively by Schuyler and Putnam counties. The bridge is technologically representative - one of numerous such pinned Pratt through trusses found throughout Missouri.

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**NAME(S) OF STRUCTURE**

Waldo Bridge (Chariton River Bridge)

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 274000.1; Schuyler County Court Record 14: page 501 (19 May 1903), page 519 (18 June 1903); Schuyler County Court Record 15: page 119 (3 September 1904) - located at Schuyler County Courthouse, Lancaster MO; field inspection by Clayton Fraser and Ron Ashbach-Sladek, 7 October 1991.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

4 February 1992

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# SULLIVAN COUNTY

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INCLUDED: [Significant feature(s) of bridge given in boldface]  
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
*SULL01	J 772	Medicine Creek Bridge	1-140' <b>riveted Pratt through truss</b> 1932 Gerard Knutson Constr. Co.
*SULL02	051001.3	Crumpacker Bridge	1- 75' <b>pinned Pratt pony truss</b> 1912 Dildine Bridge Co., Cameron MO
*SULL03	062002.7	McNealey Bridge	1- 80' <b>pinned Pratt pony truss</b> 1910 Canton Bridge Co., Canton OH
SULL04	119000.2	North Spring Creek Bridge	1- 40' <b>pinned Pratt half-hip pony truss</b> c1910 Canton Bridge Company (prob.)
*SULL05	128001.6	Jones Bridge	1- 75' <b>pinned Pratt pony truss</b> 1912 Illinois Steel Bridge Company
SULL06	140000.9	Yellow Creek Bridge	1- 45' <b>pinned Pratt half-hip pony truss</b> c1910 Canton Bridge Company (prob.)
SULL07	200R01.4	Locust Creek Bridge	1- 80' <b>pinned Pratt pony truss</b> c1910
*SULL08	228001.3	Mewmaw Bridge	1- 45' <b>pinned Warren pony truss</b> 1908 county work force
SULL09	275001.3	Dewitt Bridge	1- 75' <b>pinned Pratt pony truss</b> 1913 county work force
SULL10	308000.7	Cook School Bridge	<b>(replaced)</b>
SULL11	321000.1	Holloway Bridge	1- 50' <b>pinned Pratt half-hip pony truss</b> 1909 Canton Bridge Co., Canton OH
*SULL12	322000.4	Little Yellow Creek Bridge	1- 45' <b>pinned Pratt pony truss</b> c1910
SULL13	323001.7	Maggart Bridge	1- 75' <b>pinned Pratt pony truss</b> 1908 Canton Bridge Company (prob.)
*SULL14	341001.6	Bridge	1- 45' <b>pinned Pratt half-hip pony truss</b> c1910 Canton Bridge Co., Canton OH
*SULL15	344002.0	Mussel Creek Bridge	1- 80' <b>pinned Pratt pony truss</b> c1910 Canton Bridge Co., Canton OH
SULL16	385001.1	Locust Creek Bridge	1- 50' <b>pinned Pratt pony truss</b> c1910 Canton Bridge Company (prob.)
SULL17	417002.0	Mock Bridge	1- 60' <b>pinned Pratt half-hip pony truss</b> 1907 Canton Bridge Co., Canton OH
*SULL18	435000.1	Jack Hill Bridge	<b>(replaced)</b>

EXCLUDED:

Pratt pony truss

J 548	K 298	K 299	K 300	K 301	K 302	K 303
T 928	313R00.6	417R00.9				

Warren pony truss

J 951	002000.3	003001.0	041001.8	072000.7	129001.5	156000.6
197000.4	230001.6	315002.0	342000.8	352000.2	359001.2	373000.5
379000.8	412001.5	445001.5				

# SULLIVAN COUNTY

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## EXCLUDED (cont.):

Lattice bedstead  
302000.3 434000.9

### Steel stringer

J 379	J 691	S 199	S 582	S 583	T 482	T 485
X 996	X 997	001000.5	014001.7	015001.2	019000.5	021R00.5025000.5
037001.9	052001.1	088002.7	089000.8	102000.4	147002.5	157000.2165000.8
166000.1	178000.1	179001.0	199003.4	230002.4	252R00.7	261003.3263000.1
278001.1	296R00.6	309001.5	310000.8	331000.8	337000.9	339R02.0
345000.9	348001.0	355000.5	365001.6	395000.2	398000.1	399000.7404001.1
413001.4	424004.2	429R02.8	432000.3	451001.8		

### Concrete girder

G 587	G 630	H 805	H 806	J 687	154000.2
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### Concrete box culvert

J 484	J 485	J 486	J 671	J 672	P 8	P 13
P 14	P 172	T 484	T 486	X 820	X 821	X 892
X 893	X 994					

## SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	1	15	0	0	16
Excluded	32	63	0	0	95
	<hr/>				
	33	78	0	0	111 structures

# Medicine Creek Bridge

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SULL01

## GENERAL DATA

<b>structure no.:</b>	J 772	<b>city/town:</b>	3.1 miles west of Humphreys
<b>county:</b>	Sullivan	<b>feature inters.:</b>	Medicine Creek
		<b>cadastral grid:</b>	S34, T62N, R22W
		<b>highway route:</b>	Missouri State Highway 6
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Missouri Highway and Transportation Department

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 8-panel, rigid-connected Pratt through truss, skewed; 2 rigid-connected Warren pony truss approach spans		
<b>substructure:</b>	concrete abutments and wingwalls; concrete bullnose spill-through piers		
<b>span number:</b>	1; 2	<b>condition:</b>	good
<b>span length:</b>	140.0'; 60.0'	<b>alterations:</b>	none
<b>total length:</b>	271.0'	<b>floor/decking :</b>	asphalt on concrete deck over steel stringers
<b>roadway width:</b>	22.0'	<b>other features:</b>	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates; vertical: 4 angles with lacing; diagonal: 2 angles with batten plates; lateral bracing: 2 riveted square bars (lower), 2 angles (upper); strut: 2 angles with lacing; floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels

## HISTORICAL DATA

<b>erection date:</b>	1932
<b>erection cost:</b>	\$21,144.12
<b>designer:</b>	Missouri State Highway Department
<b>fabricator :</b>	Inland Steel Company, East Chicago IN (main span); Scullin (approach spans)
<b>contractor:</b>	Gerard Knutson Construction Company, Kansas City MO
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. J 772; Missouri Primary System Bridge Record, located at the Missouri Highway and Transportation Department, Jefferson City MO; field inspection by Clayton Fraser, 6 October 1991.
<b>sign. rating:</b>	55
<b>evaluation:</b>	NRHP determined non-eligible (skewed example of MSHD truss bridge design)

Inventoried by: Clayton B. Fraser 6 February 1992

# Crumpacker Bridge

SULL02

## GENERAL DATA

structure no.: 051001.3      city/town: 8.9 miles north of Milan  
county: Sullivan      feature inters.: Locust Creek  
cadastral grid: S22, T64N, R20W  
highway route: County Road 51  
highway distr.: 2  
current owner: Sullivan County

## STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss; 2 timber stringer approach spans  
substructure: timber pile bent abutments, wingwalls and pier; concrete-filled steel cylinder piers

span number: 1      condition: fair  
span length: 75.0'      alterations: steel beams and plates added to east abutment  
total length: 108.0'      floor/decking : timber deck over steel stringers  
roadway width: 11.8'      other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: 2 square eyebars with turnbuckles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 latticed angles

## HISTORICAL DATA

erection date: 1911-12  
erection cost: \$945.00  
designer: John Dildine Bridge Company, Cameron MO  
fabricator : John Dildine Bridge Company, Cameron MO  
contractor: John Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 051001.3; **Standard Atlas of Sullivan County, Missouri** (Chicago: Geo. A. Ogle & Co. 1897); Sullivan County Court Record, Book 17: page 436 (9 August 1911), page 626 (9 November 1912) - located at Sullivan County Courthouse, Milan MO; field inspection by Clayton Fraser, 6 October 1991.

sign. rating: 37  
evaluation: NRHP determined non-eligible (typically configured example of common truss configuration)

inventoried by: Clayton B. Fraser      6 February 1992

# McNealey Bridge

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SULL03

## GENERAL DATA

structure no.:	062002.7	city/town:	6.4 miles northwest of Milan
county:	Sullivan	feature inters.:	Locust Creek
		cadastral grid:	S4/5, T63N, R20W
		highway route:	County Road 62
		highway distr.:	2
		current owner:	Sullivan County

## STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	steel pile bent piers and abutments with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	80.0'	alterations:	none
total length:	124.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.5'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1909-10
erection cost:	\$1300.00
designer:	Canton Bridge Company, Canton OH
fabricator :	Canton Bridge Company, Canton OH
contractor :	Canton Bridge Company, Canton OH
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 062002.7; <b>Standard Atlas of Sullivan County, Missouri</b> (Chicago: Geo. A. Ogle & Co., 1897); Sullivan County Court Record, Book 17: page 125 (6 August 1909), page 193 (9 February 1910), page 249 (4 May 1910) - located at Sullivan County Courthouse, Milan Missouri.
sign. rating:	44
evaluation:	NRHP non-eligible (typically configured example of common structural type)

Inventoried by: Clayton B. Fraser 6 February 1992

# North Spring Creek Bridge

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SULL04

## GENERAL DATA

<b>structure no.:</b> 119000.2	<b>city/town:</b> 7.7 miles north of Green City
<b>county:</b> Sullivan	<b>feature inters.:</b> North Spring Creek
	<b>cadastral grid:</b> S6, T64N, R18W
	<b>highway route:</b> County Road 119
	<b>highway distr.:</b> 2
	<b>current owner:</b> Sullivan County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 3-panel, pin-connected, Pratt half-hip pony truss	
<b>substructure:</b> unknown	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 40.0'	<b>alterations:</b> unknown
<b>total length:</b> 40.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.6'	<b>other features:</b> steel lattice guardrails

## HISTORICAL DATA

<b>erection date:</b> c1910
<b>erection cost:</b> unknown
<b>designer:</b> Canton Bridge Company, Canton OH (probable)
<b>fabricator :</b> Canton Bridge Company, Canton OH (probable)
<b>contractor :</b> Canton Bridge Company, Canton OH (probable)
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 119000.2.
<b>sign. rating:</b> 28
<b>evaluation:</b> NRHP non-eligible (undocumented, undistinguished, small-scale structure, lacking technological significance)

**inventoried by:** Clayton B. Fraser    6 February 1992

# Jones Bridge

SULL05

## GENERAL DATA

structure no.: 128001.6      city/town: 4.7 miles northeast of Green City  
county: Sullivan      feature inters.: North Fork Spring Creek  
cadastral grid: S26, T64N, R18W  
highway route: County Road 128  
highway distr.: 2  
current owner: Sullivan County

## STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss; 1 steel stringer approach span at each end  
substructure: timber pile bent abutments, wingwalls and piers

span number: 1      condition: poor  
span length: 75.0'      alterations: deck removed  
total length: 132.0'      floor/decking : timber deck over steel stringers  
roadway width: 11.6'      other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 latticed angles; cast iron hip blocks

## HISTORICAL DATA

erection date: 1912  
erection cost: \$869.00  
designer: Illinois Steel Bridge Company, Jacksonville IL  
fabricator : Illinois Steel Bridge Company, Jacksonville IL;  
Illinois Steel Company, Chicago IL  
contractor: Illinois Steel Bridge Company, Jacksonville IL

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 128001.6; **Standard Atlas of Sullivan County, Missouri** (Chicago: Geo. A. Ogle & Co., 1897; Sullivan County Court Record, Book 17: page 509 (6 February 1912), page 585 (7 August 1912) -located at Sullivan County Courthouse, Milan MO; field inspection by Clayton Fraser, 6 October 1991.

sign. rating: 38  
evaluation: NRHP non-eligible (typically configured, poorly preserved example of a mainstay structural type)

inventoried by: Clayton B. Fraser      6 February 1992

# Yellow Creek Bridge

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SULL06

## GENERAL DATA

<b>structure no.:</b> 140000.9	<b>city/town:</b> 2.1 miles northwest of Green City
<b>county:</b> Sullivan	<b>feature inters.:</b> Yellow Creek
	<b>cadastral grid:</b> S1/12, T63N, R19W
	<b>highway route:</b> County Road 140
	<b>highway distr.:</b> 2
	<b>current owner:</b> Sullivan County

## STRUCTURAL DATA

**superstructure:** steel, 3-panel, pin-connected, Pratt half-hip pony truss  
**substructure:** concrete back- and wingwalls; steel pile bent piers

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 45.0'	<b>alterations:</b> none
<b>total length:</b> 45.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 11.7'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars, or 2 looped round eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: square eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guard-rail: 2 latticed angles

## HISTORICAL DATA

**erection date:** c1910  
**erection cost:** unknown  
**designer:** Canton Bridge Company, Canton OH (probable)  
**fabricator :** Canton Bridge Company, Canton OH (probable); Cambria Steel Company, Pittsburgh PA  
**contractor:** Canton Bridge Company, Canton OH (probable)

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 140000.9; field inspection by Clayton Fraser, 6 October 1991.

**sign. rating:** 30  
**evaluation:** NRHP non-eligible (typical example of common truss type; with its construction history undocumented, the bridge is of extremely limited interpretive value)

**inventoried by:** Clayton B. Fraser 6 February 1992

# Locust Creek Bridge

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SULL07

## GENERAL DATA

<b>structure no.:</b> 200R01.4	<b>city/town:</b> 3.0 miles west of Milan
<b>county:</b> Sullivan	<b>feature inters.:</b> Locust Creek
	<b>cadastral grid:</b> S8, T62N, R20W
	<b>highway route:</b> County Road 200
	<b>highway distr.:</b> 2
	<b>current owner:</b> Sullivan County

## STRUCTURAL DATA

<b>superstructure:</b> steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
<b>substructure:</b> timber pile bent abutments, wingwalls and piers	
<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 80.0'	<b>alterations:</b> unknown
<b>total length:</b> 147.0'	<b>floor/decking :</b> timber deck
<b>roadway width:</b> 11.3'	<b>other features:</b> steel lattice guardrails

## HISTORICAL DATA

<b>erection date:</b> c1910	
<b>erection cost:</b> unknown	
<b>designer:</b> unknown	
<b>fabricator :</b> unknown	
<b>contractor:</b> unknown	
<b>references:</b> Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 200R01.4.	
<b>sign. rating:</b> 30	
<b>evaluation:</b> NRHP non-eligible (typically configured, inadequately documented example of common structural type)	

**inventoried by:** Clayton B. Fraser    6 February 1992

# Mewmaw Bridge

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SULL08

## GENERAL DATA

<b>structure no.:</b>	228001.3	<b>city/town:</b>	8.9 miles northwest of Milan
<b>county:</b>	Sullivan	<b>feature inters.:</b>	West Fork of Locust Creek
		<b>cadastral grid:</b>	S4/9, T63N, R21W
		<b>highway route:</b>	County Road 228
		<b>highway distr.:</b>	2
		<b>current owner:</b>	Sullivan County

## STRUCTURAL DATA

<b>superstructure:</b>	steel, 3-panel, pin / rigid-connected Warren pony truss		
<b>substructure:</b>	timber pile bent piers and abutments with timber back- and wingwalls		
<b>span number:</b>	1	<b>condition:</b>	fair
<b>span length:</b>	45.0'	<b>alterations:</b>	none
<b>total length:</b>	45.0'	<b>floor/decking :</b>	timber deck over steel stringers
<b>roadway width:</b>	12.0'	<b>other features:</b>	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 square bars with threaded ends; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: none

## HISTORICAL DATA

<b>erection date:</b>	1908
<b>erection cost:</b>	unknown
<b>designer:</b>	T.J. Morelock, County Highway Engineer
<b>fabricator :</b>	unknown
<b>contractor:</b>	county work force
<b>references:</b>	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 228001.3; Sullivan County Court Record, Book 16: page 373 (11 May 1907), page 537 (10 April 1908) - located at Sullivan County Courthouse, Milan MO.
<b>sign. rating:</b>	64
<b>evaluation:</b>	NRHP possibly eligible (unique example of uncommon structural type)

inventoried by: Clayton B. Fraser    6 February 1992

# Dewitt Bridge

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SULL09

## GENERAL DATA

structure no.:	275001.3	city/town:	5.8 miles southwest of Milan
county:	Sullivan	feature inters.:	West Fork of Locust Creek
		cadastral grid:	S23/24, T62N, R21W
		highway route:	County Road 275
		highway distr.:	2
		current owner:	Sullivan County

## STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	75.0'	alterations:	unknown
total length:	75.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel lattice guardrails

## HISTORICAL DATA

erection date:	1913
erection cost:	unknown
designer:	Canton Bridge Company, Canton OH (probable)
fabricator :	Canton Bridge Company, Canton OH (probable)
contractor :	county work force
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 275001.3; <b>Standard Atlas of Sullivan County, Missouri</b> (Chicago: Geo. A. Ogle & Co., 1897); Sullivan County Court Record, Book 18: page 72 (10 April 1913) - located at Sullivan County Courthouse, Milan MO.
sign. rating:	38
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Clayton B. Fraser 6 February 1992

# Holloway Bridge

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SULL11

## GENERAL DATA

structure no.:	321000.1	city/town:	7.6 miles southeast of Milan
county:	Sullivan	feature inters.:	Yellow Creek
		cadastral grid:	S35, T62N, R19W
		highway route:	County Road 321
		highway distr.:	2
		current owner:	Sullivan County

## STRUCTURAL DATA

**superstructure:** steel, 3 panel, pin-connected, Pratt half-hip pony truss, with one short steel stringer approach span

**substructure:** timber pile bent abutments, wingwalls and pier

span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	55.0'	floor/decking :	timber deck
roadway width:	12.0'	other features:	steel angle guardrails

## HISTORICAL DATA

**erection date:** 1909

**erection cost:** \$750.00

**designer:** Canton Bridge Company, Canton OH

**fabricator :** Canton Bridge Company, Canton OH

**contractor:** Canton Bridge Company, Canton OH

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 321000.1; **Standard Atlas of Sullivan County, Missouri** (Chicago: Geo. A. Ogle & Co., 1897); Sullivan County Court Record, Book 17: page 68 (4 March 1904), page 102 (6 May 1909), page 125 (6 August 1909), page 249 (4 May 1910) - located at Sullivan County Courthouse, Milan MO.

**sign. rating:** 39

**evaluation:** NRHP non-eligible (typical example of a common truss configuration, with standard detailing, unremarkable dimensions and an average degree of physical integrity)

**inventoried by:** Clayton B. Fraser    6 February 1992

# Little Yellow Creek Bridge

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SULL12

## GENERAL DATA

structure no.:	322000.4	city/town:	7.1 miles northwest of Winegan
county:	Sullivan	feature inters.:	Little Yellow Creek
		cadastral grid:	S25/36, T62N, R19W
		highway route:	County Road 322
		highway distr.:	2
		current owner:	Sullivan County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	45.0'	alterations:	none
total length:	45.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.0'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: looped square eye-rod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 angles

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	Lackawanna Steel Company, Pittsburgh PA
contractor :	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 322000.4; field inspection by Clayton Fraser, 6 October 1991.
sign. rating:	22
evaluation:	NRHP non-eligible (poorly documented example of an exceedingly common structural type)

inventoried by: Clayton B. Fraser 6 February 1992

# Maggart Bridge

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SULL13

## GENERAL DATA

structure no.:	323001.7	city/town:	6.2 miles north of Winegan
county:	Sullivan	feature inters.:	Mussel Creek
		cadastral grid:	S26/35, T62N, R18W
		highway route:	County Road 323
		highway distr.:	2
		current owner:	Sullivan County

## STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss, with 1 steel stringer approach span		
substructure:	unknown		
span number:	1	condition:	fair
span length:	75.0'	alterations:	unknown
total length:	98.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.8'	other features:	no guardrails

## HISTORICAL DATA

erection date:	1908
erection cost:	unknown
designer:	Canton Bridge Company, Canton OH (probable)
fabricator :	Canton Bridge Company, Canton OH (probable)
contractor:	Canton Bridge Company, Canton OH (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 323001.7; <b>Standard Atlas of Sullivan County, Missouri</b> (Chicago: Geo. A. Ogle & Co., 1897); Sullivan County Court Record, Book 16: page 519 (7 March 1908); Sullivan County Court Record, Book 17: page 102 (6 May 1909) - located at Sullivan County Courthouse, Milan MO.
sign. rating:	42
evaluation:	NRHP non-eligible (technologically undistinguished, undocumented example of pinned Pratt pony truss construction)

Inventoried by: Clayton B. Fraser    6 February 1992

# Bridge

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SULL14

## GENERAL DATA

**structure no.:** 341001.6      **city/town:** 3.9 miles northwest of Winegan  
**county:** Sullivan      **feature inters.:** unnamed stream  
**cadastral grid:** S8/17, T61N, R18W  
**highway route:** County Road 341  
**highway distr.:** 2  
**current owner:** Sullivan County

## STRUCTURAL DATA

**superstructure:** steel, 3-panel, pin-connected, Pratt half-hip pony truss, with steel stringer approach span  
**substructure:** timber pile bent abutments, wingwalls and pier

**span number:** 1      **condition:** fair  
**span length:** 45.0'      **alterations:** none  
**total length:** 57.0'      **floor/decking :** timber deck over steel stringers  
**roadway width:** 11.0'      **other features:** upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped square or round eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 angles

## HISTORICAL DATA

**erection date:** c1910  
**erection cost:** unknown  
**designer:** Canton Bridge Company, Canton OH  
**fabricator :** Canton Bridge Company, Canton OH  
**contractor:** Canton Bridge Company, Canton OH

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 341001.6; field inspection by Clayton Fraser, 6 October 1991.

**sign. rating:** 35  
**evaluation:** NRHP non-eligible (undocumented example of common structural type)

**inventoried by:** Clayton B. Fraser      6 February 1992

# Mussel Creek Bridge

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SULL15

## GENERAL DATA

<b>structure no.:</b> 344002.0	<b>city/town:</b> 2.9 miles northeast of Winegan
<b>county:</b> Sullivan	<b>feature inters.:</b> Mussel Creek
	<b>cadastral grid:</b> S14, T61N, R19W
	<b>highway route:</b> County Road 344
	<b>highway distr.:</b> 2
	<b>current owner:</b> Sullivan County

## STRUCTURAL DATA

**superstructure:** steel, 5-panel, pin-connected Pratt pony truss  
**substructure:** timber abutments and wingwalls

<b>span number:</b> 1	<b>condition:</b> fair
<b>span length:</b> 80.0'	<b>alterations:</b> none
<b>total length:</b> 80.0'	<b>floor/decking :</b> timber deck over steel stringers
<b>roadway width:</b> 13.0'	<b>other features:</b> upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: none

## HISTORICAL DATA

**erection date:** c1910  
**erection cost:** unknown  
**designer:** Canton Bridge Company, Canton OH  
**fabricator :** Canton Bridge Company, Canton OH  
**contractor:** Canton Bridge Company, Canton OH

**references:** Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 344002.0; field inspection by Clayton Fraser, 6 October 1991.

**sign. rating:** 40  
**evaluation:** NRHP non-eligible (longer than average, but typically configured example of common structural type)

**inventoried by:** Clayton B. Fraser 6 February 1992

# Locust Creek Bridge

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SULL16

## GENERAL DATA

structure no.:	385001.1	city/town:	6.5 miles south of Milan
county:	Sullivan	feature inters.:	Little East Fork Locust Creek
		cadastral grid:	S1/12, T61N, R20W
		highway route:	County Road 385
		highway distr.:	2
		current owner:	Sullivan County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach span		
substructure:	timber pile bent abutments, wingwalls and pier		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	66.0'	floor/decking :	timber deck
roadway width:	12.0'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Canton Bridge Company, Canton OH (probable)
fabricator :	Canton Bridge Company, Canton OH (probable)
contractor:	Canton Bridge Company, Canton OH (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 385001.1.
sign. rating:	33
evaluation:	NRHP non-eligible (typically configured example of pinned Pratt pony truss design)

inventoried by: Clayton B. Fraser    6 February 1992

# Mock Bridge

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SULL17

## GENERAL DATA

structure no.:	417002.0	city/town:	5.6 miles east of Humphreys
county:	Sullivan	feature inters.:	West Fork Locust Creek
		cadastral grid:	S11, T61N, R21W
		highway route:	County Road 417
		highway distr.:	2
		current owner:	Sullivan County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected, Pratt half-hip pony truss, with steel stringer approach span		
substructure:	unknown		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	73.0'	floor/decking :	timber deck over steel stringers
roadway width:	12.0'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1907
erection cost:	\$970.00
designer:	Canton Bridge Company, Canton OH
fabricator :	Canton Bridge Company, Canton OH
contractor:	Canton Bridge Company, Canton OH
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 417002.0; <b>Standard Atlas of Sullivan County Missouri</b> (Chicago: Geo. A. Ogle & Co., 1897); Sullivan County Court Record, Book 16: page 386 (12 June 1907), page 396 (7 August 1907), page 475 (9 January 1908), page 489 (4 February 1908), page 567 (7 May 1908) - located at Sullivan County Courthouse, Milan MO.
sign. rating:	42
evaluation:	NRHP non-eligible (partially documented, slightly longer-than-average example of pinned Pratt pony truss construction)

inventoried by: Clayton B. Fraser 6 February 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Medicine Creek Bridge  
MHTD: J 772

SULL01

**DATE(S) OF CONSTRUCTION**

1932

**LOCATION**

Missouri State Highway 6 over Medicine Creek; S34, T62N, R22W  
3.1 miles west of Humphreys; Sullivan County, Missouri

**USE (ORIGINAL / CURRENT)**

highway bridge / highway bridge

**RATING** NRHP determined non-eligible (score: 41)

**CONDITION**

good

**OWNER**

Missouri Highway and Transportation Department

span number: 1; 2  
span length: 140.0'; 60.0'  
total length: 271.0'  
roadway wdt.: 22.0'

superstructure: steel, 8-panel, rigid-connected Pratt through truss, skewed; 2 rigid-connected Warren pony truss approach spans  
substructure: concrete abutments and wingwalls; concrete bullnose spill-through piers  
floor/decking: asphalt on concrete deck over steel stringers  
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates; vertical: 4 angles with lacing; diagonal: 2 angles with batten plates; lateral bracing: 2 riveted square bars (lower), 2 angles (upper); strut: 2 angles with lacing; floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels

This bridge is located where Missouri State Highway 6 crosses Medicine Creek—three miles west of Humphreys—just east of the Sullivan-Grundy county line. Featuring a heavily skewed Pratt through truss channel span, the structure also includes two Warren pony truss approaches. The Medicine Creek Bridge dates to 1932. Its design work was developed late that year by the Missouri State Highway Commission's Bureau of Surveys and Plans, and the project was put out to bid that year. On February 29, 1932—leap day—the contract was awarded to the Gerard Knutson Construction Company of Kansas City. Knutson erected the bridge that spring for \$21,144.12. Over the years, Highway 6 has developed into a principal east-west route through north-central Missouri. As part of this route, the Medicine Creek Bridge has carried moderate to heavy traffic loads between Sullivan and Grundy Counties. Little changed from its original construction, the bridge's physical integrity remains intact.

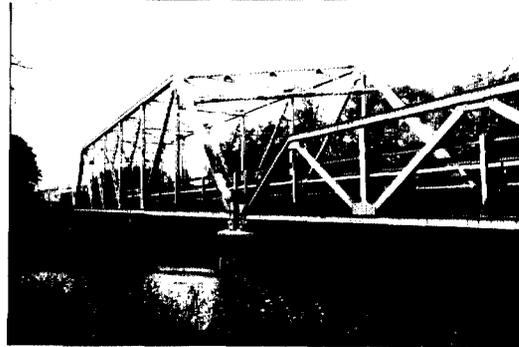
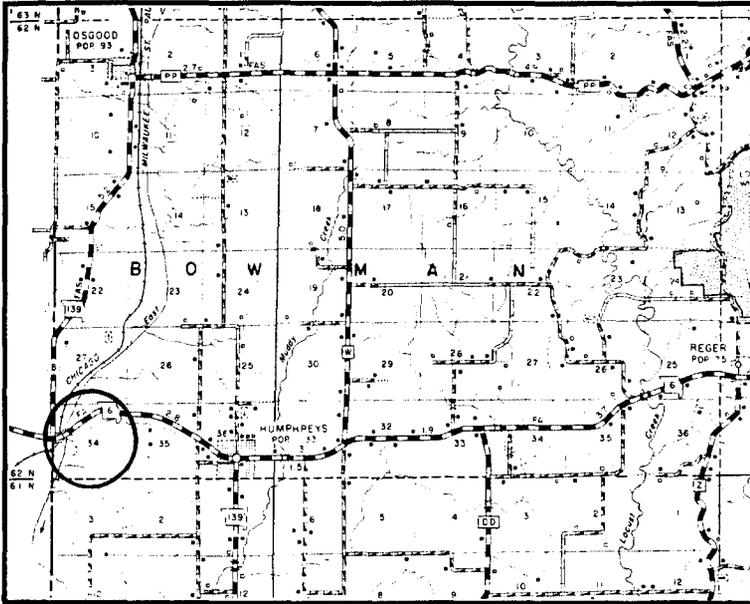
Although somewhat noteworthy for its overall structure length and its extreme skew, the Medicine Creek Bridge ranks as a typical example of MSHD truss design in the 1920s. Unremarkable in terms of its configuration and detailing, the bridge is one of over 100 other similar structures still in existence.

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**NAME(S) OF STRUCTURE**

Medicine Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. J 772; Missouri Primary System Bridge Record, located at the Missouri Highway and Transportation Department, Jefferson City MO; field inspection by Clayton Fraser, 6 October 1991.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

6 February 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Crumpacker Bridge  
MHTD: 051001.3

SULL02

**DATE(S) OF CONSTRUCTION**

1911-12

**LOCATION**

County Road 51 over Locust Creek; S22, T64N, R20W  
8.9 miles north of Milan; Sullivan County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP determined non-eligible (score: 37)

**CONDITION**

fair

**OWNER**

Sullivan County

span number: 1  
span length: 75.0'  
total length: 108.0'  
roadway wdt.: 11.8'

superstructure: steel, 5-panel, pin-connected Pratt pony truss; 2 timber stringer approach spans  
substructure: timber pile bent abutments, wingwalls and pier; concrete-filled steel cylinder piers  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: 2 square eyebars with turnbuckles; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 latticed angles

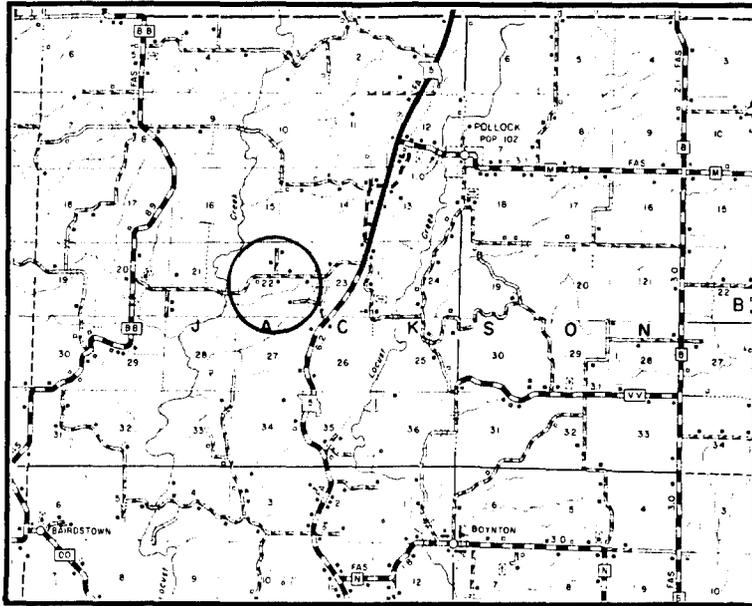
The Crumpacker Bridge carries a gravel-surfaced county road over Locust Creek some nine miles north of Milan, in northern Sullivan County. A typically configured Pratt pony truss, the structure is flanked by two timber stringer approach spans. In the summer of 1911, four bridge building firms submitted bids for the structure's fabrication and erection. The bids were opened on August 9, 1911, with the following result: Dildine Bridge Company, Cameron, Missouri - \$1090.00; Clinton Bridge Company, Clinton, Iowa - \$1157.00; Illinois Steel Bridge Company, Jacksonville, Illinois - \$1199.00; Stupp Brothers Bridge and Iron Works, St. Louis - \$1275.00. The lowest bidder, Dildine was awarded the contract. Work on the project proceeded slowly, or was perhaps put on hold for a time, because the bridge was not completed until late the following year. In November 1912 the Crumpacker Bridge was declared completed. But because it was found not to be in accordance with plans and specifications, the county deducted \$145.00 from Dildine's original contract price. Salvaged bridge beams and metal plates have been added to shore up the east abutment, but the structure remains otherwise unaltered.

The Crumpacker Bridge is a typically configured example of pinned Pratt pony truss design. With standard detailing, unremarkable dimensions, and an average degree of physical integrity, the bridge is among hundreds of similar structures still in use on Missouri's roadways.

**NAME(S) OF STRUCTURE**

Crumpacker Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 051001.3; **Standard Atlas of Sullivan County, Missouri** (Chicago: Geo. A. Ogle & Co. 1897); Sullivan County Court Record, Book 17: page 436 (9 August 1911), page 626 (9 November 1912) - located at Sullivan County Courthouse, Milan MO; field inspection by Clayton Fraser, 6 October 1991.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

6 February 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

McNealey Bridge  
MHTD: 062002.7

SULL03

**DATE(S) OF CONSTRUCTION**

1909-10

**LOCATION**

County Road 62 over Locust Creek; S4/5, T63N, R20W  
6.4 miles northwest of Milan; Sullivan County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 44)

**CONDITION**

fair

**OWNER**

Sullivan County

span number: 1	superstructure: steel, 5-panel, pin-connected Pratt pony truss, with steel stringer approach spans
span length: 80.0'	substructure: steel pile bent piers and abutments with timber back- and wingwalls
total length: 124.0'	floor/decking: timber deck over steel stringers
roadway wdt.: 11.5'	other features: steel angle guardrails

On August 6, 1909, the Sullivan County Court contracted with the Canton Bridge Company to build four steel bridges. The largest of the four was this 80-foot truss, to be built across Locust Creek, some 6½ miles northwest of Milan. Referred to as the McNealey Bridge - after adjacent landowner Virginia McNealey - its initial cost was listed as \$1000.00. The plans originally called for a wood foundation, but the county later decided to invest an extra \$300.00 to have the truss built on steel pile bents. Work on the McNealey Bridge proceeded slowly, perhaps because Canton was building numerous bridges in the county at this time. On May 4, 1910, the structure was declared completed. Still functioning as originally built, the bridge's historical integrity has not been impaired.

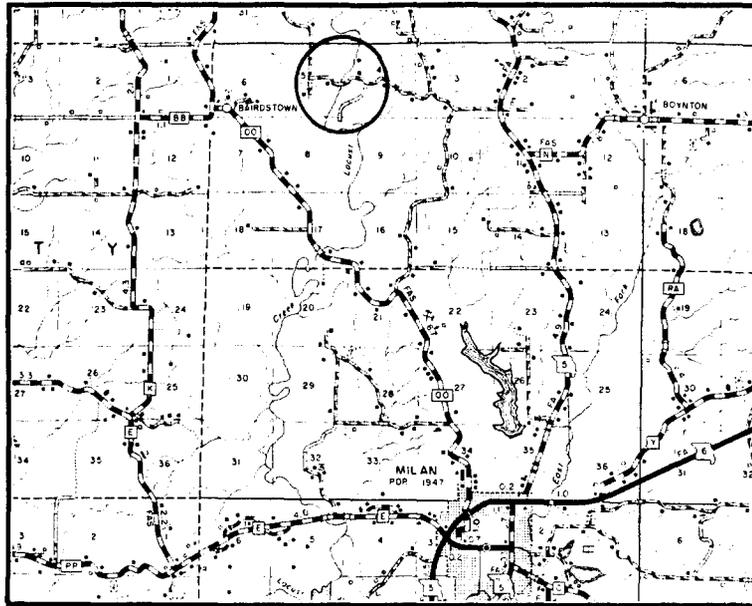
Although possessing a high degree of structural integrity, the McNealey Bridge is unremarkable in its design, dimensions and detailing. An example of a pinned Pratt pony truss, it is one of several hundred other such bridges still carrying traffic on Missouri's rural roadways.

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**NAME(S) OF STRUCTURE**

McNealey Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 062002.7; **Standard Atlas of Sullivan County, Missouri** (Chicago: Geo. A. Ogle & Co., 1897); Sullivan County Court Record, Book 17: page 125 (6 August 1909), page 193 (9 February 1910), page 249 (4 May 1910) - located at Sullivan County Courthouse, Milan Missouri.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

6 February 1992

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Jones Bridge  
MHTD: 128001.6

SULL05

**DATE(S) OF CONSTRUCTION**

1912

**LOCATION**

County Road 128 over North Fork Spring Creek; S26, T64N, R18W  
4.7 miles northeast of Green City; Sullivan County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / abandoned

**RATING** NRHP non-eligible (score: 38)

**CONDITION**

poor

**OWNER**

Sullivan County

span number: 1  
span length: 75.0'  
total length: 132.0'  
roadway wdt.: 11.6'

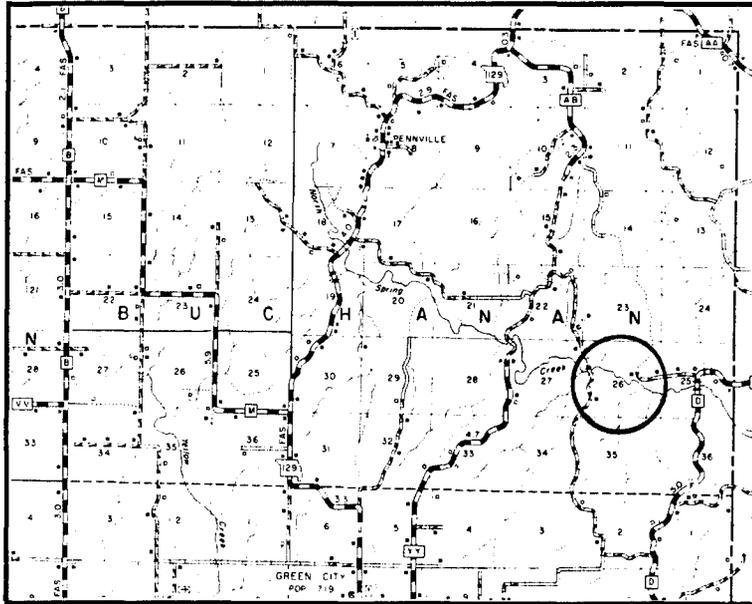
superstructure: steel, 5-panel, pin-connected Pratt pony truss; 1 steel stringer approach span at each end  
substructure: timber pile bent abutments, wingwalls and piers  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 punched rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 latticed angles; cast iron hip blocks

The Jones Bridge is located not quite five miles northeast of Green City, in northeastern Sullivan County. Carrying a secondary county road over the North Fork of Spring Creek, the structure has been in service since the summer of 1912. In early February 1912 Sullivan County opened bids from seven firms to erect two 75-foot trusses, known as the Jones and Montgomery Bridges. Tendering the lowest overall bid, the Illinois Steel Bridge Company of Jacksonville, Illinois, won contracts to build both bridges. Illinois Steel Bridge completed the structures in early August. The cost of the Jones Bridge—named for adjacent landowner James Jones—was \$869.00. In the late 1980s, the bridge's north abutment began to collapse. In the spring of 1990 part of the deck was removed, and the structure was closed to traffic. It is a typically configured, poorly preserved example of a mainstay structural type.

**NAME(S) OF STRUCTURE**

Jones Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 128001.6; *Standard Atlas of Sullivan County, Missouri* (Chicago: Geo. A. Ogle & Co., 1897; Sullivan County Court Record, Book 17: page 509 (6 February 1912), page 585 (7 August 1912) -located at Sullivan County Courthouse, Milan MO; field inspection by Clayton Fraser, 6 October 1991.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

6 February 1992

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Mewmaw Bridge  
MHTD: 228001.3

SULL08

**DATE(S) OF CONSTRUCTION**

1908

**LOCATION**

County Road 228 over West Fork of Locust Creek; S4/9, T63N, R21W  
8.9 miles northwest of Milan; Sullivan County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 64)

**CONDITION**

fair

**OWNER**

Sullivan County

span number: 1  
span length: 45.0'  
total length: 45.0'  
roadway wdt.: 12.0'

superstructure: steel, 3-panel, pin / rigid-connected Warren pony truss  
substructure: timber pile bent piers and abutments with timber back- and wingwalls  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 square bars with threaded ends; lateral bracing: round rod with threaded ends; floor beam: I-beam; guardrail: none

The Mewmaw Bridge carries a gravel road over the West Fork of Locust Creek some nine miles northwest of Milan. Named after adjacent landowner James Mewmaw, the bridge's history dates to spring of 1907. In May of that year, Sullivan County Bridge Commissioner, E.L. Caffey reported to the county court that he had viewed and estimated the cost of this and one other small bridge. The court took no action, however, and by the following April, the bridge was still unbuilt. Finally, on April 8, 1908, the court directed T.J. Morelock (Caffey's successor as bridge commissioner), to purchase material and build a 45-foot span over West Locust Creek at this location. Rather than using pinned Pratt pony or truss-leg bedstead design, Morelock instead opted to build an esoteric Warren truss featuring both pinned and riveted connections. No subsequent references to the structure were found in county records, but it was evidently built as planned. Now in its eighth decade of service, the Mewmaw Bridge continues to carry local traffic in rural northwest Sullivan County.

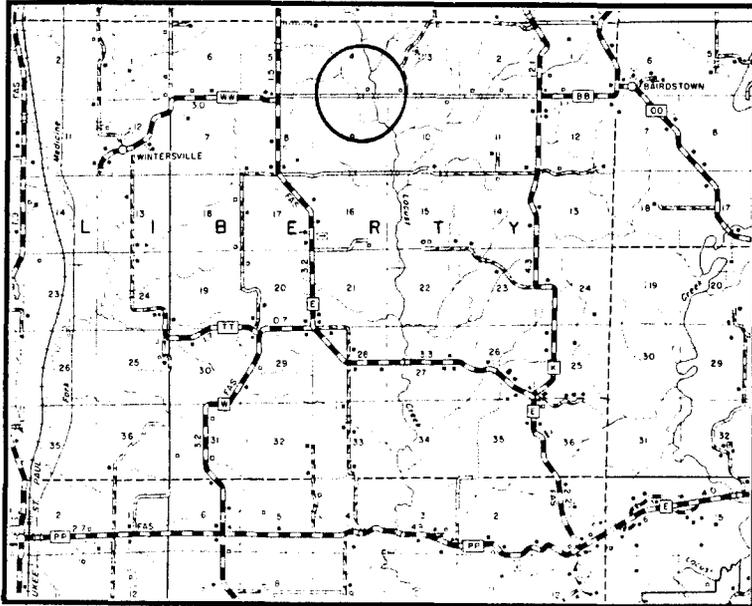
Patented in 1848 by Captain James Warren and Theobald Monzani, the Warren truss in its classic form features a web configuration that relies on simple triangulation for its rigidity. "The term Warren truss or Warren girder was originally applied only to the particular case of the Triangular truss in which the web triangles are all equilateral; but later writers generally use the name for any triangular truss" noted bridge engineer J.A.L. Waddell stated in his 1916 *Bridge Engineering*. "As there is no special advantage in making the web triangles equilateral, there does not appear to be any good *raison d'être* for the use of the true Warren type." Warrens were built sparingly in the 19th century, a period in which the pin-connected Pratt dominated the bridge industry. After the turn of the century, however, rigid-connected Warren pony trusses began to supersede earlier pinned Pratt configurations for use on short- to intermediate-span highway bridges. With both pinned and riveted connections, the Mewmaw Bridge in Sullivan County is the only bridge of its type known to exist in Missouri. A true rarity, the bridge is a well-preserved—apparently unique—example of an uncommon truss type. The Mewmaw Bridge is a significant structural anomaly of pinned truss construction.

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**NAME(S) OF STRUCTURE**

Mewmaw Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 228001.3; Sullivan County Court Record, Book 16: page 373 (11 May 1907), page 537 (10 April 1908) - located at Sullivan County Courthouse, Milan MO.

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**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

6 February 1992

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